



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

ADVISORY PLANNING COMMISSION AGENDA TUESDAY, JANUARY 19, 2015 7:00 P.M. ESQUIMALT COUNCIL CHAMBERS

MEMBERS: Nick Kovacs, Chair Mark Salter
Lorne Argyle Christina Hamer
Amy Higginbotham Graeme Dempster
David Schinbein

REGRETS:

COUNCIL LIAISON: Councillor Tim Morrison
Councillor Susan Low

STAFF LIAISON: Trevor Parkes, Senior Planner

SECRETARY: Simone Manchip

I. CALL TO ORDER

II. LATE ITEMS

III. ADOPTION OF AGENDA

IV. ADOPTION OF MINUTES – DECEMBER 15, 2015

V. STAFF REPORTS

(1) TEMPORARY USE PERMIT

856 Esquimalt Road and 858 Esquimalt Road
PID 026-691-418, Lot A Section 11 Esquimalt District Plan VIP80973
PID 002-925-966, Lot 2, Section 11, Esquimalt District, Plan 23904

Purpose of the Application

The property owner is requesting a Temporary Use Permit in order to legitimize the use of the northern portion of the existing parking lot at 856 Esquimalt Road for the parking of large commercial trucks [moving trucks] not associated with the Cambie Pub and Liquor Store. And the northern portion of 858 Esquimalt Road for the parking of large commercial trucks [moving trucks] associated with the business currently located at this address [2 Burley Men Moving Ltd.].

A similar application was brought before the APC on March 31, 2015 which attempted to deal with the many illegal uses that were occurring on the 856 Esquimalt Road property at that time, and were generating numerous police and bylaw complaints. That application was considered by Council in August and the direction to staff and the applicant was to consult with the neighbours and find a way to allow 2 Burley Men Moving Ltd. to temporarily continue operating at this location without causing further disturbance to the neighbourhood.

The existing building at 858 Esquimalt Road is currently occupied by 2 Burley Men Moving Ltd., who are operating a business office with onsite storage space solely for their goods and services (not clients). There is also one single residential dwelling unit located upstairs in this building that is independent of the business. The building at 856 Esquimalt Road continues to be used as a Licensed Liquor Establishment [Cambie Pub and Liquor Store].

The Esquimalt Official Community Plan and the Zoning Bylaw allow Council to consider approving a commercial or industrial land use, on a temporary basis for a period of up to two years, with appropriate notifications (as required by the *Local Government Act*).

If necessary, the temporary commercial or industrial use permit may be renewed for a second two year term, however, only one renewal is permitted.

The *Local Government Act* allows the local government to ‘specify conditions under which the temporary use may be carried on’. Staff are recommending the following conditions would form part of the Temporary Use Permit:

1. ‘Commercial and Industrial Parking’ means the parking of ‘Moving Trucks’ is allowed on the northern portion of the properties at 856 Esquimalt Road and 858 Esquimalt Road subject to the following conditions:
 - a. ‘Business office with associated storage’ for the business at 858 Esquimalt Road shall occupy no less than 125 square metres of the building at this location;
 - b. That the sixty-seven [67] parking spaces required for the Cambie Pub and Liquor Store shall be maintained at all times;
 - c. That two [2] parking spaces for the business office located at 858 Esquimalt Road shall be maintained at 856 Esquimalt Road [PID 026-691-418, Lot A Section 11 Esquimalt District Plan VIP80973] during regular daytime business hours;
 - d. That the moving trucks and commercial vehicles will be located away from the neighbouring residential properties and toward the northern and western most property lines;
 - e. That no trucks weighing over 10 000 kg, no semi-trailers or tandem axle trucks, shall be parked on the subject properties;
 - f. That all vehicles parked on the subject properties overnight will be single axle, fully licensed for road transit, and in operational condition;
 - g. That no more than twenty-seven [27] Moving Trucks will be parked on the northern portion of the 856 Esquimalt Road property at any time;
 - h. That no more than four [4] Moving Trucks will be parked on the northern portion of the 858 Esquimalt Road property at any time;
 - i. That a maneuvering aisle will be maintained adjacent to the rear of the Cambie Pub and Liquor Store building, allowing for fire truck access at all times;

- j. Moving Truck access and egress from the northern portion of the property at 856 Esquimalt Road would be limited by the Esquimalt Maintenance of Property, Unsightly Properties and Nuisance Bylaw, Bylaw No. 2826;
 - (1) Notwithstanding Bylaw No. 2826, the site will only be used:
 - (i) Saturdays: 9:00 am to 7:00 pm;
 - (ii) Sundays and Statutory Holidays: 9:00 am to 5:00 pm; and
 - (iii) Weekdays: 7:00 am to 7:00 pm;
- k. Moving Truck access and egress from the northern portion of the property at 858 Esquimalt Road shall be permitted 24 hours a day 7 days per week;
- l. No commercial activity associated to the Moving Trucks (ie. no selling from the vehicles) is permitted;
- m. No washing or maintenance of vehicles is allowed on the subject properties;
- n. No fuel storage, or storage of waste fluids for the vehicles is allowed on the subject properties;
- o. No parking or storage of vehicles containing 'dangerous goods', as defined by the *Transport of Dangerous Goods Act* allowed on the subject properties;
- p. Within fourteen days [14] of Council's approval of this Permit the parking lots of both properties will be painted and signs posted, delineating spaces dedicated for the Moving Trucks and other current uses of the subject properties, or the Permit will not be issued;
- q. Within fourteen days [14] of Council's approval of this Permit the continuous cedar fence, as proposed in the landscape plan of DP No. 06/2011, will be erected along the eastern property line of 856 Esquimalt Road, or the Permit will not be issued;
- r. Within fourteen days [14] of Council's approval of this Permit a security deposit in the sum of \$10 000.00 (ten thousand) will be deposited with the Township of Esquimalt, [to guarantee performance to the Terms and Conditions of the Permit], or the Permit will not be issued;
- s. Permit shall be valid until _____ ____, 2018 [2 years from the date of issuance];
- t. All 'Moving Trucks' shall be removed from the subject properties within forty-eight [48] hours of the expiry of the Temporary Use Permit.

RECOMMENDATION:

That the Advisory Planning Commission recommends to Council that the application for a Temporary Use Permit [TUP] authorizing the Commercial/Industrial Parking for Moving Trucks on the northern portion of the properties located at PID 026-691-418, Lot A Section 11 Esquimalt District Plan VIP80973 [856 Esquimalt Road] and PID 002-925-966, Lot 2, Section 11, Esquimalt District, Plan 23904 [858 Esquimalt Road], be forwarded to Council with a recommendation **to either approve with conditions, or deny the application.**

(2) REZONING APPLICATION

“West Bay Triangle”

468 Head Street [Lot 8, Block H, Section 11, Esquimalt District, Plan 292]

470 Head Street [Lot 5, Block H, Section 11, Esquimalt District, Plan 292]

472 Head Street [Lot 4, Block H, Section 11, Esquimalt District, Plan 292]

515 Gore Street [Lot 1, Block H, Section 11, Esquimalt District, Plan 292]

509 Gore Street [Lot 2, Block H, Section 11, Esquimalt District, Plan 292]

922 Lyall Street [Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509]

920 Lyall Street [Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509]

918 Lyall Street [Lot 6, Block H, Section 11, Esquimalt District, Plan 292]

912 Lyall Street [Lot 7, Block H, Section 11, Esquimalt District, Plan 292]

Purpose of the Application

The applicant is requesting a change in zoning from the current mix of RS-1 [Single Family Residential], RD-2 [Two Family Small Lot Residential], and C-7 [West Bay Commercial] zoning which currently regulates the 9 subject properties to a Comprehensive Development zone [CD] to facilitate the consolidation of the subject properties and authorize a new 6 storey, commercial mixed use building containing ground floor commercial space oriented toward Head Street and 73 residential units.

Evaluation of this application should focus on issues relevant to zoning such as the appropriateness of the proposed height, density and massing, proposed unit sizes, setbacks, lot coverage, useable open space, how the building relates to adjacent and surrounding sites and whether the proposed uses are appropriate and consistent with the overall direction contained within the Official Community Plan and the West Bay Neighbourhood Design Guidelines [attached], endorsed by Esquimalt Council on November 16, 2015.

Specific form and character issues relating to the aesthetics of the building, such as cladding materials, window materials, doorways, streetscape improvements and landscaping will be evaluated in a separate application for Development Permit should this rezoning application be approved by Council.

RECOMMENDATION:

The Esquimalt Advisory Planning Commission recommends that the application for rezoning to facilitate consolidation of nine properties located between Head Street, Gore Street and Lyall Street and to permit a new 6 storey, commercial mixed use building containing ground floor commercial space oriented toward Head Street and 73 residential units, sited in accordance with the survey plan

prepared by McIlvaney Riley Land Surveying Inc., stamped “Received December 23, 2016”, and incorporating height and massing consistent with architectural plans prepared by Hillel Architecture, stamped “Received December 8, 2016” be forwarded to Council with a recommendation to either **approve, approve with conditions, or deny the application.**

VI. PLANNER’S STATUS REPORT

VII. COUNCIL LIAISON

VIII. INPUT FROM APC TO STAFF

X. NEXT REGULAR MEETING

Tuesday, February 16, 2016

XI. ADJOURNMENT



CORPORATION OF THE TOWNSHIP OF ESQUIMALT
ADVISORY PLANNING COMMISSION MEETING MINUTES
HELD ON
TUESDAY DECEMBER 15, 2015
ESQUIMALT COUNCIL CHAMBERS

MEMBERS PRESENT: Nick Kovacs, Chair Graeme Dempster
Christina Hamer Lorne Argyle

REGRETS: David Schinbein
Amy Higginbotham
Mark Salter

STAFF LIAISON: Trevor Parkes, Senior Planner
Krystal Wilson, Planning Technician

COUNCIL LIAISON: Councillor Susan Low
Councillor Tim Morrison

SECRETARY: Simone Manchip

I. CALL TO ORDER

The meeting was called to order at 7:05 p.m. by the Chair.

II. LATE ITEMS

826 Esquimalt Rd Parking Study received December 15, 2015.

III. ADOPTION OF AGENDA

Moved by Lorne Argyle seconded by Christina Hamer that the agenda be adopted as distributed.

The Motion **CARRIED UNANIMOUSLY.**

IV. ADOPTION OF MINUTES – NOVEMBER 17, 2015

Moved by Graeme Dempster seconded by Christina Hamer that the minutes of the Advisory Planning Commission held November 17, 2015 be adopted as distributed.

The Motion **CARRIED UNANIMOUSLY.**

V. BUSINESS FROM MINUTES

There was no outstanding business from the Minutes.

VI. STAFF REPORTS

(1) DEVELOPMENT VARIANCE PERMIT

933 Admirals Road

PID 000-673-463, Strata Lot 1, Section 2, Esquimalt District, Strata Plan 382

Purpose of the Application

Krystal Wilson, Planning Technician, introduced the application stating that the applicant is proposing to construct a 'Freestanding Sign' on the subject property similar to the existing sign that needed to be removed due to the Admirals Road Construction Project. The original sign was put in place without benefit of a Sign Permit and was larger than the Sign Regulation Bylaw permits. A Development Variance Permit is required because the Sign Regulation Bylaw does not allow a Freestanding Sign over 1 square metres in area for properties zoned Multi Family Residential [attached as Schedule F]

Ms. Wilson stated that the owners have indicated that they would like their sign to be larger than what's permitted because the entrance to the Strata is off of Luscombe Place rather than Admirals Road and it can be challenging to locate.

The owners are requesting a Development Variance Permit to allow the freestanding sign to be 1.32 metres squared larger than the permitted 1 square metre.

Denise Bragg, Strata President was in attendance addressed APC regarding the application.

APC Comments:

Members commented that they liked the new design of the sign and that it is an improvement from the current signage. They agreed that it would be easier to see from Admirals Road.

RECOMMENDATION:

Moved by Christina Hamer, seconded by Lorne Argyle that the Advisory Planning Commission recommends to Council that the application for a Development Variance Permit authorizing the construction as shown on the drawing prepared by Tran Sign stamped 'Received November 3, 2015 [attached as Schedule A] and including the following relaxations to Sign Regulation Bylaw, 1996, No. 2252, for the development located at PID 000-673-463, Strata Lot 1, Section 2, Esquimalt District, Strata Plan 382 (Phase 1) together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on form 1 [933 Admirals Road], be forwarded to Council with a recommendation for approval.

Sign Regulation Bylaw, 1996, No. 2252, Section 9.8.3 (c) – Sign Placement, Area and Dimensions: An increase in the size permitted for apartment and multi-family dwellings for a Freestanding Sign by 1.32 square metres [from 1 square metre to 2.23 square metres] as detailed on the drawing attached as Schedule A.

The Motion **carried unanimously.**

(2) DEVELOPMENT VARIANCE PERMIT
808 Viewfield Road
PID 023-000-473 Lot A Section 11 Esquimalt District Plan VIP60198

Purpose of the Application

Trevor Parkes, Staff Liaison, The owners of the property are preparing for new tenants, and are proposing to divide the building into 3 separate units. Units 101 and 102 will be used primarily for warehousing and storage, and unit 103 will be used by Axis Trampoline Parks as a recreation or 'Fitness Centre'. Fitness Centre is a permitted use within the Light Industrial zone so a rezoning application or temporary use permit are not required for this use to occur on this property. There is however an issue with the number of parking spaces required for this use so the owner is requesting a variance to *Parking Bylaw, 1992, No. 2011*.

The parking requirements for a Fitness Centre [Trampoline Park] within *Parking Bylaw, 1992, No. 2011* are categorized as 'Other Industrial' and requires one [1] parking space per 25 square metres of gross floor area. The parking bylaw makes provision to allow a decrease in required parking spaces by up to two spaces if bicycle parking, shower facilities and local transit are available

Alex Campbell, Axis Trampoline was in attendance.

Stephen Henderson, CRD presented a PowerPoint presentation.

Carolynn Wilson, Moore Wilson Architects Inc. presented the proposed parking layout.

Dan Casey, Boulevard Transportation summarized the parking study analysis conducted.

APC Comments:

Members welcomed the new proposed business to Esquimalt and commented that they are looking forward to the business starting up. Members expressed their appreciation to the owners and applicant for conducting the parking study and presenting it to the Commission.

RECOMMENDATION:

Moved by Lorne Argyle, seconded by Graeme Dempster that the Advisory Planning Commission [APC] recommends to Council that the application for a Development Variance Permit authorizing the parking layout as shown on the site plan prepared by Moore Wilson Architects., stamped "Received November 27, 2015, and including the following relaxations to Parking Bylaw, 1992, No. 2011, for the development located at PID 023-000-473 Lot A Section 11 Esquimalt District Plan VIP60198 [808 Viewfield Road], be forwarded to Council with a recommendation **for approval**.

Parking Bylaw, 1992, No. 2011, Section 13(1)(a)(iv) – Parking Requirements – Number of Off-Street Parking Spaces – a reduction to the number of required off-street Parking Spaces, from 97 spaces to 59 spaces. [for a net reduction of 38 spaces].

The Motion **carried unanimously**.

**(3) DEVELOPMENT PERMIT
973 Wollaston Street
Strata Lot A, Section 11, Esquimalt District, Strata Plan VIS6589**

Purpose of the Application

Krystal Wilson, Planning Technician, outlined that the applicant has built an addition to the deck located at the rear of the principal building, without the benefit of a Building Permit. This application for a Development Permit including Variances is required as the illegally installed deck contravenes the Development Permit No.19/2007 as well as zoning regulations pursuant to the CD-65 zone.

The subject property was rezoned in 2008 allowing a subdivision of the property into two strata parcels. A new infill home has been constructed at 971 Wollaston Street. The infill parcel proposed in 2008 was smaller than the permitted parcel size within the Single Family Residential zone therefore Comprehensive Development District [CD No. 65] was created and site specific zoning regulations were written. Site specific zoning is written to accommodate a specific development proposal for a specific piece of land.

CD No. 65 states that Lot Coverage shall cover not more than 22% of the parent lot. The expanded deck exceeds the permitted Lot Coverage by one percent [1%]. The minimum rear setback for a principal building in CD No. 65 is required to be not less than 7.9 metres. This measurement corresponded to the siting of the existing house prior to the recent expansion to the existing deck. The applicant is requesting a 2.25 metre reduction to this requirement to 5.65 metres to accommodate the expanded deck area.

Single Unit Infill Housing projects are subject to OCP Development Permit Guidelines and a Development Permit must be approved prior to the issuance of a Building Permit which must be consistent with the approved Development Permit. Development Permit No 19/ 2007 was issued December 3, 2007 and clearly shows the pre-existing deck as part of the landscape plan. As the expanded deck is inconsistent with the approved Development Permit No. 19/ 2007, a new Development Permit must be issued to replace the original prior to the consideration of a Building Permit. As the expanded deck also contravenes both the permitted Lot Coverage and the required Rear Setback regulations in the CD-65 zone, two variances to relax these requirements must also be approved.

Roman Olariu, Owner/applicant presented a written submission to the Advisory Planning Commission. The members took a moment to review the document submitted by the applicant.

Mr. Olariu addressed the membership by reading his prepared statement. He advised that he is here as a result of Councils direction to submit a new application. APC recommended denial of previous application to change Development Permit No.19/2007. He felt that the Commission decision was made without any supporting rational for why it was denied. He commented that the staff report indicated that the expanded deck increased the intrusion onto the privacy of the adjacent properties contrary to the OCP guidelines for single unit infill housing.

The Chair politely interrupted the applicant to remind him that the Commission Members had reviewed his written submission, and that this was his opportunity to speak to the Commission. The Chair asked the applicant if there was anything further beyond his written statement he wished to add. Mr. Olariu responded that he had nothing new to add.

In response to a member's question, Mr. Olariu commented that the exterior stairs were built onto the deck for ease of life and access to the rear yard. Mr. Olariu stated that an internal stairwell was created within the last year to have access to the lower level back door, and clarified that the work was done with a Building Permit.

APC Comments:

In response to a member's inquiry regarding the applicant's letter with regard to the process for handling this application, Trevor Parkes, Senior Planner, responded that there is an avenue available to relax regulations contained in any zone that is contained in any zoning bylaw and this is done through the Development Variance Permit process. Mr. Parkes clarified that the statement relating to "wobble space" in a zone was quoted from a response made to a question that he had received at a previous meeting relating to the nature of a Comprehensive Development zone. Mr. Parkes elaborated stating Comprehensive Development Districts are written to accommodate the specific application presented at that time. The zoning that governs this property was written to accommodate the application that was presented to APC and then Council in 2008 and that application was a development consistent with the Development Permit that was later issued and is attached to the APC agenda package. Accordingly, the rear set back was written at 7.9 metres and the lot coverage was limited to 22% of total lot coverage.

Mr. Parkes stated that the avenue available to the applicant to alter his building, which the applicant is appropriately pursuing, is to secure a Development Permit including Variances to accommodate the changes made to his building. The OCP clearly specifies that a Development Permit is required for alteration of a building that is within a Development Permit Area and Bylaw No. 2791 sets out that procedure. Mr. Parkes stated that the application before APC is seeking to amend the existing Development Permit and this is achieved by issuing a new Development Permit to overwrite the old one. The new Development Permit includes variances to the existing zoning requirements to accommodate the expansion of the deck that was constructed without a Building Permit and in contravention of the Development Permit that is applied to the property currently. In order for the applicant to get a Building Permit for this deck, he first needs to be successful in getting the new Development Permit including the requested variances to accommodate the siting of this deck.

Member's comments varied from stating support for the application, to hesitation regarding the proposal, to opposition.

Graeme Dempster made a motion to approve. The motion was debated and was denied.

A second motion was made by Lorne Argyle for denial. The motion carried with a 3-1 vote to deny the application.

RECOMMENDATION:

Moved by Graeme Dempster, seconded by Christina Hamer that the Esquimalt Advisory Planning Commission [APC] recommends to Council that the application for a Development Permit including Variances authorizing the construction as shown in the photos, stamped "Received December 8, 2015", and sited as detailed on the survey plan prepared by Powell and Associates, stamped "Received November 9, 2015", and including the following relaxations to Zoning Bylaw, 1992, No. 2050, for Strata Lot A, Section 11, Esquimalt District, Strata Plan VIS6589 [973 Wollaston Street], be forwarded to Council with a recommendation for approval.

Zoning Bylaw, 1992, No. 2050, Section 67.52 (5) – Lot Coverage A 1% increase to the requirement that all principal buildings, accessory buildings and structures combined shall not cover more than 22% of the area of the parent lot [i.e. increase from 22% to 23% lot coverage].

Zoning Bylaw, 1992, No. 2050, Section 67.52 (7)(a)(iii) Rear Setback - A 2.25 metre reduction to the requirement that no principal building shall be located within 7.9 metres of

any rear lot line [i.e. from 7.9 metres to 5.65 metres] as outlined on the survey attached as Schedule A.

The Motion was **defeated** (Graeme Dempster in favour).

Moved by Lorne Argyle, seconded by Christina Hamer that the Esquimalt Advisory Planning Commission [APC] recommends to Council that the application for a Development Permit including Variances authorizing the construction as shown in the photos, stamped “Received December 8, 2015”, and sited as detailed on the survey plan prepared by Powell and Associates, stamped “Received November 9, 2015”, and including the following relaxations to Zoning Bylaw, 1992, No. 2050, for Strata Lot A, Section 11, Esquimalt District, Strata Plan VIS6589 [973 Wollaston Street], be forwarded to Council with a recommendation **for denial**.

Zoning Bylaw, 1992, No. 2050, Section 67.52 (5) – Lot Coverage A 1% increase to the requirement that all principal buildings, accessory buildings and structures combined shall not cover more than 22% of the area of the parent lot [i.e. increase from 22% to 23% lot coverage].

Zoning Bylaw, 1992, No. 2050, Section 67.52 (7)(a)(iii) Rear Setback - A 2.25 metre reduction to the requirement that no principal building shall be located within 7.9 metres of any rear lot line [i.e. from 7.9 metres to 5.65 metres] as outlined on the survey attached as Schedule A.

The Motion **carried** (Graeme Dempster opposed).

(4) REZONING APPLICATION

826 Esquimalt Road

PID 006-075-495 Lot 2, Section 11, Esquimalt District, Plan 4225

Purpose of the Application

Trevor Parkes, Staff Liaison outlined that the applicant is requesting a change in zoning from the current RD-3 [Two Family/ Single Family Residential] zone to a Comprehensive Development District zone [CD]. This change is required to accommodate the proposed 6 storey, 30 unit, multiple family residential building including a grade level podium structure containing a lobby, utility areas and a 22 space parking garage. The podium would extend to the, north, west and east property lines but would maintain a 5.0 metres setback from the street while the 5 storey tower section would step modestly inward on the north, west and east sides thereby somewhat mitigating the perceived mass of the building. Two additional parking spaces dedicated to visitors are proposed to be located in front of the building bringing the total parking to 24 spaces.

This site is located within Development Permit Area No. 1 – Multi-Unit Residential. Should the rezoning application be approved, the applicant would need to obtain a Development Permit respecting the character of the development, including landscaping, and the siting, form, exterior design and finish of the proposed 6 storey, 30 unit, multiple family residential building which would be considered by both the DRC and Council in the future.

Staff request the Advisory Planning Commission review this proposal with regard to the proposed siting, height, mass, density, lot coverage, useable open space and parking and provide comments for staff and the applicant to consider as well as a recommendation to Council.

Mark Eraut, Owner/applicant, Michael Levin, Praxis Architects, Kristin Schullter, Praxis Architects and Dan Casey, Boulevard Transportation were in attendance.

Mark Eraut, property owner gave a brief history of the proposed project. He explained that he owns this property as well as the Esquimalt Bingo Hall and two associated properties along Old Esquimalt Road. He has lived in the community for 30 years and is a builder / developer and has looked at doing a project in Esquimalt for long time now. He has tried for a number of years to purchase the property at 822 Esquimalt Road located between the subject property and the Bingo Hall and the owners have no interest in selling. This project would provide affordable market housing and be economically viable for clients that do not have vehicles. There will be a car share program for all units.

Dan Casey, Boulevard Transportation presented the parking study analysis.

Kristin Schulter, Praxis Architects gave a PowerPoint presentation detailing the site plan and an overview of the building design and materials, elevations, setbacks, parking and landscaping for the project.

APC Comments:

Members identified some concerns with the lack of parking for visitors and suggested an increase to the number of Visitor Parking spaces to more than the recommended 3 spaces would be desirable. A member commented that the reduction in the number of units and the increase in the number of parking spaces from the initial design was appreciated and recommended the applicant consider providing all 5 spaces in front of the security gate as Visitor parking. Members stated they liked that a Car Share program would be implemented for all units.

RECOMMENDATION:

Moved by Lorne Argyle, seconded by Graeme Dempster that the Esquimalt Advisory Planning Commission [APC] resolves that the application for rezoning, authorizing a 18 metre [6 storey], 30 unit, multiple family residential building sited in accordance with the BCLS Site Plan provided by J.E. Anderson and Associates Surveyors-Engineers, stamped "Received November 10, 2015, and incorporating height and massing consistent with the architectural plans provided by Praxis Architects Inc., stamped "Received December 2, 2015", detailing the development proposed to be located at PID 006-075-495 Lot 2, Section 11, Esquimalt District, Plan 4225 [826 Esquimalt Road], be forwarded to Council with a recommendation **for approval with the condition that the three or more parking spaces be dedicated to Visitor Parking.**

The Motion **carried unanimously.**

VII. STAFF LIAISON

322 Plaskett Place: [Rezoning] The amendment Bylaw was presented to Council on December 7, 2015 and the amendment bylaw was granted 1st and 2nd reading. Public Hearing is tentatively scheduled for January 18, 2016.

Interim Zoning Bylaw Amendments: [Zoning Bylaw Revisions] Public Hearing was held December 7, 2015. Council directed minor changes to the bylaw.

VIII. COUNCIL LIAISON

Councillor Morrison commented that responses have been received for the Esquimalt Village Plan Request for Proposal.

IX. INPUT FROM APC TO STAFF

None

X. NEW BUSINESS

None

XI. NEXT REGULAR MEETING

Tuesday, January 19, 2016

XII. ADJOURNMENT

On motion the meeting adjourned at _____ P.M.

CERTIFIED CORRECT:

CHAIR, ADVISORY PLANNING COMMISSION

ANJA NURVO,
CORPORATE OFFICER

THIS DAY OF , 2015

DRAFT



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

APC Meeting: January 19, 2016

STAFF REPORT

DATE: January 14, 2016

TO: Chair and Members of the Advisory Planning Commission

FROM: Karen Hay, Planner
Bill Brown, Director of Development Services

SUBJECT: TEMPORARY USE PERMIT
856 Esquimalt Road and 858 Esquimalt Road
PID 026-691-418, Lot A Section 11 Esquimalt District Plan VIP80973
PID 002-925-966, Lot 2, Section 11, Esquimalt District, Plan 23904

RECOMMENDATION:

That the Advisory Planning Commission recommends to Council that the application for a Temporary Use Permit [TUP] authorizing the Commercial/Industrial Parking for Moving Trucks on the northern portion of the properties located at PID 026-691-418, Lot A Section 11 Esquimalt District Plan VIP80973 [856 Esquimalt Road] and PID 002-925-966, Lot 2, Section 11, Esquimalt District, Plan 23904 [858 Esquimalt Road], be forwarded to Council with a recommendation **to either approve with conditions, or deny the application.**

BACKGROUND:

Context

Owner/ Applicant: Esquimalt Holdings Corp., Inc. No. 600168 / Sam Yehia

Property Size: 856 Esquimalt Rd.: Metric: 5923 m² Imperial: 63756 ft²
858 Esquimalt Rd.: Metric: 1109 m² Imperial: 11937 ft²

Existing Land Use: 856 Esquimalt Road: Licensed Liquor Establishment/ Liquor Store
858 Esquimalt Road: Business and Professional Office, 1 residential unit

Surrounding Land Uses:

North: Multiple Family Residence

South: Multiple Family Residence

East: Multiple Family Residence

West: Commercial/ Institution (Victor Brodeur playing field) and Drive-in Restaurant

Existing Zoning: CD-80 [Comprehensive Development District]

Purpose of the Application

The property owner is requesting a Temporary Use Permit in order to legitimize the use of the northern portion of the existing parking lot at 856 Esquimalt Road for the parking of large commercial trucks [moving trucks] not associated with the Cambie Pub and Liquor Store. And the northern portion of 858 Esquimalt Road for the parking of large commercial trucks [moving trucks] associated with the business currently located at this address [2 Burley Men Moving Ltd.].

A similar application was brought before the APC on March 31, 2015 which attempted to deal with the many illegal uses that were occurring on the 856 Esquimalt Road property at that time, and were generating numerous police and bylaw complaints. That application was considered by Council in August and the direction to staff and the applicant was to consult with the neighbours and find a way to allow 2 Burley Men Moving Ltd. to temporarily continue operating at this location without causing further disturbance to the neighbourhood.

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The Esquimalt Official Community Plan and the Zoning Bylaw allow Council to consider approving a commercial or industrial land use, on a temporary basis for a period of up to two years, with appropriate notifications (as required by the *Local Government Act*). If necessary, the temporary commercial or industrial use permit may be renewed for a second two year term, however, only one renewal is permitted.

The *Local Government Act* allows the local government to 'specify conditions under which the temporary use may be carried on'. Staff are recommending the following conditions would form part of the Temporary Use Permit:

1. 'Commercial and Industrial Parking' means the parking of 'Moving Trucks' is allowed on the northern portion of the properties at 856 Esquimalt Road and 858 Esquimalt Road subject to the following conditions:

- a. 'Business office with associated storage' for the business at 858 Esquimalt Road shall occupy no less than 125 square metres of the building at this location;
- b. That the sixty-seven [67] parking spaces required for the Cambie Pub and Liquor Store shall be maintained at all times;
- c. That two [2] parking spaces for the business office located at 858 Esquimalt Road shall be maintained at 856 Esquimalt Road [PID 026-691-418, Lot A Section 11 Esquimalt District Plan VIP80973] during regular daytime business hours;
- d. That the moving trucks and commercial vehicles will be located away from the neighbouring residential properties and toward the northern and western most property lines;
- e. That no trucks weighing over 10 000 kg, no semi-trailers or tandem axle trucks, shall be parked on the subject properties;

- f. That all vehicles parked on the subject properties overnight will be single axle, fully licensed for road transit, and in operational condition;
- g. That no more than twenty-seven [27] Moving Trucks will be parked on the northern portion of the 856 Esquimalt Road property at any time;
- h. That no more than four [4] Moving Trucks will be parked on the northern portion of the 858 Esquimalt Road property at any time;
- i. That a maneuvering aisle will be maintained adjacent to the rear of the Cambie Pub and Liquor Store building, allowing for fire truck access at all times;
- j. Moving Truck access and egress from the northern portion of the property at 856 Esquimalt Road would be limited by the Esquimalt Maintenance of Property, Unightly Properties and Nuisance Bylaw, Bylaw No. 2826;
 - (1) Notwithstanding Bylaw No. 2826, the site will only be used:
 - (i) Saturdays: 9:00 am to 7:00 pm;
 - (ii) Sundays and Statutory Holidays: 9:00 am to 5:00 pm; and
 - (iii) Weekdays: 7:00 am to 7:00 pm;
- k. Moving Truck access and egress from the northern portion of the property at 858 Esquimalt Road shall be permitted 24 hours a day 7 days per week;
- l. No commercial activity associated to the Moving Trucks (ie. no selling from the vehicles) is permitted;
- m. No washing or maintenance of vehicles is allowed on the subject properties;
- n. No fuel storage, or storage of waste fluids for the vehicles is allowed on the subject properties;
- o. No parking or storage of vehicles containing 'dangerous goods', as defined by the *Transport of Dangerous Goods Act* allowed on the subject properties;
- p. Within fourteen days [14] of Council's approval of this Permit the parking lots of both properties will be painted and signs posted, delineating spaces dedicated for the Moving Trucks and other current uses of the subject properties, or the Permit will not be issued;
- q. Within fourteen days [14] of Council's approval of this Permit the continuous cedar fence, as proposed in the landscape plan of DP No. 06/2011, will be erected along the eastern property line of 856 Esquimalt Road, or the Permit will not be issued;
- r. Within fourteen days [14] of Council's approval of this Permit a security deposit in the sum of \$10 000.00 (ten thousand) will be deposited with the Township of Esquimalt, [to guarantee performance to the Terms and Conditions of the Permit], or the Permit will not be issued;
- s. Permit shall be valid until _____, 2018 [2 years from the date of issuance];
- t. All 'Moving Trucks' shall be removed from the subject properties within forty-eight [48] hours of the expiry of the Temporary Use Permit;

Comments From Other Departments:

This proposal was circulated to other departments and the following comments were received:

Fire Services: Concern raised that the contents of the trucks not include hazardous materials and that their parking not obstruct access to the back of the Cambie building.

Bylaw Enforcement: Though there have been numerous complaints in the past, Esquimalt Bylaw Department has received no complaints since Sept/2015.

Engineering Services: Proposed use will not have an impact on the Engineering aspects of the property, and any damage to infrastructure as a result of the proposed use shall be the sole responsibility of the property owner.

PUBLIC NOTIFICATION:

As this application includes a Temporary Use Permit application, should it proceed to Council, a notice will be mailed to tenants and owners of properties within 100 metres (328 feet) of the subject property. A notice would also be placed in one edition of the Victoria News; as required by the *Local Government Act*.

ALTERNATIVES:

1. Forward the application for a Temporary Use Permit to Council with a **recommendation of approval with conditions**.
2. Forward the application for a Temporary Use Permit to Council with a **recommendation of denial**.

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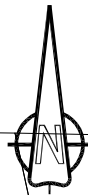
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OLD ESQUIMALT RD



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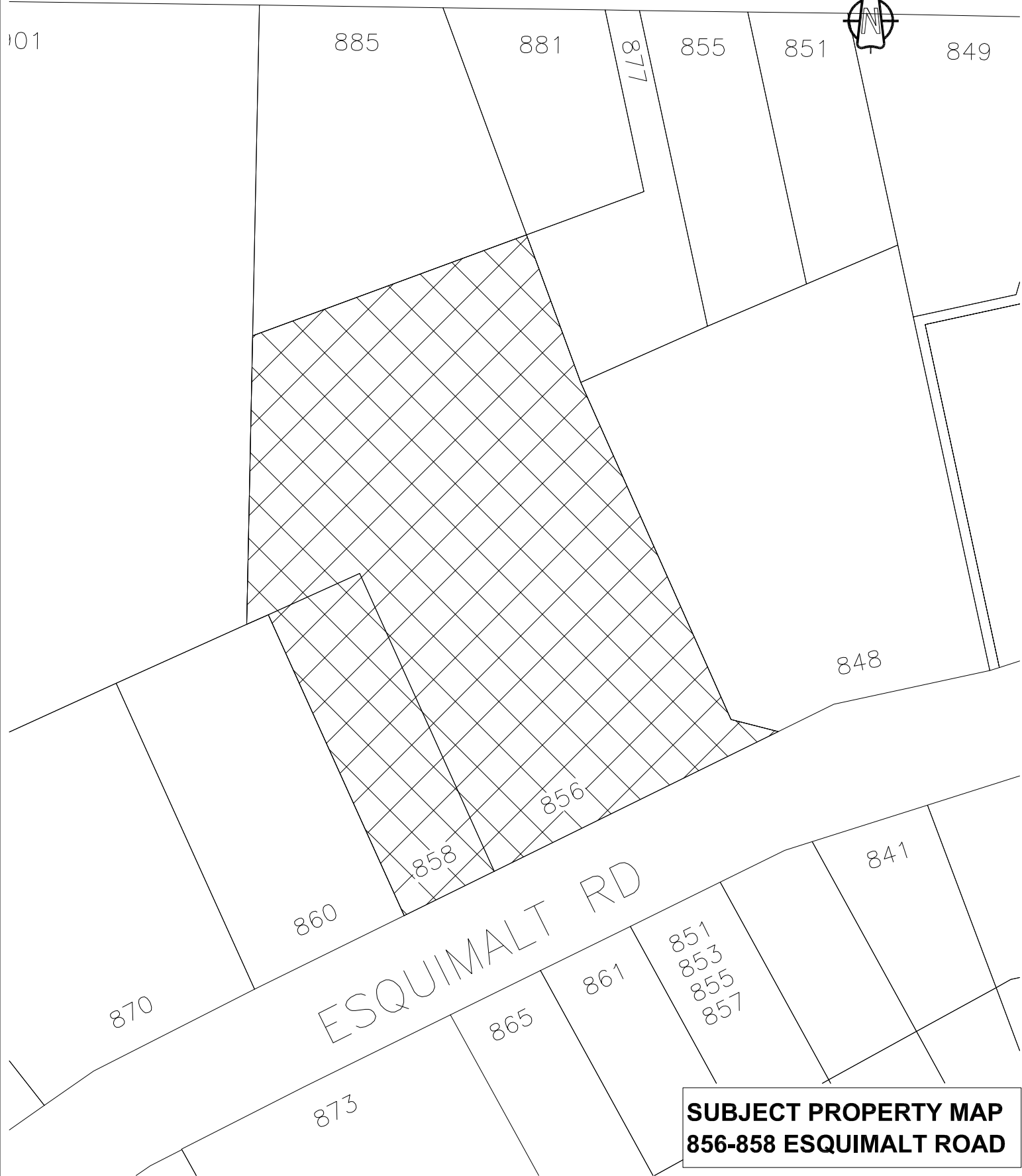
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**SUBJECT PROPERTY MAP
856-858 ESQUIMALT ROAD**

856 and 858 Esquimalt Road



21 moving trucks – 856 Esquimalt Road – August 2015



2 Moving trucks – 858 Esquimalt Road – January 2016





CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

APC Meeting: January 15, 2016

STAFF REPORT

DATE: January 15, 2016

TO: Chair and Members of the Advisory Planning Commission

FROM: Trevor Parkes, Senior Planner

SUBJECT: REZONING APPLICATION
“West Bay Triangle”
468 Head Street [Lot 8, Block H, Section 11, Esquimalt District, Plan 292]
470 Head Street [Lot 5, Block H, Section 11, Esquimalt District, Plan 292]
472 Head Street [Lot 4, Block H, Section 11, Esquimalt District, Plan 292]
515 Gore Street [Lot 1, Block H, Section 11, Esquimalt District, Plan 292]
509 Gore Street [Lot 2, Block H, Section 11, Esquimalt District, Plan 292]
922 Lyall Street [Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509]
920 Lyall Street [Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509]
918 Lyall Street [Lot 6, Block H, Section 11, Esquimalt District, Plan 292]
912 Lyall Street [Lot 7, Block H, Section 11, Esquimalt District, Plan 292]

RECOMMENDATION:

The Esquimalt Advisory Planning Commission recommends that the application for rezoning to facilitate consolidation of nine properties located between Head Street, Gore Street and Lyall Street and to permit a new 6 storey, commercial mixed use building containing ground floor commercial space oriented toward Head Street and 73 residential units, sited in accordance with the survey plan prepared by McIlvaney Riley Land Surveying Inc., stamped “Received December 23, 2016”, and incorporating height and massing consistent with architectural plans prepared by Hillel Architecture, stamped “Received December 8, 2016” be forwarded to Council with a recommendation to either **approve, approve with conditions, or deny the application.**

BACKGROUND:

Purpose of the Application

The applicant is requesting a change in zoning from the current mix of RS-1 [Single Family Residential], RD-2 [Two Family Small Lot Residential], and C-7 [West Bay Commercial] zoning which currently regulates the 9 subject properties to a Comprehensive Development zone [CD] to facilitate the consolidation of the subject properties and authorize a new 6 storey, commercial mixed use building containing ground floor commercial space oriented toward Head Street and 73 residential units.

Evaluation of this application should focus on issues relevant to zoning such as the appropriateness of the proposed height, density and massing, proposed unit sizes, setbacks, lot

coverage, useable open space, how the building relates to adjacent and surrounding sites and whether the proposed uses are appropriate and consistent with the overall direction contained within the Official Community Plan and the West Bay Neighbourhood Design Guidelines [attached], endorsed by Esquimalt Council on November 16, 2015..

Specific form and character issues relating to the aesthetics of the building, such as cladding materials, window materials, doorways, streetscape improvements and landscaping will be evaluated in a separate application for Development Permit should this rezoning application be approved by Council.

Context

Applicant/Owner: Triangle Estates Ltd. [Mark Lindholm]

Architect: Hillel Architecture Inc. [Peter Hardcastle]

Property Size: Metric: 4080 m² Imperial: 43,917 ft²

Existing Land Use: Single Family and Two Family residences and vacant Commercial land

Surrounding Land Uses:

North: Neighbourhood Commercial/ West Bay Commercial
South: DND Federal Land [Work Point]
West: Single Family and Two Family Residential
East: Marine Commercial

Existing Zoning: RS-1 [Single Family Residential] – 5 Lots
RD-2 [Two Family Small Lot Residential] – 1 Lot
C-7 [West Bay Commercial] – 2 Lots

Proposed Zoning: CD [Comprehensive Development District]

Existing OCP Designation: Commercial Mixed-Use [No change required]

Comments from Other Departments

The plans for this proposal were circulated to other departments and the following comments were received by the submission deadline:

Building Inspection: Construction must satisfy BC Building Code and Esquimalt's Building Code Bylaw, 2002, No. 2538 requirements. Detailed review of plans will occur when at time of Building Permit application.

Parks Services: Parks staff completed a site review of the area and determined that the proposed development would impact 9 trees located on municipal property adjacent to the site noting that all of these trees would be removed, should this development be approved. As no detailed landscape plan has been provided at this time, Parks staff reserve comments until a plan is presented as part of the Development Permit application. Staff note that the applicant will need to provide a Tree Survey so that appropriate tree replacement requirements can be determined consistent with the Township's Tree Protection Bylaw.

Fire Services: Fire Services staff have completed a preliminary evaluation of the proposed new buildings and no objection to the proposed rezoning. Detailed additional comments will be provided as part of the consideration of the Building Permit should Council approved this

rezoning application.

Engineering Services: Engineering staff have completed a preliminary evaluation of Works and Services that would be required for the proposed commercial mixed use development including commercial space and 73 residential units located on 9 properties within West Bay. The applicant is responsible for retaining the services of a qualified professional for the design, construction and supervision of all Works and Services required to service the proposed development as indicated in Esquimalt’s Subdivision and Development Control Bylaw No. 2175. Capacity study should verify all main sizes, material, etc, and to determine if the Sewer and Drain mains are of adequate size and condition to handle the increased flows introduced as a result of the development. Existing municipal infrastructure may need to be relocated or SRWs will need to be secured. Existing properties are to remain fully serviced during construction. Additional review comments will be provided when detailed engineering drawings are submitted.

ISSUES:

Zoning

Density, Lot Coverage, Setbacks, Height and Parking:

The following chart details the setbacks, lot coverage and floor area ratio and parking requirements of this proposal.

	Comprehensive Development Zone	
Floor Area Ratio	1.25	Zoning Bylaw, 1992, No. 2050 does not contain a zone that could accommodate this commercial mixed-use proposal. Staff present this summary table as the basis for a site specific zone written to accommodate this proposal should it be forwarded in the development review process.
Lot Coverage		
• 1 st Storey	55%	
• 2 nd Storey	63%	
• Above 2 nd Storey	23%	
Setbacks		
• Front	5.8 m plus 3.0 m [Canopy]	
• Rear	6.7 m plus 1.8 m [Canopy]	
• Interior Side	0.9 m plus 0.9 m [Canopy]	
• Exterior Side	3.2 m plus 0.5 m [Canopy]	
Building Height	7.5m [Head Street and Gore Street] 22 m (6 Storeys) [Lyll Street]	
Off Street Parking	Total spaces required for all uses = 114 Total proposed = 121	
Screening/Landscaping	2 m of fencing or vegetation on property line adjacent to residential development consistent with Zoning Bylaw, 1992 No. 2050.	
Useable Open Space	15% [Including 2 nd Storey Terrace]	
Commercial Floor Area	Not less than 400 square metres	

Parking: The applicant proposes to provide modestly greater parking than the minimum required for this project, exceeding the minimum number of spaces by 7 spaces. With the exception of the 14 spaces accessed off Lyall Street serving as Visitor Parking for the residential units, all parking associated with this development is screen from the public realm or located underground.

Height: The applicant proposes to construct a building 2 storeys in height along the Head Street and Gore Street frontages and would include a residence block fronting Lyall Street that

would rise 6 storeys with a total residential density [Floor Area Ratio] of 1.25. While approval of this rezoning application would make Triangle Estates one of the tallest buildings in Esquimalt, the proposed height is substantially less than initially considered for this Commercial Mixed-Use site and is consistent with the recently adopted West Bay Neighbourhood Design Guidelines.

Setbacks: Commercial development best practices encourage placing commercial retail uses in proximity to the public realm. Accordingly, the proposed siting of the portions of the building containing retail/ office commercial space is appropriate as the large canopies proposed along Head Street will serve to create a covered transitional space between the indoor uses and the outdoor public realm. The setback associated with the residential block facing Lyall Street is consistent with a residential building setback and offers adequate separation between the public realm and the building.

Permitted Uses: The West Bay Area is identified as a Commercial Node in the Official Community Plan therefore it is appropriate to consider an application that proposes a mix of commercial retail, office, and boutique commercial spaces. Permitted Uses in the proposed Comprehensive Development District may include but are not limited to the following uses:

- a) Dwelling – Apartment
- b) Home Occupation
- c) Business and Professional Office
- d) Financial Institution
- e) Personal Service Establishment
- f) Retail Store
- g) Restaurant
- h) Entertainment
- i) Convenience Store
- j) Group Children's Daycare

Traffic Study: The applicant provided a detailed Traffic Impact Assessment for the original 97 unit mixed-use proposal, dated February 20, 2012. Boulevard Transportation Group [BTG] determined at that time that the proposed project would have little local impact, adding an estimated 38 trips in the AM peak hour and 44 trips in the PM peak hour. BTG also determined that it was most likely that the existing traffic flow pattern, including DND traffic, would not change with the addition of this building, as additional delays would be very minor. BTG concluded that no improvements or changes would be required to the existing key intersections to accommodate the new traffic but recommended that sidewalks should be provided along the proposed development frontages of Head Street, Gore Street and Lyall Street.

Based on this assessment, staff are confident that this revised 73 unit, commercial mixed use, proposal would not result in any material changes to these recommendations.

Fit with Neighbourhood: The West Bay Triangle lands are located within a predominantly low rise neighbourhood consisting of a mix of commercial properties, single and two family residential homes, a marine community, and military uses including training facilities and housing. The exception to the established one to three storey built environment is the DND "Accommodations" building located at Work Point to the south of the subject properties. This building is approximately 7 storeys [22 metres] in height and commands a substantial presence in the area due to the combination of height and massing of the building.

Should this rezoning application be approved, the West Bay Triangle would integrate into the existing urban fabric by complimenting the DND Accommodations building while presenting a 2 storey built form sympathetic to the existing residential and commercial buildings along Gore Street and Head Street.

The current Land Use Designation of properties adjacent to the West Bay Triangle site include Commercial Mixed-use for lands to the east, adjacent to the water, Park and Open Space for Captain Jacobson Park and Townhouse Residential for properties on the north side of Gore Street and the south side of Paradise Street. Based on these land use designations, it is not unreasonable to expect some of these properties adjacent to the proposed West Bay Triangle building to redevelop in the future.

Official Community Plan

The current Esquimalt Official Community Plan contains policies and statements relevant to the West Bay Triangle proposal under the following broad categories:

General Land Use and Development Objectives: The Township encourages a mix of land uses that facilitate multiple modes of transportation and reduce non-essential trips by private motor vehicles.

Commercial Mixed Land Use: The Township encourages mixed-use projects in areas designated Commercial Mixed-Use, generally up to 12 storeys in height with a Floor Area Ratio of 3.0, but limited to 6 storeys in the West Bay Neighbourhood. The Township encourages the provision of local amenities including street furniture, artworks and decorative lighting in commercial areas as well as the provision of cycling amenities such as end of trip facilities for employees, lockers, and secure bicycle storage in addition to general bicycle parking integrated into the development.

Public Art: The Township encourages the private sector to include artworks in new and existing developments.

Smart Design and Construction: The Township encourages the use of sustainable technology in the design of all new buildings, encourages design teams to achieve LEED or equivalent rating and encourages the incorporation of Crime Prevention through Environmental Design [CPTED] principles and measures in new projects.

Parking: The Township will explore further opportunities for on and off street public parking in the vicinity of West Bay waterfront so as to ensure that local residential streets are not overburdened by West Bay’s commercial uses

Utilities: The Township will require underground wiring for hydro, street-lighting, telecoms and cable for all new subdivisions and infill development in Esquimalt.

Stormwater, Sewer Systems and Source Pollution: The Township will require intervening /mitigating measures such as oil traps for development proposal adjacent to or near the water to preserve or rehabilitate shoreline habitat.

Economic Development: The Township supports expansion and commercial waterfront development around West Bay Harbour, provided it is planned to be compatible with adjacent residential areas.

Keep Urban Settlement Compact – Regional Growth Strategy [RGS]: The RGS encourages densification through a combination of infill and redevelopment to higher densities particularly for areas near transit corridors.

West Bay Neighbourhood Design Guidelines

As a direct result of the controversy surrounding previous development proposals on the subject properties, Esquimalt Council directed staff to coordinate the creation of the West Bay Neighbourhood Design Guidelines. This document was to be created including input from local residents, business owners, staff, planners, developers and architects to create a vision for West Bay as well as guidelines for achieving this vision to ensure new development enhances West Bay's unique character.

Staff engaged a consulting group and the guidelines were crafted via a collaborative and iterative process through the spring and summer of 2015. Esquimalt Council received and endorsed the West Bay Neighbourhood Design Guidelines on November 16, 2015.

The West Bay Triangle development proposal has been specifically tailored to address these guidelines resulting in an application that is overwhelmingly consistent with the West Bay Neighbourhood Design Guidelines.

Green Building Features

The applicant has completed the Esquimalt Green Building Checklist [attached].

Public Notification

As this is a rezoning application, should it proceed to a Public Hearing, notice would be mailed to tenants and owners of properties within 100m (328 ft) of the subject property. Signs indicating that the properties are under consideration for a change in zoning have been placed on the frontages of all the subject properties and would be updated to reflect the date, time and location of the Public Hearing. Additionally, notice of the Public Hearing would be placed in two editions of the Victoria News.

ALTERNATIVES:

1. Forward the application for Rezoning to Council with a **recommendation of approval**.
2. Forward the application for Rezoning to Council with a **recommendation of approval** including specific conditions.
3. Forward the application for Rezoning to Council with a **recommendation of denial**.

Trevor Parkes
Senior Planner, Staff Liaison to the Advisory Planning Commission

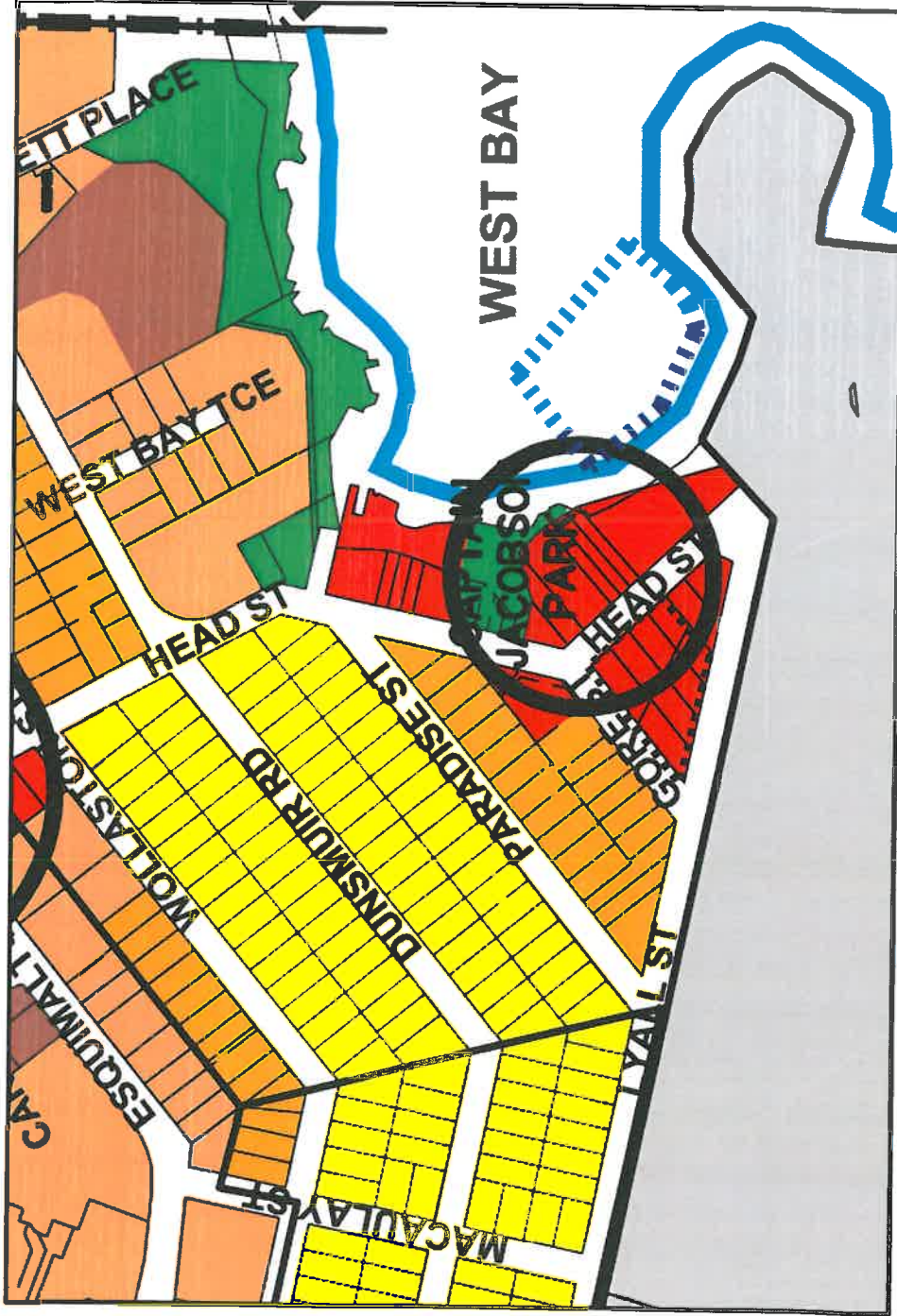
468 Head Street – ‘Triangle Estates’



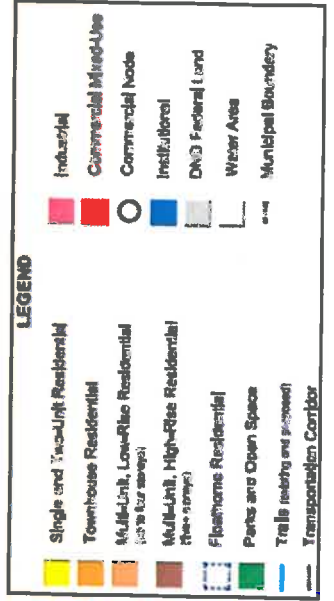
Subject Property Boundary: 

Extract from Official Community Plan Bylaw No. 2646 Schedule 'A'

Land Use Designations



Subject Property:



2.3 Commercial—Mixed Land Use

Commercial activity in Esquimalt is clustered in four main groupings:

- Esquimalt Village;
- Esquimalt Road/Head Street;
- Craigflower Road/Tillicum Road; and
- West Bay Harbour.

2.3.1 General Commercial - Mixed Use Objectives

- a) To create a diversified commercial and employment sector that provides a wide range of goods and services to residents of Esquimalt.
- b) To foster the creation of an identifiable and vibrant Esquimalt Village that successfully integrates commercial, public and residential activity.
- c) To encourage growth through revitalization and redevelopment of commercial areas.
- d) To encourage a mix of ground-level commercial and upper-level(s) residential.

2.3.2 General Commercial - Mixed Use Policies

- a) The Township encourages a mix of commercial and multi-unit residential developments in all commercial-mixed use areas denoted on "Schedule A". These will have commercial uses on the ground floor and residential uses above.
- b) All commercial-mixed use areas are designated Development Permit Areas, as shown on "Schedule C" in order to ensure that future development and infill contributes positively to the visual and aesthetic character of its site, setting and surrounding properties.
- c) The Township encourages public and private sector initiatives to improve streetscapes and accessibility for pedestrians and cyclists to all commercial areas.
- d) The Township will develop signage guidelines for each commercial area, as part of design guidelines for these areas. The Township, in partnership with interested members of the community, will take the initiative to design and install entrance signage at key street locations, including entrances to Esquimalt.
- e) The Township encourages the provision of amenities such as mini-parks/plazas, street furniture, public art and decorative lighting on private lands in all commercial areas. The Township is amenable to using density bonusing, or providing variances to zoning or parking regulations for redevelopment proposals.
- f) To encourage the use of bicycles, provision should be made in new commercial buildings for bicycle parking for employees and visitors. Secure bicycle parking for employees should be provided in the ratio of one (1) parking space per ten (10) full-time employees with a minimum of one (1) space for each new building. In all new commercial buildings, six (6) bicycle parking spaces should be available for the use of temporary visitors.
- g) End of trip facilities for cyclists such as secure bicycle parking/storage, lockers, change rooms and showers, should be provided to encourage cycling as a viable form of transportation.

- h) Where all of the following criteria are met in a commercial building, Council may reduce the off-street parking requirement through the Development Permit:
 - i) Two (2) or more secure bicycle storage spaces are provided;
 - ii) Shower and change rooms are provided;
 - iii) Six (6) visitor bicycle parking spaces are provided; and
 - iv) The building is located within 200 metres of a regional bus route.
- i) Lands outside the Commercial–Mixed Use designation on “Schedule A” will not be considered for commercial zoning unless the following criteria are met:
 - i) The project is needed to serve tourists or local residents and cannot be appropriately located within established commercial areas;
 - ii) The density and scale of the project is sensitive to the prevailing character of surrounding lands;
 - iii) The project, through its exterior finishes enhances the aesthetics of the neighbourhood;
 - iv) The project’s parking requirements can be satisfied on-site and will not unduly affect neighbouring residences; and
 - v) The proponent demonstrates that the neighbourhood has been consulted and residents have had an opportunity to express their concerns.

[Amendment Bylaw [No. 6], 2010, No. 2730 - Adopted May 25, 2010]

- j) In mixed commercial and multi-unit residential developments, buildings up to 12 storeys in height and with a floor area ratio of up to 3.0 for the residential portion of the building may be acceptable.
- k) Development proposals with heights and/or densities greater than those set out in Section 2.3.2 (j) may be considered, where appropriate, through variances to zoning and/or parking regulations and density bonusing of floor space where new commercial buildings provide affordable, accessible, or special needs housing units or amenities for the benefit of the community.

9.4 Development Permit Area No. 2 – Commercial

9.4.1 Scope

All lands designated Commercial on Schedule "C" are part of DPA No. 2.

9.4.2 Category

Section 919(1)(f) of the *Local Government Act* – form and character, commercial.

9.4.3 Justification

Traditionally, Esquimalt's commercial areas have not been developed on the basis of a particular theme or concept. The design and form of commercial development has been rather haphazard and, as a result, the Esquimalt Village and other local commercial areas do not have the cohesiveness nor the attractiveness they could have.

When asked in a recent questionnaire to identify what they disliked most about Esquimalt, an overwhelming number of respondents identified the lack of a downtown commercial area, with appropriate shops and services, and the appearance of Esquimalt Road in the village core.

Where new development is to occur within Esquimalt's commercial core, that development should add to the pedestrian appeal and overall appearance of the street through features such as easily accessible entrances, street furniture and public art, landscaping and attractive exterior finishing materials, and by their orientation to the street rather than to a parking lot or internal square.

The goals for Development Permit Area No. 2 are:

- a) to enhance the aesthetic image of Esquimalt's commercial district, particularly those areas that are considered community focal points, such as the Village, the Head Street/Esquimalt Road intersection and major entrance points to the municipality;
- b) to revitalize existing commercial areas by encouraging a variety of businesses;
- c) to encourage growth in the tax base through diversified commercial development and redevelopment of existing commercial areas; and
- d) to encourage integrated residential/institutional/commercial uses in commercial areas.

9.4.4 Requirements of Owners of Land within the Development Permit Area

- a) Owners of land within Development Permit Area No. 2 must not do any of the following without first obtaining a development Permit in accordance with the guidelines for this Development Permit Area:
 - i) subdivide lands; or
 - ii) construct or alter a building or structure;without first obtaining a Development Permit in accordance with the guidelines of this Development Permit Area.
- b) Exemptions:

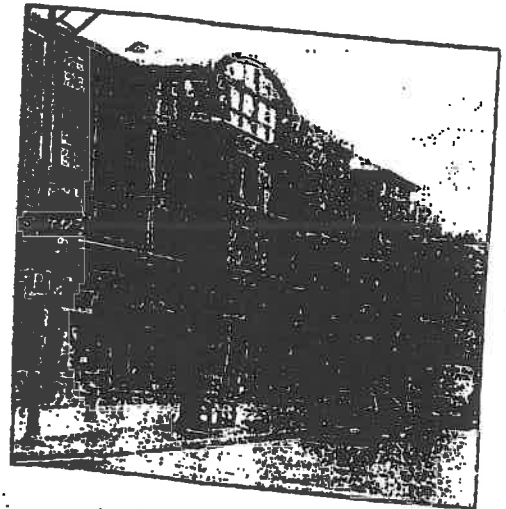
The following do not require a development permit:

- i) construction of buildings or structures less than 10 square metres in area;

- ii) minor additions to existing structures where the floor area of the addition does not exceed 10 percent of the ground floor area of the structure;
- iii) emergency repairs to existing structures and public walkways where a potential safety hazard exists;
- iv) fences;
- v) the cutting of trees as permitted by the municipal tree protection bylaw; and
- vi) replacement or changing of existing signs, provided the sign area is not to be increased.

9.4.5 Guidelines for Owners of Land within the Development Permit Area

- a) Commercial building facades should be appropriate to a pedestrian shopping area with windows facing the street and doors opening onto the street rather than onto a courtyard or laneway. (See image)
- b) Ornamental lighting that not only highlights the building but also increases the amount of light falling onto pedestrian areas should be used wherever possible. However, commercial lighting should not create unnecessary glare or shine directly into neighbouring residential properties.
- c) Buildings should be designed and sited to minimize the creation of shadows on public spaces.
- d) Where possible, weather protection (i.e. awnings and canopies) should be provided above all pedestrian walkways including walkways to on-site parking areas.
- e) Off-street parking areas should be located either at the rear of commercial buildings or underground. Surface parking should be screened with landscaping. Large parking areas should contain additional islands of landscaping.
- f) The design of new commercial buildings, including areas use for parking, should incorporate Crime Prevention through Environmental Design (CPTED) principles.
- g) Buildings may be located at the front property line in order to create a pedestrian-oriented environment, except where vehicle visibility is affected and on those streets that have been identified as requiring future road widening.
- h) Landscape screening and fencing should be located around outdoor storage areas and garbage and recycling receptacles.
- i) Retention and protection of trees and the natural habitat is encouraged wherever possible.





West Bay Neighbourhood Design Guidelines



We would like to thank the following members of the West Bay Design Guidelines Stakeholders Group whose local insight and involvement in the process was of great benefit to the development of these design guidelines.

West Bay Design Guidelines Stakeholders Group:

Brian Emmett
Kris Samuels
Lieutenant-Colonel Ulpiano Honorio
Carolyn Gisborne
Christina Clarke
Laurie Hurst
Jeff Miller
Janice Rose
Scott Hartman
Liz Dill
Jim Witter
Peter Hardcastle
Mark Lindholm
Sally Reid
Katrina Dwulit
Julie Flatt
Carole Witter

Submitted: 10 September, 2015.

Table of Contents

1. Purpose & Overview	1
2. Design Vision & Principles	7
3. Design Guidelines	8
Overview	8
A Friendly Face to the Street	9
Guidelines: Commercial and Mixed-Use Buildings	9
Guidelines: Residential Buildings	13
Neighbourliness.....	16
Architectural Concept: Achieving a Human Scale.....	18
Height	21
Green Healthy Buildings & Open Spaces	22
Public Realm Design: Pedestrian Comfort & Safety	24
Recommended Public Realm Improvements.....	25
4. Site Specific Guidelines: Harbourfront Urban Village High Street	26

1. Purpose & Overview

Purpose and Overview

These design guidelines were developed through a collaborative, community-based process to ensure new development enhances rather than detracts from West Bay's unique and rich character and identity. These design guidelines focus on the interface between and integration of public and private lands towards the preservation and enhancement of the identity, social vitality and overall liveability that characterizes the West Bay neighbourhood. This includes integration with federal Department of National Defense (DND) lands, integration of local ecosystems, and connection to the harbourfront and the range of recreational and other activities associated with the working harbour.

This document includes:

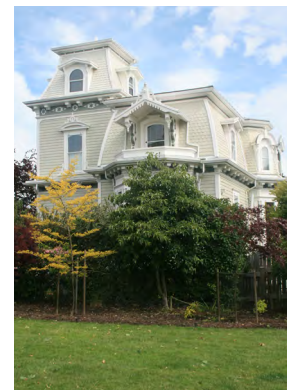
- An inventory and analysis of neighbourhood design characteristics;
- A vision statement and set of design principles as the rationale for the design guidelines;
- Development Permit Area design guidelines for Form and Character;
- A set of general design guidelines and recommendations for public realm design and improvements; and,
- Specific guidance on site planning and design for development opportunity sites along Head Street south adjacent to the harbourfront and including the "Triangle Lands", are also included.

The Spirit of West Bay

During the West Bay Design Guidelines Stakeholder Workshop, we asked: "What words capture the spirit of West Bay?" The following is a summary of responses:



Neighbourhood Characteristics: Photo Essay



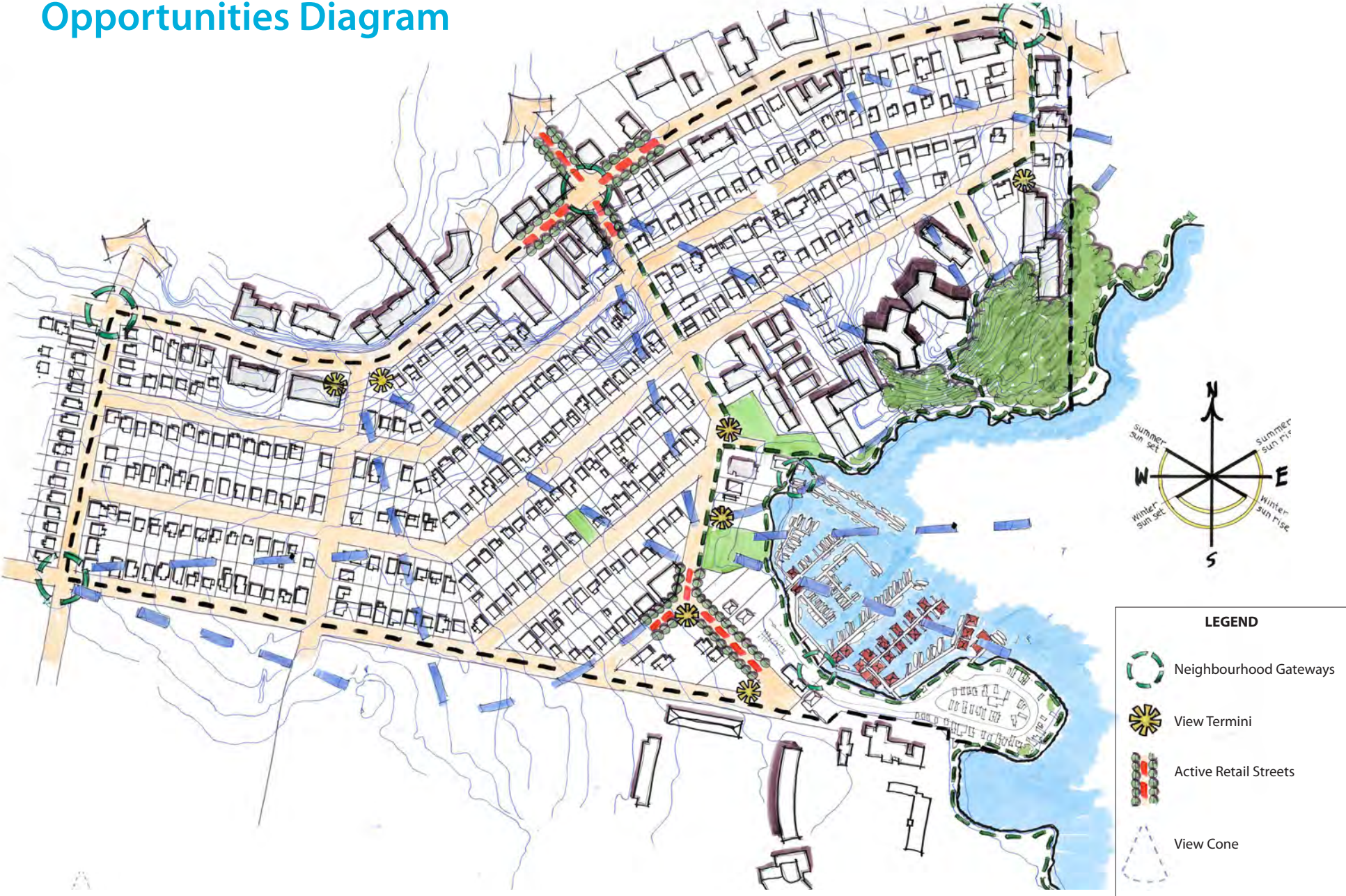
West Bay Ortho Photo and Study Area



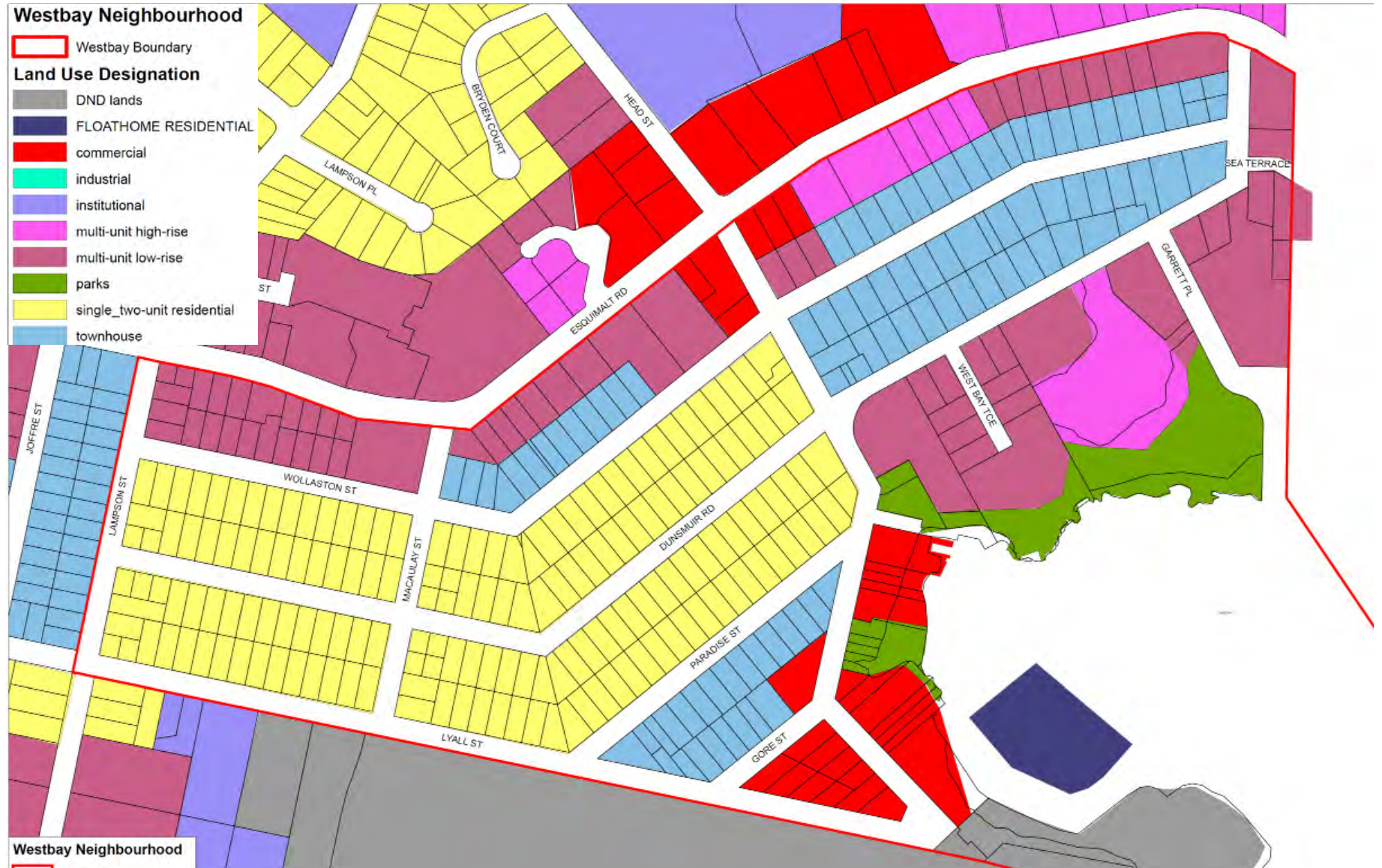
Neighbourhood Characteristics: Topography and Building Massing (Existing)



Neighbourhood Characteristics: Opportunities Diagram



West Bay Neighbourhood - OCP Land Use



2. Design Vision & Principles

Vision

West Bay is a thriving, attractive and walkable harbourfront neighbourhood. Its unique identity and sense of place is shaped by the natural and human history of its marine environment. A diverse mix of local, pedestrian-oriented shops, businesses and housing, together with active and attractive streets, harbourfront recreation, and sunny open spaces with strong connections to the harbour, make this a truly liveable neighbourhood and a popular destination for locals and visitors. The built, historical and natural environments are sensitively integrated to ensure a healthy shoreline and upland ecology.



Photo courtesy: Josh McCulloch, PictureBC

Design Principles

The following design directions for the public realm were identified through the workshop to help maintain and enhance the unique identity of West Bay:

- **Strong connection to the harbourfront:** Maintain and enhance visual and physical connections to the harbour and harbourfront trail network.
- **A walkable neighbourhood:** Publicly accessible buildings present a friendly face to streets and open spaces to make them active, attractive and inviting to pedestrians. Make connections within and to adjacent neighbourhoods to make walking easy.
- **Human scale:** Use architectural features, details, and site design elements that are of human proportion and clearly oriented for public, pedestrian activity. A building has good human scale if its details, elements, and materials allow people to feel comfortable using and approaching it on foot.
- **Marine ecology:** Enhance, restore and celebrate the unique marine and upland ecology. Layer and integrate mobility, recreation, stormwater, habitat and ecological systems in the context of the working harbour.
- **Harbourfront community & identity:** Ensure building and open space design that supports and enhances the unique sense of community and identity, including its architectural and marine heritage. More broadly, this includes the rich natural and human history of the area and associated expression of local culture, festivals, and gathering at and near the harbour front and water's edge.
- **Neighbourliness:** Ensure new development responds positively to the existing context by ensuring a sensitive transition in scale, by minimizing view and shadow impacts, and by responding to the positive design characteristics, if only subtly, of adjacent development.

3. Design Guidelines

Overview

Urban design is the comprehensive and cohesive combination of buildings, streets, open spaces and the natural environment and has, as its objective, the creation of memorable public spaces.

The essence of good urbanism is determined by the relationship between:

- the built and natural environments, and
- the public and private realm - the areas between buildings and public open spaces - at street level.

Buildings, streets, and other public open spaces scaled for human comfort and use are essential to the creation of a functional, aesthetically rich and vibrant neighbourhood. Building fronts and their orientation to streets and public open spaces are therefore the strongest determinants of the character and quality of neighbourhoods.

The design elements and approaches on the following pages provide a starting point and preliminary framework in maintaining and enhancing what is special, unique and wonderful about West Bay. They also provide a foundation to ensure new development and public realm improvements contribute to a vibrant, safe and accessible neighbourhood environment that encourages walking and street life while creating a unique social and physical environment that celebrates the beautiful natural setting of West Bay.



Photo: Joaquin Karakas



Photo: Joaquin Karakas

A Friendly Face to the Street

INTENT

- To define residential and commercial streets with active and attractive building fronts that have a positive orientation to streets and other public open spaces.
- To ensure buildings are sited and designed to be welcoming, and to encourage street vitality, visual interest, and safety.

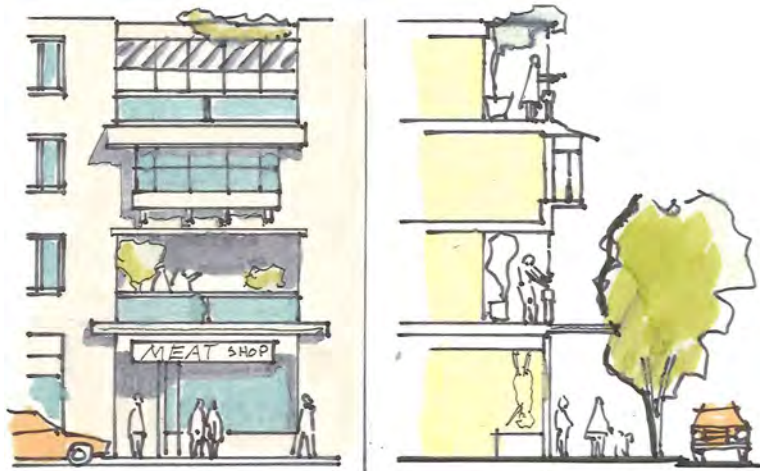
GUIDELINES: COMMERCIAL AND MIXED-USE BUILDINGS

- Locate publicly oriented active uses at grade and at or near the sidewalk edge.
- Incorporate transparent shop-front windows, frequent entrances, weather protection and pedestrian oriented signage into ground floor facades;
- A signage and lighting program for any commercial development should be designed as a totality, with signs, lighting, and weather protection architecturally integrated from the outset.

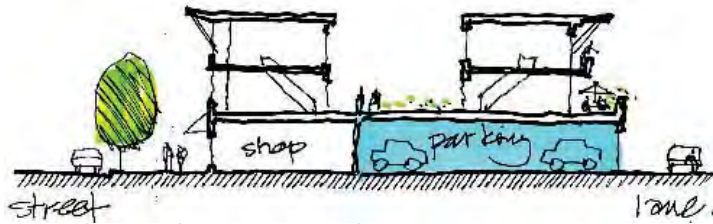


Active uses oriented to public streets and openspaces creates vibrant and attractive neighbourhoods (above, below and below left)





Frequent entrances along the street and upper storey balconies overlooking public open spaces adds to street vitality.



A shop front buffers public space from above ground structured parking.



Orient active frontages towards streets.

- Provide pedestrian access to storefronts and businesses from the adjacent public street, and orient upper-storey windows and balconies to overlook adjoining public open spaces.
- On corner sites, develop street-facing façades for both streets. Design front elevations with pronounced entrances oriented to the corner and/or primary streets.
- Locate on-street parking at the curb in front of shops.
- Avoid locating off-street surface or structured parking adjacent to active public streets and open spaces. Locate off-street parking behind or underneath buildings. Laminate or wrap any above ground structured parking with active (residential or commercial) uses to buffer structured parking from public open spaces.
- Achieve a minimum glazing area of 75% for frontages at grade along all commercial streets. Clear site lines from inside buildings to open public spaces should allow for casual surveillance of the street and sidewalk, and store interiors should be visible from the street.



Photo: Joaquin Karakas

On corner sites, develop street-facing façades for both streets. .

- Incorporate frequent entrances into commercial frontages facing public streets with a desired maximum spacing of 10 m.
- Recessed entrances to buildings from the sidewalk or property line are encouraged in order to provide for door swings, to protect the entrance from rain or snow, and to emphasize building entrances.
- Incorporate plantings, attractive lighting, signage, paving details, furnishings, street trees and other landscape details to create a comfortable, attractive, unique and well defined public realm.



Photo: Joaquin Karakas

Transparent shop fronts with lots of glazing and frequent entrances help create a welcoming and attractive streetscape (above and right)



Photo: Joaquin Karakas

Paving details, pedestrian oriented signage, furnishings and other landscape details add character.

Photo: Dan Burden



Incorporate plantings and other landscape details such as banners.

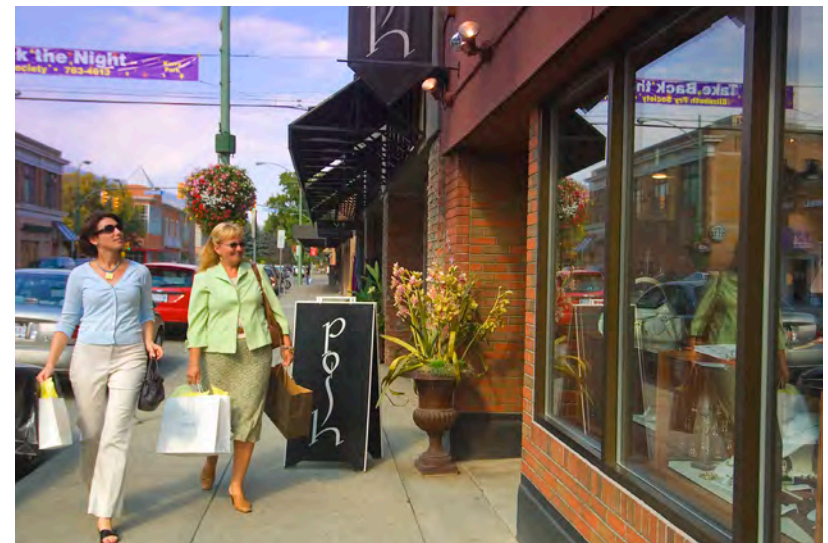


Screen



When unavoidable, blank walls should be screened with landscaping or through incorporate of a patio cafe or special materials to make it visually more interesting.

- Avoid expansive blank walls (over 5 m in length) and retaining walls adjacent to public streets. When blank walls and retaining walls are unavoidable, use an appropriate design treatment, such as the following:
 - » Install a vertical trellis in front of the wall with climbing vines or other plant material
 - » Set the wall back slightly to provide room for evergreens and conifers to provide year-round screening
 - » Provide art (a mosaic, mural, relief, etc.) over a substantial portion of the wall surface
 - » Employ quality materials of different textures and colours to make the wall more interesting visually
 - » Provide special lighting, canopies, awnings, horizontal trellises or other human-scale features that break up the size of the blank wall surface and add visual interest
 - » Incorporate walls into a patio or sidewalk café space
 - » Terrace (step down) retaining walls



GUIDELINES: RESIDENTIAL BUILDINGS

- Site and orient multi-plex, townhouse and apartment buildings to overlook public streets, parks, walkways, and communal spaces, while ensuring the security and privacy of residents.
- incorporate individual entrances to ground floor units in residential buildings that are accessible from the fronting street. This provides easy pedestrian connections to buildings, encourages street activity and walking, and enhances safety.
- Residential entries should be clearly visible and identifiable from the fronting public street to make the project more approachable and create a sense of association amongst neighbours.
- Emphasize front doors by incorporating a front patio or stoop and orienting front entryways prominently towards public streets and open spaces.
- Incorporation of a semi-elevated front entry way (1 m - 1.5 m) is encouraged to create a semi-private entry or transition zone to individual ground floor units. For these units, ensure an alternate access point that is accessible by wheelchair.

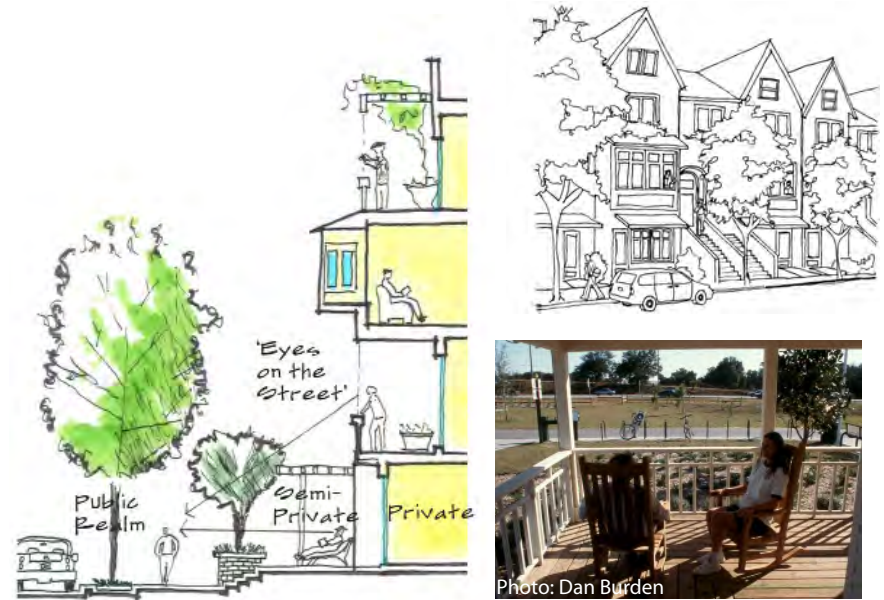


Photo: Dan Burden

Incorporate a front patio or stoop to create street activity and association among neighbours (above, below and below left)



Photo: Joaquin Karakas



Photo: Joaquin Karakas



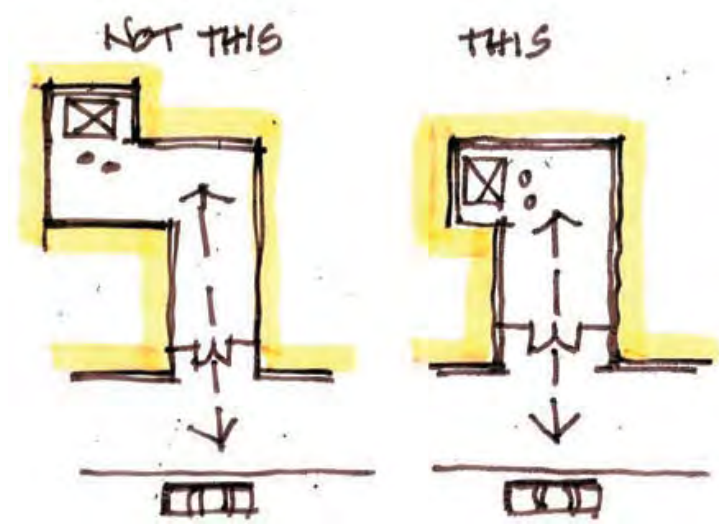
Locate off-street parking behind or underneath buildings, but never between the front face of a building and the fronting public street. .



Photo: Joaquin Karakas

Consider using a landscape transition zone between entryways and public sidewalks.

- Locating off-street surface parking behind or underneath buildings. Off-street surface parking located between the front of the building and the public sidewalk or adjacent to other public openspaces is strongly discouraged and should be avoided. When parking is accessed from the fronting public street, recess parking garages and entrances from the front face of buildings.
- A landscaped transition zone in between the entryway and public sidewalk should be considered on streets with high traffic volumes.
- Apartment lobbies and main building entries should be clearly visible from the fronting street with direct sight lines into them. Where possible, apartment lobbies should have multiple access points to enhance building access and connectivity with adjacent open spaces.



Direct sight lines into elevator lobbies are safer

VISUAL & PHYSICAL CONNECTIONS TO THE HARBOUR

- Physical and visual connections to landmark buildings, landscape features, the harbour, seascape, and other surrounding natural features are important components of West Bay's character and identity and therefore should be preserved and enhanced.
- New development and landscaping should frame rather than block public views of parks and openspaces, natural features, prominent buildings, public art, and the harbour.
- Locate and design buildings to preserve public street-end views (and where possible private views) to the harbour
- Where possible, create new public connections to harbourfront uses and activities at the waters edge, specifically Sailor's Cove, Hidden Harbour, and West Bay Marina's
- Mark/celebrate corners and street-end views through building and open space design.
- Water access and views to the West Bay harbourfront and upland neighbourhood from the water are equally important elements of West Bay's identity. Therefore future development must consider visual and physical connections to the neighbourhood from the water in considering future development.

- New development adjacent or near to the harbourfront should respond to relevant sections of the The Province of B.C. "Flood Hazard Area Land Use Management Guidelines" (2004) (http://www.env.gov.bc.ca/wsd/public_safety/flood/pdfs_word/guidelines-2011.pdf).



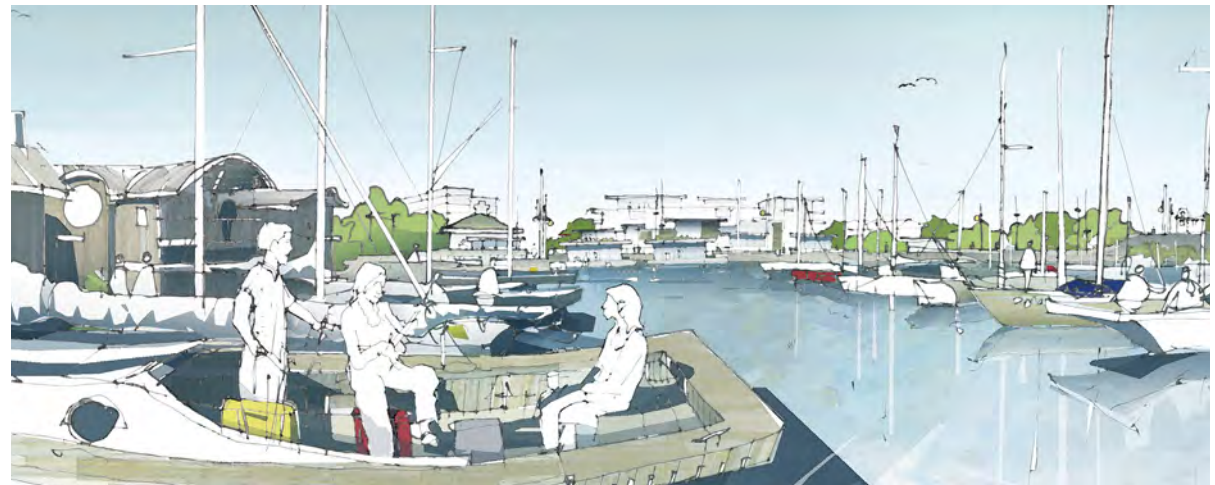
Photo: Bill Brown

Maintain street-end views to the harbour-front



Photo: Franc D'Ambrosio Architecture and Urbanism

Create new public connections to harbourfront uses and activities.



Neighbourliness

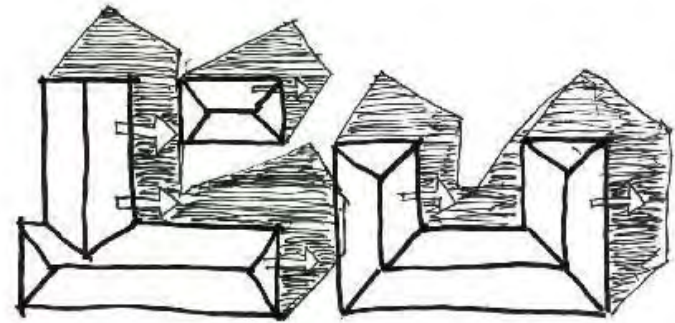
Buildings should respect adjacent properties by siting and designing new development to minimize disruption of the privacy and out-door activities of residents in adjacent buildings, and by ensuring buildings are sited to compliment the type, scale, and use of adjacent buildings.

GUIDELINES

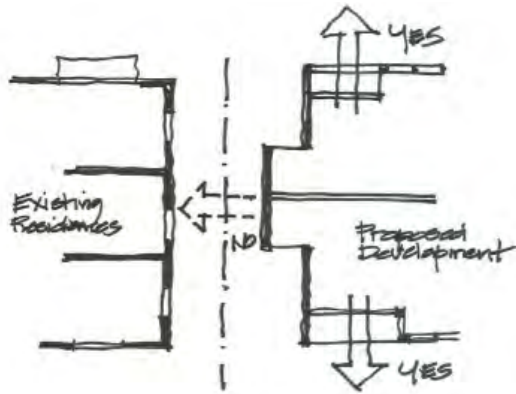
- New projects should provide a sensitive transition to near-by, less-intensive zones or areas with different uses. Projects on zone edges should be developed in a manner that creates a step in actual or perceived height, bulk, and scale between the anticipated development potential of adjacent zones.
- Buildings and groups of buildings should step down to be similar in height to adjacent buildings. This allows for an effective transition in scale and adequate sunlight penetration into open spaces and adjacent properties.
- In a mixed use project adjacent to a less intensive zone, the more compatible use and building type should be sited near the zone edge.
- Face similar uses across the street and at compatible scales; avoid building scale differences of more than 2 storeys across streets.
- Locate development to minimize view impacts on existing and planned future development.
- Buildings should be positioned and scaled to minimize the impact of shadows on adjacent open spaces, buildings, and within the project.
- Sun shade diagrams for the Winter Solstice will be required only when significant impacts are expected such as shading solar panels
 - » Winter Solstice: 9 a.m., 12 noon, 3 p.m.



Transition in scale to existing context



Sun shade analysis and diagrams help determine the siting of buildings to minimize overshadowing of adjacent open spaces and buildings



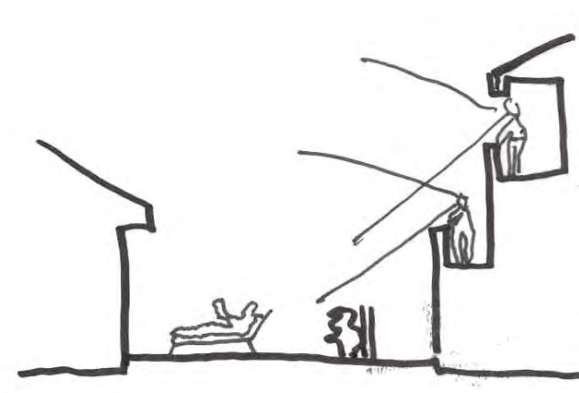
Protect privacy by placing primary (view) windows towards front and rear yards rather than interior side yards.

Photo: Joaquin Karakas



This building steps down to the slope to reduce the visual impact from it's massing

- Locate open space (plazas, parks, patios, cafes, etc.) south of permanently shading structures.
- Locating off-street surface parking in front of buildings, at prominent corners or intersections, immediately adjacent to public sidewalks and open spaces, and other public oriented active openspaces is strongly discouraged and should be avoided.
- Minimize impacts from sloping sites on neighbouring development. Examples of treatments to minimize impacts include using terraced retaining walls of natural materials, or stepping a building to respond to the slope.
- Views from upper stories of new buildings should minimize overlook into adjacent private yards, especially in less intensive areas. Following are some strategies which can be used to achieve this guideline:
 - » Increase building separation so that the face of the building and hence the windows are setback farther from the property line.
 - » Take advantage of site design that reduces impacts by using, for example, an adjacent ground floor area for an entry court.
 - » Stagger windows to not align with adjacent, facing windows.
 - » Primary windows into habitable spaces should not face interior side-yards



Minimize overlook into adjacent private yards

Architectural Concept: Achieving a Human Scale

OVERVIEW AND INTENT

These general guidelines for architectural are not intended to be prescriptive, but rather to encourage flexibility and innovation in building design and character. The overall intent is to create buildings and other structural elements that are scaled to the pedestrian, encourage pedestrian activity and welcome users.

Human Scale

Achieving human scale refers to the use of architectural features, details, and site design elements that are of human proportion and clearly oriented for pedestrian activity. A building has good human scale if its details, elements, and materials allow people to feel comfortable using and approaching it.

Building Articulation

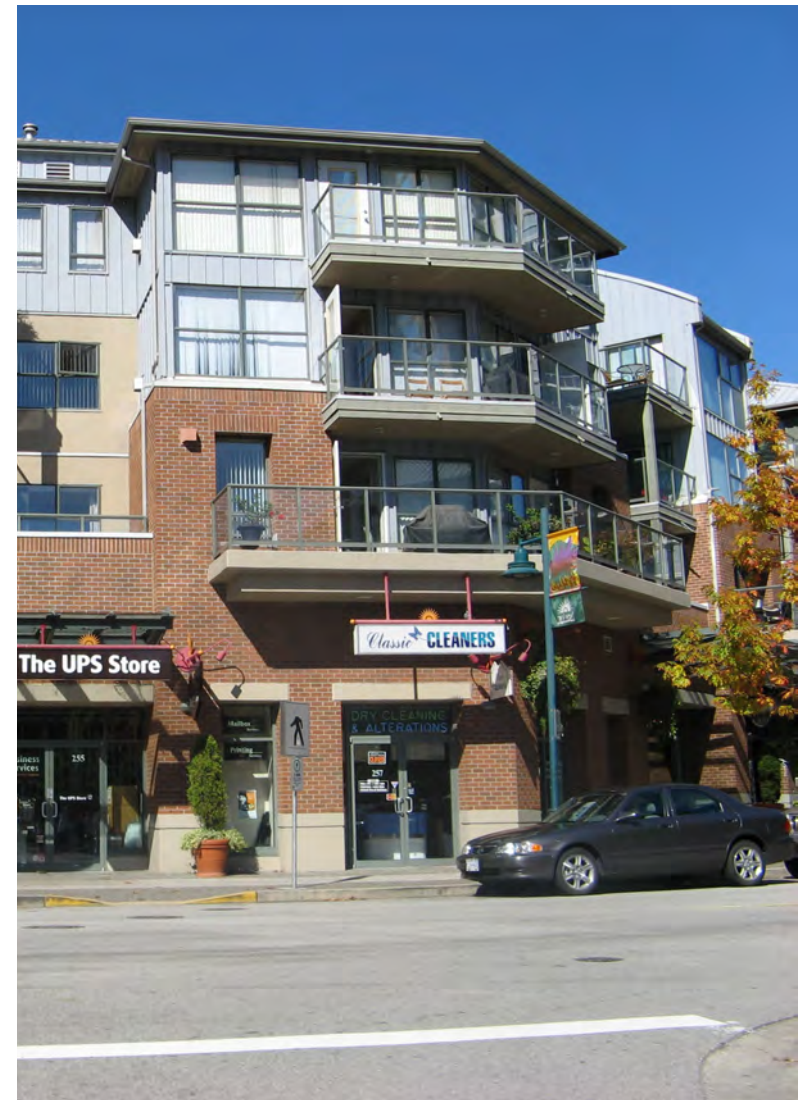
Many street frontage design elements, both horizontal and vertical, help to create an interesting and welcoming streetscape. These include building materials, special ground floor design treatments, façade modulation, corner treatments, building step-backs for upper storeys, and façade elements such as window treatments, building entries, and other architectural details. All of these help define the public realm as a welcoming place.



Human scale elements and building articulation arranged in a modern composition.

GENERAL GUIDELINES

- The design of new buildings and renovated existing buildings should express a unified architectural concept that incorporates both variation and consistency in façade treatments (for example, by articulating façades into a series of intervals).
- Design buildings to express their internal function and use.
- Incorporate into building façades a range of architectural features and design details that are rich and varied to create visual interest when approached by pedestrians.
- Examples of architectural features include:
 - » Building height, massing, articulation and modulation
 - » Bay windows and balconies
 - » Corner features accent, such as turrets or cupolas
 - » Decorative rooflines and cornices
 - » Building entries
 - » Canopies and overhangs
- Examples of architectural details include:
 - » Treatment of masonry (ceramic tile, paving stones, brick patterns, etc.)
 - » Treatment of siding (for example, the use of score lines, textures, and different materials or patterning to distinguish between different floors)
 - » Articulation of columns and pilasters
 - » Ornament or integrated artwork
 - » Integrated architectural lighting
 - » Detailed grilles and railings
 - » Substantial trim details and moldings
 - » Trellises and arbors



Incorporation of a range of architectural details and features make this mixed-use building attractive when approached by pedestrians



Photo: Franc D'Ambrosio Architecture and Urbanism

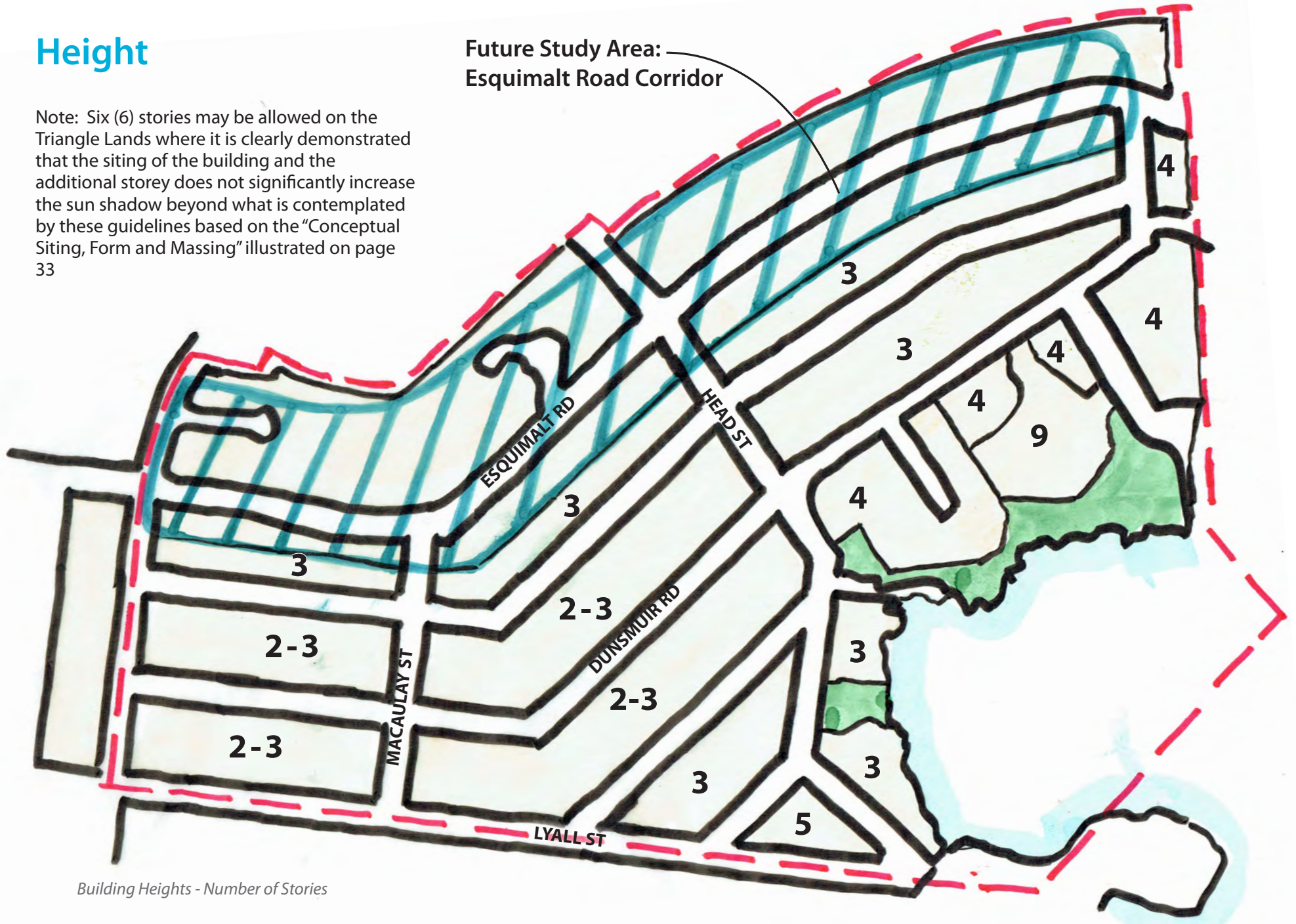
Design entrances to distinguish between individual commercial and residential.

- Locate and design entrances to create building identity and to distinguish between individual commercial and/or residential ground floor units. Use a high level of architectural detail and, where appropriate, landscape treatment to emphasize primary entrances and to provide “punctuation” in the overall streetscape treatment.
- Design balconies as integral parts of buildings and to maximize daylight access into dwellings through the use of glazed or narrow metal spindle guardrails.
- Clearly distinguish the roofline from the walls of buildings (for example, through the use of a cornice, overhang, or decorative motif).
- Windows can be used to reinforce the human scale of architecture by incorporating individual windows in upper storeys that:
 - » Are vertically proportioned and approximately the size and proportion of a traditional window
 - » Include substantial trim or molding
 - » Are separated from adjacent windows by a vertical element
 - » Are made up of small panes of glass
 - » Are separated with moldings or jambs but grouped together to form larger areas of glazing
- The use of figured or frosted glass or tinted glazing is discouraged for windows facing the street except for compatible use of stained glass or where figured or frosted glass comprises a maximum 20% of the glazing. This creates a welcoming, visually interesting and transparent street frontage.
- In general, new buildings should incorporate natural building materials into façades to avoid a “thin veneer” look and feel, and combined with more modern treatments, such as glass, concrete, and steel.
- Vinyl siding, large expanses of stucco, swirl type stucco, and vinyl for window frames are generally discouraged.

Height

Note: Six (6) stories may be allowed on the Triangle Lands where it is clearly demonstrated that the siting of the building and the additional storey does not significantly increase the sun shadow beyond what is contemplated by these guidelines based on the "Conceptual Siting, Form and Massing" illustrated on page 33

Future Study Area:
Esquimalt Road Corridor



Building Heights - Number of Stories

Green Healthy Buildings & Open Spaces

- Building design and site planning should reduce the overall “ecological footprint” (energy use, waste, and pollution) of new development while also maximizing liveability. This can be achieved by maximizing passive lighting, heating and cooling, providing usable outdoor amenity spaces, and being responsive to the existing ecosystems and natural context.
- Design residential buildings to receive daylight and natural ventilation from at least two sides of the building, or from one side and a roof. Where possible, dwellings should have a choice of aspect: front and back, or on two sides (for corner units).
- Dwelling units with exterior access on only one side should always face a good view or the direction of the sun (ideally both) and are most suitable as wide frontages with shallow floor plans to allow adequate penetration of daylight.

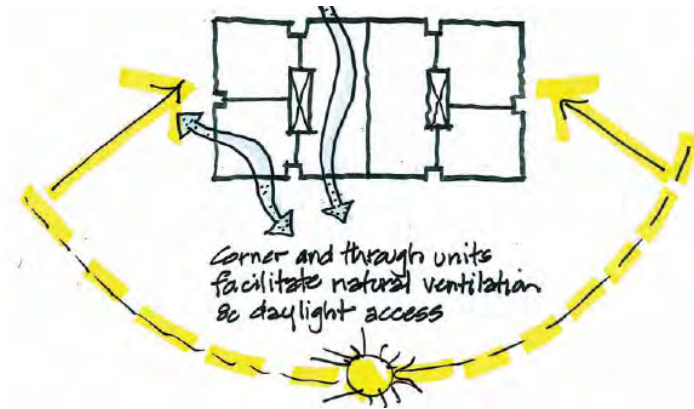
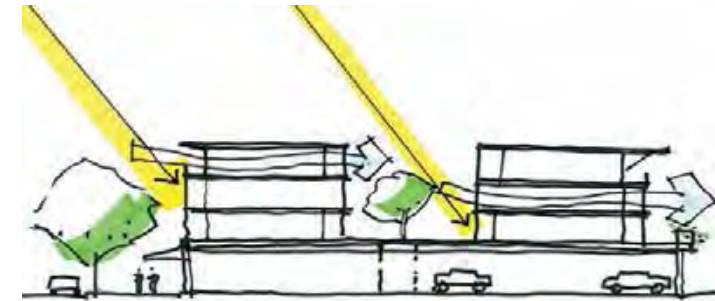


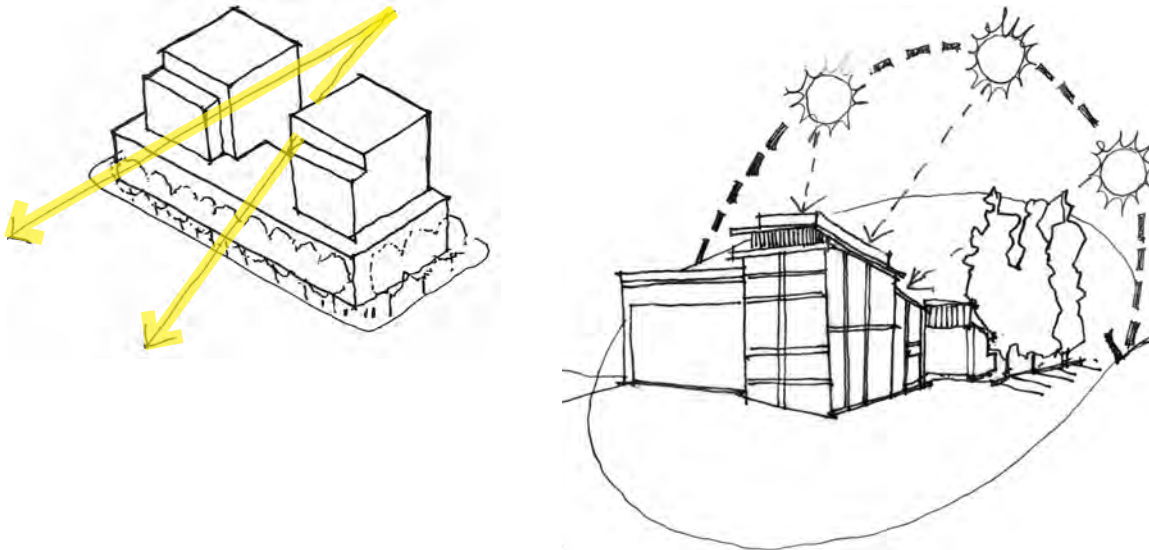
Photo: Joaquin Karakas

Incorporate common areas as defining elements of projects.



Photo: Joaquin Karakas

A landscaped pathway located adjacent to this residential building partially screens the lower floor units from the park (foreground) while also softening the building's appearance, helping to integrate it into the landscape.



- New buildings should not block significant views or solar access to adjacent buildings and open spaces.
- Incorporate courtyards, greenways, gardens and other common areas as defining elements of projects.
- Where at-grade space is limited, rooftop patios, gardens and courtyards are encouraged.
- Retention and infiltration best management practices for rainwater should be used as appropriate.
- Residential buildings should incorporate direct access to a usable private outdoor space such as a patio, balcony, or upper level terrace.

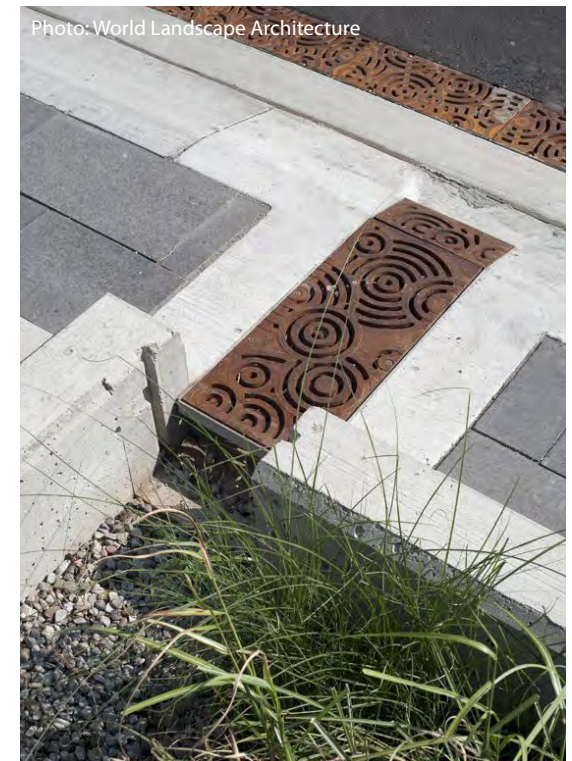


Photo: World Landscape Architecture

Use best management practices for rainwater where appropriate.

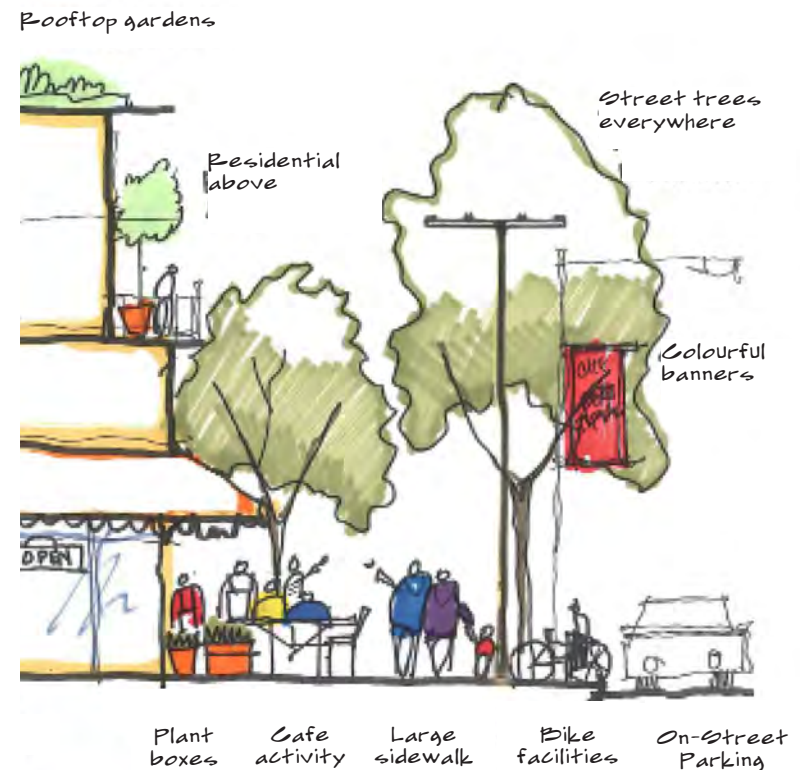
Public Realm Design: Pedestrian Comfort & Safety

INTENT

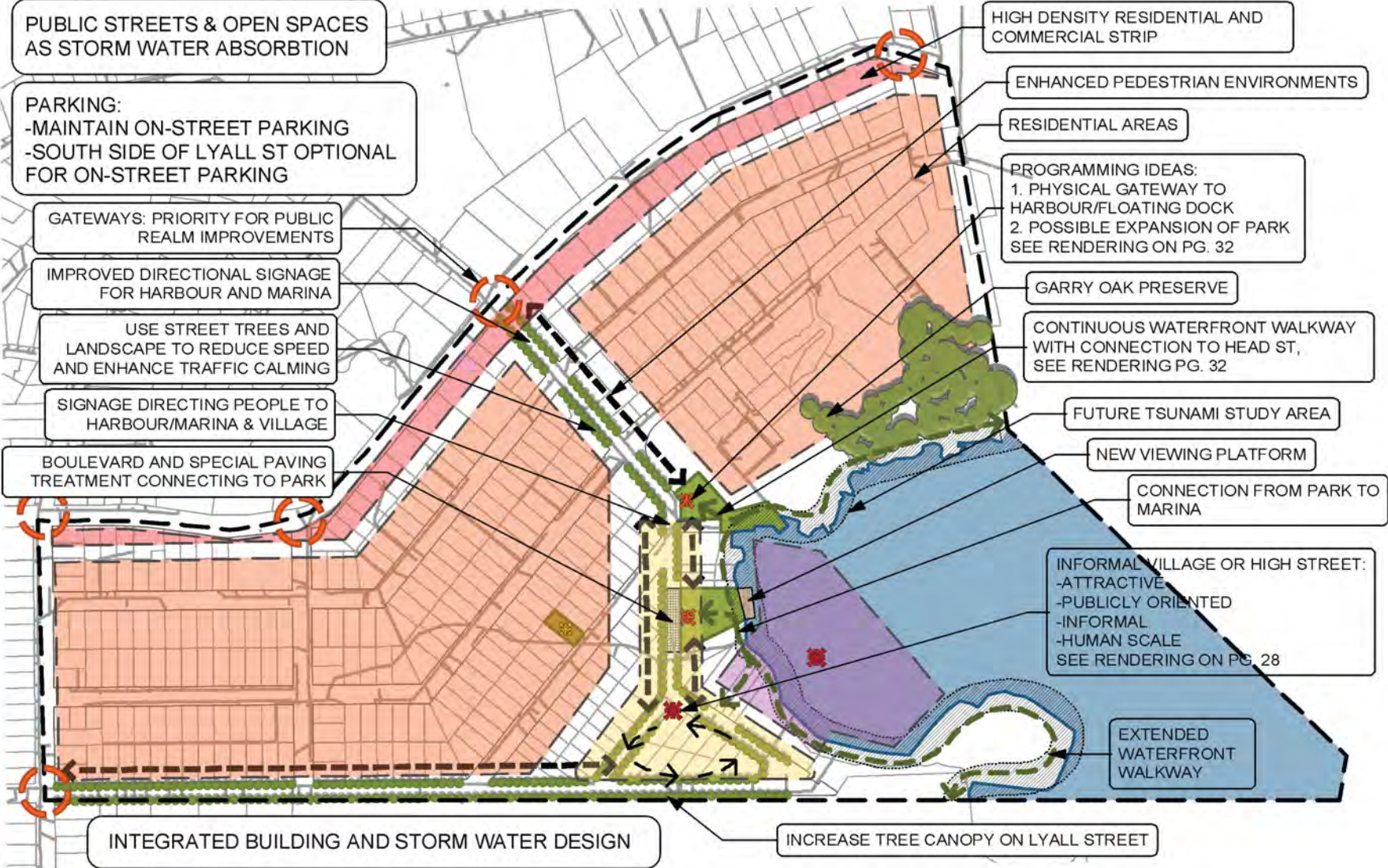
To ensure that the design of streets and open spaces creates visual interest, comfort, and safety for pedestrians and contributes to a unique local identity and sense of place for West Bay.

GUIDELINES

- Provide a continuous planting of street trees along both sides of neighbourhood streets, with priority for the Head Street South Village High Street and Esquimalt Road as indicated in the Recommended Public Realm Improvements map on page 25.
- Provide zebra- or ladder-painted crosswalks, or crosswalks made of special paving materials, at all key pedestrian crossings to increase driver awareness.
- Incorporate corner bulges into streetscape design in key pedestrian areas to enhance pedestrian crossings and provide space for landscaping, seating, rain gardens and public art.
- Provide adequate public streetscape amenities including benches, planters, garbage receptacles, bike racks, and distinctive bus shelters in areas with high pedestrian activity.
- Café tables are permitted and encouraged on public sidewalks in retail areas
- Pedestrian-oriented lighting should be provided with priority for high-activity pedestrian areas such as along Head Street and Esquimalt Road.
- Small plazas and squares should be incorporated in prominent locations with good solar gain, such as at Lyall and Head Street.
- Public art should be incorporated into parks, plazas and other key activity areas, especially at gateways and entry points, to enhance the sense of identity and entry into the neighbourhood.



Recommended Public Realm Improvements



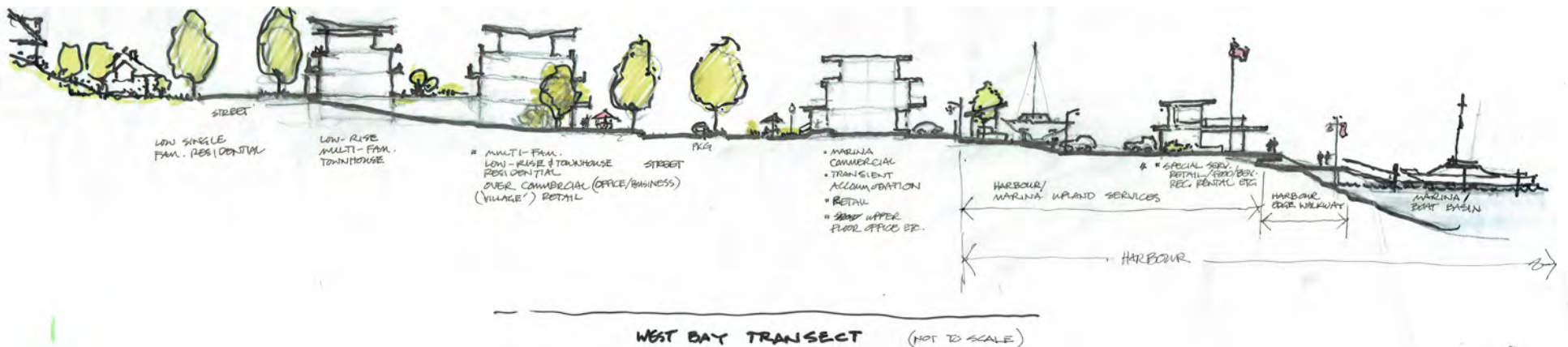
4. Site Specific Guidelines: Harbourfront Urban Village High Street

An important objective in the development of the design guidelines to address local concerns about density, height and massing, shadowing and view impacts, as well as the interface between federal lands and local ecosystems, specifically, in the harbourfront urban village area centred on Head Street South between Lyall and Paradise Streets. To this end, these guidelines were developed with input and feedback from neighbourhood stakeholders. This included an integrated design workshop (charrette) where design concepts and approaches for both public and private realms were developed. The sketch examples on this page were produced during this charrette.

This section is intended to provide site planning and design guidance and recommendations for this part of the West Bay Neighbourhood, based on stakeholder input. It is important to note that the concepts and illustrations on the following pages do not represent a development proposal, but rather a conceptual implementation of the preceding West Bay Neighbourhood Design Vision, Principles and Guidelines.



Sketches from the West Bay Design Workshop (above and below)



URBAN VILLAGE HIGH STREET LOOKING NORTH-WEST AT CORNER OF HEAD AND LYALL STREET

Maximum 5 storey building height



Urban Village high street defined by low scale buildings with active fronts and lush street tree canopy

Harbourfront retail, food and beverage Uses

New public connection to harbourfront and Hidden Harbour Marina

Landmark corner plaza located at south gateway to urban village with views to harbourfront

URBAN VILLAGE HIGH STREET (HEAD STREET SOUTH) LOOKING SOUTH

Street trees, pedestrian oriented lighting and banners line either side of the street.

Continuous planting of street trees along south-west side of Head Street

Maximum 3 storey street wall along Head Street south

A rhythm of pedestrian oriented shop fronts and entryways line the street with sidewalk cafes and other uses spilling out onto the sidewalk

Landscaped bulb-outs and parking bays on north-east side of Head Street



Marked (zebra striped) pedestrian crossings at Gore and Lyall Streets

LOOKING NORTH TO HARBOUR AND URBAN VILLAGE HIGH STREET

*Publicly oriented active uses
activate the harbourfront*



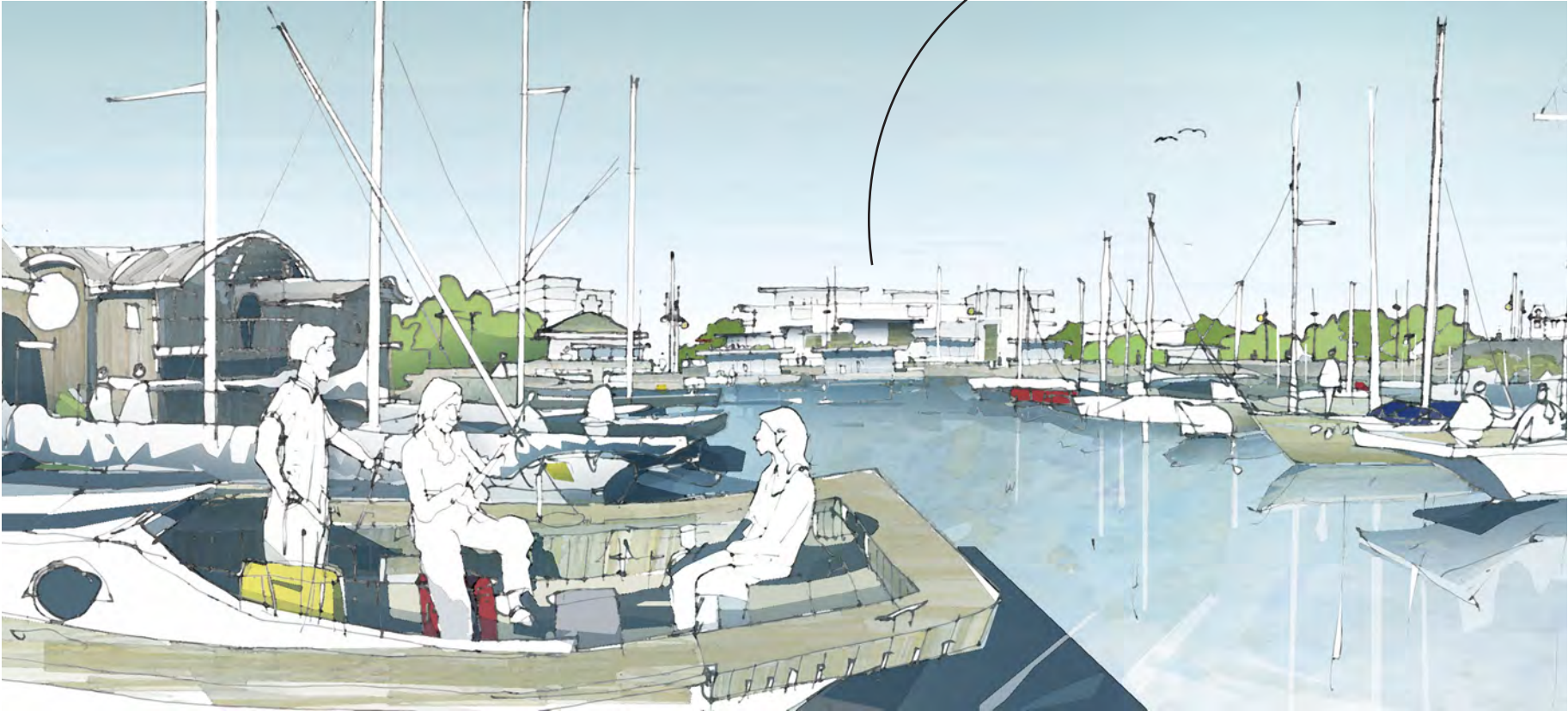
*Parking areas along the
harbourfront represent great
redevelopment opportunities where
appropriate*

*Continuous walking connecting
Songhees to West Bay Urban
village/marina and beyond*

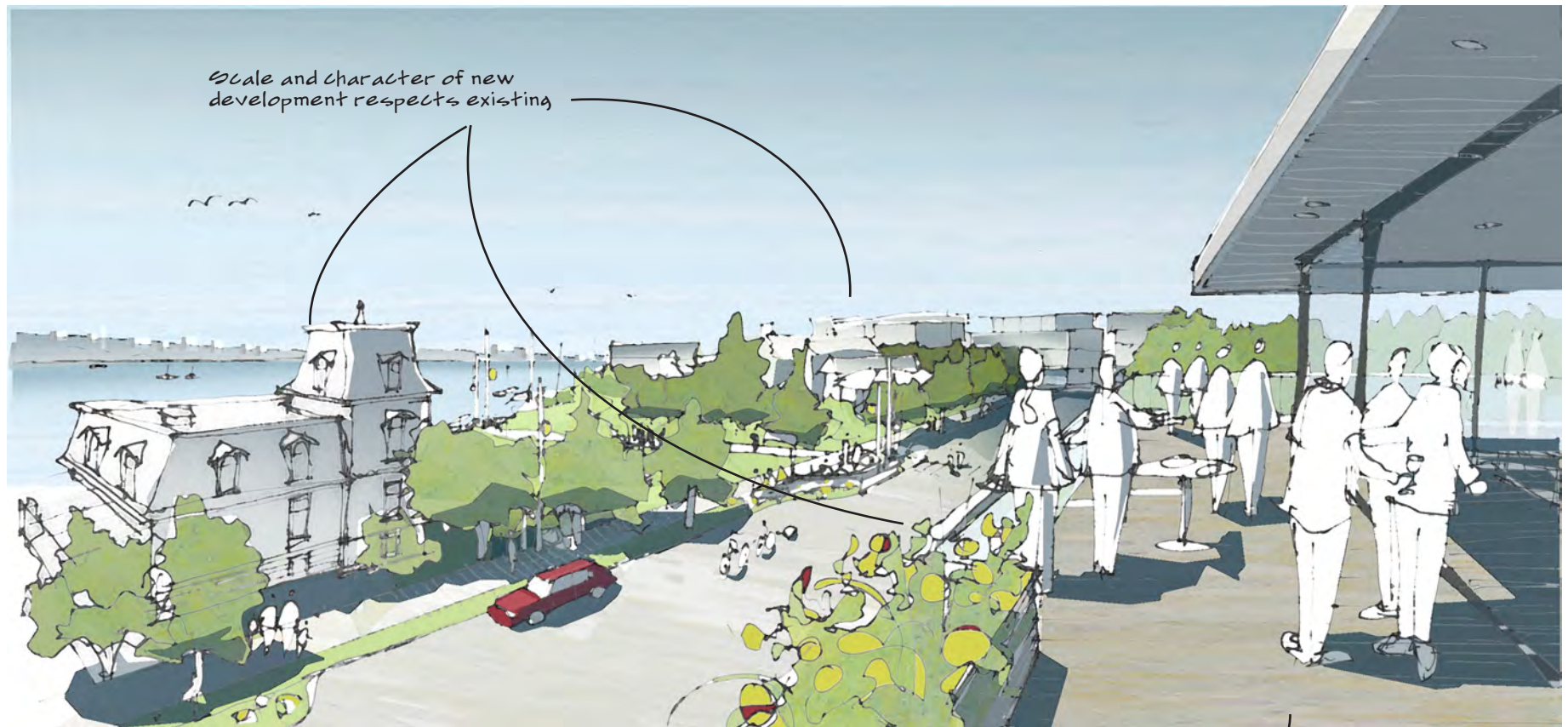
VIEW TO TOWARDS URBAN VILLAGE HIGH STREET FROM THE HARBOUR LOOKING WEST

Water access and views to the West Bay harbourfront and upland neighbourhood from the water are equally important elements of West Bay's identity.

New development scaled to be compatible with, and not overwhelm, harbourfront character and identity. Also exhibits the transition massing from existing 7 storey on DND land.



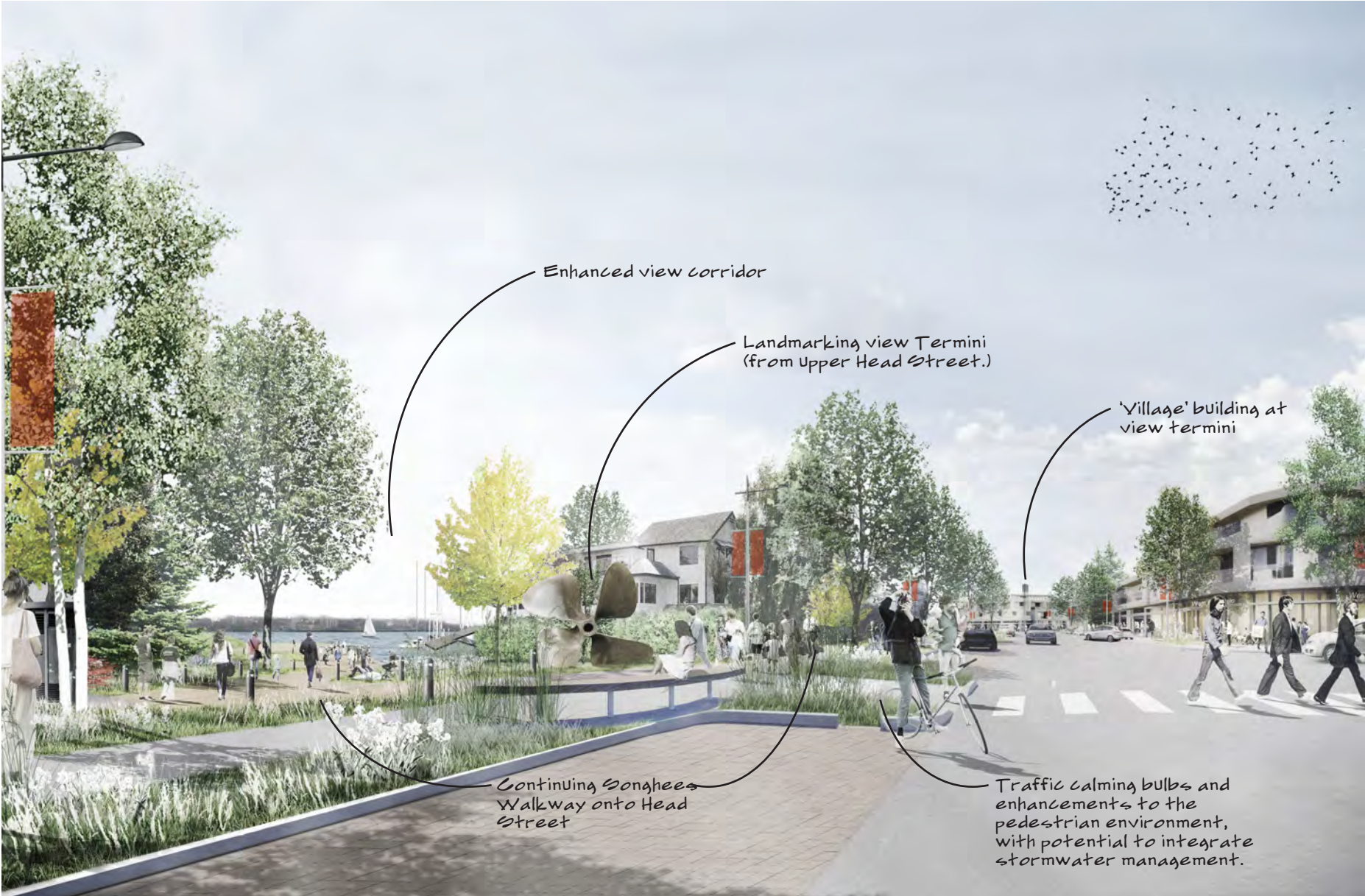
VIEW FROM FUTURE BUILDING LOOKING SOUTH-EAST TO HARBOUR ACROSS URBAN VILLAGE HIGH STREET AND CAPTAIN JACOBSON PARK



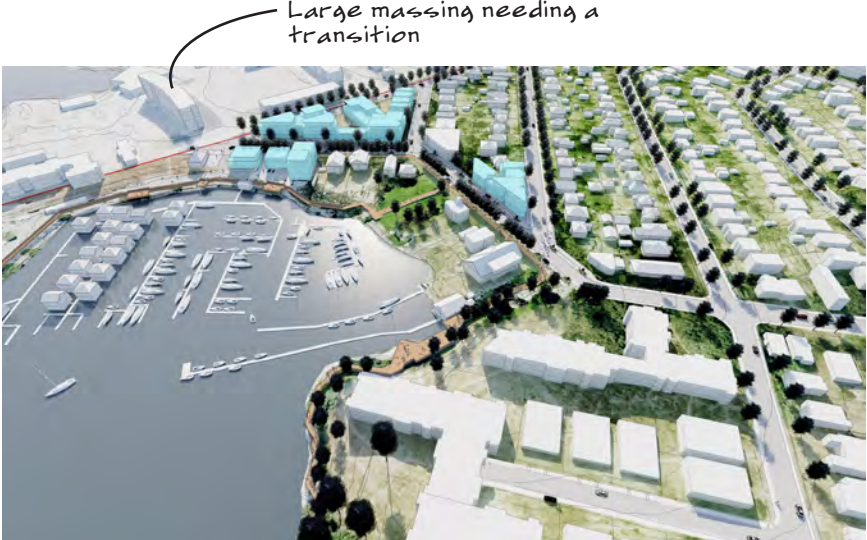
Scale and character of new development respects existing

Upper storeys step back to let sun onto urban village high street

HEAD STREET LOOKING SOUTH TOWARDS GORE AND LYALL



URBAN VILLAGE HIGH STREET: CONCEPTUAL SITING, FORM AND MASSING



Large massing needing a transition

- Legend**
- Conceptual massing
 - Continuous Boardwalk/Walkway



Continue walkway onto Head Street

Masses oriented to let southern light into spaces between buildings



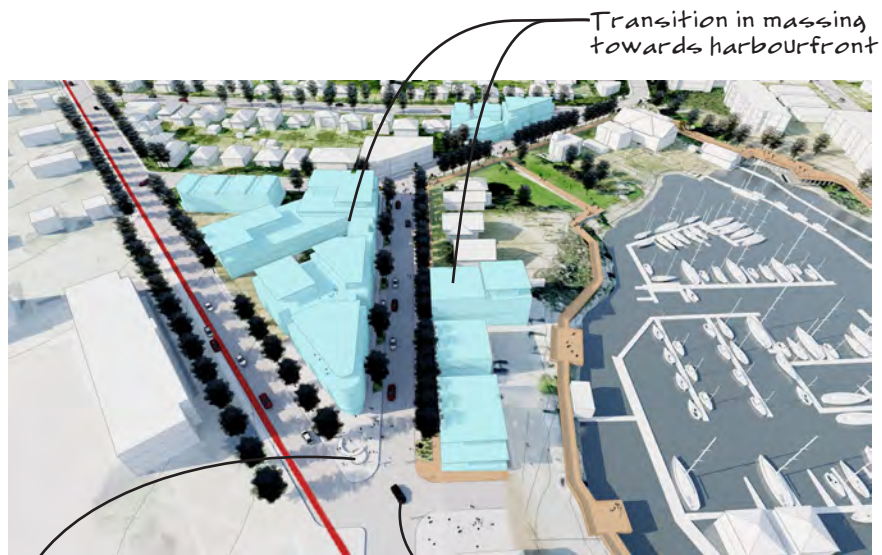
Continuous walkway

Public realm enhancements opportunity areas

New intersection

Improved view corridor

View on page 32.



Transition in massing towards harbourfront

Landmark corner plaza located at South gateway to urban village, with views to harbourfront. See sketch on page 27.

New intersection



View on page 28

Preserve and highlight heritage building

View termini



Streetscape Guidelines

OVERVIEW

The design, construction and maintenance of streetscapes in the West Bay Neighbourhood should serve to reinforce its unique identity by incorporating:

1. Strong connection to the harbourfront;
2. A walkable neighbourhood;
3. Human scale;
4. Harbourfront ecology;
5. Harbourfront community & identity, and
6. Neighbourliness.

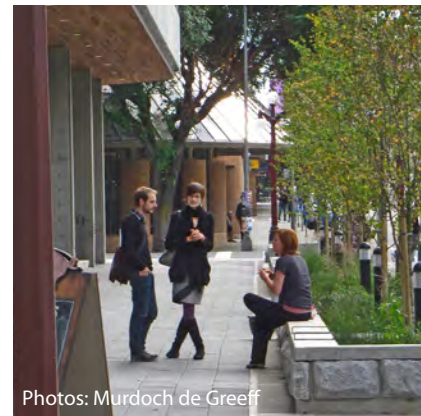
To illustrate the integration of these principles four typical streetscape sections and plans are presented below. These include Head Street North, Head Street South, Lyall Street West, and Lyall Street East. The sections focus on Public Realm improvements within two distinct areas of the West Bay Neighbourhood; West Bay Urban Village Gateways and West Bay Urban Village.

WEST BAY URBAN VILLAGE GATEWAY

West Bay Urban Village is accessible by Lyall Street and Head Street. Both streets currently have 18m wide Right of Ways (R.O.W) and accommodate two way traffic in standard 4.25m wide lanes. Objectives for the Lyall Street and Head Street Gateways include: improving the pedestrian environment, maintaining on-street parking, improved wayfinding, increased tree canopy, and integrated stormwater management. The Lyall Street West section shows a typical treatment through an existing residential lot on Lyall across from the Department of Defense (DND) lands. The Head Street North section shows a typical treatment near the intersection of Head and Esquimalt.

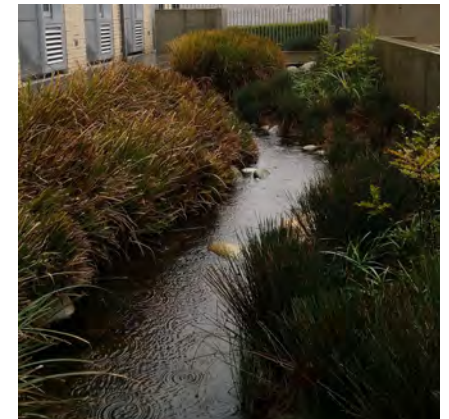
WEST BAY URBAN VILLAGE

West Bay Urban Village is located on the harbour front adjacent to the Marina. The area is identified for potential future development with a primary focus on the 'Triangle Property'. Some of the objectives for the Urban Village include: increased density, creation of places to gather, an inviting and active public realm, and reduced speed/traffic calming. Both the Lyall Street East section and Head Street South section show typical treatments at the triangle property and suggest a 'Skinny Streets' approach with reduce travel lane widths (3.0m)



Photos: Murdoch de Greeff

Integrated Storm Water Management, The Atrium building, Victoria BC



Integrated Storm Water Management, Reliable Controls building, Victoria BC

Integrated Storm Water Management

Integrate rain gardens, swales, tree planting, and other suitable BMP's to capture, slow, and treat storm water before in enters the harbour.



Photos: Murdoch de Greeff

Storm Water Plaza,
Cecelia Road, Victoria BC



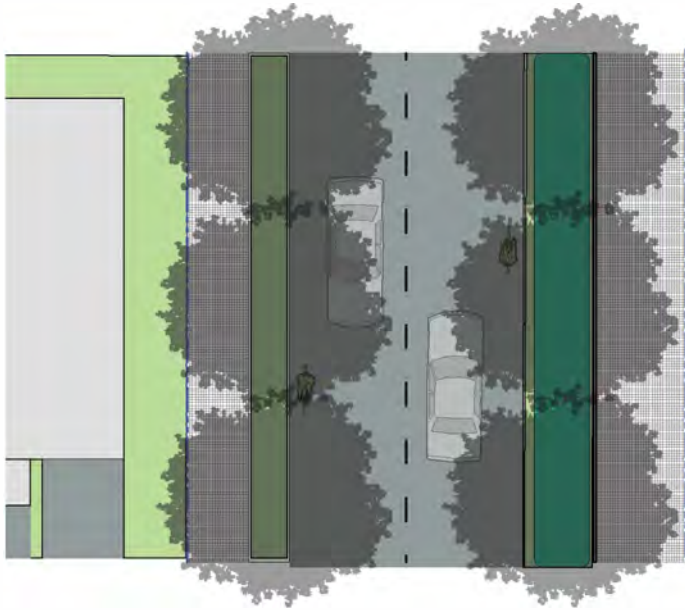
Photo: Paul de Greeff

Storm Water Streetscape,
Portland, Oregon



Photo: Murdoch de Greeff

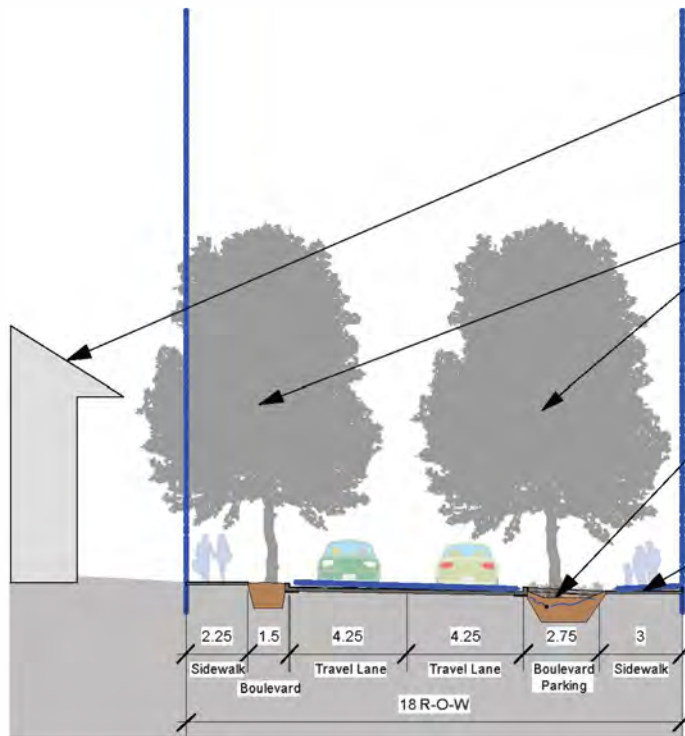
Storm Water Traffic Bulge,
Trent St., Victoria BC



Head Street North

Head street North, near the intersection of Head and Esquimalt Road, is intended to act as a Gateway to the Village, Harbour, and Marina. The intent on this section of Head is to increase the appeal of the pedestrian environment, employ clear wayfinding strategies to direct traffic towards the Village and/or Harbour/Marina.

Strategies to achieve this include: Increased sidewalk width to encourage walking, a double row of trees to accentuate Head as a linear corridor to the harbour/village, source controls such as onstreet rain gardens to treat stormwater, and similar building scales to reinforce the street end/harbour as a focal point. Development fronting onto the east side of this section of Head street should maintain physical and visual permeability to the waterfront by integrating paseos and achieving a minimal building separation of 6 metres



Neighbourliness-

Transition building heights from shoreline to upland areas to minimize shadow and view impacts.

Human Scale-

Utilize a double row of trees to accentuate the linearity of Head Street as a Gateway and the Village as a destination at the terminus.

Upland Ecology-

Install onstreet rain gardens to capture, treat and slow street run off before it enters the harbour downstream.

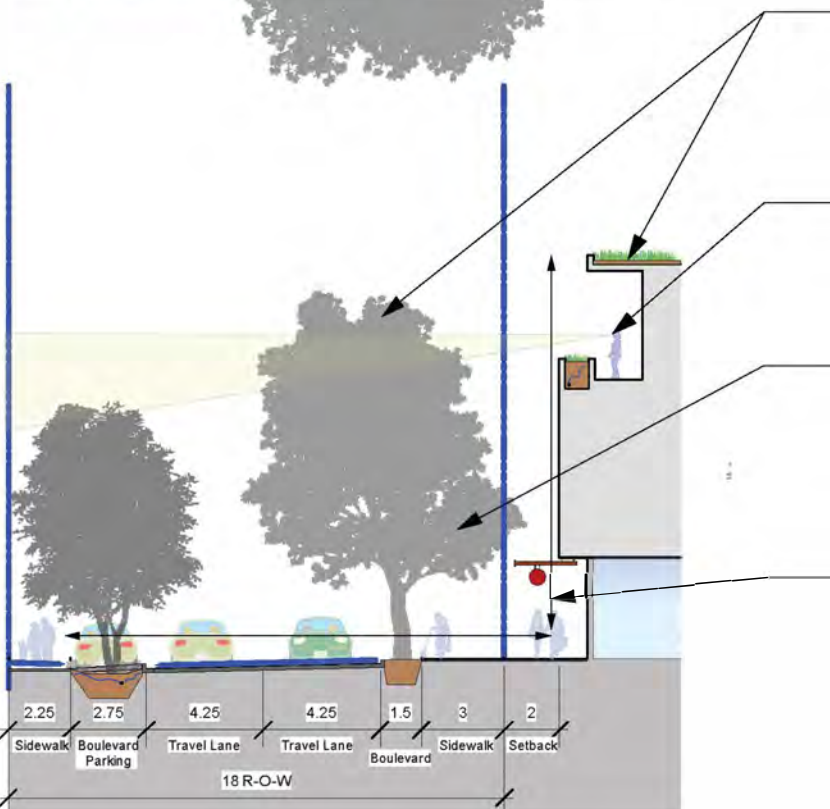
Walkable Neighbourhood-

Utilise vegetated boulevard strips to provide separation from vehicular traffic and increase sidewalk widths on gateway streets to promote wayfinding while contributing to attractive and inviting pedestrian environments.

Head Street South

Head street South, is intended to act as the communities Village or High Street. The intent on this section of Head is to provide public open spaces that contribute to a sense of being on the waterfront while supporting the commercial viability of the area. The *West Bay Village* public space should include but is not limited to waterfront access, space for gathering and festivals, and an attractive pedestrian oriented environment.

Strategies to achieve this include: *Distance to Building Height Ratios* of between 1:1 and 3:1, use of green building techniques to preserve the health and visual aesthetic of the shoreline, and transitions in scale to perserve upslope and adjacent views.



Waterfront Community and Identity-

Green Roofs, street trees and building heights support each other to reinforce the identity of West Bay as a Green Waterfront Community. Designs acknowledge views both to and from the inner harbour.

Strong Connection to the Waterfront/Neighbourliness-

Buildings step back and transition in scale to maximize potential for harbour views while minimizing shadow impacts on adjacent properties.

Waterfront Ecology-

Tree plantings intercept rainwater, reduce storm water flows through evapotranspiration, and provide valuable bird habitat contributing significantly to the shoreline ecology.

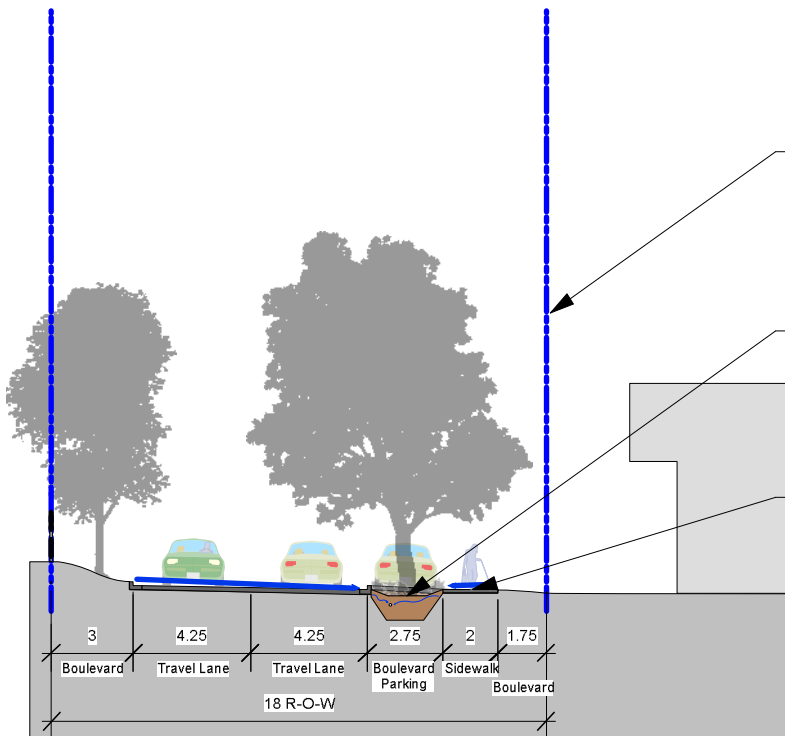
Human Scale-

Buildings should be between 1:1 and 3:1 *Distance to Building Height Ratio* to create intimate and inviting public open spaces.



Lyall Street West- Existing Residential

West Lyall Street acts as an important connection from the harbour to the surrounding residences, Esquimalt village, and the Esquimalt Rec. Center. Future developments along this corridor should focus on improving the pedestrian environment to increase the walkability of the neighbourhood. Strategies to achieve this include: Tree planting to reduce the visual scale of the street and provide shade, green infrastructure such as rain gardens to intercept stormwater flows before they enter the harbour downstream, and separated boulevards to increase the attractiveness of the pedestrian environment.



Human Scale-

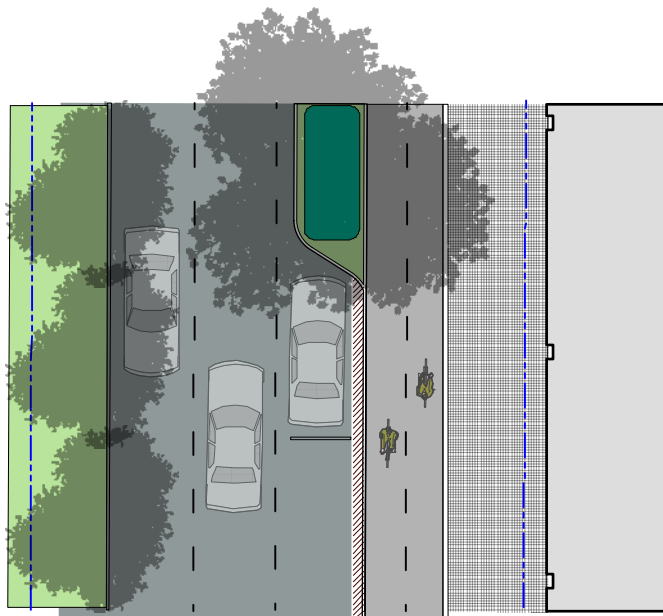
Tree planting can reduce the visual scale of the street while offering summer shade for the street and sidewalk.

Waterfront Ecology-

Integrated rain gardens to treat stormwater and provide habitat for birds and pollinators.

A Walkable Neighbourhood-

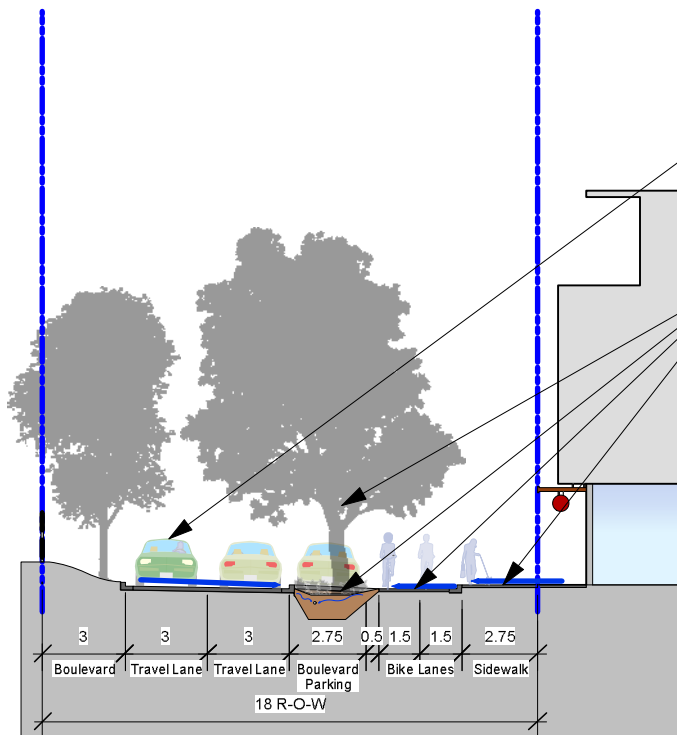
Use separated boulevards to remove sidewalks from vehicular traffic and increase the attractiveness of the pedestrian environment.



Lyall Street East- West Bay Village

East Lyall Street at the harbour interface is meant to act as part of the *West Bay Village*. The intent here is to contribute to the sense of a waterfront village while providing an attractive pedestrian environment with places for festivals and gatherings. Located adjacent to the DND lands Lyall street, in general, should focus on pedestrian improvements on the North side which is more heavily used by the public.

Strategies to achieve this include: Tree planting to reduce the visual scale of the street, vehicular travel lane reductions to provide space for bike lanes and encourage alternative modes of transportation, green infrastructure such as rain gardens to intercept stormwater flows before they enter the harbour, and seperated boulevards to increase the attractiveness of the pedestrian environment.



Walkable Neighbourhood

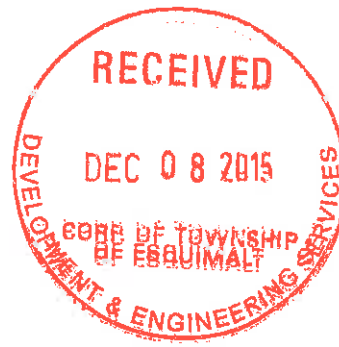
Skinny Streets help to maximize pedestrian oriented space and allow for increased sidewalk widths as well as seperated bike lanes.

Waterfront Ecology-

Significant tree planting, green infrastructure, bike lanes, access corridors to waterfront recreation, clear wayfinding, and attractive pedestrian spaces should be layered and integrated to create a vibrant community, ecologically healthy shoreline, and healthy upland ecology.

3b

Lyall Street- West Bay Village
Scale: 1:200



08 December 2015

Mayor and Council
c/o Trevor Parkes, Senior Planner
Development Services
Township of Esquimalt



101 1831 Oak Bay Avenue
Victoria BC V8R 1C3

phone 250 . 592 . 9198
fax 250 . 592 . 9178

**RE: Westbay Triangle Redevelopment,
Westbay, Esquimalt, BC**

Rezoning Application: Project Description

Mayor and Council,

Please find enclosed a redevelopment proposal for the Westbay Triangle Lands prepared by Hillel Architecture Inc. as a revision to, and continuation of the Rezoning Application process for the following residential lots being amalgamated into a single land holding:

468 Head Street [Triangle Estates],

PID 001-843-991 Lot 8, Block H, Section 11, Esquimalt District, Plan 292 [468 Head Street];

PID 006-720-439, Lot 5, Block H, Section 11, Esquimalt District, Plan 292 [470 Head Street];

PID 000-036-722, Lot 4, Block H, Section 11, Esquimalt District, Plan 292 [472 Head Street];

PID 009-175-024, Lot 1, Block H, Section 11, Esquimalt District, Plan 292 [515 Gore Street];

PID 009-175-016, Lot 2, Block H, Section 11, Esquimalt District, Plan 292 [509 Gore Street];

PID 000-704-580, Strata Lot 1, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 [922 Lyall Street];

PID 000-704-598, Strata Lot 2, Section 11, Esquimalt District, Strata Plan 509, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1 [920 Lyall Street];

PID 009-175-008, Lot 6, Block H, Section 11, Esquimalt District, Plan 292 [918 Lyall Street];

and PID 009-174-885, Lot 7, Block H, Section 11, Esquimalt District, Plan 292 [912 Lyall Street].

Development History

Earlier development proposals, prepared by others, were presented at public hearing June 23rd, 2014 for public commentary and council discussion. The conclusion of that public hearing was a suspension of the rezoning process, and a request that the design direction presented that evening was revised to a development direction more in keeping with comments received from both the public and the councillors.

Hillel Architecture Inc. was retained to prepare that revised Development Proposal. Hillel Architecture Inc. reviewed all written commentary, public input from that public hearing, and councillor commentary. Hillel Architecture engaged in a public consultation with the Westbay Neighbourhood residents and local business members. Following those earlier engagement meetings a potential design direction emerged. As those meetings continued the project modelling continued to adjust as commentary was recorded. The consultation process expanded to Esquimalt Planners, the Director of Economic Development, Esquimalt Engineering staff for both civil services and transportation. Meetings were held with representatives of the Westbay Residents Association and the Esquimalt Chamber of Commerce.

The public consultation process has involved over a dozen meetings one on one with key individuals, Esquimalt staff, presentations to several audiences, and each time the project evolved and benefited. The public consultation process concluded with records in both powerpoint presentations and written materials which formed the basis of the booklet submitted herein as a part of this submission package and titled "The Westbay Triangle Development Guidelines". As copies have already been made available for each councillor, and planning staff – one record copy is submitted herein.

At this same time, Esquimalt initiated the documentation of the Westbay Neighbourhood Development Guidelines to reflect the current neighbourhood's opinions and concerns to shape their future throughout the larger Westbay Neighbourhood. This too had a distinct focus on these triangle lands. The Director of Economic Development assembled a broad based Stakeholders Committee of residents and local businesses, and a consulting team to independently document the greater Westbay Neighbourhood Design Guidelines. Hillel Architecture was a participant as a member of that Stakeholder Committee.

Description of Development

The Triangle Lands redevelopment proposal enclosed herein offers a comprehensive design solution that incorporates;

- a village for hosting local business to create a dynamic central gathering place in Westbay;
- a dynamic engaging setting for residents, businesses and patrons to engage, share, and socialize;
- a community that would also include multiple housing options, suiting many lifestyles, income levels, and aspirations;
- in a manor that would also demonstrate community leadership in extent of, and variations of green spaces available;
- advanced methods for gathering and harvesting rainwater for later irrigation on site;
- roof systems configured to take advantage of solar orientation for potential solar power generation and domestic water preheating
- advanced methods for gathering and harvesting rainwater for later irrigation on site;
- advanced modelling for shadow development and community impact assessment;
- multiple public social spaces, at 3 distinctly different corner settings;
- multiple park like settings for residents and leaseholders offering both raised ocean view park areas, wind protected conversation parks, sun protected shade gardens for hot weather respite for people, but year round environments suitable to many indigenous coastal plant and animal species;
- designed in a manor that creates a focus to a community, and encourages its growth and development by others.

Most importantly however, is that the design grew out of consultation. The solution evolved after many conversations, and over numerous contributions from a neighbourhood that cares deeply about its character today. Members of the neighbourhood clearly stated they were ready for a new center of Westbay, but that they clearly needed it to be designed carefully by those prepared to listen. We hope in the coming presentations that everyone can see a piece of their personal contributions in this proposal.

Description of Building Proposal

The proposed design offers a two storey commercial streetscape on Head Street designed to create a main street like village focus of activities, yet with architecture and building volumes designed to protect residents of Gore Street around the corner and residents above from the noise and busyness of that dynamic environment. This "main street" composition offers 6 commercial tenant spaces, sized specifically for this neighbourhood, and when combined with existing businesses in the vicinity will create a shopping and socializing destination.

Raised above this village center is a park like central courtyard setting reflecting the shape of the triangle lands, and creating a private enclave of six townhouses overlooking the park while the park itself provides privacy for those dwellings from each other and the residents of the multi-family building completing the Triangle.

Along Lyall Street, this residential building shares a similar footprint and personality with the larger volumes of the multi-family and multi-use buildings of the Pacific Fleet base. The residential building is six stories, offering the lower floors with generous outdoor terraces,

private entries. Interim floor levels with park like views, ocean and marina views, and culminating with the upper most floors offering panoramic ocean views in almost all directions.

Each Streetscape is respected as a front yard. Each streetscape developed to suit its current scale and massing, and in many ways the personalities of each street. In addition, no street is unfairly burdened with being a rear yard. All typical "back of house" activities are handled entirely internally – out of sight – and out of mind - of surrounding residents.

Further layers of purposeful and meaningful design response to the needs of a Main Street as is intended on Head Street, a Village setting like Gore Street , or an Avenue like setting along Lyall Street, are contained in the enclosed Westbay Triangle Development Guidelines. Even with this booklet, where many pages can describe some of the design intent, the design team knows even more layers of design and thought are still not yet expressed. We encourage people to look deeper and to ask questions. Our considerations for lighting, plantings, materials, signage, and many others have simply not had the space for expression, and time or page space does not permit.

In addition to the drawing package, and in exchange for what a massing model can provide, this submission package includes a computer modelled 3D massing that has been recorded in two distinctly different flight paths circling the building; one providing the viewpoint of a pedestrian both in front of some buildings and behind others, and secondly; a flight path just above the neighbouring buildings to ensure an unbroken view and understanding of the proposal. These included videos are viewable in all media, and can be played in public settings, be viewable on multiple screens, for staff councillors and the public equally.

Zoning

The proposal is submitted with the request for a spot zone to be created to respect the unique nature of this location, its role in the neighbourhood's future development, similar to proposals that predate this design direction. This correspondence therefore does not to compare this design proposal with the current land use bylaws of existing Lots, as these differ even in their own zoning. The general Project Data covering the required categories of data disclosure is located on Drawing Sheet A1.1.

Site Size: 4075m ²	
Site Coverage: 63%	<i>as measured by Amendment Bylaw 2861 to current Esquimalt Zoning Bylaw 2050</i>
Floor Space Ratio: 1.39 : 1	<i>as measured by Amendment Bylaw 2861 to current Esquimalt Zoning Bylaw 2050</i>
Building Height:	Bldg 1: 7.6m above average grade. Bldg 2: 21.75m above average grade. <i>Bldg 1: 15.0m geodetic to top of low sloped assembly.</i> <i>Bldg 2: 29.150m geodetic to top of low sloped assembly</i>
Number of Stories:	Building 1: 2 storeys. Building 2: 6 stories.
Setbacks:	5.8m @ Head Street (bldg wall finishes, not including canopies) 3.2m (stair only) and 5.8m main bldg @ Gore Street (bldg wall finishes, not including canopies),and 6.8m @ Lyall Street (bldg wall finishes, not including canopies).
Parking:	121 stalls where 114 are required.

In addition, as new definitions and clarifications are proposed under Bylaw 2861 for the Esquimalt Zoning Bylaw 2050, this project has been prepared based on those new definitions, and Sheet A1.2 graphically provides a data summary for key calculations such as floor area, FSR, and total site coverage. This same sheet demonstrates shadow studies at various recommended times as requested in the new Westbay Community Design Guidelines and those advocated by this office, or those offering an explanation for our building orientation.

Parking

The enclosed development proposal honours the Esquimalt Parking Bylaw without request for variances. In addition, the proposal goes on to provide additional motorcycle and scooter parking. In a demonstration of environmental concern, the design provides for alternative transportation forms with electric charging stations for both cars, and motorcycles and internally to the bike rooms, for electric bicycles.

Climate Change Considerations

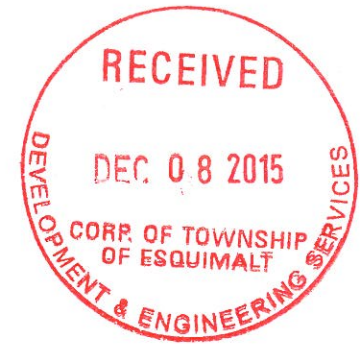
The green building checklist is enclosed in this project package. In general a wide range of designed features have been incorporated into this project that reflect contemporary concerns for the environment. Hillel Architecture has also participated in several green buildings and counts some of its clients as outstanding corporate citizens such as the Mountain Equipment Coop. The project contains;

- roof top orientation and angles, designed to aid the potential of main roof planes to serve for solar power generation;
- roof top orientation and angles, designed to aid the potential of main roof planes to serve for domestic hot water preheating;
- the harvesting of portions of the rain fall within these property boundaries;
- the thermal inertial and energy savings effect from planted green roofs;
- the general increase in green space and plant coverage over typical buildings of this form or role;
- extensive use of materials considered for their lesser environmental impacts;
- the use of appliances and equipment known for their lesser energy needs; and
- building orientation known to lessen the affects of overheating.

We hope that the enclosed drawing package and submission documents describe the proposal for these Triangle Lands adequately for the rezoning purposes of this submission. We also hope that they inspire. We wish that everyone imagine a new dynamic center of Westbay.

Regards

Peter Hardcastle
Hillel Architecture Inc.



Westbay Triangle Development Guidelines

Record of Community Consultation &
Concept Development
For

The Westbay Triangle

Assembled April 15, 2015

A conversation with the Community

The enclosed Westbay Triangle Development Guidelines are the result of multiple meetings with community Stakeholders started in the summer of 2014.

Township of Esquimalt Director of Economic Development
Township of Esquimalt Planning Staff
President of the Westbay Residents Association
President of the Chamber of Commerce
Township of Esquimalt Engineering (Civil Utilities)
Township of Esquimalt Engineering (Roads and Traffic)
& several Westbay residents and members of the Chamber of Commerce

These guidelines incorporate commentary provided by residents and local business owners as recorded by Esquimalt at the Public Hearing of earlier site solutions prepared by others.

This “conversation with the community” represents multiple presentations, amalgamated into this record, that represents both listening too, and preparing a response for, those residents and businesses of the neighbourhood.

Taken as a whole, this conversation provides the essence of a “Design Guideline” for this unique site. As these were exchanges, the text has kept the tone of a conversation.

The Westbay Triangle Neighbourhood Context



The Westbay Triangle; the people, the neighbourhood, and the businesses that define this unique ocean side community.



The Westbay Triangle Town site is a unique ocean side community, small in scale, quiet in nature, and proudly hosting a diversity of homes and businesses. Residences from single family through to multifamily homes define the residential side of Westbay. The commercial business center of Head Street is defined by several ocean side businesses, from resort like Bed and Breakfasts to offices. Some focusing on marine industries and some that do not. Some hosting expected marina based operations from classic marina services for sail and power boaters, to those of the floating residential communities.

The community is diverse, inclusive of many cultures, lifestyles, and businesses, and is close. Most residents and businesses present now, have coexisted for many years. Although the opinions are diverse, the community as a whole is cohesive.



In close proximity to the Westbay Triangle site are heritage homes, and heritage parks.

The Triangle is located in a pivotal central area able to serve the interests of its residential neighbourhood, the Pacific Military Navel Base, and the larger urban scale of multifamily homes and offices along Esquimalt Road.

The existing community of buildings specific to the Westbay Marine and ocean side environment are distinctly separate to the character of Esquimalt Road. At this water's edge, the setting contains the smaller buildings associated with those originally built and now playing new roles in the community.

Several original ocean side single family homes now house businesses, resorts, offices and retailers. Those offering services locally and those offering professional services to locations farther a field.



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The Triangle at Westbay



Westbay Marina based businesses include;

The sailboat community,

The power boat community, and....





... The floating residential community.

Which just like any other land based residential neighbourhood has a unique internal relationship with issues of access, privacy, noise, security, parking and streetscapes.



The current water front business district is low in elevation (2 stories) and affords several sightlines through and around properties on the water. The center of town is uniformly thought of as Head Street from Gore Street to its conclusion at the entry to the Naval Base. The "face" of the Westbay Triangle.

The Westbay Triangle Site Outline and Description





Our proposal should respect where the neighbourhood has the character of "main street". Head Street is this location. The sights & sounds, people and places, that make great destinations. Shops and services,
People & pets,
Bikes & trikes.

Dynamic. The pulse of energy.



Our proposal should respect where the neighbourhood behaves like a village.

The term village encompasses a wider definition: Where people feel at home, they reside and socialize, yet enjoy that the corner store is close at hand, the dynamic village core just around a corner, or just over a hill.



Our proposal must respect that some locations feel like avenues - the slightly busier flow of people and cars going to and fro, a different personality, a more urban scale. To behave cohesively both sides of an "avenue" should reflect this larger building type, and flow of people, cyclists, and vehicles.



Respecting the three faces of the triangle:

The "main" street of Head Street

The "village" of Gore Street

The "avenue" of Lyall Street

Existing front yard setbacks along these streetscapes were reviewed, considered for their application and appropriateness to a new solution to this sites development. Proposed front yard setbacks respect that each road is a front yard, each street is a prime elevation.

Average front yard surrounding the Triangle is $\pm 6m$. We applied a new 6m front yard setback on all three street faces of the Westbay Triangle.

But there is more to the role of a front yard setback: its function must be considered. Again 6m becomes a functionally correct setback when the roles ascribed to these areas are outlined herein.



The Westbay Triangle Precedence and Programming

The value of Setbacks

For those new businesses: We need to WANT to see you!

A business needs to actively draw our attention. A business therefore needs enough outdoor realm for them to create the environment suitable to enthruse patrons and draw them in. Destinations are created when groupings of like minded businesses work together to actively seek out and attract people.

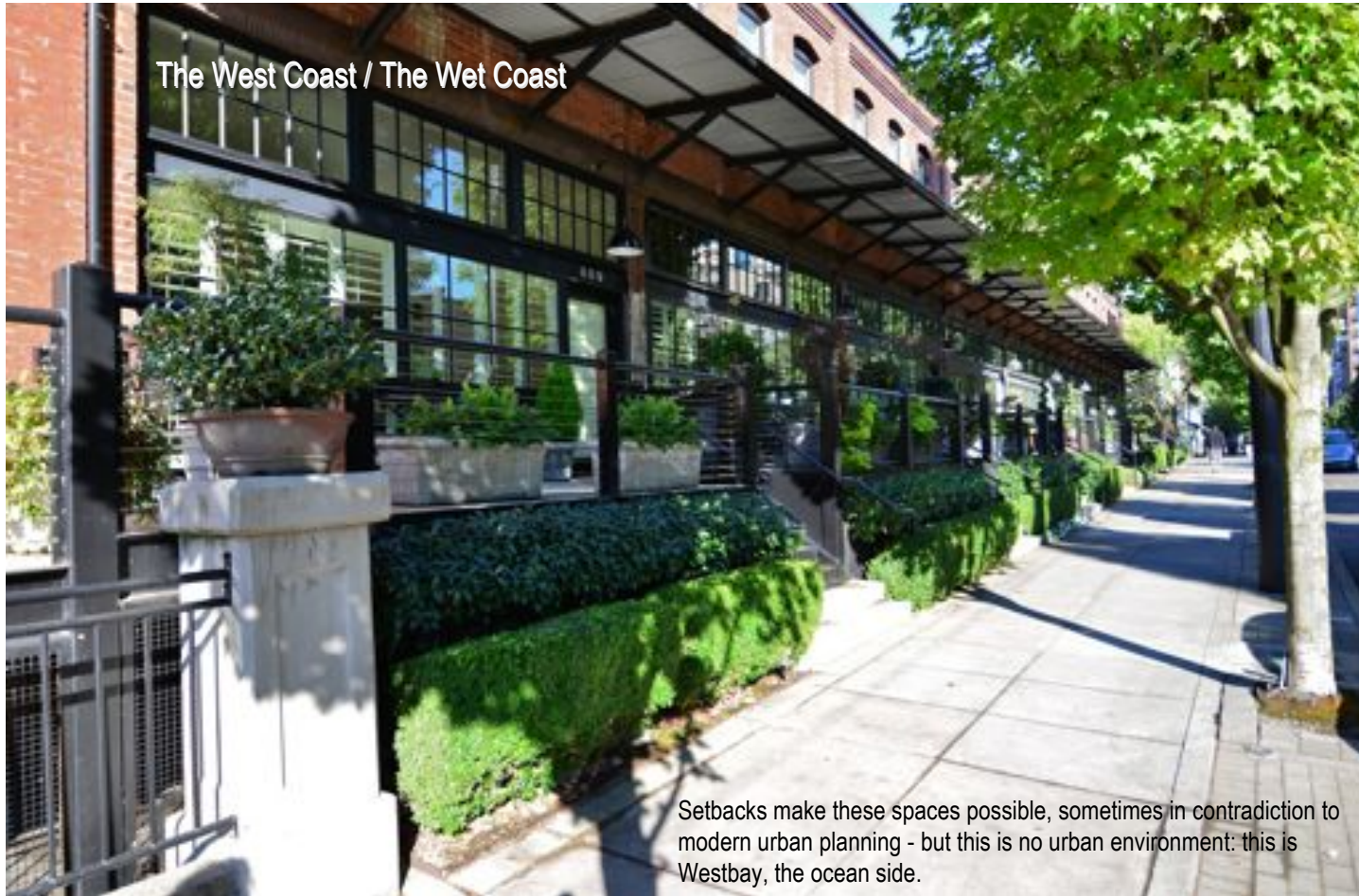


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The Triangle at Westbay

The West Coast / The Wet Coast



Setbacks make these spaces possible, sometimes in contradiction to modern urban planning - but this is no urban environment: this is Westbay, the ocean side.

Residences and businesses alike that benefit from outside spaces benefit from protection from the elements. Canopies, overhangs, and roof projections all can aid to protect these valuable outdoor areas.



Roof projections,

Overhangs,

Canopies.

They all create and define an outside space.
It communicates to the audience / the patron.

It can set a tone, create an atmosphere, set a stage, and define the character of a business or the character of a street face.

The Triangle at Westbay

Canopies protect goods, and also the patrons.

They draw attention.

They cool a space in summer.

They defend it in the winter.

They deflect noise.

Two items of note here:

One; the canopy here is defending a municipal sidewalk. Not only does this sidewalk not request this protection, but now this valuable outdoor space is in the public realm and not available in a manner that is helpful to a business displaying goods or services.

Secondly; the second floor is separated from the "main street environment" and is subordinate to the ground floor. It can aid in creating character, and enriching the setting, but its roles can be quite independent: a residence or a dance studio, an office or a home.

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It takes several meters of canopy to defend and protect an outdoor space.

The higher the canopy the greater the span should be. The height and scale of the canopy can communicate the scale of the services and activities enclosed



Note:
Our climate includes many cloud filled days, and short winter days. Our canopies should permit daylight to enter our buildings for both natural lighting and display needs. Keeping generous canopies transparent or high in elevation aids this need.

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The Triangle at Westbay



There are endless design outcomes to roof projections, overhangs, and canopies.

They should be an honest expression of;

- the neighbourhood's intentions,
 - the local people and local materials,
 - the local architecture form and character,
- and can still be unique.



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Elevating a space or recessing a space can serve the same role: to enclose and define a space.

Enveloping the potential patron, and infusing them with your products or services.

Several items of note:

Elevating a new space must be done in a manner that does not deter the elderly or the physically disabled and meet new building codes. A shared outdoor pedestrian space with HC access could join each of these store fronts and create a special environment that is still accessible. At the same time each retailer could have steps to each shop front for their own personal identity and character as shown here.

Secondly, each of these spaces were once enclosed differently. As this shopping environment grew over time, demand for space increased to a point where outside porch space was enclosed. In today's terms, designing for future growth is "future proofing". A good design today should understand its future potential needs. Some time from now that may be very important.

The Triangle at Westbay

Protection extends our use of the outside, our appreciation for the fresh air, and our terrific climate , through multiple seasons.

This example could be a social space, an outdoor room for a home, a residence...

... just as easily, it could be a display space for an artist, a retailer, or seating space for a coffee shop.

Without adequate protection it would have a greatly diminished value. If it was within municipal property boundaries it would similarly have a greatly diminished value, and very limited potential role for the business.





This entry could be a welcoming entry to a designer's office,

A professional office,

A personal service,

A retail shop,

A townhouse and home.

The Triangle at Westbay

Retail goods benefit from the protection of a canopy, be it a display of food, a boat, or a bike.

Those goods may be those that are for sale, and could also be those available for hire.



There can be a large variety of “services” or items for hire which need space for their effective display, from the small to the quite substantial.

From renting

Kayaks & Scooters, to

Bikes and Boats.



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The Triangle at Westbay



The sizes of these goods must be planned for in storefronts and business entries. These businesses could be displaying goods, some of which might be quite sizeable; a zodiac or kayak for example. From offshore gear to road gear.

In Westbay the residents speak openly about their healthy lifestyle, and need of a new town center. Residents have also emphasized that their neighbourhood goes beyond the marina: “we are more than just the water and boats”. So building solutions must consider a wider definition of the recreational lifestyle of Westbay.





Interactive displays draw attention, draw involvement, and animates a destination. Even if a retailer can not move their products outside, a good retailer creates displays of testing equipment, a climbing wall, a fitness centre for example. Destinations are defined by this outdoor interstitial space, the interconnectedness between the public realm and private business realm.

New internet shopping habits are making this essential. Retailers must draw patrons with diverse products, and provide exceptional levels of service and knowledge. Outdoor displays of testing equipment communicate this knowledge level and performance level of goods, staff, products. It also creates a statement about the neighbourhood - it exemplifies that here we care, here we excel.



Note:
Seeing to and from our main street is important to "main street".
Outdoor plantings should have both high canopies for shade and protection,
and low plants for colour and variety, changing through the seasons.
None should obscure the view to or from the pedestrian or those in vehicles.

An outdoor café needs to be outdoors all year round.
Planters and plants protect patrons from cars, noise, harsh sun,
and work in consort with the building's canopies or roof overhangs.
Defining spaces but not hiding them.



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Enveloping patrons, defining atmosphere



Setbacks would create sufficient space outside to envelop a patron in an environment suitable to enjoy a meal, a beverage, or a moment shopping.

The Triangle at Westbay



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No marketing plan can overcome some shortfalls in urban planning, Bylaws, or architectural design.

Buildings no matter how well designed, can not make the “stage set” by themselves. Our modern business is theatre: we need to think accordingly.

This building is quite exceptional, a unique combination of heritage, modern, is both thoughtful and animated, but....

...this is not how we sell food!





This is how we sell food!



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Even businesses like a bank or wine shop, that may not be able to display their goods or services directly still benefit from outdoor spaces, and therefore “main street” still benefits.

Think of the outdoors as a reception area or waiting room. Planned outdoor spaces, for interaction and socializing.

A chair is a service.

A tool to the designer of the “stage”.

Multiple options for seating help create a stage set for a destination. Just like multiple parking options are essential, so are multiple forms of seating; some facing out, some facing in, some for short term, and some to encourage simply sitting.



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The Adirondack Chair:
From Halifax Nova Scotia to South Beach Miami.

So what is our language for public seating in the Westbay Triangle and along Head Street?

Not a manufactured product, but one that naturally grows out of our community, our history, or our materials. An honest reflection of our history and our future, our shared culture and shared character.

The Triangle at Westbay

As it is with public seating, it is with “sidewalks”. Multiple types of sidewalks help define a pedestrian environment that is responsive to our needs, and therefore aids in creating destinations;

We need “sidewalks” for pedestrians who are quick movers;

- the commuters,
- those on a mission,
- those in cruise mode.

Municipal sidewalks service this role well. Municipal sidewalks are avenues for the flow of people.

We need a separate “walkway” for browsers,

- the slow movers,
- the shoppers,
- those staying.

Having both is dynamic. You need both. Destinations in some form must have both. People stop “at” destinations!





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Another example:

Here a building façade captures a heritage building, enlivens its façade with thoughtful contemporary touches, and fits well with the streetscape. Architecture is however static. A backdrop. No matter how interesting, the “building” alone does not create a destination.

Architecture can contain the right palette of pieces to help set the stage. A good main street is dynamic. It is animated by people, plants, the comings and goings, the to and fro, the goods and services, colour and life. It draws businesses and patrons alike - and those that are present animate the space and draw more interest towards it.

It must work also work in consort with bylaws and regulations, guidelines and community aspirations, to create an atmosphere that draws attention, and people.

This example seems NOT to be a successful retail environment or shopping destination, however interesting the façade may be.

But just around the corner...

The Triangle at Westbay



A more successful dynamic main street.

Trees with high canopies and plants with a lower nature, not obstructing sightlines.

Public furnishings that face in, and those that face out, some for leisure and some for waiting, some protected and some exposed.

High signage for vehicles and low signage for pedestrians. Long term parking close by, and short term parallel parking street side.

A municipal sidewalk for the cruisers, and a separate more leisurely walkway in the shopping environment.

The Westbay Triangle Response
Combining Neighbourhood Consultation with Building Concept



On the lowest level, aligning with Head Street are the commercial spaces.

Shops and markets, offices and services. The sights and sounds of a dynamic meeting place and market space.

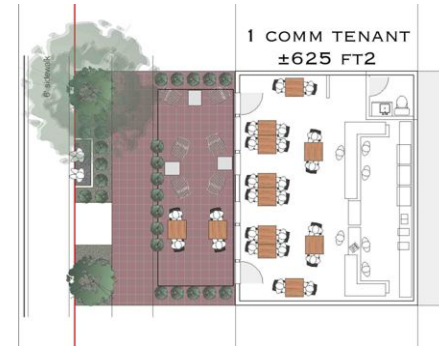
The rhythm of the existing business faces were used to aid defining the width of new business fronts designed into the Westbay Triangle.

Destinations

One or two businesses do not create that critical mass of variation to create a destination. 5 or 6 shops and businesses meets this goal, when combined with existing businesses on Head Street.

To encourage new local, small scale businesses, these tenancies are planned to be approximately ± 600 ft² each.

This size is well suited to numerous business types and occupancies.



Proof of concept drawings were prepared to demonstrate to owners, financiers, potential businesses and local residents, the value of this multi-use size of tenancies.

Group D business and personal services occupancies, Group E retail uses, and Group A assembly use spaces were planned, as was the outside potential of the 6m setbacks to building faces.

Over time these drawings were coordinated with parking and structure and other design alignments. These spaces are currently ±635 ft2.



Naturally if a retailer wished to obtain a larger space, simply leasing two spaces creates a 1200 ft2 space - well suited to a neighbourhood grocer. But it is most important that the smaller scale, smaller lease holding, smaller business uses are considered first and paramount.

Smaller lease holdings represent smaller risk, and smaller overhead.

It is a consideration beyond space fit. The size incorporates several layers of criteria important to small businesses.



Continuing along Gore Street, the activity of the “Main Street” gives way to the quieter setting of homes and residences.

The rhythm of the existing residences faces were used to aid defining the width of new residential street fronts designed into the Westbay Triangle.

These private ground floor entry street front residences are planned to be ±600 - 650 ft² each, and were test fit in one and two bedroom configurations.

This size is well suited to numerous lifestyles, ages, and smaller family units.



Those same features: Setbacks, canopies, landscaping and their planters, used another way create a sense of privacy and protection, and can define a more private and intimate social space.

It can create a sense of invitation, of welcome, and of “home”. One should feel it when they have stepped into a residential environment.

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The “Avenue” of Lyall Street was considered for the location of a multifamily residential building, a character it would share with buildings on its neighbouring Naval Base.

Oriented in this direction, the multifamily building would offer only its narrow profile to the neighbourhood and would therefore minimize its impact on the views and sightlines of others.

The Triangle at Westbay



At the ocean's edge, in the area of 2 storey buildings, the sightlines are towards the interior of the cove, and centered on Coville Island.



As you progress up the hill, or higher in elevation, the view corridor opens to a more panoramic view, enclosed by the naval buildings and Swallows Landing, and is oriented towards the axis of the new multifamily building. The proposed building therefore offers only its narrow end elevation and has a limited impact on other properties. As one can see above, if the building were proposed 90° to this orientation its impact would be much higher.



We heard the concern over building shadows created by an earlier proposal provided by others. This proposal is responding by demonstrating a similar program over a reduced number of stories.

Only two sample shadows are shown here, however these two shadows studies are most telling: Our proposal is six floors along Lyall Street in company with similar bldgs in proximity, and its shadows fall almost entirely on site. Most importantly, in the summer afternoon social hours, critical to shadow studies, the shadows falls towards, but does not meet, the commercial properties. Not in the direction of private homes.





Along the Head Street and Gore Street the response is two stories.

One, a dynamic “Main Street” of food & shops, businesses & services. The dynamic town center, and social nature of neighbourhood gathering places.

The other, along Gore Street, the concept provides residences to respect existing homes and their more quiet and private nature.

Along Lyall Street, one storey above Head Street, the multifamily building completes the Triangle. The proposal offers three front facades to these three street faces.

Captured at this elevation, one storey above Head Street, is a courtyard greenspace. A walk on park setting offering ocean views at one end above and overlooking Head Street, and at the other end offering a leisure setting suitable to the quiet of local residents.



The park level is accessed from the “Avenue” of Lyall Street as a walk on park level.

With pathways, the park is accessible from Gore Street as a walk on park level.

With stairs, the park is accessible from Head Street.

Residents with their private garden terraces along the park therefore will be able to quickly access the shops of Head Street or the Marina.



Roof top park level offers one area designed for quiet leisure use, in a quieter location on the site oriented towards the residential homes of Gore Street.

With an area of twice this size, another park setting provides ocean views, views over the activities of Head Street, and has gathering places designed to share a coffee or a conversation in small gatherings.

Internally, the Triangle Park provides a landscaped entry to many homes, a shared common space, in a wind protected environment, a naturalized courtyard.



The value of those building setbacks increases as we layer landscaping considerations over these spaces. These green corridors permit generous street side planters, and planting beds on all three street faces.

In addition, the Park Terrace and these planted areas have a performance role environmentally to play, by permitting rainwater harvesting, on site rainwater retention, and reuse for on site irrigation throughout this project.



The Gateways to the Westbay Triangle.

Three main street corners, each containing a sculptural greenspace define and announce the Triangle.

Three locations, three distinct characters, serving three different audiences.



Successful urban greenspaces must provide multiple experiences, one moment sculptural in nature, another more naturalized in nature.

Some suitable for small gatherings, and some suitable for private quiet moments.

Some for people and their needs, and some more suitable for the "birds and bees".

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The Triangle at Westbay

Rainwater Harvesting

Both residential buildings; the multifamily building along Lyall, and the townhouses viewing over the Park Terrace are designed with gull wing roofs to aid in rain water harvesting to irrigate the substantial plantings throughout the property and its roof top parks.

Solar Orientation

The planned gull wing roof pre-slopes a significant roof area of the multi-family building towards the primary sun gathering angle of “due South” permitting solar hot water preheating and solar power collection. The nature of this roof design will also tend to enclose these panels from view by others.

Roof Shade

The gull wing roof style with a higher perimeter and lower central area permits the larger roof overhangs to protect outdoor private spaces without deterring bright well lit interiors.



Parralle
Short term
Parking

Municipal
Bus Stop
unchanged



Effective parking by offering variety:

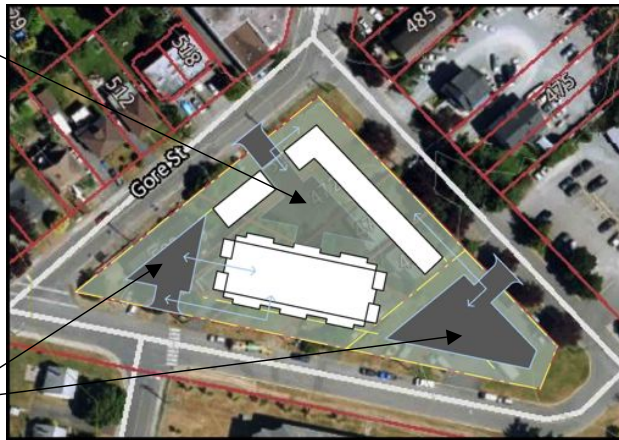
Parallel parking street side offers short term parking in close proximity to needs. These are designed into each of the three street faces. By formalizing recessed street side parking we increase safety from the current open and casual street side parking arrangement.

The municipal bus stop along Gore Street shown in grey is unchanged.

Each streets contains only one parking entry to distribute this vehicle movement equally to ensure calmer traffic overall, and increasing pedestrian safety.

Residential and
commercial
long term
parking
internal &
underground

Surface
parking &
longer term
parking



- From Lyall Street, adjacent to the residential building is the surface parking for residential guests.

- From Gore Street, aiding to define the residential realm from the commercial realm, is the entry to the more concealed internal long term parking.

- From Head Street, long term parking services commercial needs and marina needs .

Parking presented on site exceeds the municipal requirements currently by 13 stalls. With street parking, and additional surface parking we exceed predicted needs substantially.

The Westbay Triangle: The Proposal

Architectural drawings represent a refinement of the concept model responding to neighbourhood consultation, and taken to a preliminary building form.

Our concept to date

The following section outlines our program and project solution in more refined architectural drawings than program sketches shown earlier.

Esquimalt too, felt that a community consultation was required for the greater community and initiated a community wide process to review the current “Westbay Community Development Guidelines”. This process is now underway.

This building solution therefore, out of respect for that community wide consultation, is held at this level of resolution until the conclusion of that community wide process. The design team is attending and participating in that process.

When the new “Westbay Community Development Design Guidelines” are complete, our building and programming modeling will be reviewed, this building solution changed or edited to suit and respect those new community development guidelines and their intents.





Level P Underground
 Not shown is one level below grade, offering underground parking to residents, commercial leaseholders, marina float home owners, and electric vehicle charging stations for those users.

Level 1 Head Street

6 potential business faces form a dynamic "Main Street" shopping and business environment. Rounding the corner to Gore Street a porte cochere defines the end of the business realm and the start of the quieter residential component.

This level offers street side parking for short term use, and three separate longer term surface parking lots for residential guests, commercial patrons, commercial employees, and marina users.



Level 2 Lyall Street

One storey above Head Street is the Park like setting of the Westbay Triangle Residences. 6 townhouse like residences are accessed from this Park pathway network and have views over head street towards the water.

The multifamily building faces Lyall Street and contains a mixture of residences from ocean view panoramic penthouses, through two bedroom and one bedroom units, and streetlevel live / work units on one side, and Park Terrace walk out units on the Park side.



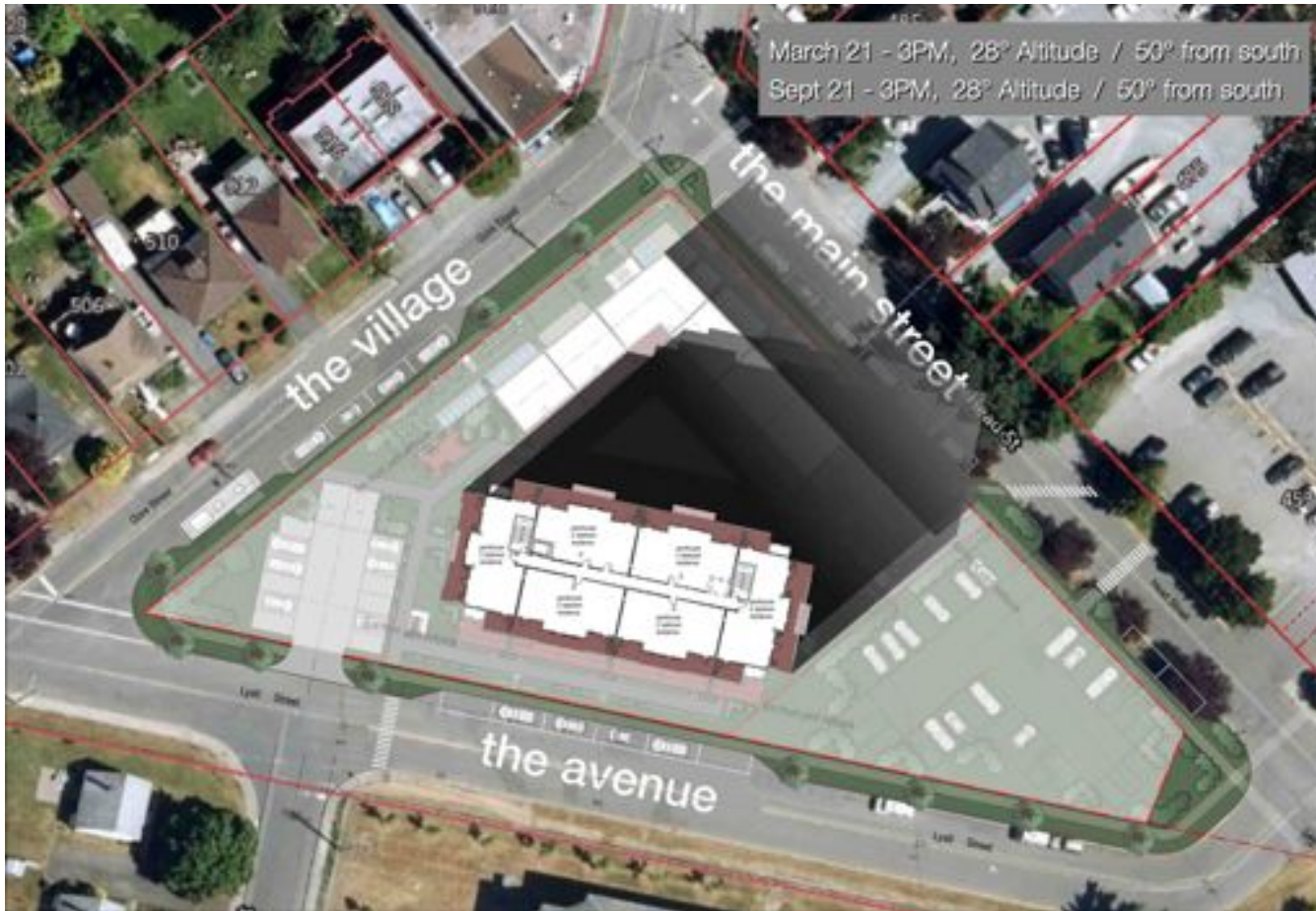
Level 3 to 5 Multifamily residential uses

Level 6 Penthouses with roof top walk out decks and gardens.

This penthouse level benefits from panoramic views in all directions. Penthouses are floor plans of larger area to accommodate the lifestyles, interests, and needs of those purchasers of these residential suites.

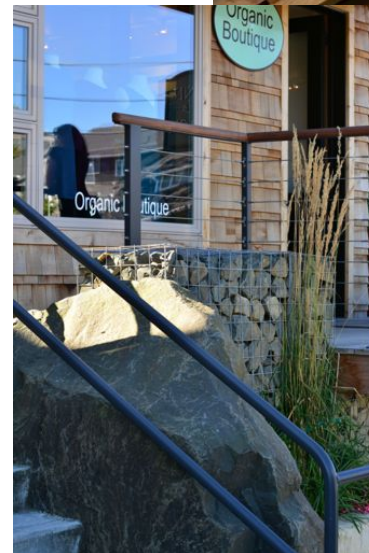


Morning shadows studies at 9AM March 21st, and September 21st, demonstrating shadows cast from both the fifth and sixth floor levels. These shadows approach but do not impose on residential properties.



Afternoon shadows studies at 3PM, demonstrating shadows cast from fifth and sixth floor levels.
At this time of day shadows are cast away from the private homes and towards, but not over, commercial properties.

The Westbay Triangle: Architectural Expression



Finding a palette of materials and features, details and textures, that are unique to this coast and capable of creating a destination.
On going research.



West Coast Architecture should reflect our coastal living and recreational lifestyles.

Entranceways and glazing systems for the Triangle need to respond to the unique needs of businesses and retailers providing goods and services to support these lifestyle choices and interests.

These goods and products are very diverse in size and shape. The nature of the services required also vary greatly in their needs, their equipment choices, and tools.

Architecture therefore must respond with uniquely suited solutions to permit businesses to move products and services inside and out. Retailers of more common goods and services will appreciate the unique setting that is created defining the Westbay Town Center.

To be a destination, to draw that level of attention and pride, one must create a unique and functional setting.



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The Triangle at Westbay



Section through Head Street Commercial Building demonstrating the value of the setbacks proposed.

From property line to public building face a distance of 6m has been provided. This permits the public street to have fully recessed parallel parking along the street edge, and a full width municipal sidewalk uncompromised. The landscaped greenspaces, and raised planters, occur on private property along this Head Street project face. This places maintenance expenses on the property owner which is preferred along this project face. With 6 metres available sufficient space is present for a defined experience inside of the planters. 3m canopies protect the outside and at sufficient ht to permit these glazed canopies to allow daylight deep into these commercial tenancies.



Envelop and finishes layered on the building section.

The protected commercial spaces below the canopies are heavy timber framed and wood sided, in an honest manor consistent with coastal working buildings in the marine industry.

Canopies shown demonstrate the scale change as building transitions from office and small retailer tenancies to larger ht. retail and assembly spaces further along Head Street.

Second floor residential spaces overlook Head Street and have entries from the Triangle Park side terrace. These residential floor levels are clean lined and contemporary in design to gently pull this area of Westbay forward and setting the stage for other buildings to be more modern in their architecture. The Westbay Triangle would remain as the "character" setting of the business district, and would tie in with existing Heritage Homes and offices along Head Street .

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The Triangle at Westbay



The Head Street Commercial Facade





Offices / Business and Personal Services spaces along Head Street



Larger retail facades / 3m glazed canopy / Daylight lit commercial spaces with high transoms along Head Street

These spaces have exterior sliding glazed 5' x 10' door sections to freely move goods and products to the outside and permit open flow of patrons during fair weather days.



People and activity of the café / sandwich shop / coffee shop on this corner location animates the Business District as patrons enter the area. This food services / beverage retailer is purposefully of limited size so as not to preclude the long term potential of a substantial water front ocean view restaurant in the future at Westbay Marina.



The Gore Street façade of the café / sandwich shop / coffee shop space.

Access stair from employee / resident / marina parking underground exits the parkade conveniently at this prime location. This serves to feed patrons past this prime corner location, the enclosure helps deflect sound from Gore Street and leave this residential street protected from this intrusion. The enclosure also provides a glazed and attractive face while concealing a commercial kitchen area beyond.



The Gore Street Residences.

Further along Gore Street the exterior materials of the commercial fronts have concluded, and changed to those materials associated with the residences above, and signify a change to this quieter residential community and this end of the Park Terrace above.





Example of a building Drive Through creating a Porte Cochere and concealing "back of house" parking and services from the Streetscape..



The Core Street Façade of the multifamily building facing Lyall Street.



The main façade of the Lyall Street residential building with ground floor Live / Work studios along a raised public walkway at the entries to private terraces.



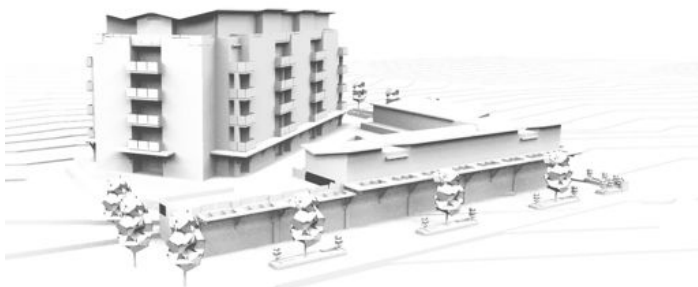
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The Triangle at Westbay

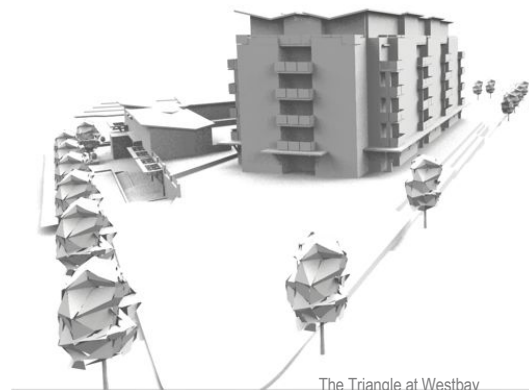
The project section:
Recessed Penthouse residences with walk out roof top gardens
Upper residential floors with clear views over lower Head Street park side residences
Lower Floors enjoy views of, and walk out terraces into the park level.

Section demonstrates "avoiding economic shadow":
The ability for a new building to be added to a community today, that benefits the neighbourhood today, and yet at the same time encourages further new community growth.





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The Triangle at Westbay



The Westbay Triangle

Preliminary 3D studies of the architectural solution responding to community consultation and this Design Guideline.

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The Triangle at Westbay

The Westbay Marina: Site Studies

Westbay Marina development considerations in the past, prepared by others, have involved the potential relocation of existing intersections, and their associated municipal roadbeds, their services and utilities.

This would have provided a neighbourhood and councilors alike, many layers of considerations to weight and concerns. It became critical therefore that our office model a potential outcome, and review its parking loads which were key drivers of those previous solutions.

In addition, with our public consultation underway, we were also able to judge a new solution relative to the neighbourhoods goals and aspirations. It allowed us to consult with engineering staff, and review current parking bylaws for their impact on a potential development scenario.

The enclosed planning is not demonstrated herein as an architectural solution, nor is it developed to the level of the Westbay Triangle demonstrated earlier. It is however a balanced and realistic approach to meeting community aspirations, and the clients aspirations.



The Westbay Marina Studies

A site analysis was conducted of the existing activities of Westbay Marina and the potential for future development. The undertaking was to predict parking loading of various development scenarios which were driving earlier design solutions.

Using the same methodology as was used in design considerations for the Triangle, Hillel Architecture modeled a scenario of a two storey outcome on this waterfront property whereby all the current needs for parking and the parking generated by the new development would be satisfied with current parking Bylaws.

A contemporary heavy timber and glass building offered a prime retail and restaurant space, and prime office and marina administration space. The program building model would sit on a very open public concourse permitting views through to the water on a frequent basis, and offering covered outdoor seating and display space.

**Parking Needs Analysis
Westbay Marina**

Parking required
31.25 stalls: 125 births at 25 stalls each
21 stalls: 21 float homes

52 for marina operations

9 marina retail
25 marina restaurant
6 marina admin
12 marina offices

52 for new marina building

Totals 104 parking stalls required

Potential available

65 lower level @ Marina
26 new underground @ Triangle II
8 surface parking @ Triangle II
13 in reserve @ Westbay Triangle

111 parking stalls available.

7 stalls surplus stalls

Permits 3 "townhouses" as
Westbay Triangle Phase II
3 stalls for residences,
3 for guests.



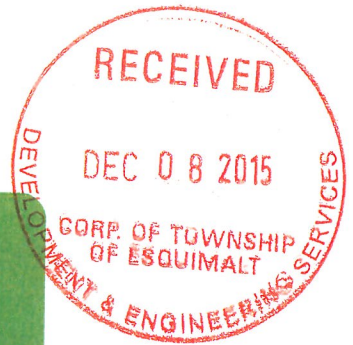
It appears that using the road intersections as they currently are, one could drive into and down an entry ramp towards the water, and in one continuous parking level, accommodate the majority of the Marina's parking needs.

For employees, and residents of the float homes an underground extension to the Triangle Parkade contributes additional stalls as does some of the earlier surface parking in this vicinity. All permitted within the parking bylaws today.

This should re-assure neighbours that a new development to house parking needs of the Marina and a moderate new building development can work within properties current boundaries and their existing roadbeds.



GREEN BUILDING CHECKLIST



The purpose of this Checklist is to make property owners and developers aware of specific green features that can be included in new developments to reduce their carbon footprints to help create a more sustainable community.

Creating walkable neighbourhoods, fostering green building technologies, making better use of our limited land base and ensuring that new development is located close to services, shops and transit are some of the means of achieving sustainability.

The Checklist which follows focuses on the use of Green Technologies in new buildings and major renovations. The Checklist is not a report card, it is a tool to help identify how your project can become 'greener' and to demonstrate to Council how your project will help the Township of Esquimalt meet its sustainability goals. It is not expected that each development will include all of the ideas set out in this list but Council is looking for a strong commitment to green development.

There are numerous green design standards, for example, Built Green BC; LEED ND; Living Building Challenge; Green Shores; Sustainable Sites Initiative. Esquimalt is not directing you to follow any particular standard, however, you are strongly encouraged to incorporate as many green features as possible into the design of your project.

As you review this checklist, if you have any questions please contact Development Services at 250.414.7108 for clarification.

**New development is essential to Esquimalt.
We look forward to working with you
to ensure that development is
as green and sustainable as possible.**

Other documents containing references to building and site design and sustainability, which you are advised to review, include:

- Esquimalt's Official Community Plan
- Development Protocol Policy
- Esquimalt's Pedestrian Charter
- Tree Protection Bylaw No. 2664
- A Sustainable Development Strategic Plan for the Township of Esquimalt

Adopted on January 10th, 2011



“One-third of Canada’s energy use goes to running our homes, offices and other buildings. The federal government’s Office of Energy Efficiency (Natural Resources Canada) reports that a corresponding one-third of our current greenhouse gas (GHG) emissions come from the built environment.”
 [Green Building and Development as a Public Good, Michael Buzzelli, CPRN Research Report June 2009]

Please answer the following questions and describe the green and innovative features of your proposed development. Depending on the size and scope of your project, some of the following points may not be applicable.

Green Building Standards

Both energy use and emissions can be reduced by changing or modifying the way we build and equip our buildings.

1	Are you building to a recognized green building standard? If yes, to what program and level? <i>HOWEVER WE BENEFIT FROM PREVIOUS EXPERIENCE IN LEED/GREEN PROJECTS</i>	Yes	<input checked="" type="radio"/> No
2	If not, have you consulted a Green Building or LEED consultant to discuss the inclusion of green features? <i>CONSULTATION ON GOING TOWARDS POSSIBLE INCLUSION @ D.P. STAGE</i>	Yes	<input checked="" type="radio"/> No
3	Will you be using high-performance building envelope materials, rainscreen siding, durable interior finish materials or safe to re-use materials in this project? If so, please describe them.	<input checked="" type="radio"/> Yes	No
4	What percentage of the existing building[s], if any, will be incorporated into the new building?		%
5	Are you using any locally manufactured wood or stone products to reduce energy used in the transportation of construction materials? Please list any that are being used in this project. <i>CONSULTATION UNDERWAY FOR DECLARATIONS @ D.P. STAGE</i>		
6	Have you considered advanced framing techniques to help reduce construction costs and increase energy savings?	<input checked="" type="radio"/> Yes	No
7	Will any wood used in this project be eco-certified or produced from sustainably managed forests? If so, by which organization? <i>WOOD PRODUCTS MAY BE ECO-CERTIFIED</i>		
	For which parts of the building (e.g. framing, roof, sheathing etc.)? <i>STRUCTURAL FRAMING.</i>		
8	Can alternatives to Chlorofluorocarbon’s and Hydro-chlorofluorocarbons which are often used in air conditioning, packaging, insulation, or solvents] be used in this project? If so, please describe these. <i>ALTERNATIVES WILL BE REVIEWED WITH SPECIFIER FOR POSSIBLE INCLUSION</i>	<input checked="" type="radio"/> Yes	No
9	List any products you are proposing that are produced using lower energy levels in manufacturing. <i>STONE VENEER, NATURAL WOOD SIDING, AND RELATED MATERIALS</i>		
10	Are you using materials which have a recycled content [e.g. roofing materials, interior doors, ceramic tiles or carpets]? A LIMITED LIST OF MATERIALS WILL CONTAIN RECYCLED CONTENT	<input checked="" type="radio"/> Yes	No
11	Will any interior products [e.g. cabinets, insulation or floor sheathing] contain formaldehyde? <i>WE CAN AVOID FORMALDEHYDE IN MOST BUILDING PRODUCTS</i>	Yes	<input checked="" type="radio"/> No

Water Management

The intent of the following features is to promote water conservation, re-use water on site, and reduce storm water run-off.

Indoor Water Fixtures

- | | | | |
|----|--|-----|----|
| 12 | Does your project exceed the BC Building Code requirements for public lavatory faucets and have automatic shut offs?
<i>WE CAN ASK TENANTS TO DO SO IN THEIR CONTRACT DWGS.</i> | Yes | No |
| 13 | For commercial buildings, do flushes for urinals exceed BC Building Code requirements?
<i>WE CAN ASK TENANTS TO DO SO IN THEIR CONTRACT DWGS.</i> | Yes | No |
| 14 | Does your project use dual flush toilets and do these exceed the BC Building Code requirements?
<i>WE CAN ENSURE USE OF DUAL FLUSH TOILETS THROUGHOUT.</i> | Yes | No |
| 15 | Does your project exceed the BC Building Code requirements for maximum flow rates for private showers?
<i>WE CAN ENSURE WATER SAVING SHOWER HEADS THROUGHOUT.</i> | Yes | No |
| 16 | Does your project exceed the BC Building Code requirements for flow rates for kitchen and bathroom faucets?
<i>WE CAN ENSURE LOW FLOW RATES FOR ALL FAUCETS</i> | Yes | No |

Storm Water

- | | | | | |
|----|---|-----|----|-----|
| 17 | If your property has water frontage, are you planning to protect trees and vegetation within 60 metres of the high water mark? [Note: For properties located on the Gorge Waterway, please consult Sections 7.1.2.1 and 9.6 of the Esquimalt Official Community Plan.] | Yes | No | N/A |
| 18 | Will this project eliminate or reduce inflow and infiltration between storm water and sewer pipes from this property?
<i>NEW CIVIL ENGINEERING WILL REMOVE/PREVENT THIS ISSUE</i> | Yes | No | N/A |
| 19 | Will storm water run-off be collected and managed on site (rain gardens, wetlands, or ponds) or used for irrigation or re-circulating outdoor water features? If so, please describe. <i>PORTIONS OF RAIN FALL ARE HELD/DELAY FED TO STORM WATER SYSTEMS. PORTIONS MAY BE USED FOR IRRIGATION</i> | Yes | No | N/A |
| 20 | Have you considered storing rain water on site (rain barrels or cisterns) for future irrigation uses?
<i>BUILDING HAS BEEN DESIGNED WITH SIMILAR CONSIDERATIONS</i> | Yes | No | N/A |
| 21 | Will surface pollution into storm drains will be mitigated (oil interceptors, bio-swales)? If so, please describe. <i>NEW CIVIL ENGINEERING UPDATES ALL EXISTING ON SITE SYSTEMS THROUGH COMPLETE REPLACEMENT</i> | Yes | No | N/A |
| 22 | Will this project have an engineered green roof system or has the structure been designed for a future green roof installation?
<i>LARGE PORTIONS ARE GREEN ROOFS</i> | Yes | No | N/A |
| 23 | What percentage of the site will be maintained as naturally permeable surfaces?
<i>LARGE PORTIONS ARE PERMEABLE GREEN ROOFS & TERRACES</i> _____ % | | | |

Waste water

- | | | | | |
|----|--|-----|----|-----|
| 24 | For larger projects, has Integrated Resource Management (IRM) been considered (e.g. heat recovery from waste water or onsite waste water treatment)? If so, please describe these. | Yes | No | N/A |
|----|--|-----|----|-----|

Natural Features/Landscaping

The way we manage the landscape can reduce water use, protect our urban forest, restore natural vegetation and help to protect the watershed and receiving bodies of water.

- | | | | | |
|----|---|-----|----|-----|
| 25 | Are any healthy trees being removed? If so, how many and what species?
<i>DECLARATIONS CAN BE MADE FOR D.P. STAGE DOCUMENTS</i>
Could your site design be altered to save these trees?
Have you consulted with our Parks Department regarding their removal?
<i>COMPLETE SITE IS EXCAVATED, HOWEVER EXTENSIVE REPLANTING IS INCORPORATED INTO PROJECT</i> | Yes | No | N/A |
|----|---|-----|----|-----|

- 26 Will this project add new trees to the site and increase our urban forest?
If so, how many and what species? YES, NUMBERS TO BE DETERMINED SEE LANDSCAPE PLAN @ D.P. STAGE. Yes No N/A
- 27 Are trees [existing or new] being used to provide shade in summer or to buffer winds? Yes No N/A
- 28 Will any existing native vegetation on this site be protected?
If so, please describe where and how. TREES REMOVED ARE REPLACED IN A HIGHER NUMBER BY INDIGENOUS SPECIES. Yes No N/A
- 29 Will new landscaped areas incorporate any plant species native to southern Vancouver Island? AN EMPHASIS IS PLACED ON NATIVE SPECIES Yes No N/A
- 30 Will xeriscaping (i.e. the use of drought tolerant plants) be utilized in dry areas? Yes No N/A
- 31 Will high efficiency irrigation systems be installed (e.g. drip irrigation; 'smart' controls)? Yes No N/A
- 32 Have you planned to control invasive species such as Scotch broom, English ivy, Himalayan and evergreen blackberry growing on the property? MAINTENANCE Yes No N/A
- 33 Will topsoil will be protected and reused on the site? SOIL TOP MAY PROVE ACCEPTABLE FOR RE-USE/EXCHANGE Yes No N/A

Energy Efficiency

Improvements in building technology will reduce energy consumption and in turn lower greenhouse gas [GHG] emissions. These improvements will also reduce future operating costs for building occupants.

- 34 Will the building design be certified by an independent energy auditor/analyst?
If so, what will the rating be? NOT KNOWN @ THIS REZONING STAGE Yes No N/A
- 35 Have you considered passive solar design principles for space heating and cooling or planned for natural day lighting? ROOF DESIGN AND ORIENTATION INCREASES POTENTIAL USE FOR SOLAR ENERGY GENERATION Yes No N/A
- 36 Does the design and siting of buildings maximize exposure to natural light?
What percentage of interior spaces will be illuminated by sunlight? DESIGN INCREASES NATURAL LIGHT LEVELS IN COMMERCIAL SPACES, Yes No N/A
- 37 Will heating and cooling systems be of enhanced energy efficiency (ie. geothermal, air source heat pump, solar hot water, solar air exchange, etc.).
If so, please describe. PRE-CONFIGURED FOR FUTURE HOT WATER PREHEAT Yes No N/A
If you are considering a heat pump, what measures will you take to mitigate any noise associated with the pump? ROOF TOP UNITS REMOVE OBJECTIONABLE NOISE
- 38 Has the building been designed to be solar ready? BUILDING HAS BEEN DESIGNED FOR FUTURE SOLAR POTENTIAL Yes No N/A
- 39 Have you considered using roof mounted photovoltaic panels to convert solar energy to electricity? YES, ROOF STYLE & ORIENTATION PURPOSEFULLY CONSIDERED FOR THIS USE Yes No N/A
- 40 Do windows exceed the BC Building Code heat transfer coefficient standards? WINDOWS WILL MEET OR EXCEED ALL CODE STANDARDS Yes No N/A
- 41 Are energy efficient appliances being installed in this project?
If so, please describe. ALL RESIDENTIAL APPLIANCES WILL BE REVIEWED FOR CONSUMPTION
- 42 Will high efficiency light fixtures be used in this project? YES. LED FIXTURES TO MANY PUBLIC SPACES Yes No N/A
- 43 Will building occupants have control over thermal, ventilation and light levels? YES Yes No N/A
- 44 Will outdoor areas have automatic lighting [i.e. motion sensors or time set]? YES, MANY OUTDOOR LOCATIONS WILL HAVE LIGHT SENSORS Yes No N/A
- 45 Will underground parking areas have automatic lighting? YES Yes No N/A

Air Quality

The following items are intended to ensure optimal air quality for building occupants by reducing the use of products which give off gases and odours and allowing occupants control over ventilation.

46	Will ventilation systems be protected from contamination during construction and certified clean post construction?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
47	Are you using any natural, non-toxic, water soluble or low-VOC [volatile organic compound] paints, finishes or other products? If so, please describe. <u>THESE PRODUCTS WILL BE DETERMINED AT A LATER STAGE</u>	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
48	Will the building have windows that occupants can open? <u>YES</u>	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
49	Will hard floor surface materials cover more than 75% of the liveable floor area? <u>YES. BETWEEN 60% AND POTENTIALLY UP TO 75%.</u>	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
50	Will fresh air intakes be located away from air pollution sources? <u>YES</u>	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A

Solid Waste

Reuse and recycling of material reduces the impact on our landfills, lowers transportation costs, extends the life-cycle of products, and reduces the amount of natural resources used to manufacture new products.

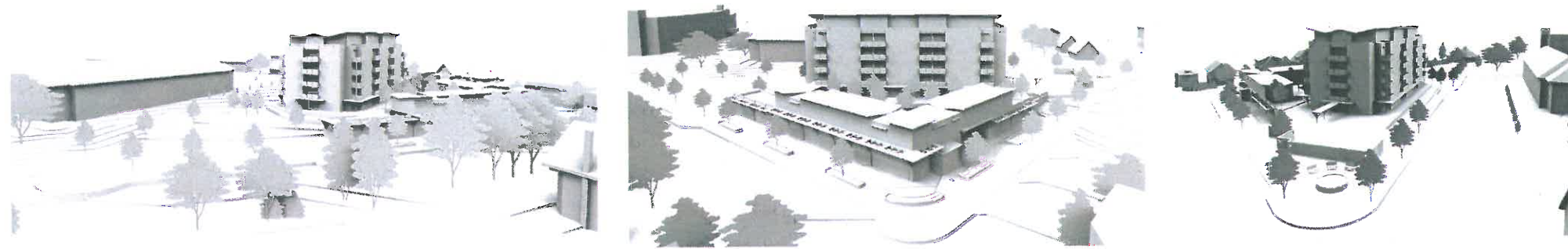
51	Will materials be recycled during demolition of existing buildings and structures? If so, please describe. <u>LUMBER MATERIALS CAN BE MADE AVAILABLE</u>	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
52	Will materials be recycled during the construction phase? If so, please describe. <u>A LIMITED LIST OF MATERIALS & PACKAGING WILL BE RECYCLED DURING CONST. PHASE</u>	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
53	Does your project provide enhanced waste diversion facilities i.e. on-site recycling for cardboard, bottles, cans and or recyclables or on-site composting?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
54	For new commercial development, are you providing waste and recycling receptacles for customers?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A

Green Mobility

The intent is to encourage the use of sustainable transportation modes and walking to reduce our reliance on personal vehicles that burn fossil fuels which contributes to poor air quality.

55	Is pedestrian lighting provided in the pathways through parking and landscaped areas and at the entrances to your building[s]?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
56	For commercial developments, are pedestrians provided with a safe path[s] through the parking areas and across vehicles accesses?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
57	Is access provided for those with assisted mobility devices?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
58	Are accessible bike racks provided for visitors?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
59	Are secure covered bicycle parking and dedicated lockers provided for residents or employees?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
60	Does your development provide residents or employees with any of the following features to reduce personal automobile use [check all that apply]: <input type="checkbox"/> transit passes <input type="checkbox"/> car share memberships <input type="checkbox"/> shared bicycles for short term use <input checked="" type="checkbox"/> weather protected bus shelters <u>OFF SITE, PROVIDED BY OTHERS</u> <input checked="" type="checkbox"/> plug-ins for electric vehicles			

Is there something unique or innovative about your project that has not been addressed by this Checklist? If so, please add extra pages to describe it.



DRAWING LIST

Architectural Drawings Enclosed

- A1.1 Existing Site and Site Data
 - A1.2 Graphic Project Summary
 - A1.3 Site Development Layout
 - A1.4 Site Development Finishes
 - A1.5 Graphic Summary of Setbacks

 - A2.0 Underground Parkade Layout
 - A2.1 Bldg 1 - Ground Floor @ Head Street Layout
 - A2.2 Bldg 1 - Ground Floor @ Head Street Finishes
 - A2.3 Bldg 1 & 2 Ground Floor @ Lyall Street Layout
 - A2.4 Bldg 1 & 2 Ground Floor @ Lyall Street Finishes
 - A2.5 Bldg 2 Floors 2nd through 5th floors
 - A2.6 Bldg 2 Penthouse (Sixth) Floor Plan

 - A2.7 Building 1 - Suite Floor Plans
 - A2.8 Building 1 - Suite Floor Plans
 - A2.9 Building 2 - Suite Floor Plans
 - A2.10 Building 2 - Suite Floor Plans
 - A2.11 Building 2 - Suite Floor Plans

 - A3.0 Proposed Project Perspectives
 - A3.1 Neighbourhood Perspectives
 - A3.2 Building 1 - Head Street [northeast] & Gore Street [northwest] Elevations
 - A3.3 Building 1 - [southeast] & [southwest] Elevations
 - A3.4 Building 1 - Detail Elevations and Finishes

 - A3.5 Building 2 - Lyall Street [south] Elevation
 - A3.6 Building 2 - North Elevations
 - A3.7 Building 2 - Gore Street [west] & East Elevations
 - A3.8 Building 2 - Detail Elevations and Finishes

 - A4.1 Building 1 & 2 - Section Facing [northwest] Gore Street
 - A4.2 Building 1 - Section Facing [south] Lyall Street
 - A4.3 Building 2 - Section Facing [northeast] Head Street

 - A4.4 Building 1 & 2 - Material Board
- Drawings not included in returning submission package
 Material Board prepared for development permit submission (includes material finishes included on elevations A3.2 & A3.4)

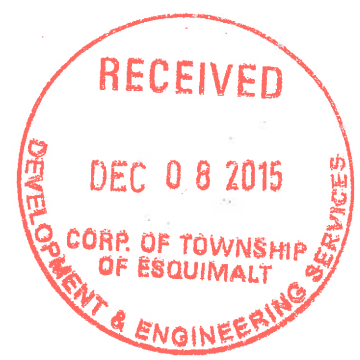
CONTACT LIST

Prime Consultant & Architect of Record
Hillel Architecture Inc.
 101 1831 Oak Bay Ave, Victoria, BC V8R 1C3
 Contact: Peter Hardcastle
 Phone: (250) 592.9198
 Fax: (250) 592.9178
 Email: peter@hillelarch.ca

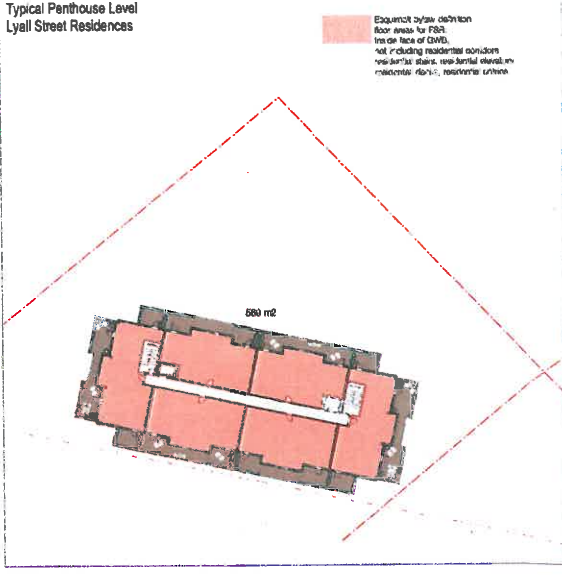
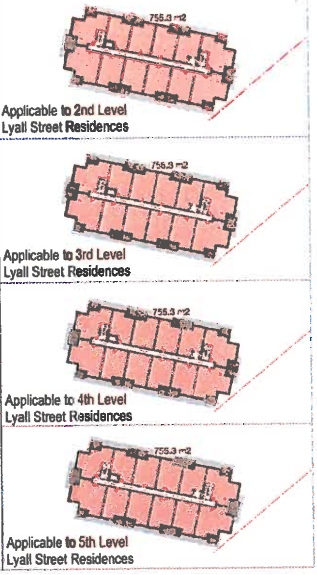
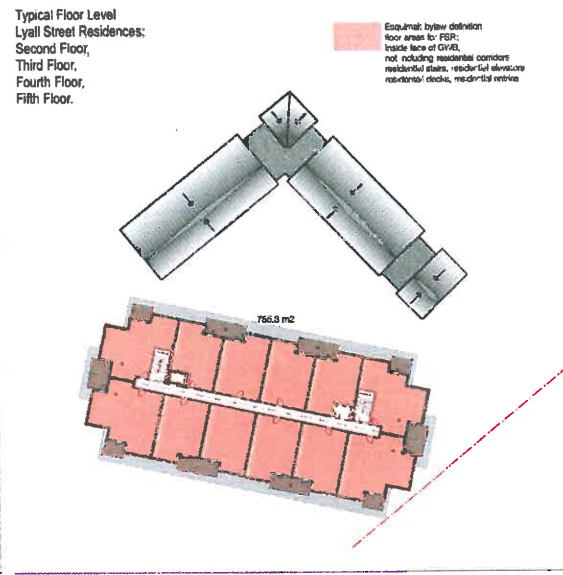
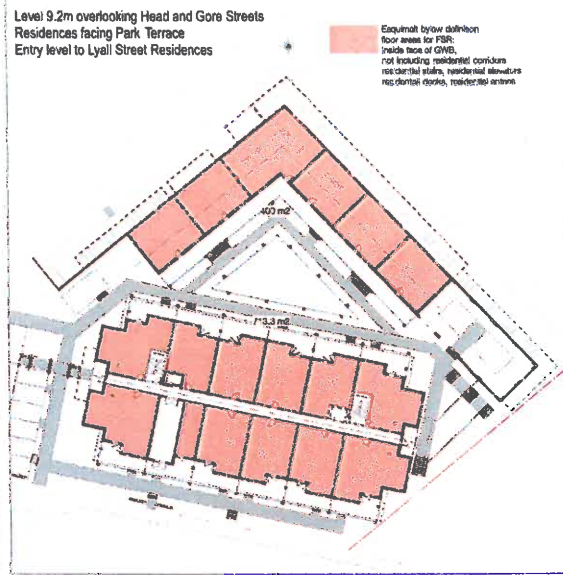
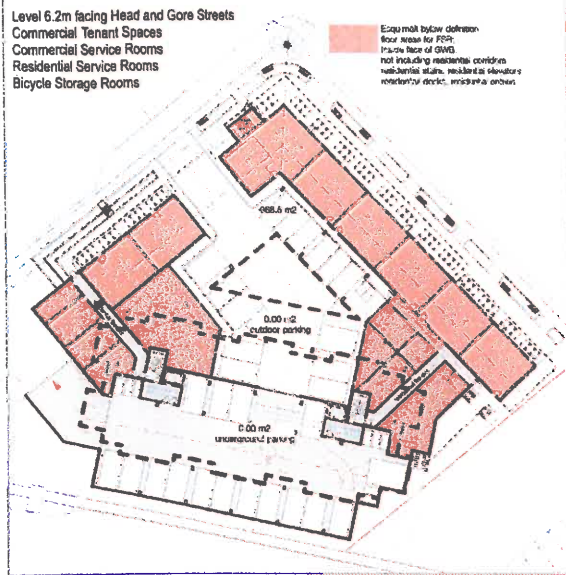
Property Owner
Lindholm Land & Investment Corporation
 7081 Central Saanich Road, Victoria, British Columbia, Canada V8M 1Y3
 Contact: Mark Lindholm
 Phone: (250) 383.5678
 Fax: (250) 383.8565
 Email: mark.lindholm@lindholmiland.ca

THE TRIANGLE AT WESTBAY

ESQUIMALT, BRITISH COLUMBIA



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ZONING BY-LAW 1992, NO. 2050
Amendment: Bylaw No. 2861

NEW DEFINITION USED IN THESE DOCUMENTS:
"Floor Area" when used in reference to a floor, Storey or Building means the entire area which in plan is enclosed by the interior face of the exterior walls of the Storey or floor level, calculated in accordance with Section 14.

(6) in PART 4 - GENERAL REGULATIONS Section 14.

NEW DEFINITION USED IN THESE DOCUMENTS:
14. CALCULATION OF FLOOR AREA AND FLOOR AREA RATIO

(1) The following shall not be included as Floor Area:

- (a) balconies;
- (b) open decks and sun decks;
- (c) unenclosed porches and verandas;
- (d) chimneys forming part of an exterior wall;
- (e) projecting bay windows where the bottom of the window is located a minimum 0.3 metres above the floor, and no area of the bay window in plan exceeds 1.0 square metres.

(2) In addition, the following shall not be included as Floor Area in Multiple Family Dwellings and in residential portions of Mixed Commercial/ Residential Buildings:

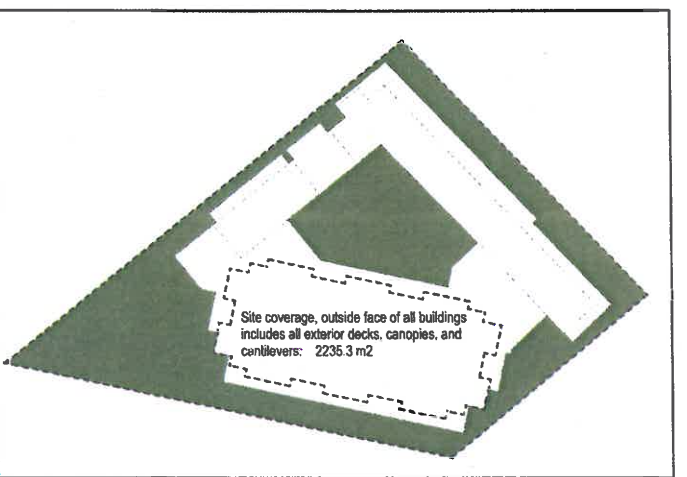
- (a) the area of the floor of any and all portions of the Building used or intended to be used for the parking or movement of vehicles;
- (b) secure bicycle storage facilities (bike lockers);
- (c) the area of the floor of any and all portions of the Building intended to be used by all residents of the building, including but not limited to the following:
 - (i) mezzanines;
 - (ii) corridors and hallways;
 - (iii) landings and foyers;
 - (iv) staircases and stairwells; and
 - (v) elevator shafts.

(3) In addition, the following shall not be included as Floor Area for the purposes of computing Floor Area Ratio:

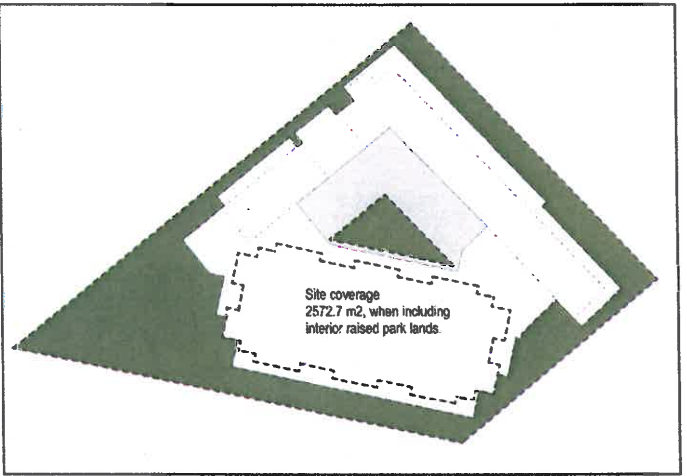
- (a) Any portion of a penthouse containing elevator or venting machinery;
- (b) Any portion of a Building where the ceiling height is less than 1.2 metres above the floor.

NEW DEFINITION USED IN THESE DOCUMENTS:
"Floor Area Ratio" means the figure obtained when the Floor Area of all floors and Storeys in all Buildings on a Parcel is divided by the Area of the Parcel, calculated in accordance with Section 14.

1 Floor Area Graphic Summary
A1.2 Scale 1:500



2 Site Coverage Graphic Summary
A1.2 Scale 1:500



3 Daily Shading Summary: March, June, Sept.
A1.2 Scale 1:4900



4 Shadows / 9AM and 3PM, March 21st, Sept 21st
A1.2 Scale 1:4900



LINDHOLM LAND & INVESTMENT CORPORATION
7081 Central Street, Victoria, British Columbia, Canada V8M 1Y3
mark.lindholm@lindholm.ca

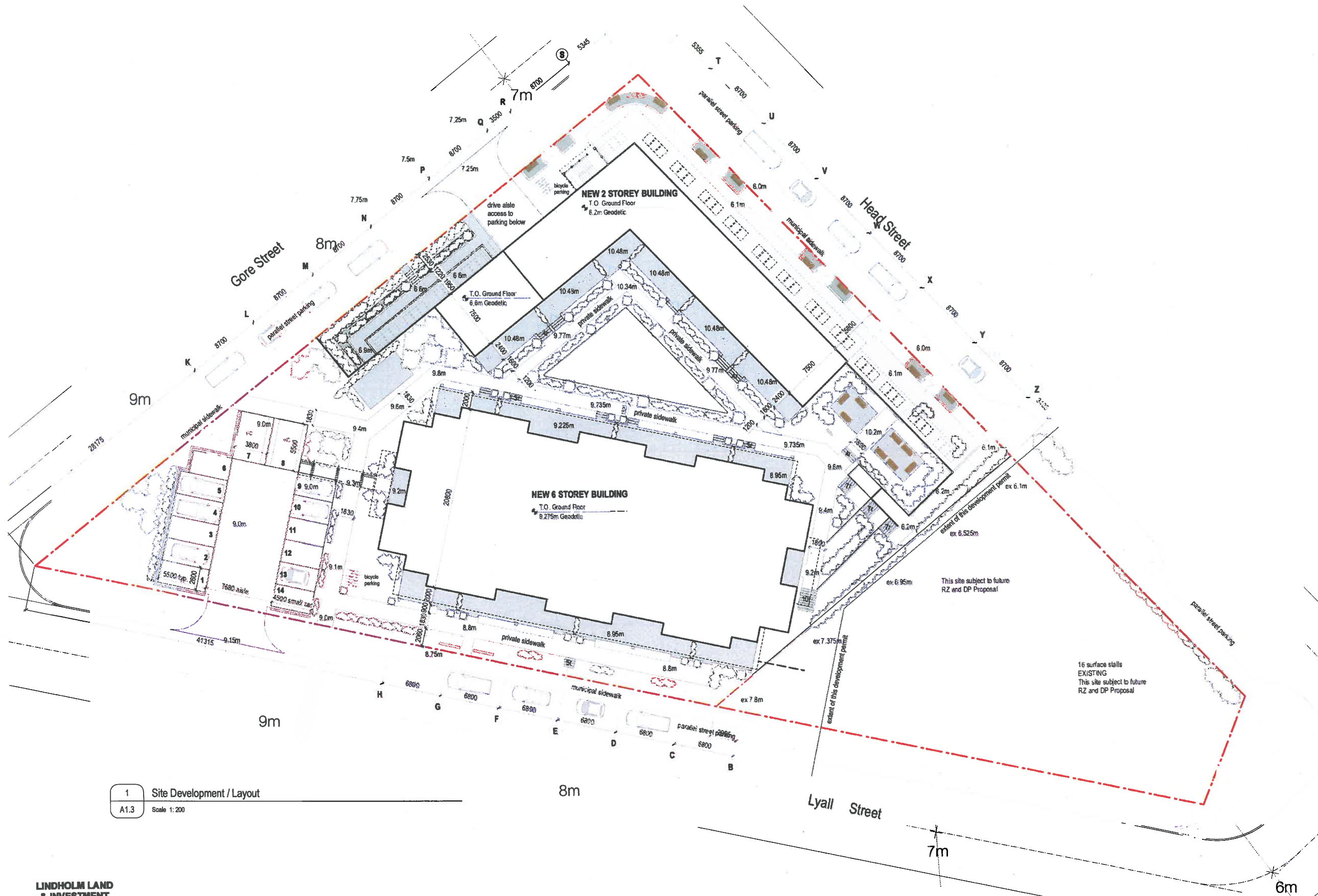
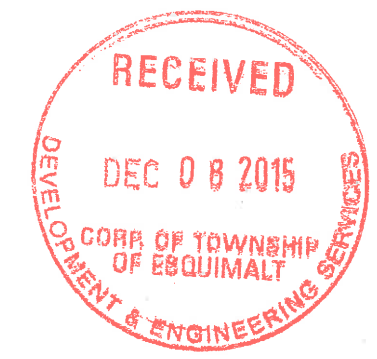
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CORP. OF TOWNSHIP OF ESQUIMALT
DEVELOPMENT & ENGINEERING SERVICES

Hillel architecture
101 18th Ave, Victoria BC V8K 1K3
phone 250-392-9988 fax 250-392-4729

Project: THE WESTBAY TRIANGLE HEAD, GORE & LYALL STREETS, ESQUIMALT
Graphic Project Summary
Drawing No. A1.2

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1 Site Development / Layout
A1.3 Scale 1:200



THE TRIANGLE @ WESTBAY

ESQUIMALT • BRITISH COLUMBIA

Hillel architecture

101 15th Cak Day Avenue
Victoria BC V8K 1K3
phone 250.592.9998
fax 250.592.9978

Project:	THE WESTBAY TRIANGLE HEAD, GORE & LYALL STREETS, ESQUIMALT
Drawing title:	Site Development Layout
Revision no.:	
Drawing no.:	A1.3

10/1/2014



no.	date	description
1	08 December 2010	Preliminary Submittal
2	25 September 2014	Public Review
3	June 2014	Review
4	no entry	no entry
5	no entry	no entry

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Site Finishes Legend
List of Finishes typical of all elevations

- (ET) existing trees to be undisturbed during construction
- (CP) common planting area - see landscape plan for details
- (SA) seeded or sodded grass area - see landscape plan for details
- (NT) approximate location of new tree planting area - see landscape plan for details
- (RF) rock feature - see landscape plan for details
- (RP) raised concrete planter - see landscape plan for details

- (UP) unit paver area - pathways and patios
- (AS) new asphalt surface
- (MS) municipal sidewalk - poured concrete finish
- (PS) private sidewalk - coloured poured concrete finish
- (PW) private walkways / suite entry features - coloured poured concrete finish

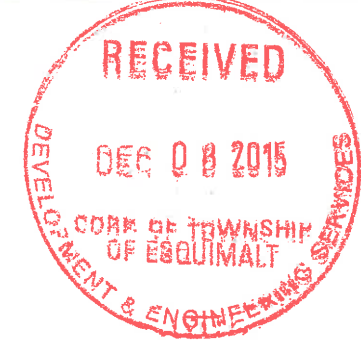
- (PT) private terrace - unit pavers finish
- (RT) roof top terrace - unit pavers finish
- (PRP) private residents park - unit pavers finish with bench seating

- (SC) site privacy screening - cedar fence - see landscape plan for details
- (LPS) landscape privacy screening - see landscape plan for details

- (RW) cultured stone veneer on concrete retaining wall with a concrete cap
- (CW) concrete retaining wall with a concrete cap

- (BP) six bike class 2 visitors bike parking / lockable
- (VP) asphalt surface vehicle parking stalls
- (PB) precast concrete parking blocks, typical all parking stalls

- (ART) public art
- (PL) parking / site lighting



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1 Site Development / Finishes
A1.4 Scale 1:200

LINDHOLM LAND & INVESTMENT CORPORATION
7081 Central Saanich Road
Victoria, British Columbia
Canada V8M 1Y3
work.lindholm@lindholm.land.ca

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Rev	Description	Date	By	Checked
01	Initial	2014.08.01		
02	02 December 2014	2014.12.02		
03	28 September 2014	2014.09.28		
04	June 2014	2014.06.01		
05	2014.05.01	2014.05.01		
06	2014.04.01	2014.04.01		
07	2014.03.01	2014.03.01		

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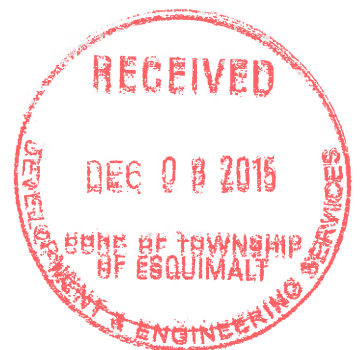
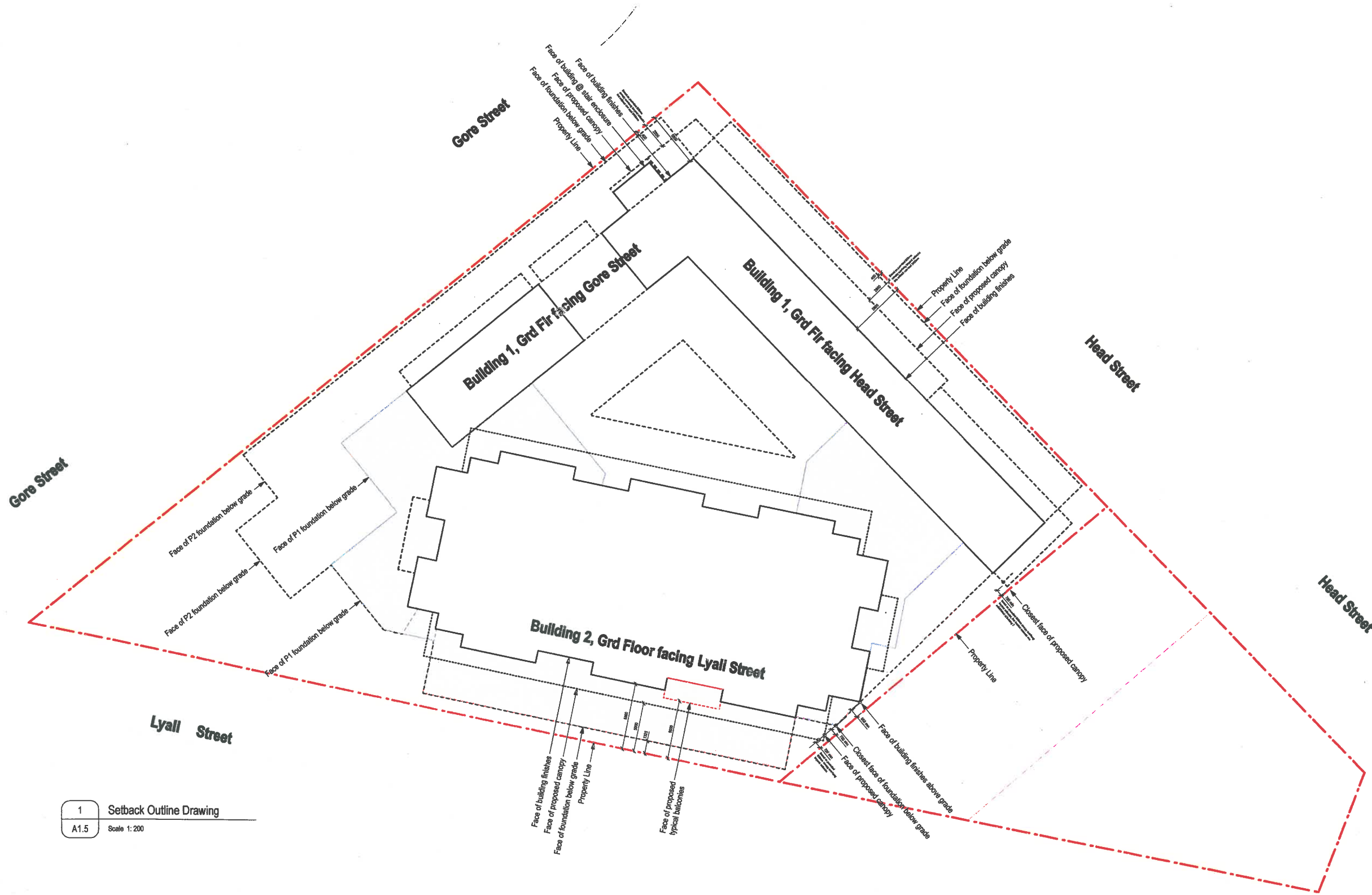
Hillel architecture

101 10th Street, Victoria, BC V8W 2G5
phone: 250-592-9900
fax: 250-592-9979

THE WESTBAY TRIANGLE
HEAD, GORE & LYALL STREETS, ESQUIMALT

project: THE WESTBAY TRIANGLE
drawing title: Site Development Finishes

sheet: A1.4 of 14



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1 Setback Outline Drawing
A1.5 Scale 1:200

no.	date	description
1	08 December 2015	Issued for Information
2	25 September 2014	Public Review
3	June 2014	Issue for
4		Drawn by
5		Checked by
6		Project number
		201430

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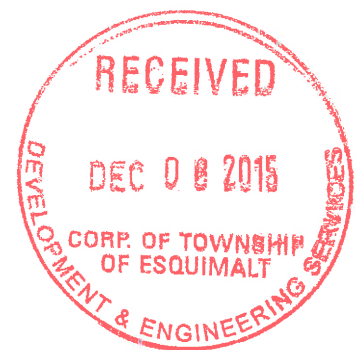
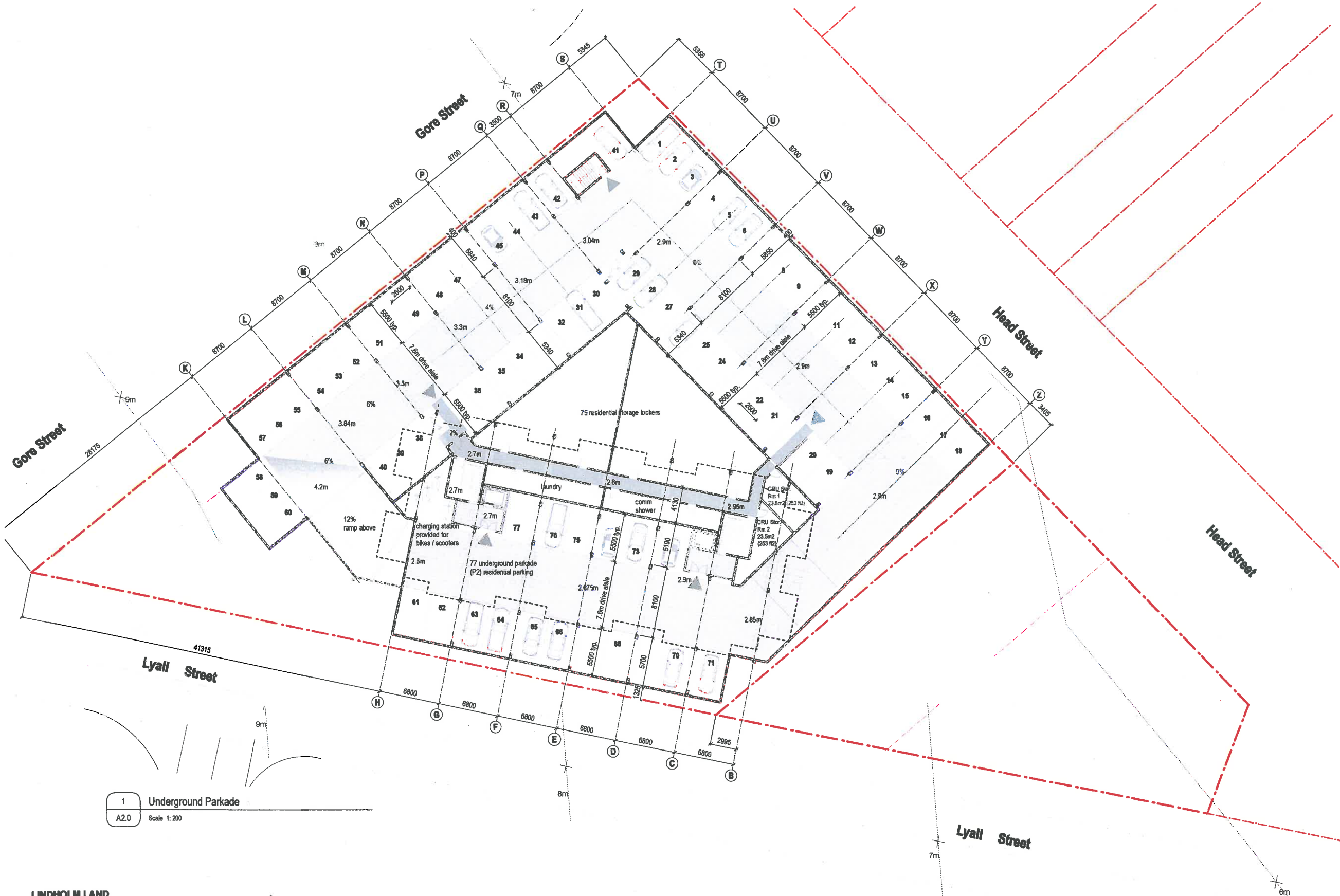
Hillel architecture

project: **THE WESTBAY TRIANGLE**
HEAD, GORE & LYALL STREETS, ESQUIMALT

drawing title: **Graphic Summary of Setbacks**

phone: 250-592-9998
fax: 250-592-9978

revision no. 1
drawing no. A1.5



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1 Underground Parkade
A2.0 Scale 1:200

no.	date	description
01	09 December 2010	Preparation
02	23 September 2014	Revised
03	June 2014	Revised
04	2014	Final

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LINDHOLM LAND & INVESTMENT CORPORATION

7081 Central Saanich Road
Victoria, British Columbia
Canada V8M 1Y3
mark.lindholm@lindholm.ca

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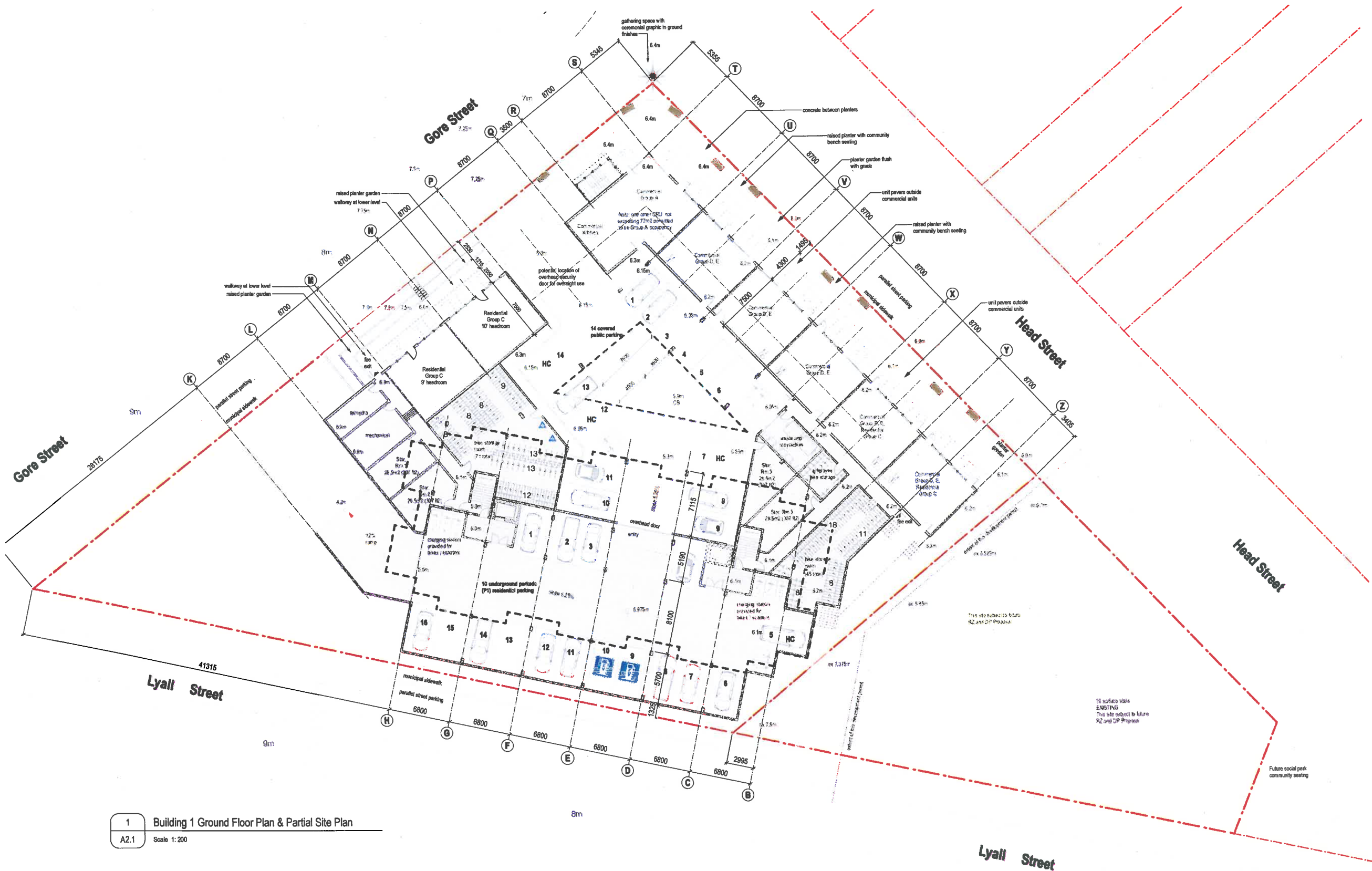
Hillel architecture

project: THE WESTBAY TRIANGLE
HEAD, GORE & LYALL STREETS, ESQUIMALT

drawing title: Underground Parkade Layout

phone: 250-592-9958
fax: 250-592-9978

revision no. 1
drawing no. A2.0



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1 Building 1 Ground Floor Plan & Partial Site Plan
 A2.1 Scale 1:200

Rev	Date	Description
01	08 December 2015	Revised Site Plan
02	25 September 2014	Final Review
03	June 2014	Final Review

201430
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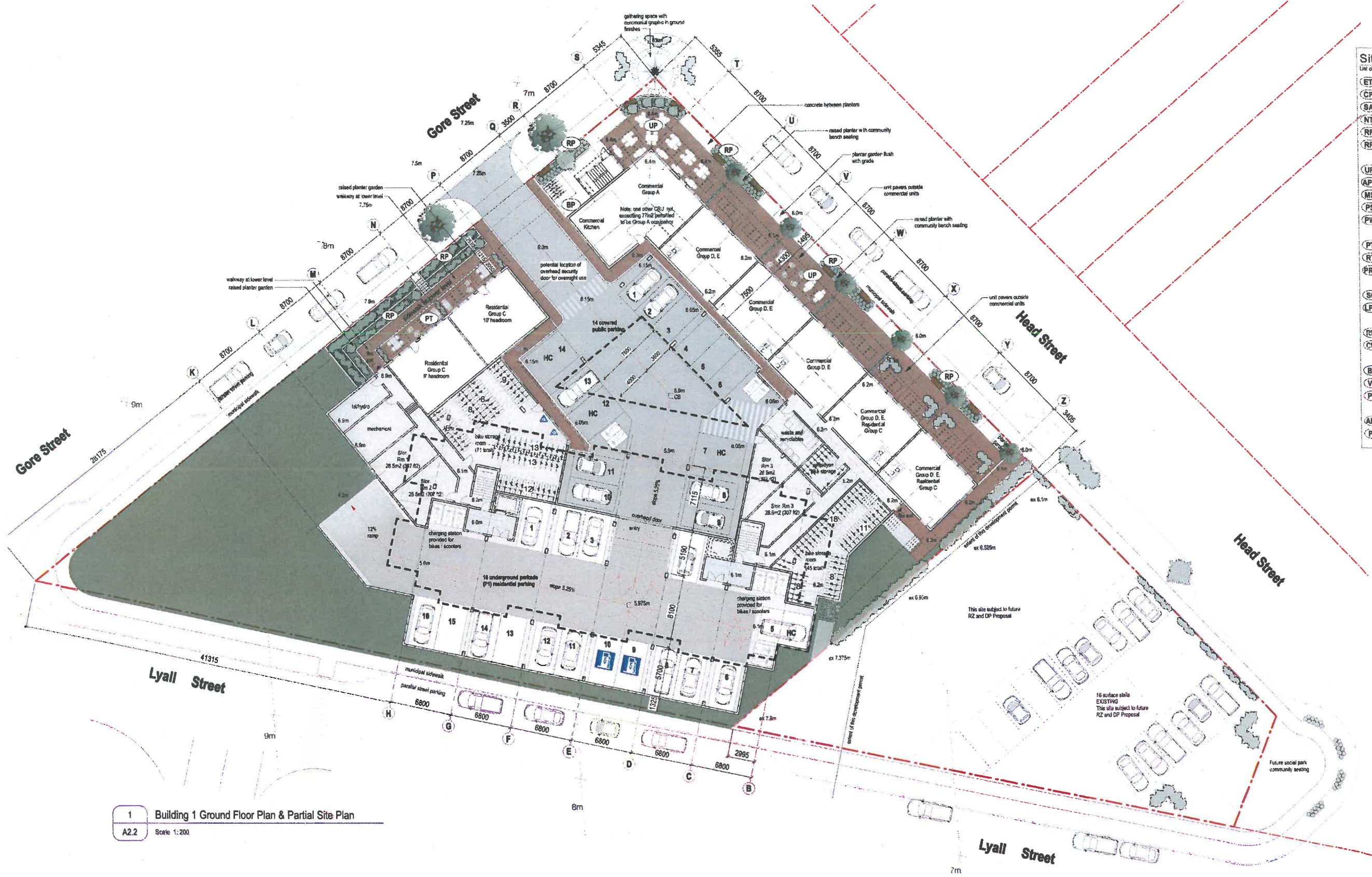


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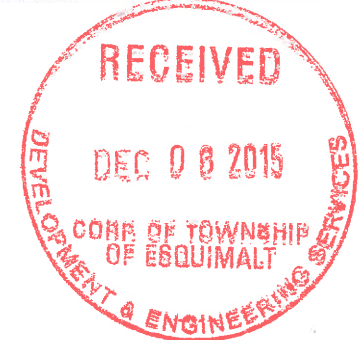
Hillel architecture

project: THE WESTBAY TRIANGLE
 HEAD, GORE & LYALL STREETS, ESQUIMALT
 drawing title: Bldg 1 - Ground Floor @ Head Street - Layout
 phone: 250.592.9998
 fax: 250.592.9978



Site Finishes Legend
List of Finishes typical of all elevations

- (ET) existing trees to be undisturbed during construction
- (CP) common planting area - see landscape plan for details
- (SA) seeded or sodded grass area - see landscape plan for details
- (NT) approximate location of new tree planting area - see landscape plan for details
- (RF) rock feature - see landscape plan for details
- (RP) raised concrete planter - see landscape plan for details
- (UP) unit paver area - pathways and patios
- (APS) new asphalt surface
- (MS) municipal sidewalk - poured concrete finish
- (PS) private sidewalk - coloured poured concrete finish
- (PW) private walkways / suite entry features - coloured poured concrete finish
- (PT) private terrace - unit pavers finish
- (RT) roof top terrace - unit pavers finish
- (PRP) private residents park - unit pavers finish with bench seating
- (SC) site privacy screening - cedar fence - see landscape plan for details
- (LPS) landscape privacy screening - see landscape plan for details
- (EWC) cultured stone veneer on concrete retaining wall with a concrete cap
- (CW) concrete retaining wall with a concrete cap
- (BP) six bike class 2 visitors bike parking / lockable
- (VP) asphalt surface vehicle parking stalls
- (PB) precast concrete parking blocks, typical all parking stalls
- (ART) public art
- (PL) parking / site lighting



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1 Building 1 Ground Floor Plan & Partial Site Plan
A2.2 Scale 1:200



THE TRIANGLE @ WESTBAY

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NO.	DATE	DESCRIPTION	BY	CHECKED
01	04 December 2015	Issued for Review		
02	25 December 2015	Approved for Construction		

Permitted under: 2014.00
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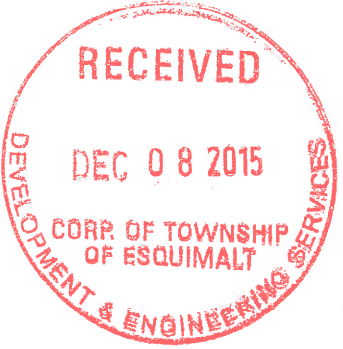
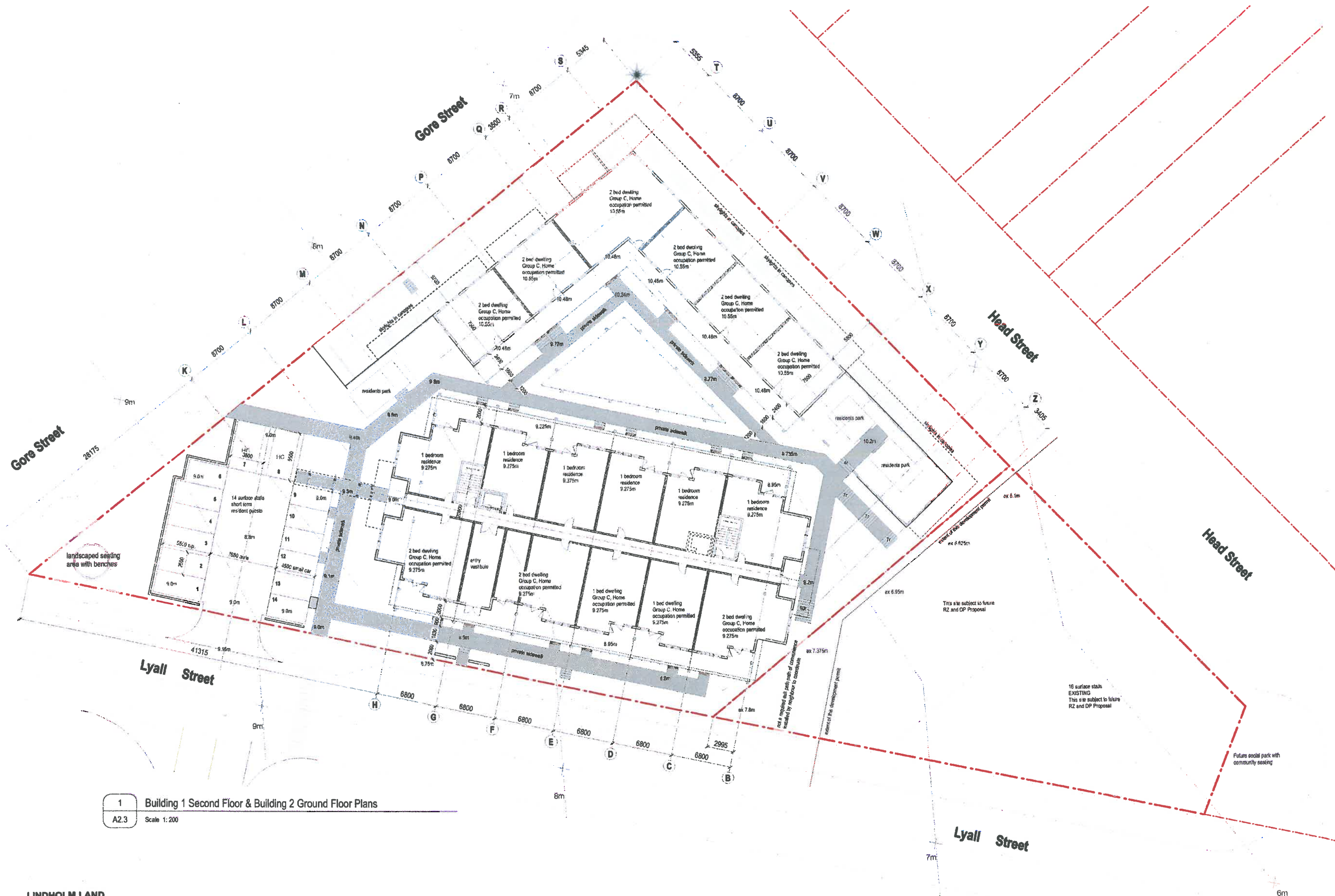
Hillel architecture

9-5011
THE WESTBAY TRIANGLE
 HEAD, GORE & LYALL STREETS, ESQUIMALT

Working title: **Bldg 1 - Ground Floor @ Head Street - Finishes**

101 1551 Oak Bay Avenue
 Victoria, BC V8R-1K3
 phone: 250.992.8994
 fax: 250.992.9178

DATE: 2015.12.08
 DRAWING NO: A2.2



1 Building 1 Second Floor & Building 2 Ground Floor Plans
A2.3 Scale 1:200



THE TRIANGLE @ WESTBAY

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Hillel architecture

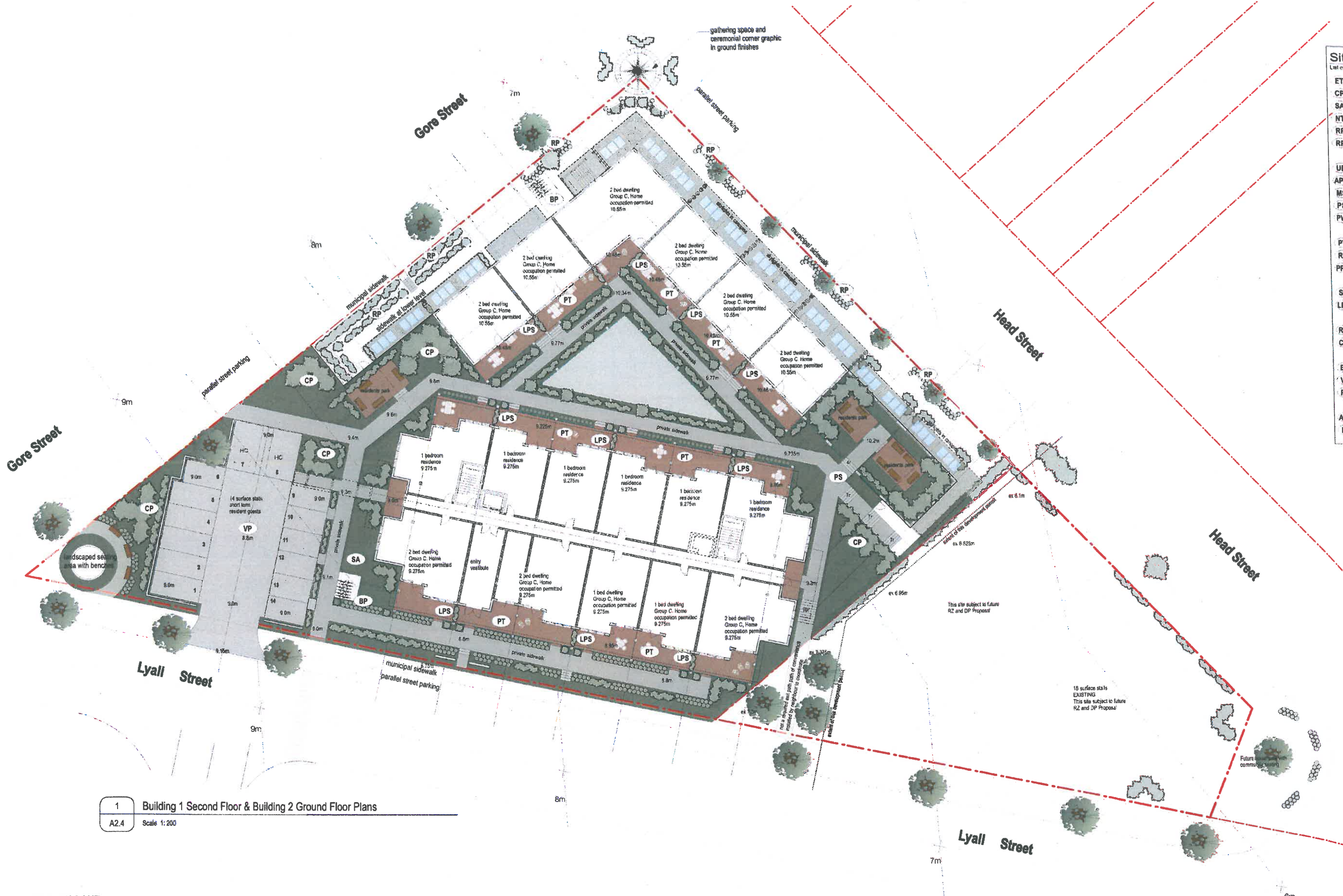
Project: THE WESTBAY TRIANGLE
HEAD GORE & LYALL STREETS, ESQUIMALT
Drawing No: Building 1 & 2 Ground Floor @ Lyall Street - Layout

Project No: 230-574-9976
Task: 250-558-9976

Scale: A2.3

Rev	By	Date	Description
1	AS	05 December 2015	Building Submission
2	AS	05 September 2014	Permitting Review
3	AS	JUNE 2014	Final

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Site Finishes Legend
 List of finishes typical of all elevations

- ET existing trees to be undisturbed during construction
- CP common planting area - see landscape plan for details
- SA seeded or sodded grass area - see landscape plan for details
- NT approximate location of new tree planting area - see landscape plan for details
- RF rock feature - see landscape plan for details
- RP raised concrete planter - see landscape plan for details

- UP unit paver area - pathways and patios
- APS new asphalt surface
- MS municipal sidewalk - poured concrete finish
- PS private sidewalk - coloured poured concrete finish
- PW private walkways / suite entry features - coloured poured concrete finish

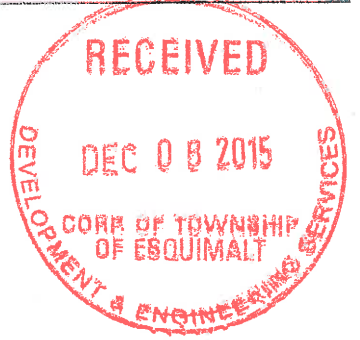
- PT private terrace - unit pavers finish
- RT roof top terrace - unit pavers finish
- PRP private residents park - unit pavers finish with bench seating

- SC site privacy screening - cedar fence - see landscape plan for details
- LPS landscape privacy screening - see landscape plan for details

- RW cultured stone veneer on concrete retaining wall with a concrete cap
- CW concrete retaining wall with a concrete cap

- BP six bike class 2 visitors bike parking / lockable
- VP asphalt surface vehicle parking stalls
- PB precast concrete parking blocks, typical all parking stalls

- ART public art
- PL parking / site lighting



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1 Building 1 Second Floor & Building 2 Ground Floor Plans
 A2.4 Scale 1:200



THE TRIANGLE @ WESTBAY
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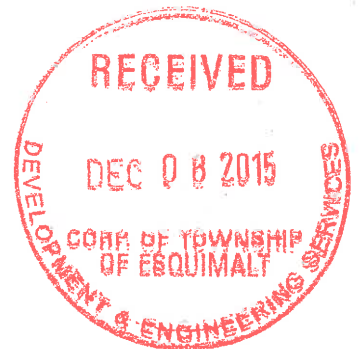
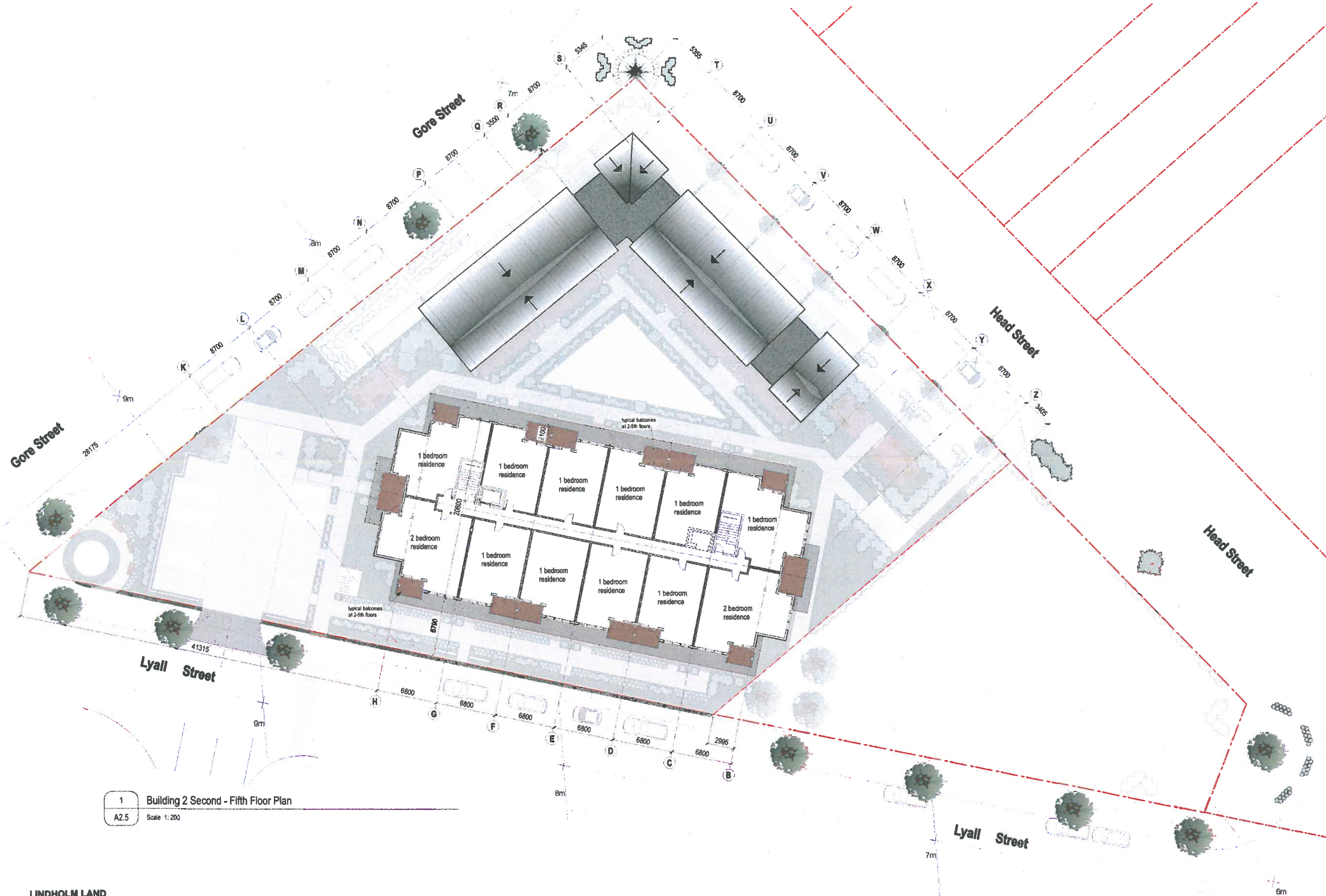


DATE	DESCRIPTION	BY	FOR
06 December 2015	Revised Submission		
25 September 2014	Preliminary Review		
June 2014	Approval		
	Approval		
05 October 2013	Approval		2/14/30

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THE WESTBAY TRIANGLE
 HEAD, GORE & LYALL STREETS, ESQUIMALT
 Building 1 & 2 Ground Floor @
 Lyall Street - Finishes

DATE: 08/12/15
 DRAWING NO: A2.4



[Handwritten signature]

1 Building 2 Second - Fifth Floor Plan
A2.5 Scale 1:200

Rev	Date	Description	By	Checked
01	08 December 2015	Revised Submission		
02	21 September 2014	Previous Review		
03	April 2014	Final		
04				
05				
06				
07				
08				
09				
10				

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LINDHOLM LAND & INVESTMENT CORPORATION




7081 Central Saanich Road
Victoria, British Columbia
Canada V8M 1Y3
mark.lindholm@lindholm.ca

THE TRIANGLE @ WESTBAY

ESQUIMALT • BRITISH COLUMBIA

Hillel architecture

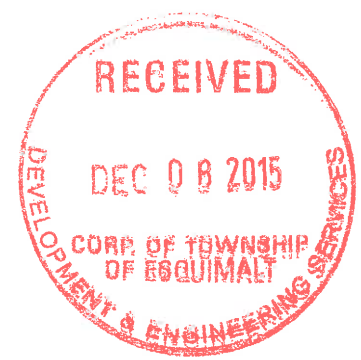
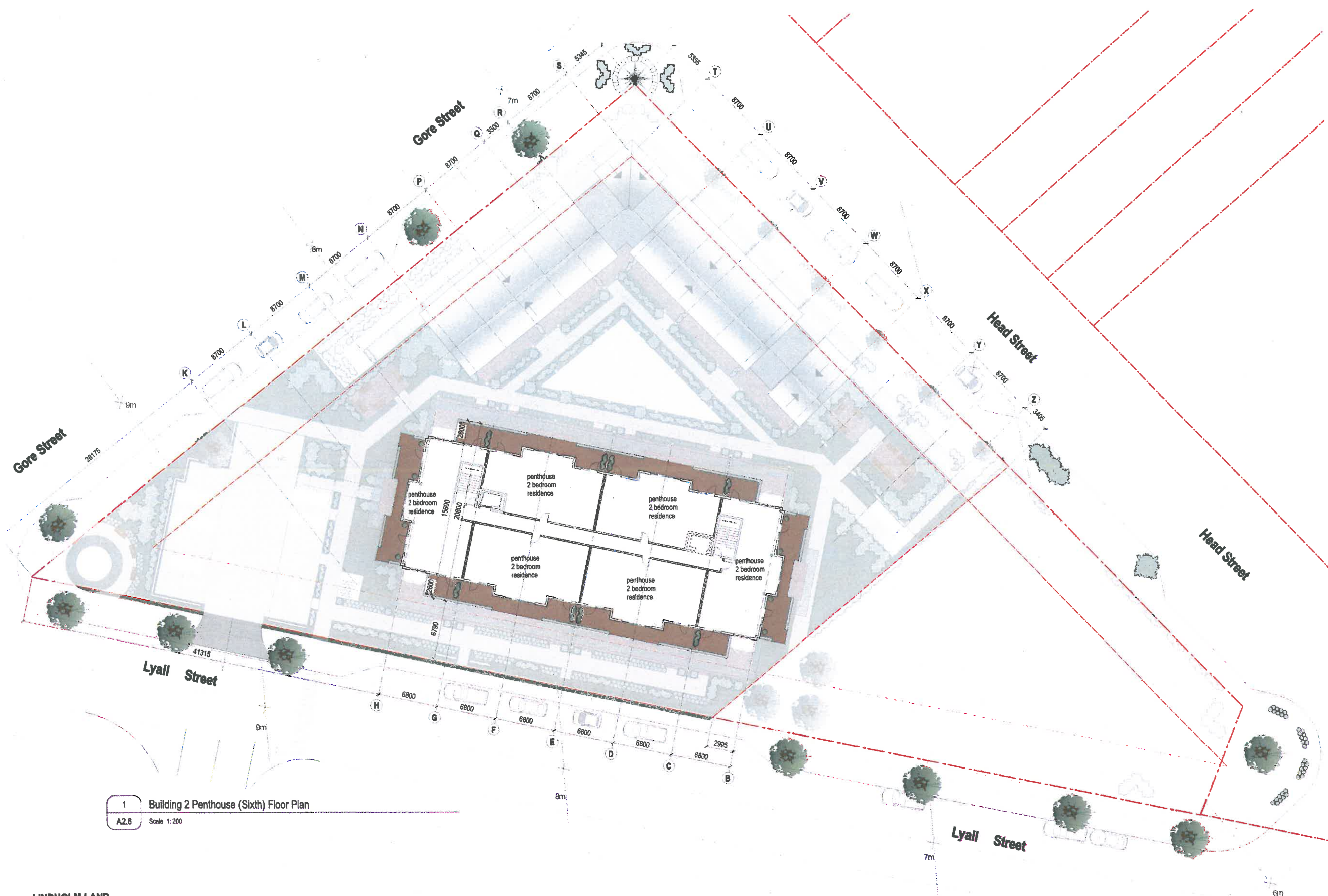


121 859 Oak Bay Avenue
Victoria BC V8R-1K3
phone 250-992-9978
fax 250-992-9978

THE WESTBAY TRIANGLE
HEAD, GORE & LYALL STREETS, ESQUIMALT

Building 2 - Typical Floor Layout
2nd through 5th Floor Plans

A2.5



Handwritten initials: #/

1 Building 2 Penthouse (Sixth) Floor Plan
A2.6 Scale 1:200

REV	DATE	DESCRIPTION	BY	CHECKED
01	08 December 2015	Revised Submittal		
02	23 September 2014	Preparation		
03	20 June 2013	Issue for Review		

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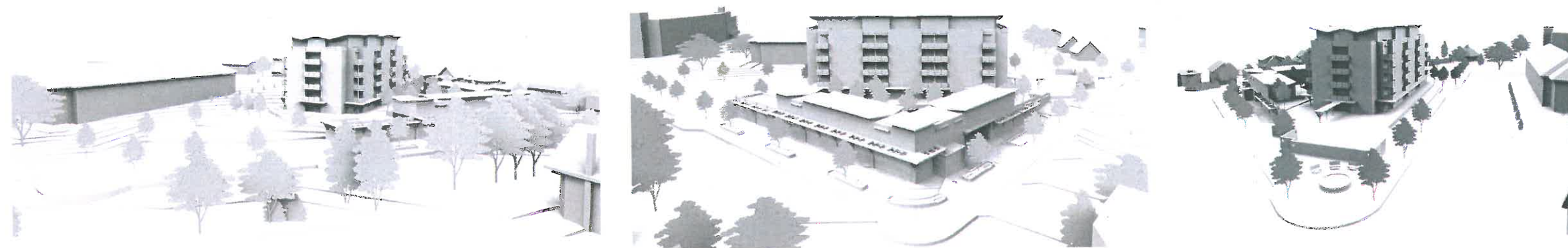
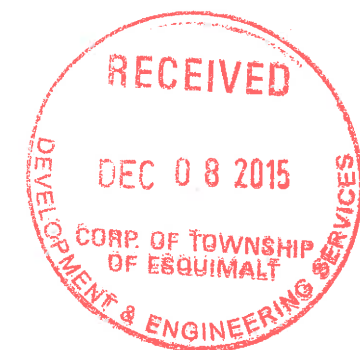
Hillel architecture

101 1851 Cook Bay Avenue
Victoria BC V8R 1K3
phone: 250.992.9988
fax: 250.992.9778

THE WESTBAY TRIANGLE
HEAD, GORE & LYALL STREETS, ESQUIMALT

Project: Bldg 2 - Penthouse (Sixth) Floor Plan

Scale: A2.6



[Handwritten signature]

LINDHOLM LAND & INVESTMENT CORPORATION
 7081 Central Saanich Road
 Victoria, British Columbia
 Canada V8M 1Y3
 mark.lindholm@lindholm.ca

THE TRIANGLE @ WESTBAY

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Hillel
 architecture
 108 854 Oak Bay Avenue
 Victoria BC V8T 1K3
 phone 250-592-9958
 fax 250-592-9978

REV	DATE	DESCRIPTION	BY
01	04 December 2015	Revised to include	Mark Lindholm
02	23 September 2015	Preliminary Review	Mark Lindholm
03	April 2014	Issue for	Mark Lindholm
04		Issue for	Mark Lindholm
05		Issue for	Mark Lindholm
06		Issue for	Mark Lindholm
07		Issue for	Mark Lindholm
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99		Issue for	Mark Lindholm
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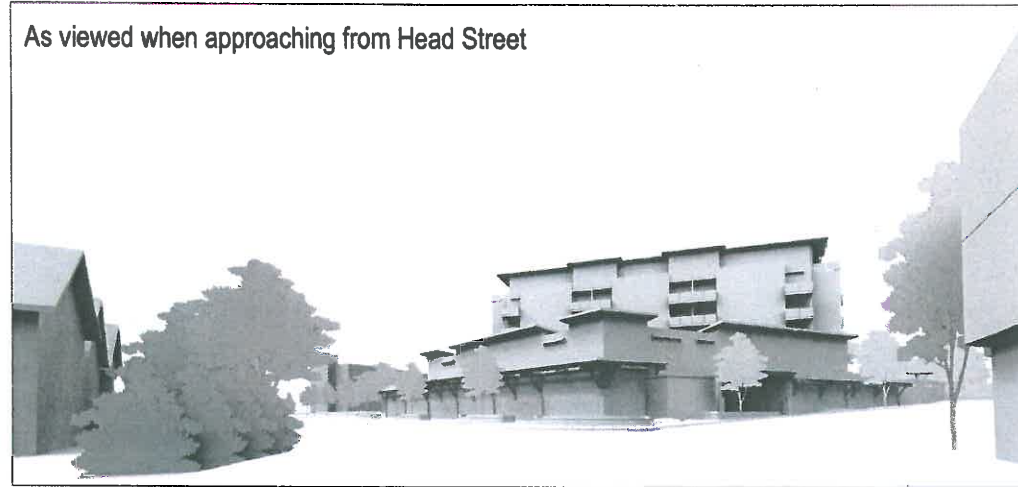
PROJECT: THE TRIANGLE AT WESTBAY
 HEAD, GORE & LYALL STREETS, ESQUIMALT

DRAWING NO: Project Perspectives

DATE: 2014.10

SCALE: A3.0

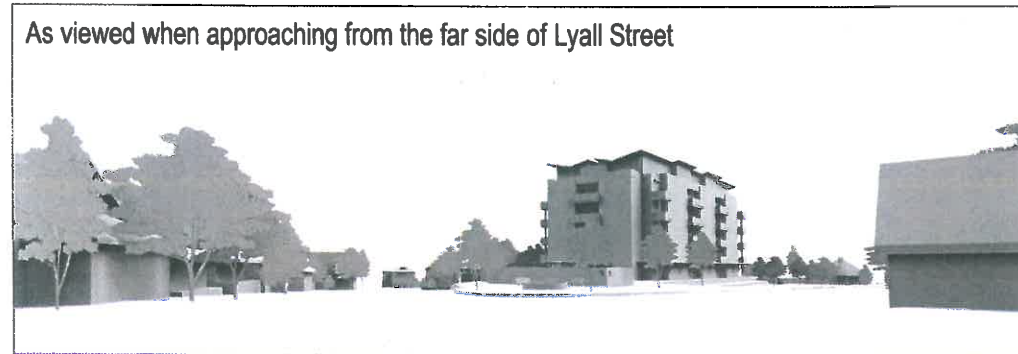
As viewed when approaching from Head Street



As viewed from Head Street



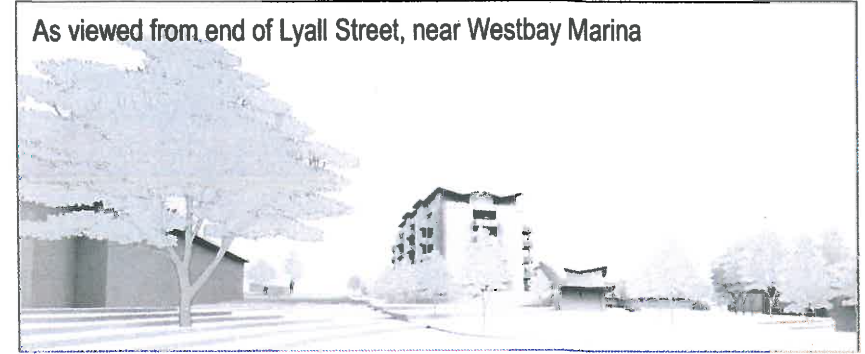
As viewed when approaching from the far side of Lyall Street



As viewed from along Lyall Street



As viewed from end of Lyall Street, near Westbay Marina



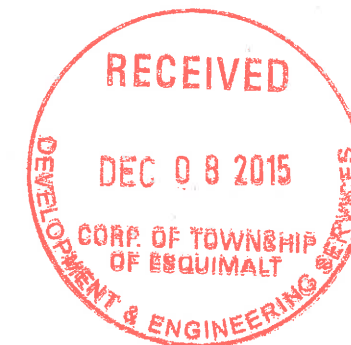
As viewed from a boat on the water in the Marina



Note: This sheet shows artistic renderings only. For exterior finishes, dimensioned elevations, and their relationship to neighboring buildings see elevation sheets A3.2 - A3.8

THE TRIANGLE @ WESTBAY

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Rev	Date	Description
01	06 December 2014	Planning Submission
02	25 September 2014	Preliminary Review
03	June 2014	Final Review
04	2014.02	Final Review

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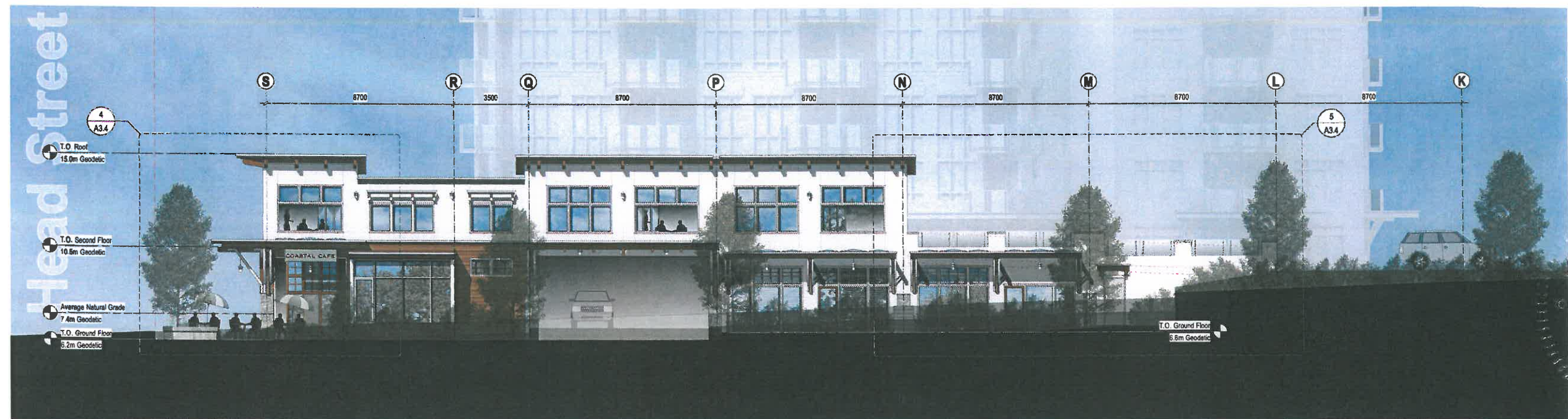
Project: THE TRIANGLE AT WESTBAY
HEAD GORE & LYALL STREETS, ESQUIMALT

Neighbourhood Perspectives

Sheet No: A3.1

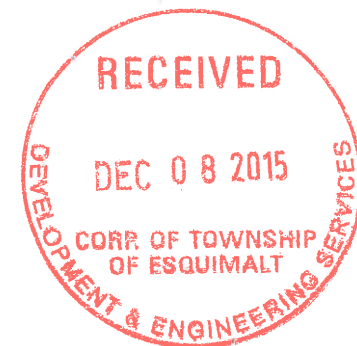


1 Building 1 Head Street [northeast] Exterior Elevation
A3.2 Scale 1:100



2 Building 1 Gore Street [northwest] Exterior Elevation
A3.2 Scale 1:100

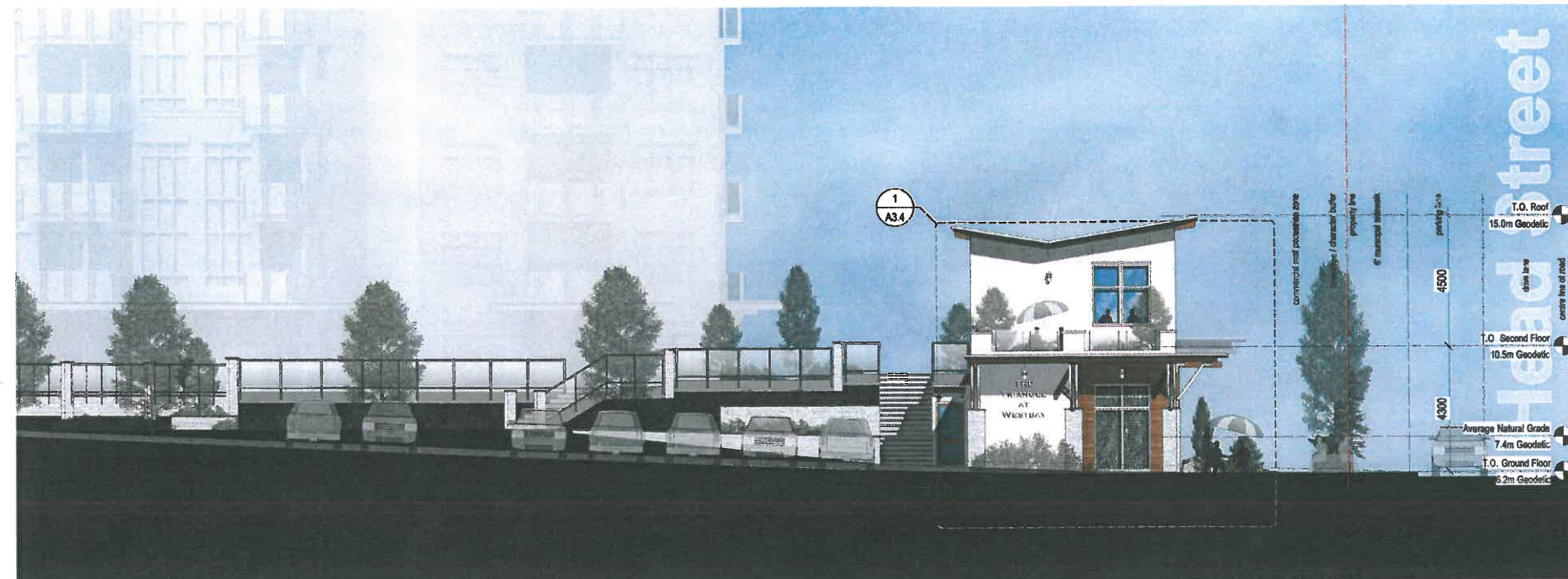
THE TRIANGLE @ WESTBAY
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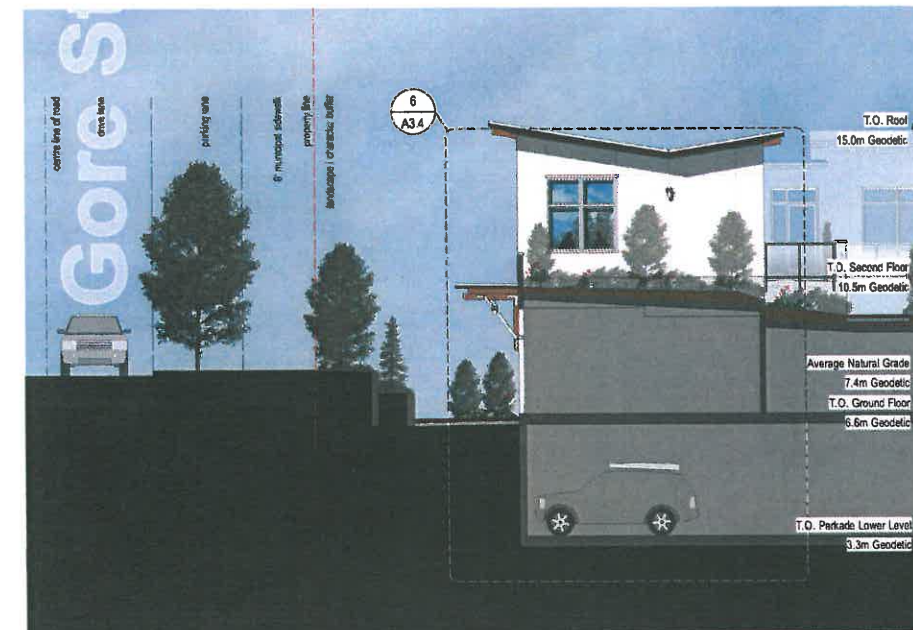
Rev	Date	Description	Author
01	02 December 2015	Revised	Planning Department
02	22 September 2015	Revised	Planning Review
03	June 2015	Revised	
04		Revised	
05	02 October	Revised	JC 4.32

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Project	THE TRIANGLE AT WESTBAY HEAD, GORE & LYALL STREETS, ESQUIMALT
Building	Building 1 - Head Street [northeast] & Gore Street [northwest] Elevations
Sheet No.	A3.2



1 Building 1 [southeast] Exterior Elevation
A3.3 Scale 1:100



2 Building 1 [southwest] Exterior Elevation
A3.3 Scale 1:100

Exterior Finishes Legend
List of Finishes typical of all elevations

Roofing

- 01 Pre-finished metallic gray steel flashing
- 02 Clear Sikksens Cetol finished exposed rafter tails
- 03 Exposed rafter tails wrapped with wood tone coloured steel flashing
- 04 Clear Sikksens Cetol finished fascia boards
- 05 non-combustible hardi (or woodtone product) fascia boards - wood tone finish to match building 1
- 06 Clear Sikksens Cetol finished exposed cedar soffit
- 07 non-combustible soffit finish - wood tone to match building 1
- 08 Structural timber framing roof canopy, clear Sikksens Cetol stain. Canopy on stone veneer column supports.
- 09 Structural timber framed sunshade canopy with clear Sikksens Cetol finish over residential windows

Exterior Finishes

- 10 Panelized, smooth stucco c/w metallic gray steel reveals, Recycled clear glass incorporated into surface Colour: 1: White
- 11 Panelized, smooth stucco c/w metallic gray steel reveals, Recycled clear glass incorporated into surface Colour: 2: Gray
- 12 Panelized, smooth stucco c/w metallic gray steel reveals, Recycled clear glass incorporated into surface Colour: 3: Graphite
- 13 Stucco accent band c/w recycled green and clear glass incorporated into surface Colour: Gray
- 14 Clear Sikksens Cetol finished 1x4 T & Groove Cedar siding installed square edge out for a bandsawn light joint surface
- 15 Non-combustible cementitious wood composite paneling, cedar mill texture, factory prefinished wood tone 1x4 ship lap siding (hardi or woodtone rustic series?)
- 16 Cultured stone veneer: Classic series country ledgestone pattern by Cultured Stone OR Limestone pattern (or cliffstone pattern) by Eldorado Stone (or similar)
- 17 Dark gray elastometric paint finish to exposed concrete components

Exterior Components

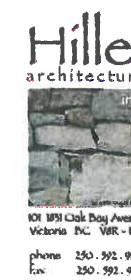
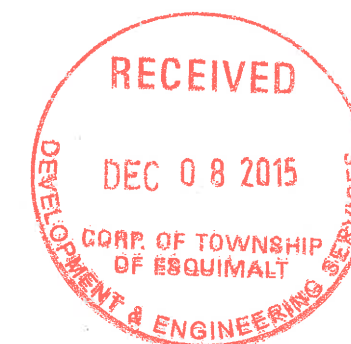
- 20 Clear Sikksens Cetol finished cedar wood and glazing storefront entry door system
- 21 Clear anodized aluminum glazed storefront entry door system
- 22 Clear anodized aluminum exterior glazing system
- 23 Coloured aluminum balcony doors c/w glazing panels with clear anodized aluminum frame
- 24 Opaque spandrel glazing system within clear anodized aluminum frame
- 25 Graphite gray solid metal door
- 26 Wall-mounted railing system: clear anodized aluminum posts and panel mounting hardware c/w frameless clear tempered glass panels
- 27 Wall-mounted railing system: clear anodized aluminum posts and panel mounting hardware c/w frameless clear tempered glass panels with clear Sikksens Cetol finished 1x4 cedar screening members
- 28 Terrace railing system: clear anodized aluminum posts and panel mounting hardware c/w clear tempered glass panels
- 29 Upstand balcony wall: panelized, smooth stucco finish c/w metallic gray steel reveals, recycled clear glass incorporated into surface and pre-finished metallic gray steel flashing cap

Exterior Features

- 30 Cultured stone veneer on concrete column with a concrete cap
- 31 Cultured stone veneer on concrete retaining wall with a concrete cap
- 32 Exposed concrete stairs - sealed
- 33 Exterior light fixture
- 34 Exterior building signage
- 35 Cut aluminum plate building signage with stand-offs
- 36 aluminum skylight



THE TRIANGLE @ WESTBAY
ESQUIMALT • BRITISH COLUMBIA



no.	rev.	description	date
01	01	Initial Issue	08 December 2015
02	02	Revised Submittal	25 September 2016
03	03	Public Review	June 2017
04	04	Final Issue	2014.30

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project: THE TRIANGLE AT WESTBAY (HEAD, GORE & LYALL STREETS, ESQUIMALT)

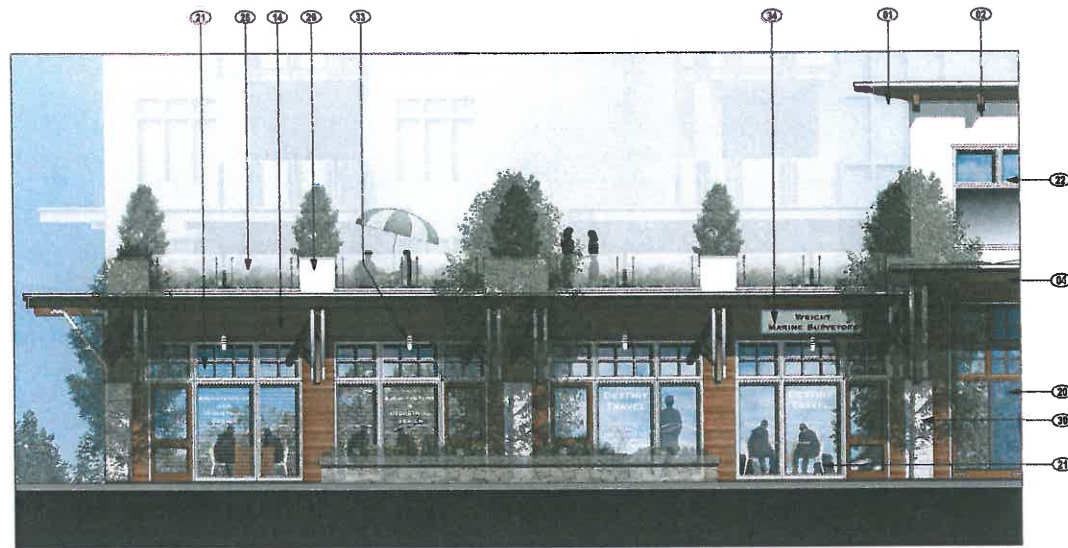
drawing: Building 1 - [southeast] & [southwest] Elevations

sheet no. A3.3





1 Building 1 [southeast] Exterior Elevation
A3.4 Scale 1:50



2 Building 1 Head Street [northeast] Exterior Elevation
A3.4 Scale 1:50



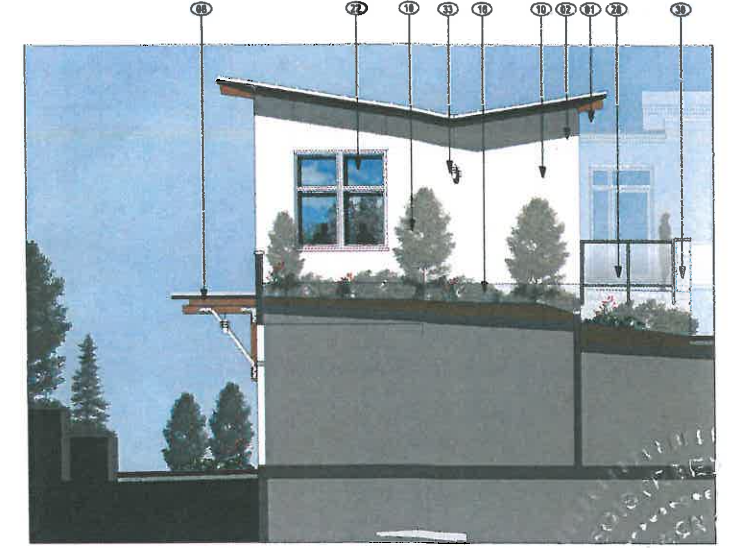
3 Building 1 Head Street [northeast] Exterior Elevation
A3.4 Scale 1:50



4 Building 1 Gore Street [northwest] Exterior Elevation
A3.4 Scale 1:50

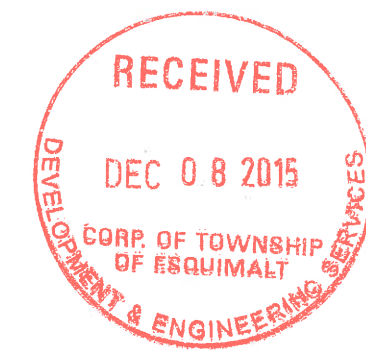


5 Building 1 Gore Street [northwest] Exterior Elevation
A3.4 Scale 1:50



6 Building 1 [southwest] Exterior Elevation
A3.4 Scale 1:50

Handwritten signature



Rev	Description	Author	Checked
1	Issue		
2	08 December 2015		Issued for Review
3	23 September 2014		Preliminary Review
4	June 2014		Issued for Review
5			
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Project: **THE TRIANGLE AT WESTBAY**
HEAD, GORE & LYALL STREETS, ESQUIMALT

Building 1 - Detail Elevations and Finishes

Sheet No: A3.4



1 Building 2 - Lyall Street [south] Exterior Elevation
 A3.5 Scale 1:100



[Handwritten signature]

DATE	DESCRIPTION	BY	CHECKED
18 December 2015	Issue	Mark Schindler	
25 September 2015	Issue	Pauline Taylor	
DATE	DESCRIPTION	BY	CHECKED

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LINDHOLM LAND & INVESTMENT CORPORATION
 7061 Central Saanich Road
 Victoria, British Columbia
 Canada V8M 1Y3
 mark.lindholm@lindholm.ca

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 ESQUIMALT • BRITISH COLUMBIA

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 DEC 08 2015
 CORP. OF TOWNSHIP OF ESQUIMALT
 DEVELOPMENT & ENGINEERING SERVICES

Hillel architecture inc.
 101 354 C. J. Day Avenue
 Victoria BC V8R 1K3
 phone 250-592-9908
 fax 250-592-9979

PROJECT			
THE TRIANGLE AT WESTBAY HEAD, GORE & LYALL STREETS, ESQUIMALT			
DATE	ISSUE		
18 December 2015	Issue		
25 September 2015	Issue		
DATE	DESCRIPTION	BY	CHECKED

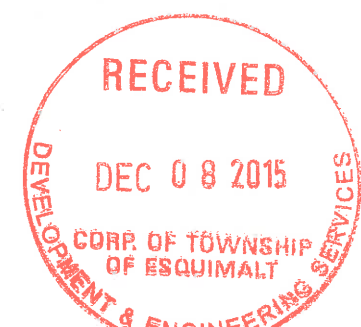


1 Building 2 - [north] Exterior Elevation
A3.8 Scale 1:100



Handwritten signature or initials in blue ink.

THE TRIANGLE @ WESTBAY
ESQUIMALT • BRITISH COLUMBIA



DATE	DESCRIPTION
08 December 2015	Building Submission
20 September 2015	Preliminary Review
June 2014	CONCEPT
DATE	DESCRIPTION
03 October 2014	2014.10

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PROJECT	THE TRIANGLE AT WESTBAY HEAD, GORE & LYALL STREETS, ESQUIMALT
DRAWING NO.	Building 2 - [north] Elevations
DATE	2014.10
DRAWING NO.	A3.6

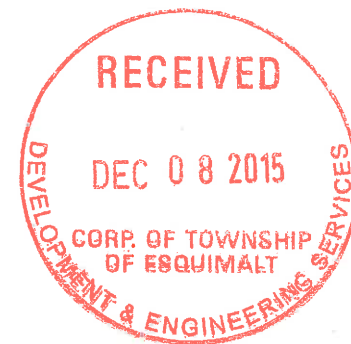


1 Building 2 - [east] Exterior Elevation
A3.10 Scale: 1:100



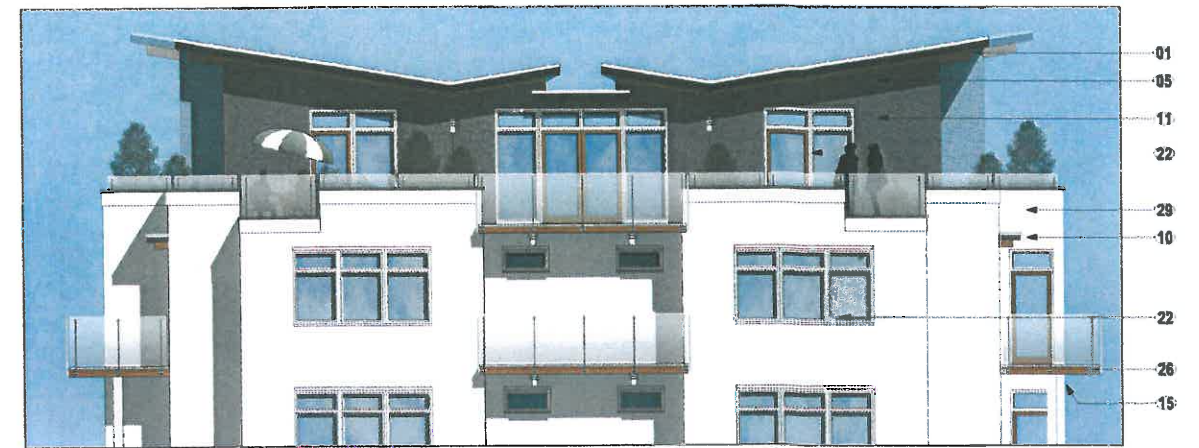
1 Building 2 - Gore Street [west] Exterior Elevation
A3.9 Scale: 1:100

THE TRIANGLE @ WESTBAY
ESQUIMALT • BRITISH COLUMBIA





1 Building 2 - Lyall Street [south] Exterior Elevation
A3.8 Scale 1:50



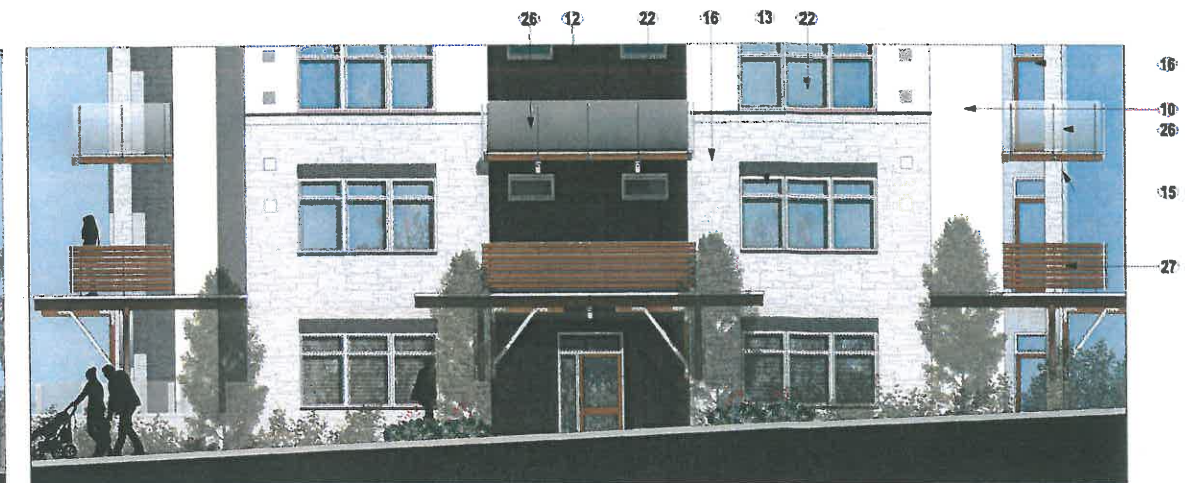
2 Building 2 - Gore Street [east] Exterior Elevation
A3.8 Scale 1:50



3 Building 2 - Lyall Street [south] Exterior Elevation
A3.8 Scale 1:50



4 Building 2 - Lyall Street [south] Exterior Elevation
A3.8 Scale 1:50



5 Building 2 - Gore Street [east] Exterior Elevation
A3.8 Scale 1:50

Exterior Finishes Legend

List of Finishes typical of all elevations

Roofing

- 01: Pre-finished metallic gray steel flashing
- 02: Clear Sikksens Cetol finished exposed rafter tails
- 03: Exposed rafter tails wrapped with wood tone coloured steel flashing
- 04: Clear Sikksens Cetol finished fascia boards
- 05: non-combustible hardi (or woodtone product) fascia boards - wood tone finish to match building 1
- 06: Clear Sikksens Cetol finished exposed cedar soffit
- 07: non-combustible soffit finish - wood tone to match building 1
- 08: Structural timber framing roof canopy, clear Sikksens Cetol stain. Canopy on stone veneer column supports.
- 09: Structural timber framed sunshade canopy with clear Sikksens Cetol finish over residential windows

Exterior Finishes

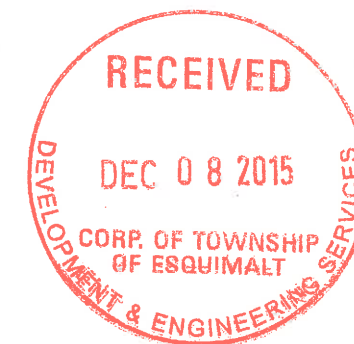
- 10: Panelized, smooth stucco c/w metallic gray steel reveals, Recycled clear glass incorporated into surface Colour 1: White
- 11: Panelized, smooth stucco c/w metallic gray steel reveals, Recycled clear glass incorporated into surface Colour 2: Gray
- 12: Panelized, smooth stucco c/w metallic gray steel reveals, Recycled clear glass incorporated into surface Colour 3: Graphite
- 13: Stucco accent band c/w recycled green and clear glass incorporated into surface Colour: Gray
- 14: Clear Sikksens Cetol finished 1x4 T & Groove Cedar siding installed square edge out for a bandsawn tight joint surface
- 15: Non-combustible cementitious wood composite paneling, cedar mill texture, factory prefinished wood tone 1x4 ship lap siding ("hardi or woodtone rustic series")
- 16: Cultured stone veneer: Classic series country ledgerstone pattern by Cultured Stone DR Limestone pattern (or cliffstone pattern) by Eldorado Stone (or similar)
- 17: Dark gray elastomeric paint finish to exposed concrete components

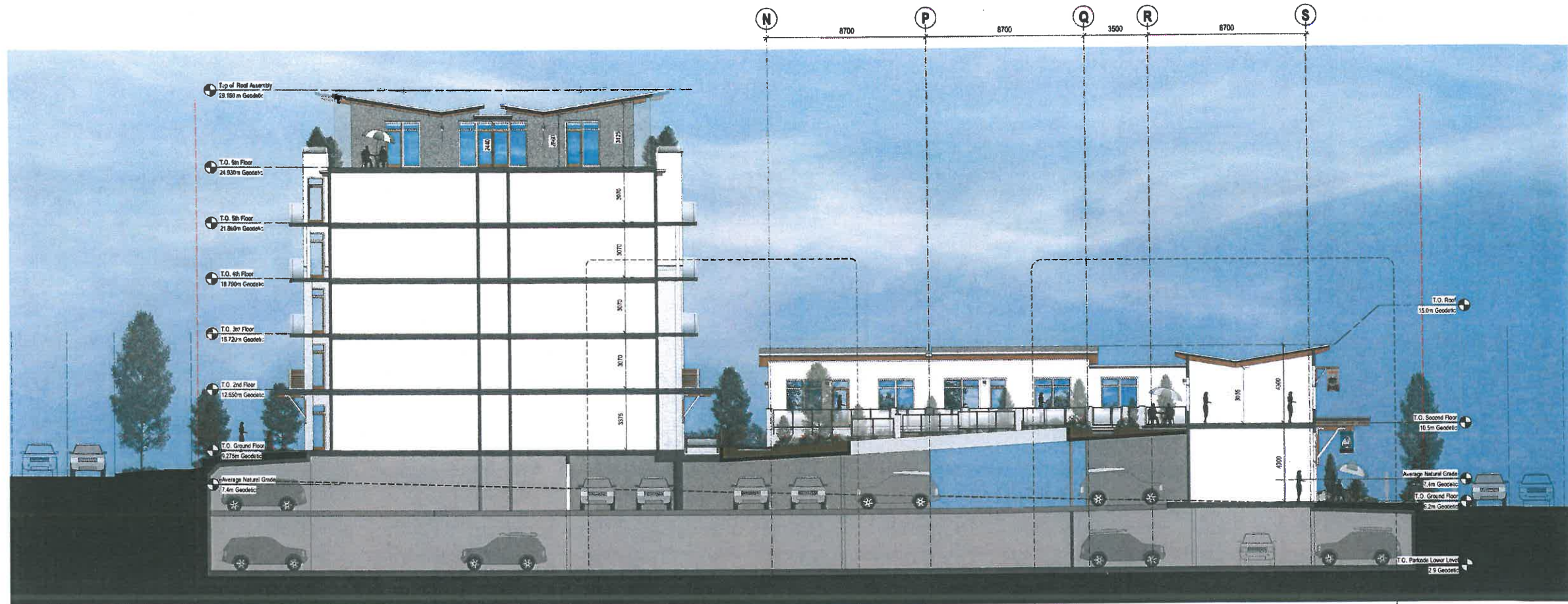
Exterior Components

- 20: Clear Sikksens Cetol finished cedar wood and glazing storefront entry door system
- 21: Clear anodized aluminum glazed storefront entry door system
- 22: Clear anodized aluminum exterior glazing system
- 23: Coloured aluminum balcony doors c/w glazing panels with clear anodized aluminum frame
- 24: Opaque spandrel glazing system within clear anodized aluminum frame
- 25: Graphite gray solid metal door
- 26: Well-mounted railing system: clear anodized aluminum posts and panel mounting hardware c/w frameless clear tempered glass panels
- 27: Well-mounted railing system: clear anodized aluminum posts and panel mounting hardware c/w frameless clear tempered glass panels with clear Sikksens Cetol finished 1x4 cedar screening members
- 28: Terrace railing system: clear anodized aluminum posts and panel mounting hardware c/w clear tempered glass panels
- 29: Upstand balcony wall: panelized, smooth stucco finish c/w metallic gray steel reveals, recycled clear glass incorporated into surface and pre-finished metallic gray steel flashing cap

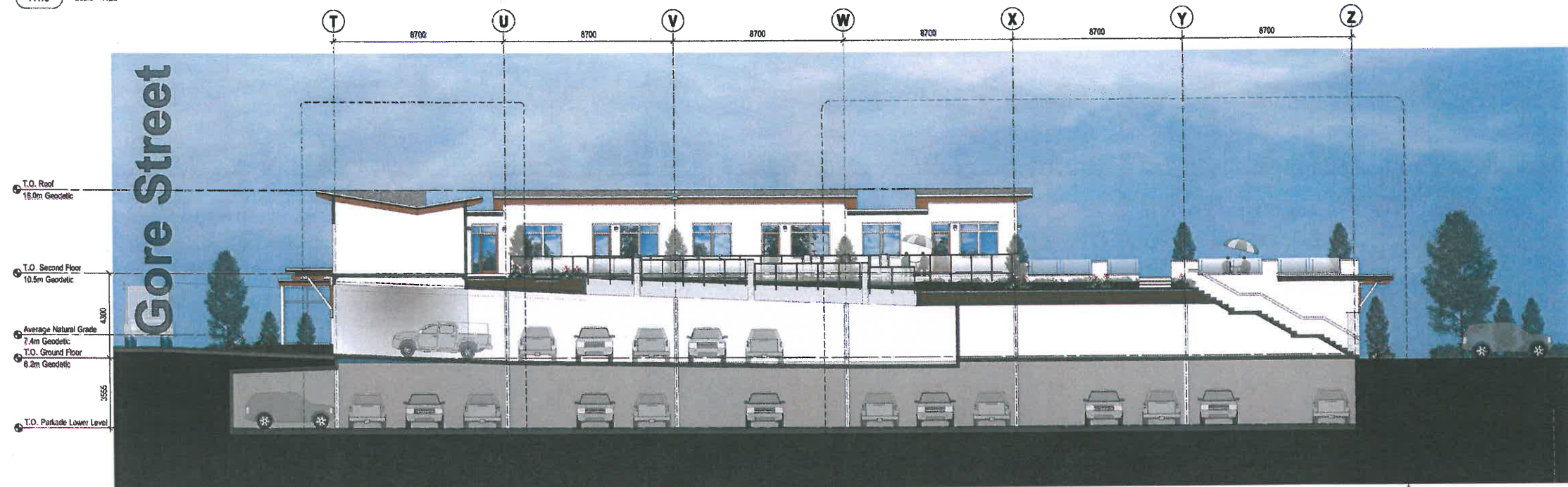
Exterior Features

- 30: Cultured stone veneer on concrete column with a concrete cap
- 31: Cultured stone veneer on concrete retaining wall with a concrete cap
- 32: Exposed concrete stairs - sealed
- 33: Exterior light fixture
- 34: Exterior building signage
- 35: Cut aluminum plate building signage with stand-offs
- 36: aluminum skylight





1 Buildings 1 & 2 - Section Facing [northwest] Gore Street
A4.0 Scale 1:125



1 Building 1 - Section Facing [northeast] Head Street
A4.1 Scale 1:125

THE TRIANGLE @ WESTBAY

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LINDHOLM LAND & INVESTMENT CORPORATION
7081 Central Search Road
Victoria, British Columbia
Canada V8M 1Y3
mart.lindholtm@lindholtm.com

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Hillel architecture
101 851 Oak Bay Avenue
Victoria BC V8K 1C3
phone 250-592-9978
fax 250-592-9978

Rev	Date	By	Checked
01	08 December 2015		Resisting/Culbertson
02	25 September 2016		Philomena/Bauer
03	June 2016		
04	05 October 2016		2016.10

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Project: THE TRIANGLE AT WESTBAY
HEAD, GORE & LYALL STREETS, ESQUIMALT
Drawing: Building 1 & 2 - Section Facing [northwest] Gore Street
Drawing No. A4.1

[Handwritten signature]



1 Building 2 - Section Facing [south] Lyall Street
 A4.2 Scale 1:100

Handwritten signature or initials in blue ink.

LINDHOLM LAND & INVESTMENT CORPORATION
 7081 Central Search Road
 Victoria, British Columbia
 Canada V8M 1Y3
 mark.lindholtm@lindholtm.ca

THE TRIANGLE @ WESTBAY
 ESQUIMALT • BRITISH COLUMBIA

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 CORP. OF TOWNSHIP OF ESQUIMALT
 DEVELOPMENT & ENGINEERING SERVICES

Rev	08 December 2015	Author	Planning Department
Rev	20 September 2015	Author	Planning Review
Rev	June 2014	Author	
Rev	March 2014	Author	

Permission is granted for these drawings to be registered by Lindholm Land and Investment Corporation.

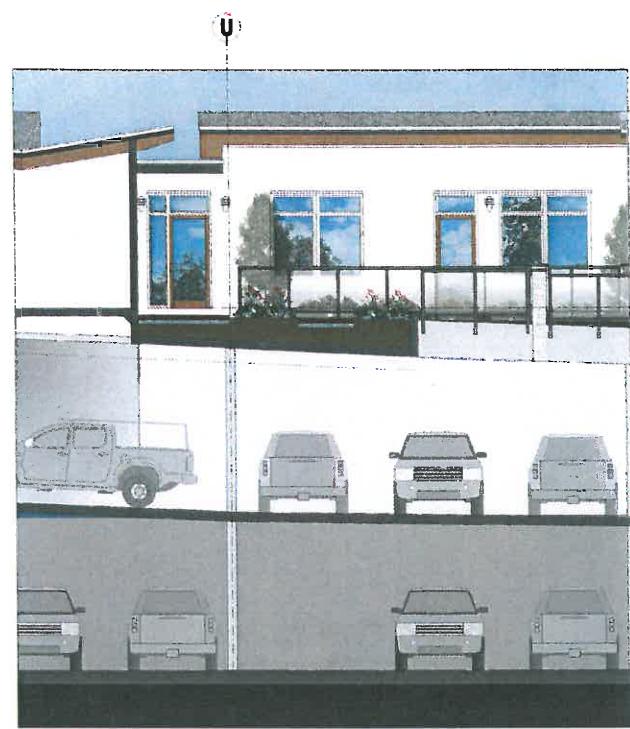
Hillel architecture
 inc.

Project: **THE TRIANGLE AT WESTBAY**
 HEAD, GORE & LYALL STREETS, ESQUIMALT

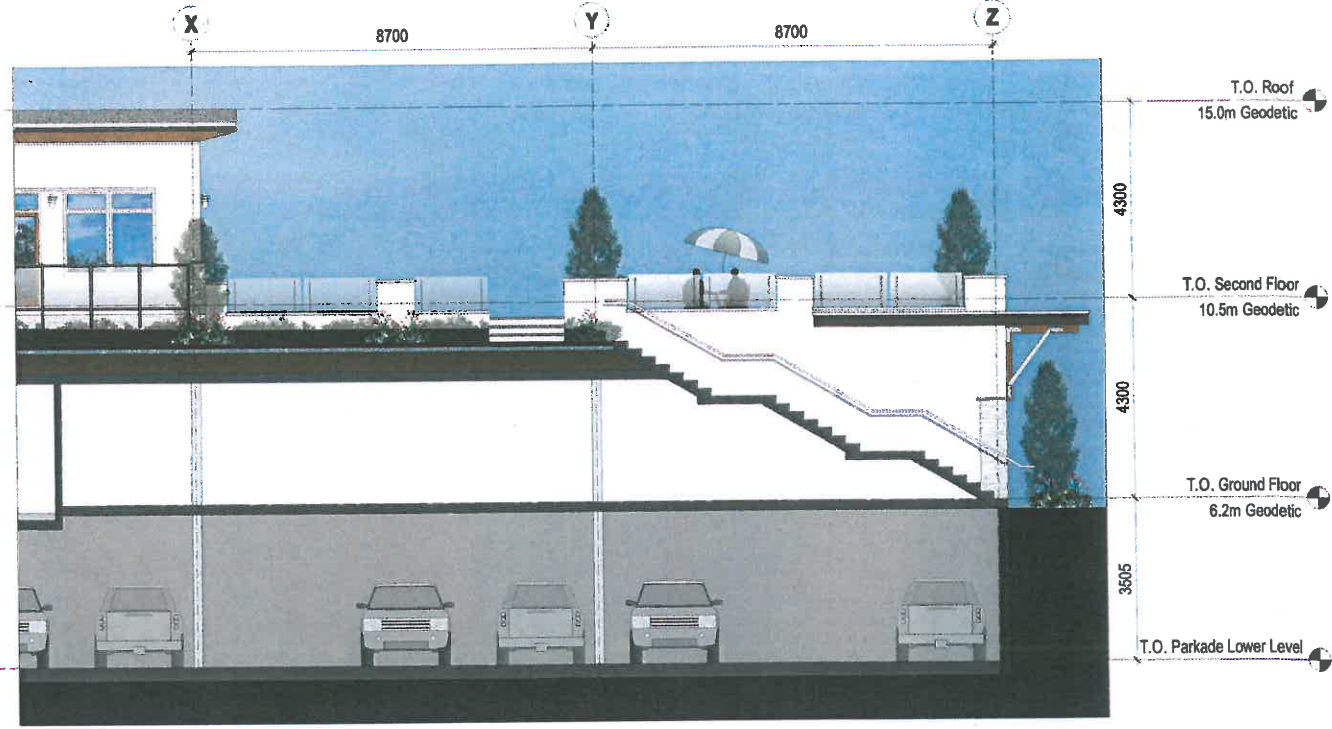
Working file: **Building 2 - Section Facing [south] Lyall Street**

1411 W. Col. Day Avenue
 Victoria, BC V8R 4K3
 phone: 250-592-9988
 fax: 250-592-9978

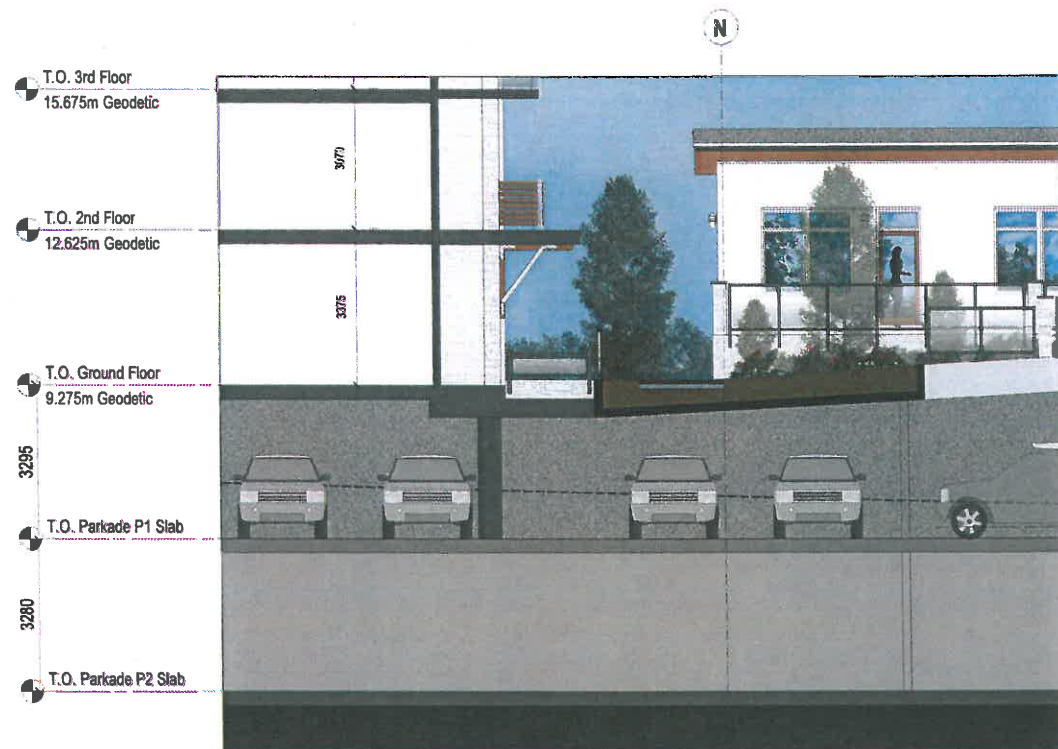
sheet no. **A4.2**



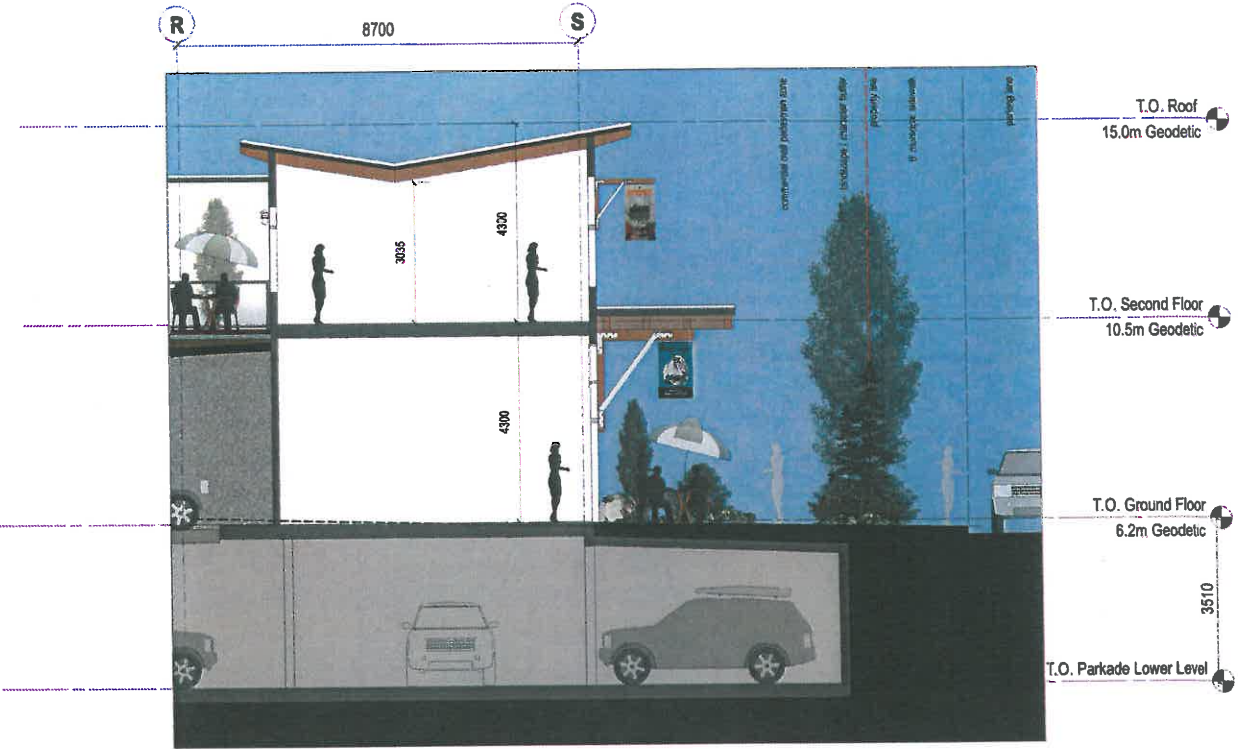
1 Buildings 1 - Detail Section
A4.3 Scale 1:75



2 Building 1 - Detail Section
A4.3 Scale 1:75



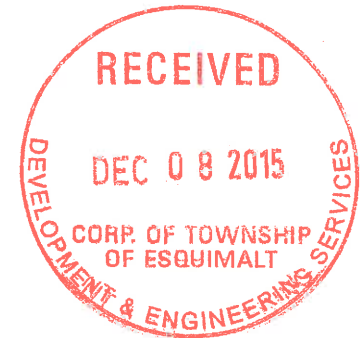
3 Buildings 1 & 2 - Detail Section
A4.3 Scale 1:75



4 Building 1 - Detail Section
A4.3 Scale 1:75

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Hillel architecture inc.

Project: THE TRIANGLE AT WESTBAY
HEAD, GORE & LYALL STREETS, ESQUIMALT

Building 1 & 2 - Detail Sections

Phone: 250-592-9808
Fax: 250-592-9778

Sheet No: A4.3

Date	Description
08 December 2015	Issued for Review
25 September 2014	Pre-Review Meeting
June 2014	Issue for Review

2014.30

Handwritten signature

Plan Showing Proposed Building Location
 Situated on Proposed Consolidation of:
 - 1, 2, 4, 5, 6, 7, 8, Block H, Plan 292;
 - Strata Plan 509;
 all of Section 11, Esquimalt District.

Township of Esquimalt



The intended plot size of this plan is 864 mm in width by 559 mm in height (D size) when plotted at a scale of 1:250

Legend:

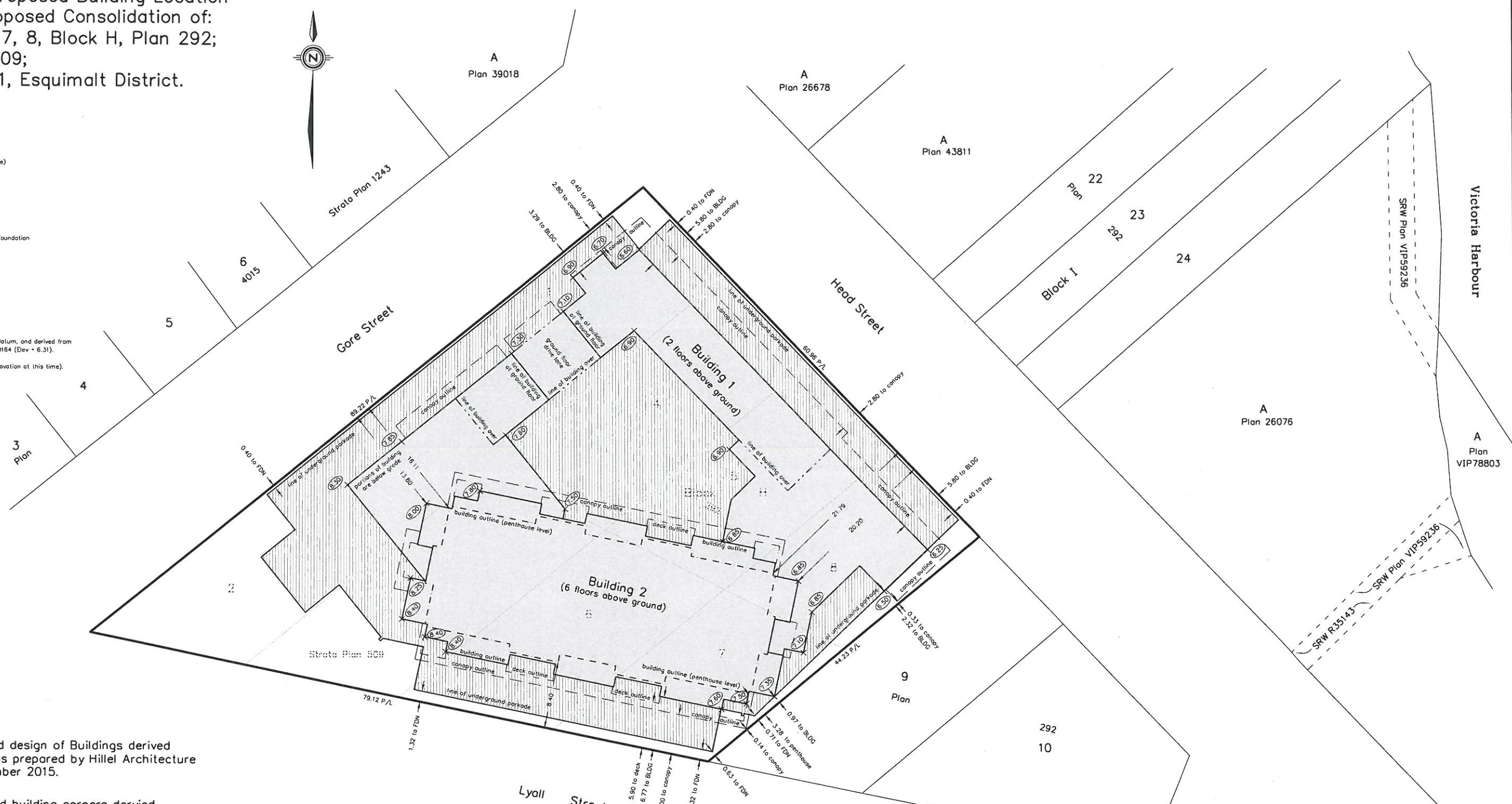
All distances are in metres and decimals thereof unless otherwise indicated.

- denotes parkade and underground foundation
- building footprint
- P/L denotes property line
- FDN denotes foundation underground
- BLDG denotes building above ground

Total Site Area = 0.408 ha

Elevations are in metres, based on geodetic datum, and derived from Bench Marks 84H0163 (Elev = 9.98) and 84H0164 (Elev = 6.31).

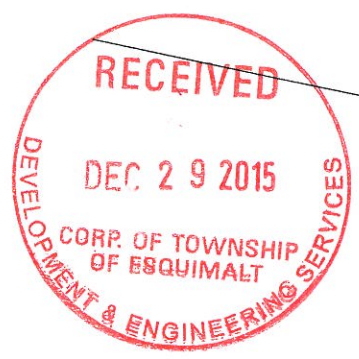
Elevation at Original Ground (no excavation at this time).



Note:

Proposed location and design of Buildings derived from autocad drawings prepared by Hillel Architecture and received 7 December 2015.

Elevations at proposed building corners derived from interpolation based on field surveys from July 2012 and April 2013.



McIlvoney Riley Land Surveying Inc.
 #113 - 2244 Sooke Road
 Victoria, B.C. V9B 1X1
 (250) 474-5538
 www.mrls.ca

File: 4175ProposedDevelopment-20151210

This plan was prepared for municipal purposes and is for the exclusive use of our client. This document shows the relative location of the PROPOSED BUILDING to be constructed with respect to the boundaries of the parcels described above. This document shall not be used to define property lines or property corners. This PROPOSED BUILDING LOCATION PLAN is certified correct this 22nd day of December 2015.

c:\A. can\Minda Riley HLKDM3,
 a=BC Land Surveyor, au=Verify ID at
 www.juricert.com LKUP.cfm?
 id=HLKDM3
 2015.12.22 13:07:49 -0800

B.C.L.S.

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