



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

ADVISORY PLANNING COMMISSION AGENDA

TUESDAY MARCH 19, 2019

7:00 P.M.

COUNCIL CHAMBER, MUNICIPAL HALL

I. CALL TO ORDER

II. LATE ITEMS

III. ADOPTION OF AGENDA

IV. ADOPTION OF MINUTES – February 19, 2019

V. STAFF REPORTS

1) **DEVELOPMENT PERMIT and DEVELOPMENT VARIANCE PERMIT**

1129 Munro Street

[PID 026-958-376, Lot 2, Section 11, Esquimalt District, Plan VIP82523]

Purpose of Application:

The applicant is proposing to construct a new detached garage. 1129 Munro Street is the original 80 year old building on the lot, which has previously been subdivided creating 2 further residential properties 325 and 327 Kinver Street. As such the only remaining space within the lot in question accessible by vehicles is to the front of the property. Therefore, the applicant has proposed to site the detached garage in front of the principal building. This is contrary to the zoning bylaw for RS-1, which requires any accessory buildings to be situated behind any primary buildings. As such, a Development Permit and Development Variance Permit are required.

The subject property falls under the following Development Permit Areas:

- Development Permit Area 1: Natural Environment,
- Development Permit Area 3: Enhanced Design Control Residential,
- Development Permit Area 7: Energy Conservation & Greenhouse Gas Reduction
- Development Permit Area 8: Water Conservation and as such this application requires a Development Permit.

Recommendation:

That the Esquimalt Advisory Planning Commission [APC] recommends to Council that the application for a Development Permit and a Development Variance Permit allowing construction of a new accessory building as illustrated in the architectural drawings and landscape plan prepared by Jeffrey A. Duffield, stamped "Received September 27, 2018", sited as detailed on the survey plan prepared by Ryan P. Hourston BCLS, stamped "Received September 27, 2018", and including the following variances for the property located at PID 026-958-376, Lot 2, Section 11, Esquimalt District, Plan VIP82523 [1129 Munro Street] **to either approve, or deny the application; including the reasons for the recommendation.**

Zoning Bylaw, 1992, No. 2050, Section 34 (9)(b)(i) – Siting Requirements – Accessory Building – Front Setback: Exemption from the requirement that no Accessory Building shall be located in front of the front face of the Principal Building

Parking Bylaw No. 1992, No. 2011, Part 4 (9)(4) – Provisions and Maintenance of Off-Street Parking and Loading Areas: Exemption to the requirement that Parking Spaces in Residential zones be located no closer to the Front Lot Line than the front face of the Principal Building [i.e. the one required parking space will be located in the new accessory building].

2) DEVELOPMENT VARIANCE PERMIT

1336 Wood Street

[PID 006-375-294, Lot 59, Suburban Lot 37, Esquimalt District, Plan VIP 2854]

Purpose of the Application:

The applicant is proposing three additions to the existing 70 year old dwelling. These extensions are to be located on the North Western corner of the existing building, the Southern elevation and the Western elevation. The steps leading to the main entrance are to be extended and a new covered entrance landing installed, to be built to the dimensions and specifications shown in the attached drawings. The proposed works will not increase the Floor Area Ratio or Lot Coverage over the acceptable limits for a single family dwelling in an RS-1 zone, however the front setback will in effect be reduced to 3.29metres. This is contrary to the zoning bylaw for RS-1, which requires a minimum setback of 7.5m. As such, a Development Variance Permit is required.

The subject property is exempt from any Development Permit Areas under paragraph 19 of Section 17.3, as the proposal is for works to a single detached, non-infill dwelling.

Recommendation:

That the Esquimalt Advisory Planning Commission recommends to Council that the application for a Development Variance Permit allowing external alterations to the existing property at 1336 Wood Street, as illustrated in the architectural drawings prepared by Adapt Design, stamped “Received January 22, 2019”, sited as detailed on the survey plan prepared by Wey Mayenburg, stamped “Received January 22, 2019”, and including the following variances for the property located PID 006-375-294, Lot 59, Suburban Lot 37, Esquimalt District, Plan VIP 2854 [1336 Wood Street] **to either approve, or deny the application; including the reasons for the recommendation.**

Zoning Bylaw, 1992 No.2050, Section 34 (9)(a)(i)- Siting Requirements- Primary Building- Front Setback: Exemption from the requirement that no principal building shall be located within 7.5 metres of the Front Lot Line [i.e from 7.5m to 3.2m].

3) DEVELOPMENT VARIANCE PERMIT

1234 Esquimalt Road [Action Motorcycles]

[PID: 026-617-340, Lot 1, Suburban Lots 40 and 41, Esquimalt District, Plan VIP80657]

Purpose of the Application:

The owner is proposing a small addition to the front of the building to facilitate improved access to the existing stairwell; allowing customers of the retail store [Action Motorcycles] to access the upper floor from within the store. The proposal for the additional floor space has required a recalculation of the parking requirements for the property.

Recommendation:

That the Esquimalt Advisory Planning Commission recommends to Council that the application for a Development Variance Permit to legitimize the existing parking as illustrated in the Parking Plan provided by Keay Architecture Ltd., stamped “Received February 14, 2019”, by authorizing the following variance to the Parking Bylaw 1992, No. 2011, for the property located at 1234 Esquimalt Road [PID: 026-617-340; Lot 1 Suburban Lot 40 and 41 Esquimalt District Plan VIP80657] be forwarded to Council with a **recommendation to either approve, approve with conditions, or deny the application; and provide reasons for the chosen recommendation;**

Parking Bylaw, 1992, No. 2011, Part 5 – Parking Requirements, Section 13 – Number of Off-Street Parking Spaces (b) Commercial (uses Retail, Restaurant, Other – repair shop) – Allow a reduction in the number of spaces, from 56 vehicle spaces required, to 18 vehicle spaces and 40 motorcycle parking spaces.

4) OFFICIAL COMMUNITY PLAN AMENDMENT AND REZONING APPLICATION

616 CONSTANCE AVENUE

[PID 000-713-465 Lot 95, Suburban Lot 44, Esquimalt District, Plan 2854]

620 Constance Avenue

[PID 000-819-832 Lot 4, Suburban Lot 44, Esquimalt District, Plan 3135]

619 Nelson Street

[PID 006-393-608 Lot 84, Suburban Lots 44 and 45, Esquimalt District, Plan 2854]

623 Nelson Street

[PID 006-278-647 Lot 3, Suburban Lot 44, Esquimalt District, Plan 3135]

and 1326 Miles Street

[PID 006-375-723 Lot 96, Suburban Lot 44, Esquimalt District, Plan 2854]

[PID 006-375-693 Lot 85, Suburban Lot 44, Esquimalt District, Plan 2854]

Purpose of the Application:

The applicant is requesting a change in Official Community Plan Proposed Land Use Designation from the current designation of High Density Residential to Commercial/Commercial Mixed-Use, and a change in zoning from the current RM-1 [Multiple Family Residential] to a Comprehensive Development District zone [CD]. The change in Official Community Plan Proposed Land Use Designation is required to accommodate the commercial uses proposed. The change in zoning is required to accommodate the proposed 12 storey, commercial mixed use, multiple family residential building including 109 residential units and a 111 space parking garage.

Evaluation of this application should focus on issues related to zoning such as the proposed height, density, massing, proposed unit sizes, siting, setbacks, lot coverage, usable open space, parking, uses, fit with the neighbourhood, and consistency with the overall direction contained within the Official Community Plan.

This site is located within Development Permit Area No. 1 - Natural Environment, No. 6 - Multi-Family Residential, No. 7 - Energy Conservation and Greenhouse Gas Reduction and No. 8 - Water Conservation of the Township’s Official Community Plan. Should the rezoning be approved, the form and character of the buildings, landscaping, and consistency with guidelines relating to natural environment protection, energy conservation, greenhouse gas reduction, and water conservation would be controlled by a Development Permit that would be considered by Council at a future date.

Recommendation:

That the Esquimalt Advisory Planning Commission recommends that the application for an Official Community Plan Amendment and Rezoning, authorizing a 45 metre [12 storeys], 109 unit, commercial mixed use, multiple family residential building, sited in accordance with the BCLS Site Plan provided by McElhanney Consulting Services, Ltd., stamped “Received March 12, 2019”, and incorporating height and massing consistent with the architectural plans provided by DIALOG, stamped “Received March 12, 2019”, detailing the development proposed to be located at 616 Constance Avenue [PID 000-713-465 Lot 95, Suburban Lot 44, Esquimalt District, Plan 2854], 620 Constance Avenue [PID 000-819-832 Lot 4, Suburban Lot 44, Esquimalt District, Plan 3135], 619 Nelson Street [PID 006-393-608 Lot 84, Suburban Lots 44 and 45, Esquimalt District, Plan 2854], 623 Nelson Street [PID 006-278-647 Lot 3, Suburban Lot 44, Esquimalt District, Plan 3135], and 1326 Miles Street [PID 006-375-723 Lot 96, Suburban Lot 44, Esquimalt District, Plan 2854] + [PID 006-375-693 Lot 85, Suburban Lot 44, Esquimalt District, Plan 2854] be forwarded to Council with a recommendation **to either approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

VI. REVIEW OF DRAFT STRATEGIC PRIORITIES AND GOALS WITH OPERATINOAL STRATEGIES 2019 – 2023

VII. NEXT REGULAR MEETING

Tuesday, April 16, 2019

VIII. ADJOURNMENT



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

**ADVISORY PLANNING COMMISSION
MINUTES OF FEBRUARY 19, 2019
COUNCIL CHAMBERS, MUNICIPAL HALL**

PRESENT: Graeme Dempster Duncan Cavens
Michael Angrove Helen Edley
Fil Ferri Marie Fidoe
Chris Munkacsi

STAFF: Bill Brown, Director of Development Services, Staff Liaison
Karen Hay, Planner
Alex Tang, Planner
Pearl Barnard, Recording Secretary

COUNCIL LIAISON: Councillor Jacob Helliwell

I. CALL TO ORDER

Graeme Dempster, Chair, called the Advisory Planning Commission meeting to order at 7:01 p.m.

II. ELECTION OF CHAIR

Nominated by Duncan Cavens and seconded by Marie Fidoe: That Graeme Dempster assume the role of Chair of the Design Review Committee. **Carried Unanimously**

III. ELECTION OF VICE CHAIR

Nominated by Duncan Cavens and seconded by Graeme Dempster: That Mike Angrove assume the role of Vice Chair of the Design Review Committee. **Carried Unanimously**

IV. LATE ITEMS

Pertaining to Agenda Item VII. **STAFF REPORTS 3) Development Variance Permit – 622 Admirals**

- Revised Site Plan

V. APPROVAL OF THE AGENDA

Moved by Fil Ferri, seconded by Mike Angrove: That the agenda be approved as circulated. **Carried Unanimously**

VI. ADOPTION OF MINUTES

Moved by Helen Edley seconded by Fil Ferri: That the minutes of December 18, 2018 be adopted as circulated. **Carried Unanimously**

VII. STAFF REPORTS

**1) Development Permit Application
1109 Lyall Street**

Rus Collins, Zebra Design and Phil Large, Large & Co., provided an overview of the Development Permit Application for 1109 Lyall Street, and responded to questions from the Committee. Megan Walker, LADR Landscape Architects spoke to the landscape plan.

Commission member comments and questions included:

- Driveway material was discussed. Concerns with loose gravel, but like its permeability
- Less off street parking spaces to be provided in future, one to one ratio preferred

RECOMMENDATION:

Moved by Duncan Cavens, seconded by Marie Fidoe: That the application for a Development Permit authorizing the form and character of the proposed development of a new two-family dwelling (duplex) to be constructed behind the existing single family dwelling, sited in accordance with the BCLS Site Plan prepared by Alan Powell, Powell & Associates, BC Land Surveyors, is consistent with the architectural plans prepared by Zebra Design, and the landscape plan prepared by LADR Landscape Architects, all stamped "Received December 17, 2018" **be forwarded to Council with a recommendation by the Esquimalt Advisory Planning Commission to approve;** as the proposal addresses concerns from a previous APC meeting and is a good addition to the neighbourhood. **Carried Unanimously**

2) REZONING APPLICATION

1158 Craigflower Road

Rus Collins, Zebra Design, provided an overview of the Rezoning Application for 1158 Craigflower Road.

Commission comments and questions included (*response in italics*):

- Why is the front yard setback so large? *For the driveway turnaround to work and also there is a right of way along the front of the property. In the process of discussing with the neighbour to eliminate the right of way. It would be great if the right of way was eliminated in order to site the house closer to the front lot line.*
- Like the option to suite the basement
- Are there any windows in the basement? *Yes, there are window wells and a rear sliding door.*

RECOMMENDATION:

Moved by Helen Edley, seconded by Fil Ferri: That the rezoning application, authorizing two new single family dwellings sited in accordance with the BCLS Site Plan prepared by Powell & Associates, BC Land Surveyors, stamped "Received February 4, 2019", and incorporating height and massing consistent with the architectural plans provided by Zebra Design, stamped "Received January 18, 2019", detailing the proposed development at 1158 Craigflower Road [PID 000-284-025 Lot 1, Section 2, Esquimalt District, Plan 5766], **be forwarded to Council with a recommendation by the Esquimalt Advisory Planning Commission to approve;** as the proposal is in keeping with the style of the new developments in the Township, and it is consistent with the proposed land use designation of low density residential.

Moved by Mike Angrove, seconded by Duncan Cravens: That the Main Motion be amended to add to the following condition for recommendation, "that the zone be written as if the right of way does not exist". **Carried Unanimously.**

The vote was taken on the Main Motion as amended and declared **Carried Unanimously.**

3) DEVELOPMENT VARIANCE PERMIT

622 Admirals Road

Bill Brown, provided an overview of the Development Variance Application for 622 Admirals Road, and responded to questions from the Committee.

Commission comments and questions included:

- The Committee questioned whether this application could be considered a hardship and processed through a Board of Variance application instead.

RECOMMENDATION:

Moved by Chris Munkacsi, seconded by Helen Edley: That the application for a Development Variance Permit, for the following variances to Zoning Bylaw 1992, No. 2050, as shown on the BC Land Surveyors Site Plan prepared by Powell & Associates BC Land Surveyors and Certified Correct on December 4, 2018 for the property located at 622 Admirals Road and legally described as Lot A, Suburban Lot 43, Esquimalt District, Plan EPP82555 [PID 030-615-992], **be forwarded to Council with a recommendation by the Esquimalt Advisory Planning Commission to approve;** given that Miles Street is considered the building frontage; hence, the variances are required to retain the existing structure as intended. **Carried Unanimously**

- 1) **Zoning Bylaw, 1992, No. 2050 Section 67.69 (8) (a) (ii) – Siting Requirements – Principal Buildings.** A 1.4 m decrease to the requirement that no principal buildings shall be located within 2.8 m of the northern Interior Side Lot Line [i.e. that the setback from the northern Interior Side Lot Line be reduced from 2.8 m to 1.4 m] in order to accommodate the balconies on the fourth to tenth floors inclusively;
- 2) **Zoning Bylaw, 1992, No. 2050 Section 67.69 (8) (a) (ii) – Siting Requirements – Principal Buildings.** A 0.5 m decrease to the requirement that no principal buildings shall be located within 2.8 m of the northern Interior Side Lot Line [i.e. that the setback from the northern Interior Side Lot Line be reduced from 2.8 m to 2.3 m] to accommodate the vertical fins on the eleventh floor;
- 3) **Zoning Bylaw, 1992, No. 2050 Section 67.69 (8) (a) (ii) – Siting Requirements – Principal Buildings.** A 0.7 m decrease to the requirement that no principal buildings shall be located within 2.8 m of the northern Interior Side Lot Line [i.e. that the setback from the northern Interior Side Lot Line be reduced from 2.8 m to 2.1 m] to accommodate the eaves and cornices;
- 4) **Zoning Bylaw, 1992, No. 2050 Section 67.69 (8) (a) (iv) Siting Requirements – Principal Buildings.** A 4.2 m reduction in the rear yard Setback (being the Setback from Admirals Road) [i.e. that the rear yard Setback be reduced from 5.5 m to 1.3 m] to accommodate those portions of the building, including balconies on floors four to ten inclusively, that encroach into the setback;
- 5) **Zoning Bylaw, 1992, No. 2050 Section 67.69 (8) (a) (iv) Siting Requirements – Principal Buildings.** A 3.3 m reduction in the rear yard Setback (being the Setback from Admirals Road) [i.e. that the rear yard Setback be reduced from 5.5 m to 2.2 m] to accommodate the vertical fins on the eleventh floor;
- 6) **Zoning Bylaw, 1992, No. 2050 Section 67.69 (8) (a) (iv) Siting Requirements – Principal Buildings.** A 3.5 m reduction in the rear yard Setback (being the Setback from Admirals Road) [i.e. that the rear yard Setback be reduced from 5.5 m to 2.0 m] to accommodate the eaves and cornices; and
- 7) **Zoning Bylaw, 1992, No. 2050 Section 67.69 (8) (a) (iv) Siting Requirements – Principal Buildings.** A 2.8 m reduction in the rear yard Setback (being the Setback from Admirals Road) [i.e. the rear yard Setback be reduced from 5.5 m to 2.7 m] to accommodate that portion of the building containing ground floor commercial space and the second floor roof deck above this space].

VIII. REVIEW OF DRAFT GREEN BUILDING CHECKLIST

Commission comments included:

- Retain previous version's option of Yes, No, N/A, instead of please check.

- Suggestion was to eliminate N/A and leave just Yes and No as options. N/A tends to get used instead of No
- Recommend an accompanying supplementary letter to justify why No or N/A was chosen
- Prioritize the guidelines

IX. REVIEW OF CAPITAL REGIONAL LOCAL GOVERNMENT ELECTRIC VEHICLE AND ELECTRIC BIKE INFRASTRUCTURE PLANNING PROJECT

Commission comments included:

- Passive encouragement without active Municipal participation
- Focus on safety of electric bikes as highlighted in the report
- Discussion of whether the provision of electric vehicle charging infrastructure should be public or private
- Concerns that monetary cost associated with EV charging could deter local business economic development

X. NEXT REGULAR MEETING

Tuesday, March 19, 2019

XI. ADJOURNMENT

The meeting adjourned 8:15 p.m.

CERTIFIED CORRECT

CHAIR, ADVISORY PLANNING COMMISSION
THIS 19th DAY OF MARCH 2019

ANJA NURVO, CORPORATE OFFICER



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

APC Meeting: March 19, 2019

STAFF REPORT

DATE: March 13, 2019

TO: Chair and Members of the Advisory Planning Commission

FROM: Fred Billingham, Planning Technician
Bill Brown, Director of Development Services

**SUBJECT: DEVELOPMENT PERMIT and DEVELOPMENT VARIANCE PERMIT
1129 Munro Street
[PID 026-958-376, Lot 2, Section 11, Esquimalt District, Plan VIP82523]**

RECOMMENDATION:

That the Esquimalt Advisory Planning Commission [APC] recommends to Council that the application for a Development Permit and a Development Variance Permit allowing construction of a new accessory building as illustrated in the architectural drawings and landscape plan prepared by Jeffrey A. Duffield, stamped "Received September 27, 2018", sited as detailed on the survey plan prepared by Ryan P. Hourston BCLS, stamped "Received October 4, 2018", and including the following variances for the property located at PID 026-958-376, Lot 2, Section 11, Esquimalt District, Plan VIP82523 [1129 Munro Street]] **to either approve, or deny the application; including the reasons for the recommendation.**

Zoning Bylaw, 1992, No. 2050, Section 34 (9)(b)(i) – Siting Requirements – Accessory Building – Front Setback: Exemption from the requirement that no Accessory Building shall be located in front of the front face of the Principal Building

Parking Bylaw No. 1992, No. 2011, Part 4 (9)(4) – Provisions and Maintenance of Off-Street Parking and Loading Areas: Exemption to the requirement that Parking Spaces in Residential zones be located no closer to the Front Lot Line than the front face of the Principal Building [i.e. the one required parking space will be located in the new accessory building].

BACKGROUND:

Purpose of the Application

The applicant is proposing to construct a new detached garage. 1129 Munro Street is the original 80 year old building on the lot, which has previously been subdivided creating 2 further residential properties 325 and 327 Kinver Street. As such the only remaining space within the lot in question accessible by vehicles is to the front of the property. Therefore, the applicant has proposed to site the detached garage in front of the principal building. This is contrary to the zoning bylaw for RS-1, which requires any accessory buildings to be situated behind any

primary buildings. As such, a Development Permit and Development Variance Permit are required.

The subject property falls under the following Development Permit Areas:

- Development Permit Area 1: Natural Environment,
- Development Permit Area 3: Enhanced Design Control Residential,
- Development Permit Area 7: Energy Conservation & Greenhouse Gas Reduction
- Development Permit Area 8: Water Conservation and as such this application requires a Development Permit.

Context

Applicant: Jeffrey A. Duffield

Owners: Jeffrey A. Duffield & Uta Gewalt

Property Size: Metric: 567 m² Imperial: 6103 ft²

Existing Land Use: Single Family Dwelling

Surrounding Land Uses:

North: Single Family Residential

South: Single Family Residential

West: Single Family Residential

East: Two Family Residential

Existing Zoning: RS-1 [Single Family Residential]

Development Permit Guidelines

- The Official Community Plan Development Permit Area No.1: Natural Environment Guidelines (attached) are intended to provide protection of the natural environment, its ecosystems and biological diversity.
 - The applicant has met Guideline 18.5.1 by not locating the structure within 10m of the high watermark of the Strait of Juan de Fuca.
 - As per Guideline 18.5.2, existing native trees, vegetation, soil and topography are being retained wherever possible. The location of the proposed accessory building will not adversely affect any public views.
 - Amendments to the existing landscape plan for the site have been provided, to conform with Guidelines 18.5.2, 18.5.3 and 18.5.4.
- The development location is within an area subject to Development Permit Area No.3: Enhanced Design Control Residential. These Guidelines (attached) are intended to establish objectives for the form and character of intensive residential development. The proposal is not for additional infill housing, but to construct an accessory building to an existing dwelling.
 - As per Guideline 20.6.2, the new structure is to be a single storey and is in keeping with the single unit residences in the immediate area. It should also be noted that the materials that are to be used for the accessory building have been chosen so that they will match those of the existing principal building.
 - To conform to Guideline 20.6.4, a landscape plan for the site has been included

as part of the submission.

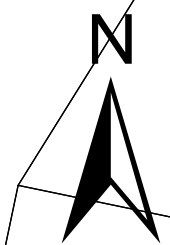
- Development Permit Area No.7: Energy Conservation & Greenhouse Gas Reduction is intended to aid the reduction of greenhouse gas emissions within the municipality, by reducing energy consumption in buildings. Any proposal that will increase Lot Coverage by the less than 5% of the parcel or 50m², whichever is smaller, is exempt from the guidelines of this DPA. The proposed accessory building will increase the lot coverage by 24.56m² or 4% of the parcel of land, and as such is exempt.
- The proposed accessory building will increase the lot coverage by 24.56m², or 4% of the total parcel, and as such is exempt from the requirements of Development Permit Area No.8: Water Conservation, as the resultant increase in Lot Coverage will be less than 50m² or 5% of the parcel.

Public Notification

As this application includes a Development Variance Permit application, should it proceed to Council, a notice will be mailed to tenants and owners of properties within 50 metres (164 feet) of the subject property.

ALTERNATIVES:

1. Forward the application for a Development Permit and Development Variance Permit to Council with a **recommendation of approval.**
2. Forward the application for a Development Permit and Development Variance Permit to Council with a **recommendation of denial.**



Subject Property Map
1129 Munro Street



Kinver Street

Munro Street

18 DPA NO. 1: NATURAL ENVIRONMENT



18.1 Area

Land within the municipal boundaries of the Corporation of the Township of Esquimalt.

18.2 Designation

Development Permit Area No. 1 is designated for the purpose of establishing objectives for:

- Section 488 (1) (a) – protection of the natural environment, its ecosystems and biological diversity.

18.3 Justification

- The gradual erosion of the natural environment incrementally degrades the value and function of local and regional ecosystem services. The average person owns a parcel of land for a relatively short time period of time (10-15 years); care can be taken to ensure larger trees and natural areas are available for the next generation.
- Landscapes and ecosystems are composed of patches of diverse habitats that may include 'species at risk', 'environmentally sensitive areas', and micro-ecosystems containing 'ecological memory'. Even small patches of native soil and vegetation support indigenous species; therefore, all areas can contribute to regional biodiversity and support ecosystem services.
- Protecting Esquimalt's natural environmental features [including but not limited to: local Garry Oak and Douglas-fir ecosystems, rock outcrops, hilly terrain, and rugged clean shorelines] contributes to natural ecosystem functioning and protection of biodiversity.
- Esquimalt has over 20 kilometres of shoreline that serves industrial, commercial and residential purposes, recreation enjoyment, and is vital habitat for numerous plants and animals (e.g. otters, whales, seal, oyster catchers, gulls, various species of waterfowl, and eagles). Shoreline ecology and fish habitat can be protected and enhanced by regulating development near shorelines, and by mitigating the impacts of stormwater entering local waterways. Keeping the urban environment absorbent helps lessen marine ecosystem damage from: introduced pollutants, sudden changes in water salinity and temperature, and shoreline erosion from surges in volume at storm-water pipe outfalls.

- The Gorge waterway is a sensitive, tidal-influenced watercourse that connects the fish-bearing fresh water of Craigflower and Colquitz Creeks with the salt waters of Victoria Harbour. Over the past few decades, significant public expenditures and efforts have gone into removing sources of pollution and contamination that once plagued this waterway. However, the removal of native shoreline vegetation and the construction of extensive seawalls have substantially diminished the quality of the shoreline as supportive habitat for fish and wildlife.
- Both private and public landowners are responsible for this loss of habitat, often through well meaning and beneficial projects such as shoreline walkways.
- Protecting and maintaining current wave energy patterns and natural patterns of erosion along Esquimalt's shorelines will contribute to the protection of natural features and dynamic processes.
- Invasive alien plant species pose a significant threat to regional biodiversity. Managing and reducing the introduction and spread of these species protects local ecosystem structure and function and biodiversity.
- Esquimalt has several natural area parks of varying sizes distributed across the municipality. These pieces of fragmented natural habitat can be better connected through the use of native plant landscaping along roadways and in private yards; thereby supporting regional biodiversity.
- Biodiversity can be enhanced in an urban region through the thoughtful selection of building methods and landscape design. Siting buildings to provide space for trees of varying species and sizes provides vertical habitat for birds, pollinators and other creatures. In addition, the plants of a traditional Garry Oak meadow ecosystem are well adapted for the seasonally dry conditions found on local building roofs, and therefore can be effectively used in a 'living roof' system (a green roof with enhanced ecosystem services). A living roof will moderate stormwater discharge while providing habitat for indigenous plants, invertebrates, and ground feeding and nesting birds.
- Native birds are an important component of urban biodiversity and provide important ecosystem services that contribute to human health. Esquimalt is located within the 'Pacific Flyway' (A chain of habitats used by at least one billion birds biannually as their migratory route along the west coast of North and South America, from Alaska to Patagonia.) and much of Esquimalt's shoreline is part of the 'Victoria Harbour Migratory Bird Sanctuary'. Reasonable actions can be taken to enhance bird habitat in an urban setting; including providing vertical habitat through protection and enhancement of the urban forest, and protecting local shorelines and waterways.
- Habitat corridors for pollinators and other wildlife will be enhanced; linking natural areas within and through the urban matrix to support biodiversity and local food gardens

18.4 Exemptions

18.4.1 Properties

For all properties:

1. Interior renovations or alterations of existing buildings where residential density is not being increased.
2. Ecological restoration projects undertaken or approved by the Township of Esquimalt.
3. Installation of unpaved paths or walking trails that are less than 1 m in width and covered in naturally permeable materials [wood chips, bark mulch, sand or loose gravel] where the soil remains undisturbed.

18.4.2 Gorge Waterway

For all lands located more than 7.5 m from the high watermark of the Gorge Waterway:

1. Repair, maintenance or reconstruction, on the same footprint, of existing legal or legally non-conforming buildings, patios, driveways, parking areas and utilities, provided there is no alteration to natural soil or native vegetation.
2. Construction of fencing where no native trees are removed and disturbance to native vegetation is negligible.
3. The addition of small temporary landscape amenities including benches, tables, garden ornaments, playground equipment, and raised garden beds (not including retaining walls).



18.4.3 High Watermark

For lands located more than 20 m from the high watermark of the Gorge Waterway, and more than 15 m from the high watermark of the Strait of Juan de Fuca:

1. Minor additions [less than 10 m² in area] to an existing legal or legally non-conforming building or structure.
2. Construction of buildings and structures less than 10 m² in area.
3. Installation of seasonal recreation equipment such as children's play equipment, patio furniture, temporary above natural ground level pools/hot tubs.
4. Temporary tent/carport structures.

18.5 Guidelines

The expertise of qualified environmental professionals (retained by applicants), is strongly encouraged and may be required in certain circumstances.

18.5.1 Lands Free of Development

Lands to remain free of development, with conditions:

1. Lands within 7.5 m of the high watermark of the Gorge Waterway shall be retained in as natural a state as possible. Where the land has been previously altered, the area shall be restored with native trees and plants.
2. New buildings/ structures shall not be located within 20 m of the high watermark of the Gorge Waterway.
3. New buildings/ structures shall not be located within 10 m the high watermark of the Strait of Juan de Fuca.
4. Replacement of, expansion of, densification and intensification of the use of existing buildings within 20 m of the high watermark of the Gorge Waterway is discouraged; detached accessory dwelling units are strongly discouraged in this location.
5. Replacement of, expansion of, densification and intensification of the use of existing buildings within 10 m of the high watermark of the Strait of Juan de Fuca is discouraged and detached accessory dwelling units are strongly discouraged in this location.
6. Variances to 'Building Height' and 'Siting Requirements' will be considered where natural areas and trees are being protected.
7. Consider the use of conservation covenants for areas having high ecosystem conservation values. Property owners are encouraged to work with local land trusts to protect natural features and valuable habitat areas through land covenants.

18.5.2 Natural Features

Natural features and areas to be preserved, protected, restored, and enhanced where feasible:

1. Retain existing healthy native trees, vegetation, rock outcrops and soil wherever possible.
2. Preserve and enhance native tree and shrub clusters that overhang the waters edge as these provide shade, protection and feeding habitat for fish and wildlife.
3. Preservation of natural topography is favoured over blasting or building of retaining walls.
4. Narrower manoeuvring aisles, fewer and smaller parking spaces can be considered where natural areas are being conserved.
5. Design new development and landscaping to frame rather than block public views.
6. Avoid disturbing, compacting and removing areas of natural soil as this can lead to invasion by unwanted plant species, poor water absorption and poor establishment of new plantings. Use of local natural soil in disturbed and restored areas will support re-establishment of ecosystem functions.

18.5.3 Biodiversity

Landscaping features that will protect, restore and enhance biodiversity. Where feasible:

1. New landscaping shall consist predominantly of native plant and tree species. Plants that are native to the Coastal Douglas-fir biogeoclimatic zone are preferred in landscape treatments as they provide habitat for threatened indigenous flora and fauna. Drought tolerant plants native to western North America, that are known to be non-invasive, are a good alternative choice for landscaped areas.
2. In residential locations plan for 'nature out front'; for new landscaping in front and exterior side yards use a variety of site-appropriate, native species; thereby contributing positively to pedestrian friendly urban streets, future greenways and habitat enhanced corridors.
3. Choose trees and plants for site conditions; consider shade, sunlight, heat, wind-exposure, sea spray tolerance, and year round moisture requirements in their placement.
4. Consider the habitat and food needs of birds, pollinators, and humans in tree and plant species selection and placement; native plantings and food gardens compliment each other.
5. Encourage native plant and food gardens to spill from private land into boulevards.
6. Avoid monoculture plantings, especially expanses of turf grass outside of playing field sites.
7. Snags, logs, driftwood and rock cairns may be used as interesting landscaping features that also provide habitat for native flora and fauna.
8. Avoid using fast-growing non-native plants to cover and retain soils as they may become invasive and a constraint to the establishment of other plants.
9. Locate civil servicing pipes/lines under driveways or other paved areas to minimize tree root damage. (Note that the majority of trees have their roots in the top 0.6 m of the soil).
10. Design retaining wall spacing and landscape planting areas of sufficient width and depth to support plantings (eg. provide larger spaces for trees).
11. Support the daylighting of portions of the stormwater system for enhanced habitat.
12. Aim to meet the Canadian Landscape Standards in all landscaping installations.

18.5.4 Natural Environment

Measures to protect, restore and enhance the natural environment (limit noise, light and air pollution). Where it is reasonable:

1. Strategically locate leafy trees/ hedges and water features to mask urban noises such as traffic, garbage collection and delivery locations. Consider that leafy rough barked trees, vine covered walls and natural ground cover materials (mulch, soil) will help dampen urban noise.
2. Use International Dark-Sky Association approved lighting fixtures in outdoor locations. Outdoor lighting shall be no brighter than necessary, be fully shielded (directed downward and designed to serve pedestrian needs), have minimal blue light emissions and only be on when needed. Avoid vanity lighting, and lighting directed into the night sky and trees tops.
3. Light spillage on to waterways is strongly discouraged.
4. Place trees and vegetation near sources of air pollution including busy roadways, to assist in reduction of air pollution through the collection of particulate matter on leaves and needles, and absorption of toxic gases, including but not limited to: ozone, nitrogen dioxide, sulfur dioxide, carbon monoxide, carbon dioxide, cadmium, chromium, nickel and lead.

18.5.5 Drainage and Erosion

Measures to control drainage and shoreline erosion. Where it is reasonable:

1. Preserve, restore and enhance treed areas. Trees are the most effective form of absorbent landscaping due to their extensive root zones and their ability to both absorb water from the soil and intercept precipitation on leaves, needles and branches. Consider that native conifers are well adapted to local wet winters.
2. Reduce the impact of surges in stormwater on shorelines by designing on-site stormwater retention systems to contain the first 3 centimetres [1.25 inches] of precipitation on site, per precipitation event; and incorporating rainwater collection systems into roof design and landscaping.
3. Consider using shared private/ public rain gardens. Direct a portion of stormwater to adjacent public open spaces, when deemed appropriate by the Director of Engineering and Public Works.
4. Maximize the ratio of planted and pervious surfaces to unplanted surfaces, and design paved areas to direct water towards vegetated areas, to help reduce surface run off. Where paved surfaces are needed, intersperse with drought resistant vegetation and trees, to help absorb stormwater, provide shade and reduce the local heat island effect.
5. Use porous surfaces to enhance stormwater infiltration, permeable paving is preferable for all open air parking areas. Ensure installation methods contribute to sustained permeability and retention of stormwater on the site.
6. Choose absorbent landscaping materials; leaf mulches, wood chips and good quality top soil, over gravel, pavers and concrete. Provide mulch of organic, locally derived materials; leaf mulch from local tree leaves is most desirable.
7. Incorporation of rain gardens, bio-swales, rain barrels, and even small depressions (puddles) into landscaping will help reduce surges of stormwater entering local waterways.
8. Planting densities should ensure that vegetated areas will have near 100% plant coverage after two full growing seasons.

18.5.6 Protect, Restore and Enhance Shorelines

Measures to protect, restore and enhance local shorelines (reducing shoreline hardening and dock development). When it is feasible:

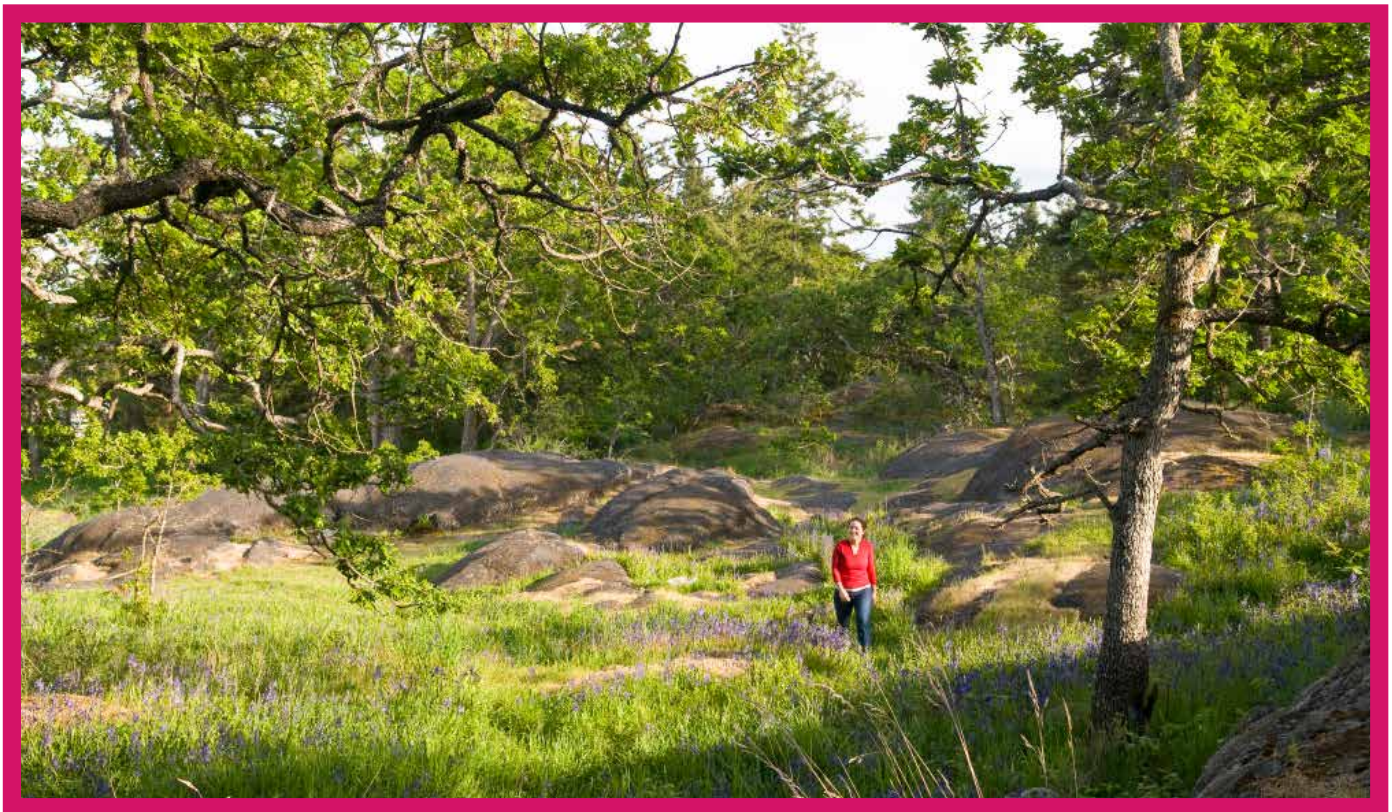
1. Waterfront property owners are encouraged to become familiar with and adopt a 'soft shore' restoration approach to the care of their foreshore property (i.e. Green Shores for Homes).
2. Avoid the expansion of dock area, bulkheads, groins or other shoreline hardening structures. Removal or reductions in the surface area of existing private docks is encouraged.
3. Where shoring methods are required to prevent erosion or the sloughing of the shoreline, choose bio-engineering methods over the use of sea-walls or retaining walls. Where sea-walls or retaining walls are the only means of effectively preventing erosion, design in consultation with qualified environmental professionals, as well as engineering professionals.

18.5.7 Native Bird Biodiversity

Measures to protect, restore and enhance native bird biodiversity. Where it is reasonable:

1. Protect and enhance habitat features like mature trees, shrub clusters, native fruit bearing shrubs, fresh water ponds and ephemeral damp areas (puddles).

2. Encourage increased front yard habitat along quieter streets to reduce bird vehicle conflicts and enhance the pedestrian experience through native plantings.
3. Sustain a mix of habitat types; including forest, shrub-land, meadow, riparian wetland and coastal shoreline ecosystems in landscaping.
4. Incorporate a vertical vegetation structure [vertical habitat] including layers of ground cover, shrub, understorey and canopy in landscape design.
5. Choose a range of native plant species and sizes; a mix of coniferous and deciduous trees will enhance bird species diversity.
6. Incorporate architectural features that limit collisions between birds and windows including patterned, frosted or tinted glass, exterior louvers, blinds, sun shades and canopies.
7. Cap and screen all ventilation pipes and grates, avoid openings greater than 2.0 x 2.0 cm.



20 DPA NO. 3: ENHANCED DESIGN CONTROL RESIDENTIAL



20.1 Area

All lands zoned for two-unit dwellings or zoned as Comprehensive Development Districts for residential developments only are designated as part of Development Permit Area No. 3 — Enhanced Design Control — Residential as shown on “Development Permit Areas Map” (Schedule “H”) of this Plan.

20.2 Designation

Development Permit Area No. 3 is designated for the purpose of establishing objectives for:

- Section 488(1) (e) – establishment of objectives for the form and character of intensive residential development.

20.3 Justification

The following guidelines were developed to allow for the better utilization and redevelopment of parcels within residential neighbourhoods and ensure that development occurs in a manner that retains the desirable physical characteristics of a neighbourhood.

Infill housing represents an important component of the housing spectrum. Because infill housing represents new development that is located within the context of an existing residential neighbourhood, it is important that there is harmonious compatibility between the form and character of the new development and the existing development

20.4 Exemptions

The following do not require a development permit:

1. Additions or renovations to one single-unit dwelling (excluding infill units) situated on a fee simple parcel where the floor area of the addition does not exceed 10 percent of the ground floor area of the dwelling; and
2. Additions or renovations to any two-unit dwelling where the floor area of the addition does not exceed 10 percent of the ground floor area of the dwelling.

20.5 Guidelines - Duplex Housing

1. The fronts of the buildings should be designed to create the appearance of smaller structures either by staggering the dwelling units or visually breaking up the façade with architectural detailing while still maintaining a complementary streetscape.
2. Innovative and creative site-specific two-unit dwellings are encouraged where usable open space is maintained either on the ground (yard) or as rooftop gardens. Setbacks to the street may be reduced to maximize property use.
3. Front to back duplexes are generally discouraged unless they can be designed to eliminate negative impacts to the immediate neighbours such as shading of gardens, overlook of outdoor amenity areas and violation of privacy.
4. Side by side, up and down, or staggered unit configurations are preferred as these result in a greater number of units facing the street, less disruption of privacy, and a more equitable division of outdoor amenity areas between the two dwelling units.
5. The use of exterior building materials similar to those used in older residential neighbourhoods (i.e. combinations of wood, brick, stucco, and stone) is encouraged.
6. Rooflines of new development should relate in height, shape and pitch to existing residential buildings in the immediate area. For corner sites, the building design should avoid having large unbroken sloped roof areas facing the street.
7. To create interest in the façade of the buildings facing the street, the incorporation of architectural elements such as bay windows, covered porches, verandas and prominent front doors is encouraged.
8. Buildings should be designed to minimize visual intrusion on to the privacy of surrounding homes. Some overlook of adjoining yards and neighbouring decks may be unavoidable; however, additional privacy should be achieved by inseting balconies, decks and patios into the building or by screening them with latticework or landscaping. Windows should be spaced so that they do not align directly with those of other buildings.
9. The height and massing of new two-unit dwellings should be designed to minimize the casting of shadows on to the private outdoor space of adjacent residential dwellings
10. A landscaping plan showing ground cover areas, planting beds, shrubbery and trees (both existing and proposed) is required. Landscaping should add to the aesthetic appeal of the streetscape as well as provide privacy between dwelling units.
11. The provision of private open space should be part of an overall site development and landscape plan and should take into consideration general site circulation patterns (including parking), existing landscape features, sun access, privacy and usability.
12. Retention and protection of trees and the natural habitat is encouraged where possible.
13. Parking areas, garages and driveways should appear as a minor component of the site when viewed from the street. The building of curving access roads and driveways helps to avoid views from the street of large expanses of paving. The use of shared driveways is encouraged.
14. The use of permeable and decorative surfacing materials, such as brick, concrete pavers, textured concrete, coloured paving or grasscrete is encouraged in place of solid expanses of asphalt or concrete.

15. Where possible, hydro meters should not be placed on the front façade of a building and, if placed in a manner which is visible from the street, will be appropriately screened by the owner in a manner consistent with BC Hydro requirements.
16. Where an existing single-unit dwelling is being converted to a two-unit dwelling both the original structure and the addition shall be in complementary architectural style and constructed of complementary exterior finishes including roofing material, window treatments, exterior finishes, door styles and trims.
17. Roof styles and pitches of the original and new portions of the building must be complimentary.
18. For new or converted two unit dwellings, garages and parking areas are encouraged to be located in the rear yard. Shared driveways are preferred to access the rear yard.
19. Where two single lane driveways serve a two unit lot, landscaping features are encouraged between the driveways.

20.6 Guidelines - Single-unit Infill Housing

20.6.1 Relationship to Existing Houses

1. Where an existing single-unit residence is to be retained and a second residence placed on the parcel, the existing dwelling is to be upgraded and made to be complementary with the new construction.
2. Where two or more new separate dwellings are situated on a parcel or within a comprehensive development zone, the buildings shall be designed as part of a comprehensive scheme with all buildings being finished in complementary materials and incorporating complimentary architectural details.
3. Where new infill single houses are proposed, the design of the new houses should be complementary in scale, size, exterior finishes, rooflines, and colours to the predominant styles of housing in the neighbourhood. It is important to ensure that the new construction fits with the overall scale and character of existing houses.

The intent of this guideline is not to encourage the replication or imitation of surrounding buildings but rather the design of structures that complement the streetscape.

20.6.2 Massing

1. New structures should be designed so that the overall massing is in keeping with other single-unit residences in the immediate area. New structures for lots other than corner or double frontage lots should be limited to one and one half storeys.
2. New structures, which are two storeys in height, should be designed so that the second storey is partially concealed within the slope of the roof to minimize the height of the building. The use of dormers set into the roof is preferred to a flat roof or a peaked roof set over the second storey.

20.6.3 Privacy/Screening/Shadowing

1. Proposed infill dwellings should have only a minimal impact on adjacent homes and be separated from neighbouring residences by vegetation, screening, natural elevation differences, or a combination of these features.
2. Windows, decks and patios should be located so as to minimize intrusion on to the privacy of adjacent properties.
3. Infill dwellings should be sited to minimize the casting of shadows on to the private outdoor space of adjacent residential dwellings.

20.6.4 Landscaping

1. Proposals for single-unit infill housing must include a landscape plan showing hard landscaping (i.e., parking areas, fences, and patios) as well as lawns, trees, shrubs, planting areas and proposed plant species.
2. Retention and protection of trees and the natural habitat is encouraged wherever possible.

20.6.5 Private Open/Yard Space

1. Any proposal for single-unit infill housing should provide for usable, private outdoor areas for each dwelling, at grade.

DIVISION 1 - RESIDENTIAL ZONES

34. SINGLE FAMILY RESIDENTIAL [RS-1]

The intent of this Zone is to accommodate Single Family Dwellings on individual Parcels of land.

(1) **Permitted Uses**

The following Uses and no others are permitted:

- (a) Single Family Residential
- (b) Home Occupation
- (c) Secondary Suite: subject to the requirements of Section 30.6
- (d) Boarding: subject to the requirements of Section 30.3
- (e) Urban Hens: subject to the requirements of Section 30.4 of this bylaw.

(2) **Parcel Size**

The minimum Parcel Size for Parcels created by subdivision shall be 530.0 square metres.

(3) **Minimum Lot Width**

The minimum width of Parcels created by subdivision shall be 16 metres measured at the Front Building Line.

(4) **Floor Area Ratio**

The Floor Area Ratio shall not exceed 0.35.

(5) **Floor Area**

The minimum Floor Area for the First Storey of a Principal Building shall be 88 square metres.

(6) **Building Height**

- (a) No Principal Building shall exceed a Height of 7.3 metres.
- (b) No Accessory Building shall exceed a Height of 3.6 metres.

(7) **Building Width**

The minimum width for any Single Family Dwelling shall be 7 metres.

(8) **Lot Coverage**

- (a) All Principal Buildings, Accessory Buildings and Structures combined, shall not cover more than 30% of the Area of a Parcel.
- (b) All Accessory Buildings and Structures combined shall not exceed 10% of the Area of Parcel.

(9) **Siting Requirements**

(a) **Principal Building**

- (i) Front Setback: No Principal Building shall be located within 7.5 metres of the Front Lot Line.
- (ii) Side Setback: No Principal Building shall be located within 1.5 metres of an Interior Side Lot Line, with the total Setback of all Side Yards not to be less than 4.5 metres. In the case where a Parcel is not served by a rear lane, one (1) Side Yard shall not be less than 3 metres. In the case of a Corner Lot, no Principal Building shall be located within 3.6 metres of an Exterior Side Lot Line
- (iii) Rear Setback: No Principal Building shall be located within 7.5 metres of a Rear Lot Line.

(b) **Accessory Building**

- (i) Front Setback: No Accessory Building shall be located in front of the front face of the Principal Building.
- (ii) Side Setback: No Accessory Building shall be located within 1.5 metres of an Interior Side Lot Line nor 3.6 metres of an Exterior Side Lot Line.
- (iii) Rear Setback: No Accessory Building shall be located within 1.5 metres of a Rear Lot Line.
- (iv) Building Separation: No Accessory Building shall be located within 2.5 metres of a Principal Building.

(10) **Fencing**

Subject to Section 22, no fence shall exceed a Height of 1.2 metres in front of the front face of the Principal Building and 2 metres behind the front face of the Principal Building.

(11) **Off Street Parking**

Off street parking shall be provided in accordance with the requirements of Parking Bylaw, 1992, No. 2011 (as amended).

150mm

3.660 (12')



89x89 Corner Stud with 38x89 studs 305 o/c

(A)

22.3 m² (240 ft²)

OVERHEAD GARAGE DOOR
2134 H 2438 W
HEADER FRAMING PER
MANUFACTURER'S INSTRUCTIONS
OR CODE REQUIREMENTS.

CT:
⊕ duplex
receptical
in ceiling.

dbl stud at
window jamb (typ)

6.710 (22')

(B)

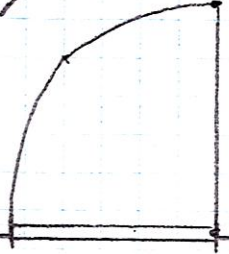
ROOF TRUSSES BY VICTORIA TRUSS



(D)

water line run from house for R/I
of sprinkler system control & Cedar
tree line watering

- 60 amp electrical panel.
- lighting & ground fault in house.
- (-) future electric car charging.

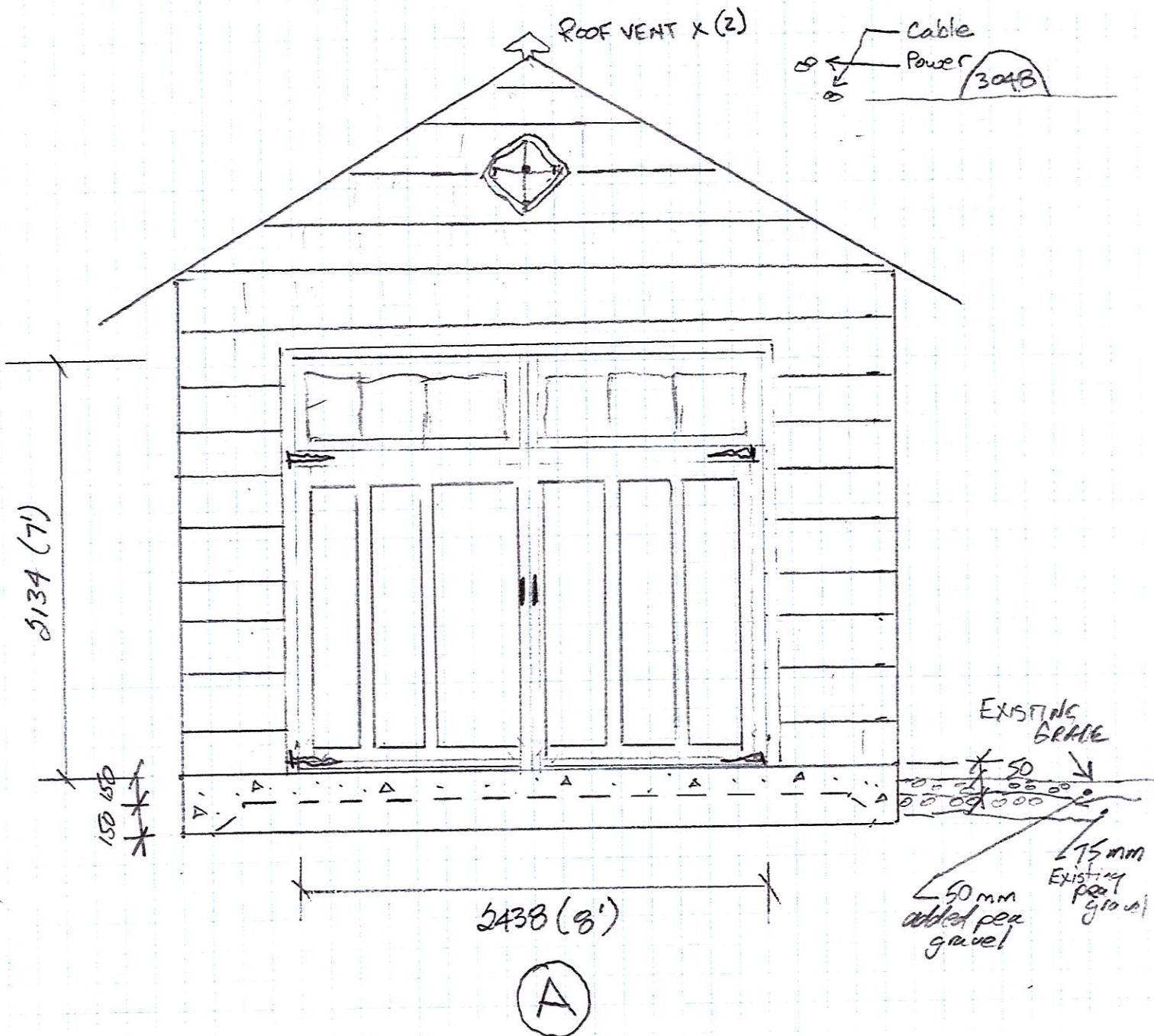


REPT 26, 2018

(C)

1129 MUNRO ST
ACCESSORY BUILDING
PLAN

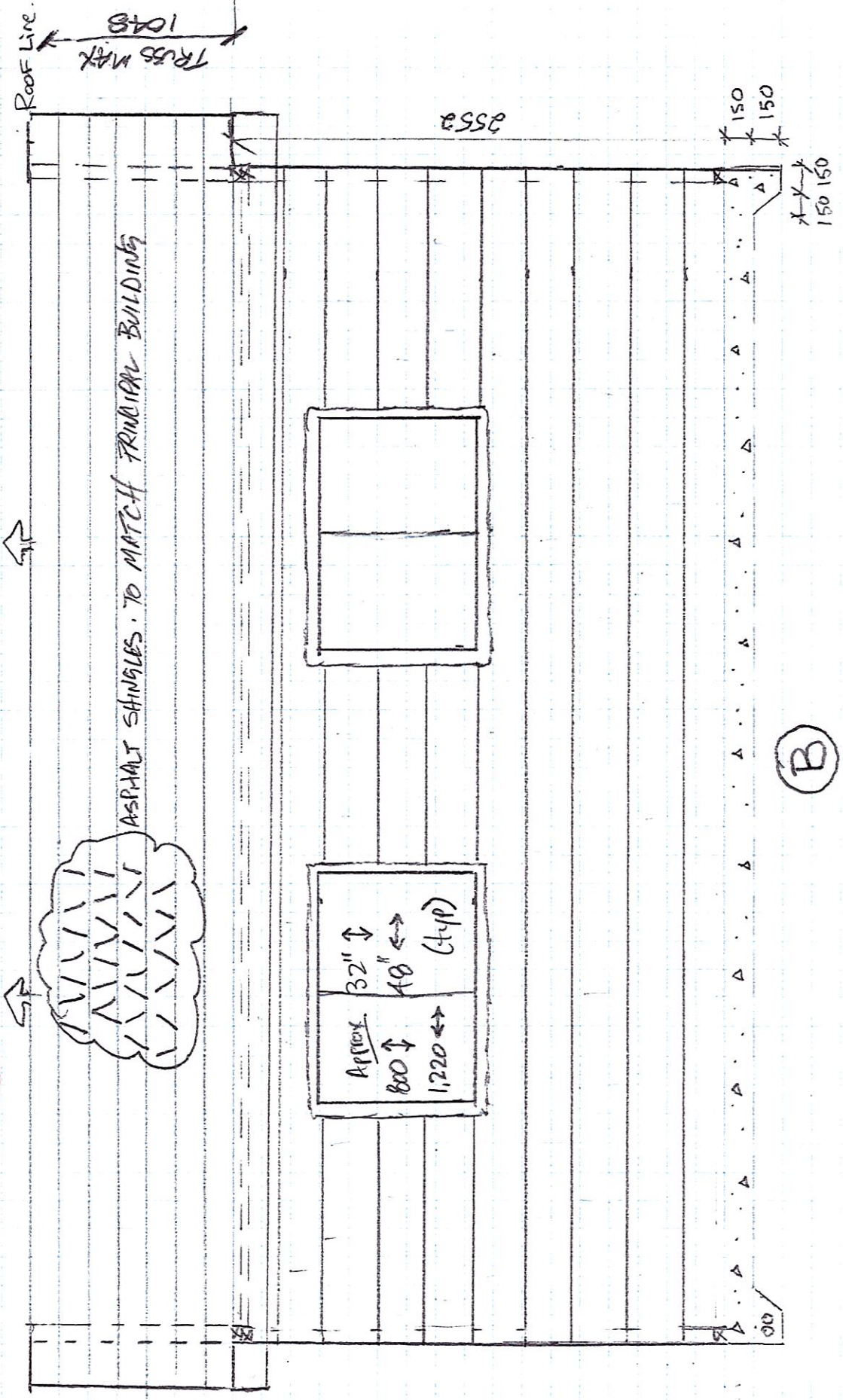
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(B)

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SEP 27 2018
CORP. OF TOWNSHIP
OF ESQUIMALT
DEVELOPMENT SERVICES

150 mm

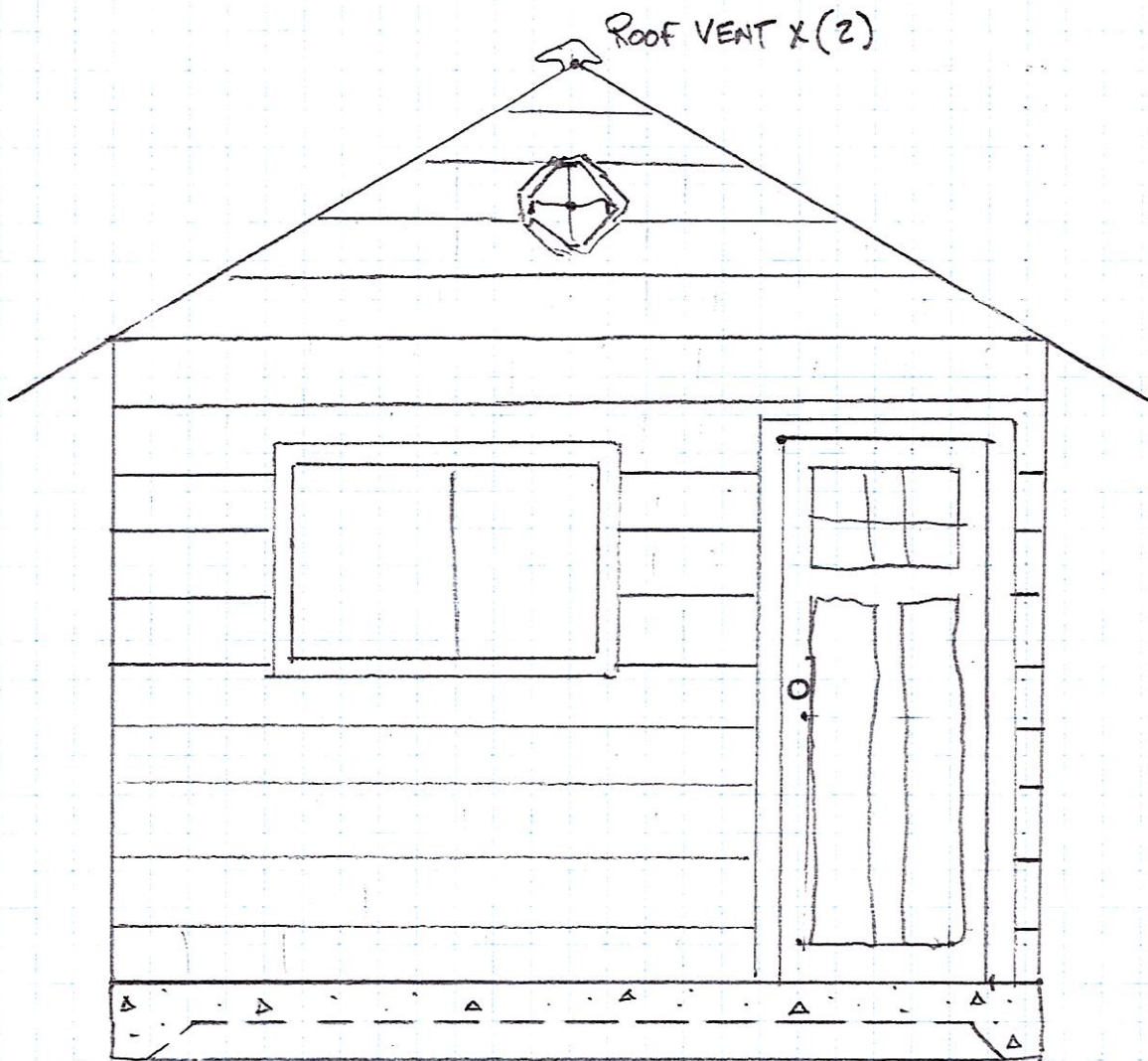


(2) 38489
TP

(1) 38489
BP

(B)

K
150MM



©

8'3 (32")

2550

1219 (4') to window sill
may vary to suit window or horizontal siding line architecture

1 1/2" CEDAR BOXED
SIDING SHINGLES
TO MATCH 80 YR OLD
PRIMARY BUILDING WITH
MITERED CORNERS.

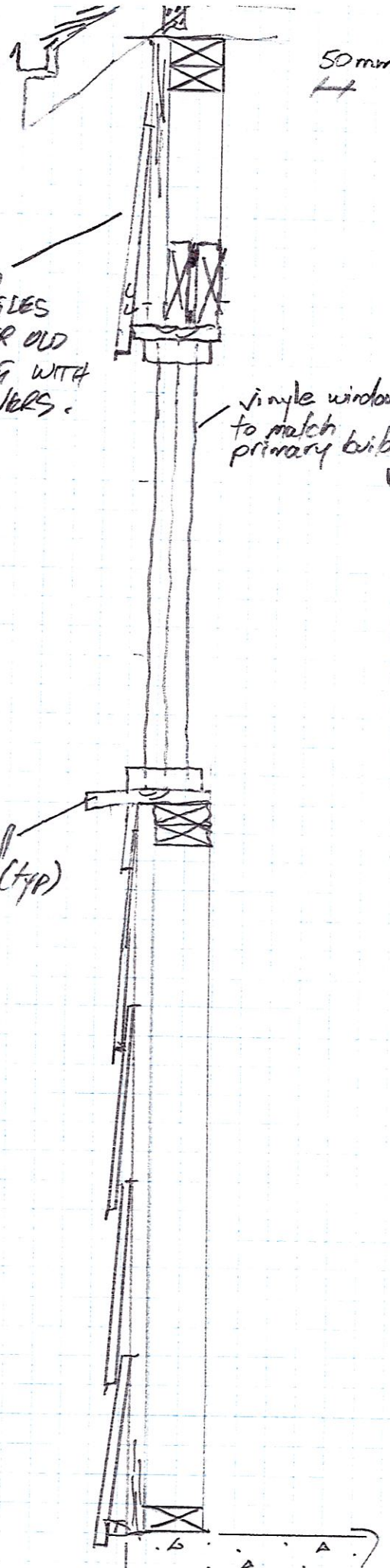
50mm
H

single windows
to match
primary building

RECEIVED
SEP 27 2018
CORP. OF TOWNSHIP
OF ESQUIMALT
DEVELOPMENT SERVICES

cedar sill
4 sides (typ)

(D)





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DEVELOPMENT SERVICES

PROPOSED ACCESSORY BUILDING
1129 Munro St.

Sketch Plan of Lot 2, Section 11, Esquimalt District, Plan VIP82523

BUILDING SUMMARY

● Lot Coverage

a) $= \frac{(76.8 + 24.6)}{567.9}$
 a) $= 17.9\% < 30\% \checkmark$
 b) $= \frac{24.6}{567.9}$
 b) $= 4.3\% < 10\% \checkmark$

● Floor Area Ratio

no change

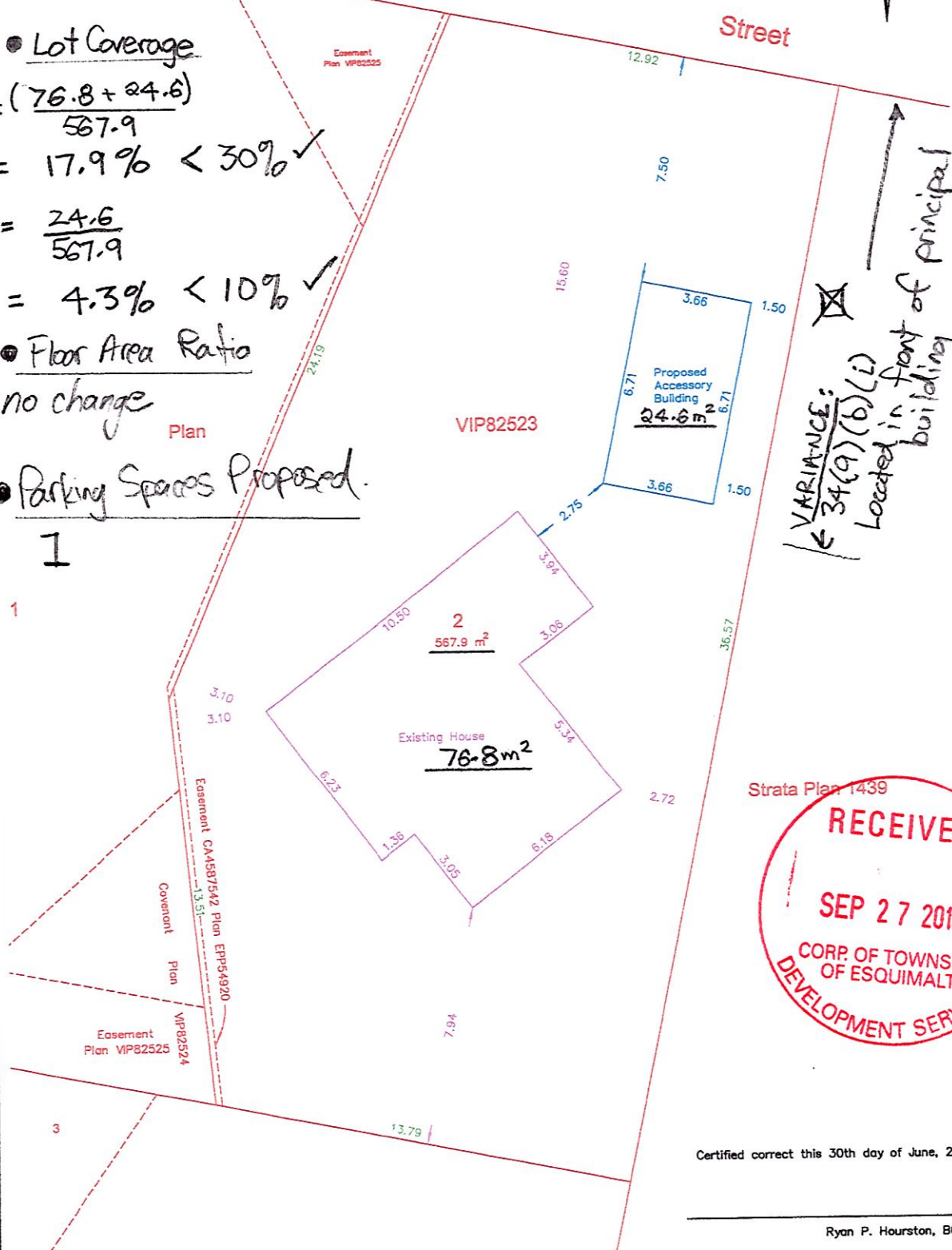
● Parking Spaces Proposed.

1

1

VIP82523

Munro Street



VARIANCE:
 ← 34(9)(b)(i)
 Located in front of building

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 SEP 27 2018
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 OF ESQUIMALT
 DEVELOPMENT SERVICES

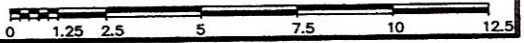
Certified correct this 30th day of June, 2016

Ryan P. Hourston, BCLS

Distances are in metres

This sketch does not constitute a redefinition of the legal boundaries hereon described and is not to be used in any manner which would assume same.

Scale 1:125 File: 29728



Development Variance Request



1129 Munro street, Esquimalt BC, V9A 5P2

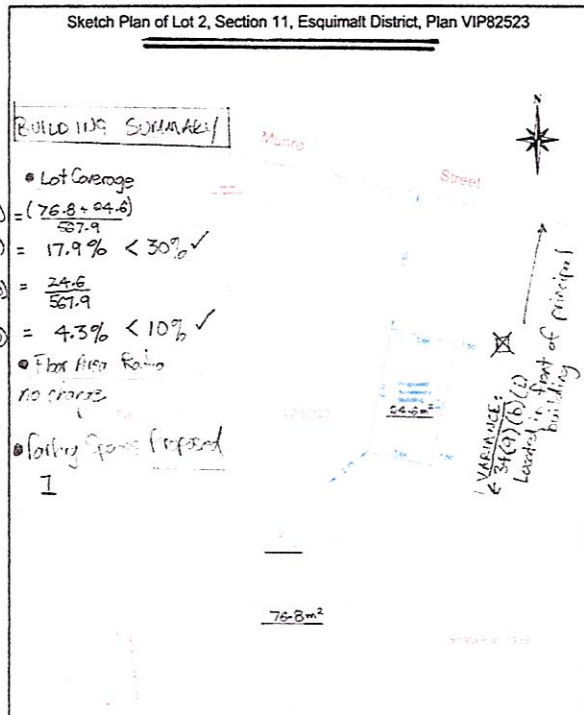
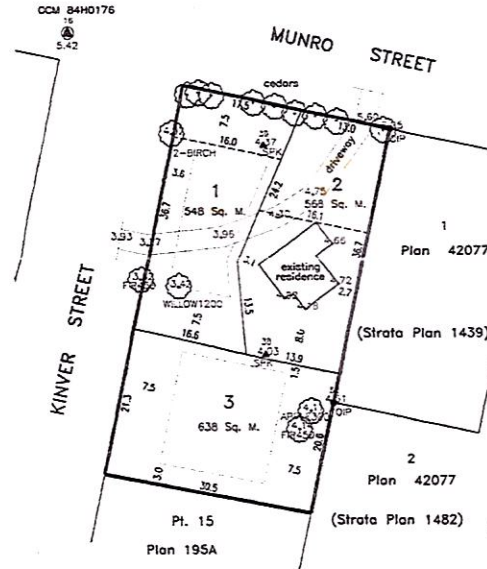
Thank you for taking your time to review this request. I will try to summarize the nature of this request as follows;

At 1129 Munro street there does not exist a garage parking stall as a result of a previous subdivision.

My wife and I are the owners of property 2 shown here, which was the original 80 year old principal building for properties 1, 2 and 3 combined.

When the subdivision was approved the garage for lot 2 was lost.

To maintain the principal building I need an accessory building (garage).



All provisions for the construction of an accessory building on this property are met with the exception of 34.9.b.i which does not allow accessory buildings from being located in front of the principal building, where the only space exists.

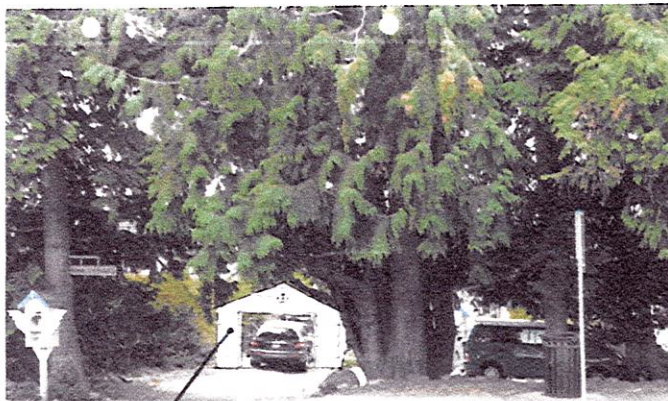
The finish on the side of the garage will be 18" boxed cedar siding shingles, sourced from a mill in Vancouver from samples off the principal building. The garage truss system will be sourced locally and engineered to best match the eaves of the principal building. The elevation of the SOG will be established so as not to change existing surface drainage, but allow for a complete skim coat of pea gravel to add to the existing. The existing principal building sanitary and storm drainage system was scoped and located (functioning well and located outside the footprint of the garage).

I have discussed the proposed garage with neighbors on both sides of our property, and across the street. Feedback was mostly positive with regard to matching the existing architecture, and the need for a garage. One observation made was with regard to the densification of the root system under the mature cedar trees lining Munro street, including both properties beside our property.

My response which seemed to be accepted, was that the garage is outside the drip line of the cedars where the surface root system is most prevalent, and the densification from vehicle traffic is well distributed by the existing pea gravel surface, and that an extra skim of pea gravel is planned for on final cleanup of the construction to further prevent densification.

The cedar trees at the front of our property are a valued asset to us in which we invest yearly in deep root fertilization, and professional trimming. The construction of a garage would also provide the opportunity to bring water to the front of our house for better maintenance of these prized trees during the extremes of summer drought.

Located near the waterfront on both Admirals and Constance there are several existing instances where properties have garages in front of their principal building. Many of these variances do not meet the current 7.5 meter setback requirement, and or, are not designed to match the principal building.



PROPOSED ACCESSORY BUILDING
1129 Munro St.

The siting of this garage proposal is approximately 1.2m below the sidewalk and largely covered by the cedar trees, will be built maintaining the character of an 80 year old house, and improves the streetscape.

I believe our request has no impact outside of our property line, and I look forward to a positive outcome.

Thank you again for your consideration of this variance.

Sincerely,

Jeff Duffield
1129 Munro Street

September 27, 2018

Dated

Sketch Plan of Lot 2, Section 11, Esquimalt District, Plan VIP82523



Munro Street

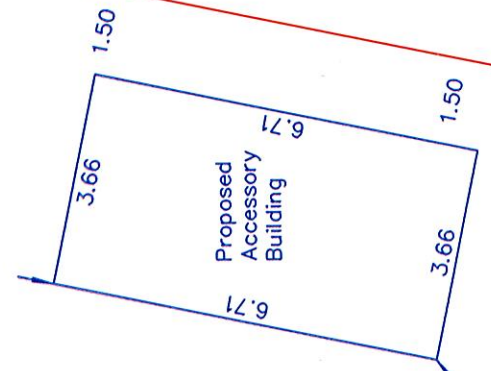
Street

12.92

Easement Plan VIP82525

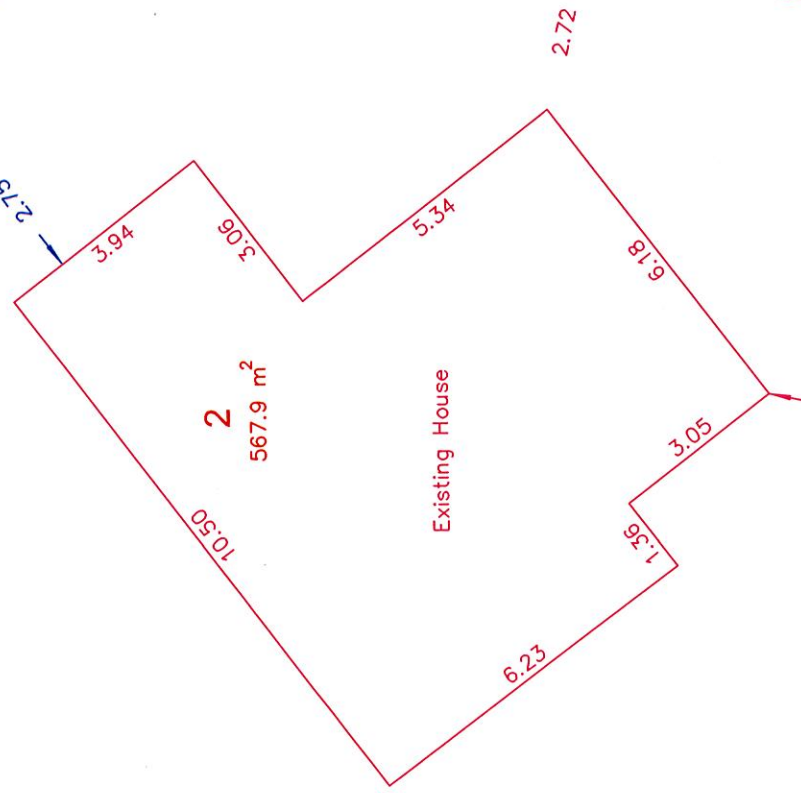
7.50

15.60



VIP82523

Plan



2

567.9 m²

Existing House

Strata Plan 1439

3.10

3.10

Easement CA4587542 Plan EPP54920

Covenant Plan

13.51

Easement Plan VIP82525

VIP82524

7.94

13.79

3

Certified correct this 30th day of June, 2016

Ryan Hourston

Ryan P. Hourston, BCLS

Distances are in metres

This sketch does not constitute a redefinition of the legal boundaries hereon described and is not to be used in any manner which would assume same.

Scale 1:125 File:29728





CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

APC Meeting: March 19, 2019

STAFF REPORT

DATE: March 11, 2019

TO: Chair and Members of the Advisory Planning Commission

FROM: Fred Billingham, Planning Technician
Bill Brown, Director of Development Services

SUBJECT: DEVELOPMENT VARIANCE PERMIT
1336 Wood Street
[PID 006-375-294, Lot 59, Suburban Lot 37, Esquimalt District, Plan VIP 2854]

RECOMMENDATION:

That the Esquimalt Advisory Planning Commission recommends to Council that the application for a Development Variance Permit allowing external alterations to the existing property at 1336 Wood Street, as illustrated in the architectural drawings prepared by Adapt Design, stamped "Received January 22, 2019", sited as detailed on the survey plan prepared by Wey Mayenburg, stamped "Received January 22, 2019", and including the following variances for the property located PID 006-375-294, Lot 59, Suburban Lot 37, Esquimalt District, Plan VIP 2854 [1336 Wood Street] **to either approve, or deny the application; including the reasons for the recommendation.**

Zoning Bylaw, 1992 No.2050, Section 34 (9)(a)(i)- Siting Requirements- Primary Building- Front Setback: Exemption from the requirement that no principal building shall be located within 7.5 metres of the Front Lot Line [i.e from 7.5m to 3.2m].

BACKGROUND:

Purpose of the Application

The applicant is proposing three additions to the existing 70 year old dwelling. These extensions are to be located on the North Western corner of the existing building, the Southern elevation and the Western elevation. The steps leading to the main entrance are to be extended and a new covered entrance landing installed, to be built to the dimensions and specifications shown in the attached drawings. The proposed works will not increase the Floor Area Ratio or Lot Coverage over the acceptable limits for a single family dwelling in an RS-1 zone, however the front setback will in effect be reduced to 3.29metres. This is contrary to the zoning bylaw for RS-1, which requires a minimum setback of 7.5m. As such, a Development Variance Permit is required.

The subject property is exempt from any Development Permit Areas under paragraph 19 of Section 17.3, as the proposal is for works to a single detached, non-infill dwelling.

Context

Applicant: Jason Wenaus & Nicole Cook

Owners: Jason Wenaus & Nicole Cook

Property Size: Metric: 557.17m² Imperial: 5997 ft²

Existing Land Use: Single Family Dwelling

Surrounding Land Uses:

North: Multiple Family Residential

South: Single Family Residential

West: Multiple Family Residential

East: Multiple Family Residential

Existing Zoning: RM-1 [Multiple Family Residential], used as RS-1 [Single Family Residential] under Zoning Bylaw, 1992 No.2050 Section 11(3).

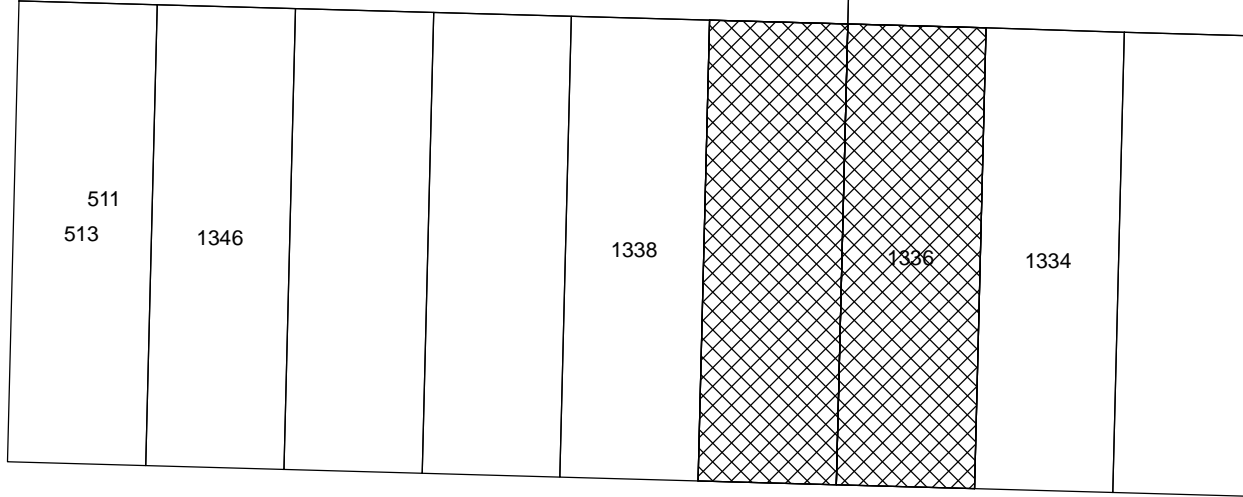
Public Notification

As this application includes a Development Variance Permit application, should it proceed to Council, a notice will be mailed to tenants and owners of properties within 50 metres (164 feet) of the subject property.

ALTERNATIVES:

1. Forward the application for a Development Permit and Development Variance Permit to Council with a **recommendation of approval.**
2. Forward the application for a Development Permit and Development Variance Permit to Council with a **recommendation of denial.**

Sturdee Street



Wood Street



Lyall Street

Nelson Street



Subject Property Map
1336 Wood Street



Wood Street

Nelson Street

DIVISION 1 - RESIDENTIAL ZONES

34. SINGLE FAMILY RESIDENTIAL [RS-1]

The intent of this Zone is to accommodate Single Family Dwellings on individual Parcels of land.

(1) Permitted Uses

The following Uses and no others are permitted:

- (a) Single Family Residential
- (b) Home Occupation
- (c) Secondary Suite: subject to the requirements of Section 30.6
- (d) Boarding: subject to the requirements of Section 30.3
- (e) Urban Hens: subject to the requirements of Section 30.4 of this bylaw.

(2) Parcel Size

The minimum Parcel Size for Parcels created by subdivision shall be 530.0 square metres.

(3) Minimum Lot Width

The minimum width of Parcels created by subdivision shall be 16 metres measured at the Front Building Line.

(4) Floor Area Ratio

The Floor Area Ratio shall not exceed 0.35.

(5) Floor Area

The minimum Floor Area for the First Storey of a Principal Building shall be 88 square metres.

(6) Building Height

- (a) No Principal Building shall exceed a Height of 7.3 metres.
- (b) No Accessory Building shall exceed a Height of 3.6 metres.

(7) Building Width

The minimum width for any Single Family Dwelling shall be 7 metres.

(8) Lot Coverage

- (a) All Principal Buildings, Accessory Buildings and Structures combined, shall not cover more than 30% of the Area of a Parcel.
- (b) All Accessory Buildings and Structures combined shall not exceed 10% of the Area of Parcel.

(9) **Siting Requirements**

(a) **Principal Building**

- (i) Front Setback: No Principal Building shall be located within 7.5 metres of the Front Lot Line.
- (ii) Side Setback: No Principal Building shall be located within 1.5 metres of an Interior Side Lot Line, with the total Setback of all Side Yards not to be less than 4.5 metres. In the case where a Parcel is not served by a rear lane, one (1) Side Yard shall not be less than 3 metres. In the case of a Corner Lot, no Principal Building shall be located within 3.6 metres of an Exterior Side Lot Line
- (iii) Rear Setback: No Principal Building shall be located within 7.5 metres of a Rear Lot Line.

(b) **Accessory Building**

- (i) Front Setback: No Accessory Building shall be located in front of the front face of the Principal Building.
- (ii) Side Setback: No Accessory Building shall be located within 1.5 metres of an Interior Side Lot Line nor 3.6 metres of an Exterior Side Lot Line.
- (iii) Rear Setback: No Accessory Building shall be located within 1.5 metres of a Rear Lot Line.
- (iv) Building Separation: No Accessory Building shall be located within 2.5 metres of a Principal Building.

(10) **Fencing**

Subject to Section 22, no fence shall exceed a Height of 1.2 metres in front of the front face of the Principal Building and 2 metres behind the front face of the Principal Building.

(11) **Off Street Parking**

Off street parking shall be provided in accordance with the requirements of Parking Bylaw, 1992, No. 2011 (as amended).

GENERAL NOTES

GENERAL NOTES

ALL MATERIALS AND CONSTRUCTION METHODS TO CONFORM TO THE CURRENT EDITION OF THE BRITISH COLUMBIA BUILDING CODE (BCBC), GOOD CONSTRUCTION PRACTICE, AS WELL AS ANY OTHER LOCAL BUILDING CODES OR BYLAWS WHICH MAY TAKE PRECEDENCE

ALL MEASUREMENTS TO BE VERIFIED ON SITE BY BUILDER PRIOR TO CONSTRUCTION. COMMENCEMENT OF CONSTRUCTION OR ANY PART THEREOF CONSTITUTES ACCEPTANCE OF THE DRAWINGS/SITE CONDITIONS AND MEANS DIMENSIONS & ELEVATIONS HAVE BEEN VERIFIED & ARE ACCEPTABLE

IF ANY DISCREPANCIES ARISE, THEY SHOULD BE REPORTED TO THE DESIGNER

DIMENSIONS SHALL TAKE PRECEDENCE OVER SCALE

FRAMING LUMBER SHALL BE GRADED #2 OR BETTER UNLESS OTHERWISE SPECIFIED

ALL INTERIOR FINISHES, CASINGS, WINDOW TYPES AND MILLWORK TO OWNERS APPROVAL

STAIR TREADS TO BE PLYWOOD OR OTHER ENGINEERED PRODUCT AND SECURED WITH SCREWS AND SUB-FLOOR ADHESIVE

TEMPORARY HEAT REQUIRED PRIOR TO DRYWALL INSTALLATION TO ASSIST IN DRYING OF FRAMEWORK. MOISTURE CONTENT OF FRAMEWORK MUST NOT EXCEED 19%

SITE PLAN

LAYOUT TO BE CONFIRMED BY A CURRENTLY REGISTERED BRITISH COLUMBIA LEGAL LAND SURVEYOR

ALL SET BACKS TO BE CONFIRMED BY THE OWNER AND BUILDER

ALL GRADE ELEVATIONS ARE THE RESPONSIBILITY OF THE OWNER AND BUILDER

VERIFY EXISTING AND PROPOSED GRADES PRIOR TO CONSTRUCTION

FOUNDATION

THE BUILDER IS RESPONSIBLE FOR LOCATING THE FOOT PRINT OF THE STRUCTURE IN THE PROPER PLACE AS PER PLANS

CONCRETE FOUNDATION WALLS NOT SUBJECT TO SURCHARGE SHALL BE INSTALLED ON COMPACTED, UNDISTURBED, INORGANIC STABLE SOILS BELOW THE DEPTH OF FROST PENETRATION WITH AN ALLOWABLE BEARING PRESSURE OF 75 kPa OR GREATER. IF SOFTER CONDITIONS APPLY, THE BEARING CAPACITY AND SIZE OF FOOTINGS ARE TO BE DESIGNED BY A QUALIFIED ENGINEER

THE SILL PLATE IS TO BE FASTENED TO THE FOUNDATION WALL WITH NOT LESS THAN 12.7mm Ø ANCHOR BOLTS SPACED NOT MORE THAN 2.4m O.C. OR FOR BRACED WALL PANELS 2 15mm Ø ANCHOR BOLTS PER BRACED WALL PANEL 500mm FROM THE ENDS OF THE FOUNDATION AND SPACED 1.7m O.C. EMBEDDED 100mm DEEP

ALL LUMBER IN CONTACT WITH CONCRETE SHALL BE TREATED OR PROTECTED BY A MOISTURE RESISTANT GASKET

IT IS THE RESPONSIBILITY OF THE OWNER/CONTRACTOR TO HAVE SITE SOIL CONDITIONS INSPECTED AND ADVISE THE DESIGNER OF ANY SOIL CONDITIONS WHICH MAY REQUIRE ENGINEERING

ALL FOUNDATION WALLS ARE 200mm THICK 20MPa CONCRETE UNLESS OTHERWISE SPECIFIED

FOUNDATION WALLS MAY BE A MAXIMUM OF 4' HIGH FROM GRADE TO UNDERSIDE OF FLOOR IF LATERALLY UNSUPPORTED AT TOP. ALL OTHER CONCRETE FOUNDATION WALLS TO BE ENGINEERED

FRAMING

ALL ENGINEERED COMPONENTS TO BE SIZED BY SUPPLIER

ALL SPANS AND LOADINGS SHALL CONFORM TO THE CURRENT VERSION OF THE BCBC. VERIFICATION OF ALL COMPONENTS IS THE RESPONSIBILITY OF THE OWNER/BUILDER. ANY COMPONENTS WHICH CANNOT BE DESIGNED WITH THE BCBC SHALL BE DESIGNED BY A QUALIFIED ENGINEER

TRUSSES AND LAYOUT ARE TO BE ENGINEERED AND INSTALLED ACCORDING TO MANUFACTURERS SPECIFICATIONS

IT IS ASSUMED THAT THE CONTRACTOR IS FAMILIAR WITH THE 2012 BCBC AND INDUSTRY STANDARDS FOR WOOD FRAME CONSTRUCTION. NOT EVERY DETAIL OF WOOD FRAMING IS SHOWN ON THESE DRAWINGS

ALL LINTELS DOUBLE 2X10 S.S. SPF FOR CLEAR SPANS UP TO 5' UNLESS OTHERWISE NOTED

EXTERIOR WALL THICKNESS SHOWN ARE MEASURED FROM OUTSIDE OF EXTERIOR SHEATHING TO INSIDE OF DRYWALL

INTERIOR WALL THICKNESS SHOWN ARE MEASURED FROM OUTSIDE OF DRYWALL TO OUTSIDE OF DRYWALL

ROOM MEASUREMENTS SHOWN ARE TO THE NEAREST INCH. DIMENSIONS SHOWN ARE TO THE NEAREST 1/2"

CONFIRM ALL VANITY'S, BATHTUBS, SHOWERS AND KITCHEN CUPBOARDS WITH OWNER PRIOR TO FRAMING AS THESE MAY REQUIRE MODIFICATIONS TO THE ROOM SIZES

ROOFING

ALL ROOFING SHALL BE APPLIED TO THE MANUFACTURERS SPECIFICATIONS AND SHALL INCLUDE EAVE PROTECTION FROM ICE DAMMING AND SNOW BUILD UP

PLUMBING AND ELECTRICAL

ANY PLUMBING AND ELECTRICAL SHOWN ON THESE PLANS IS FOR ILLUSTRATIONAL PURPOSES ONLY AND MUST BE DESIGNED AND INSTALLED BY A QUALIFIED PROFESSIONAL

FLASHING

ALL PENETRATIONS THROUGH THE ROOF WILL REQUIRE FLASHING.

ALL ROOFING TO INCLUDE STEP FLASHING.

ALL EXPOSED OPENINGS TO INCLUDE FLASHING

ALL FLASHING END DAMS TO BE 25mm (1") HIGH

DOORS

FRAME OPENING TO BE 1 1/4" WIDER THAN DOOR
FRAME HEIGHT 83" FOR EXTERIOR DOORS AND 82.5" FOR INTERIOR DOORS.
FRAME OPENING 1 1/4" WIDER THAN BIFOLD DOORS AND FRAME HEIGHT IS 81.5"
ALL INTERIOR DOORS TO BE 30" WIDE UNLESS OTHERWISE SPECIFIED

FENESTRATION

ALL WINDOWS, DOORS & SKYLIGHTS TO CONFORM TO NAFS-08 AND THE CANADIAN SUPPLEMENT TO NAFS

FENESTRATION PERFORMANCE REQUIREMENTS:

CLASS R - PG 30 - +VE/-VE DP = 1440Pa/1440Pa - WATER PENETRATION RESISTANCE = 260Pa - CANADIAN AIR INFILTRATION/EXFILTRATION = A2

WINDOW/DOOR LABELS TO BE LEFT IN PLACE UNTIL FINAL INSPECTION

SUPPLY AND INSTALL ALL WINDOW TYPES, INTERIOR CASINGS AND MILLWORK TO OWNERS APPROVAL

ALL WINDOWS ADJACENT TO BATH TUBS TO BE SAFETY GLASS

GUARDS/HANDRAILS

INSTALL GRASPABLE HANDRAIL TO ALL INTERIOR STAIRS AT 34" TO 38" ABOVE STAIR NOSING

INSTALL GUARDS AT ALL BALCONIES, DECKS AND PORCHES GREATER THAN 2' ABOVE GRADE. INSTALL GUARD AT 42" HEIGHT WHERE SURFACE IS GREATER THAN 6' ABOVE ADJACENT SURFACE, OTHERWISE 36" GUARDRAIL ALLOWABLE

TOPLESS GLASS GUARDS TO BE ENGINEERED WITH SEALED DRAWINGS

VENTILATION

PROVIDE ATTIC AND CRAWLSPACE ACCESS AND VENTILATION IN ACCORDANCE WITH BCBC

PROVIDE HEATING, MECHANICAL VENTILATION, AND AIR CONDITIONING WHERE REQUIRED IN ACCORDANCE WITH BCBC AND LOCAL BYLAWS

MECHANICAL CONTRACTOR TO PROVIDE MECHANICAL CHECKLIST COMPLETE WITH FAN & DUCT SIZES PRIOR TO FRAMING INSPECTION

MISC.

SMOKE/CARBON MONOXIDE ALARMS TO BE PROVIDED ON EVERY FLOOR AND ARE TO BE HARDWIRED AND WITHIN 5m OF EACH BEDROOM IN EVERY SUITE AND INTERCONNECTED TO ALL FLOORS. SMOKE ALARMS TO ALSO BE PROVIDED IN EVERY BEDROOM. ALL SMOKE ALARM LOCATIONS WILL HAVE BOTH PHOTOELECTRIC AND IONIC DETECTION SYSTEMS

BEDROOM WINDOWS FOR EGRESS SHALL HAVE OPENINGS WITH AREAS NOT LESS THAN 3.8m² WITH NO DIMENSION LESS THAN 15"

IT IS THE RESPONSIBILITY OF THE CONTRACTOR AND/OR OWNER TO CHECK AND VERIFY ALL ASPECTS OF THESE PLANS PRIOR TO START OF CONSTRUCTION OR DEMOLITION.

ADAPT DESIGN DOES NOT ACCEPT RESPONSIBILITY FOR THE FOLLOWING:

-INFORMATION PROVIDED ON EXISTING BUILDINGS OR SITE

-CONFORMITY OF PLANS TO SITE

-ERRORS AND/OR OMISSIONS

-ANY HOUSE BUILT FROM THESE PLANS

THESE PLANS REMAIN THE PROPERTY OF ADAPT DESIGN AND CAN BE RECLAIMED AT ANY TIME



1
A001 PERSPECTIVE VIEW 1
NOT TO SCALE



2
A001 PERSPECTIVE VIEW 2
NOT TO SCALE

Sheet Index

Site Plan and Schedules

A001	General Notes & Perspective
A002	Cover Sheet and Site Plan

Plans

101	Foundation Plan
A102	Lower level
A103	Main Level
A104	Roof Plan

Elevations

A201	Elevations
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Sections

A301	Section
------	---------

Details

A401	Details
------	---------



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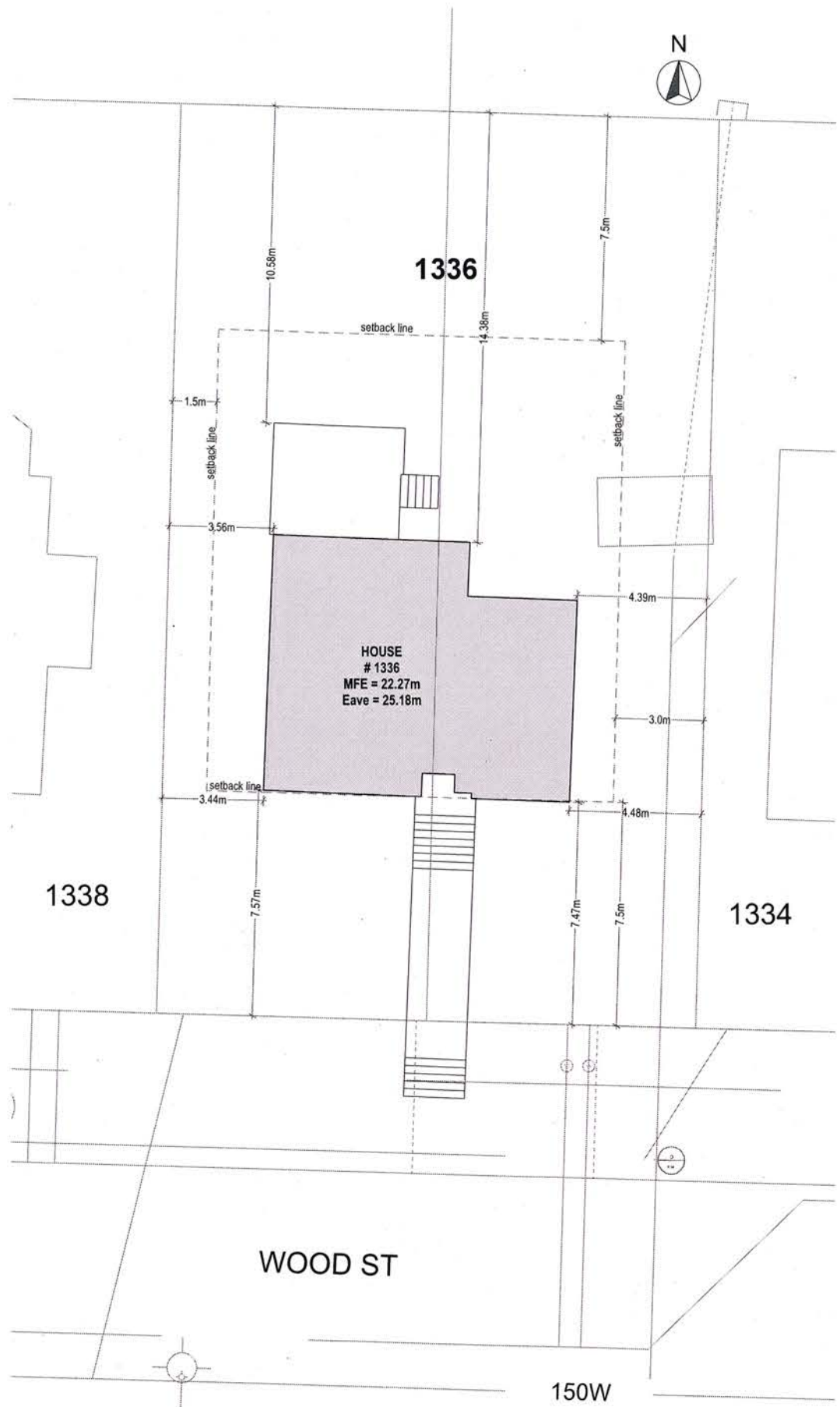
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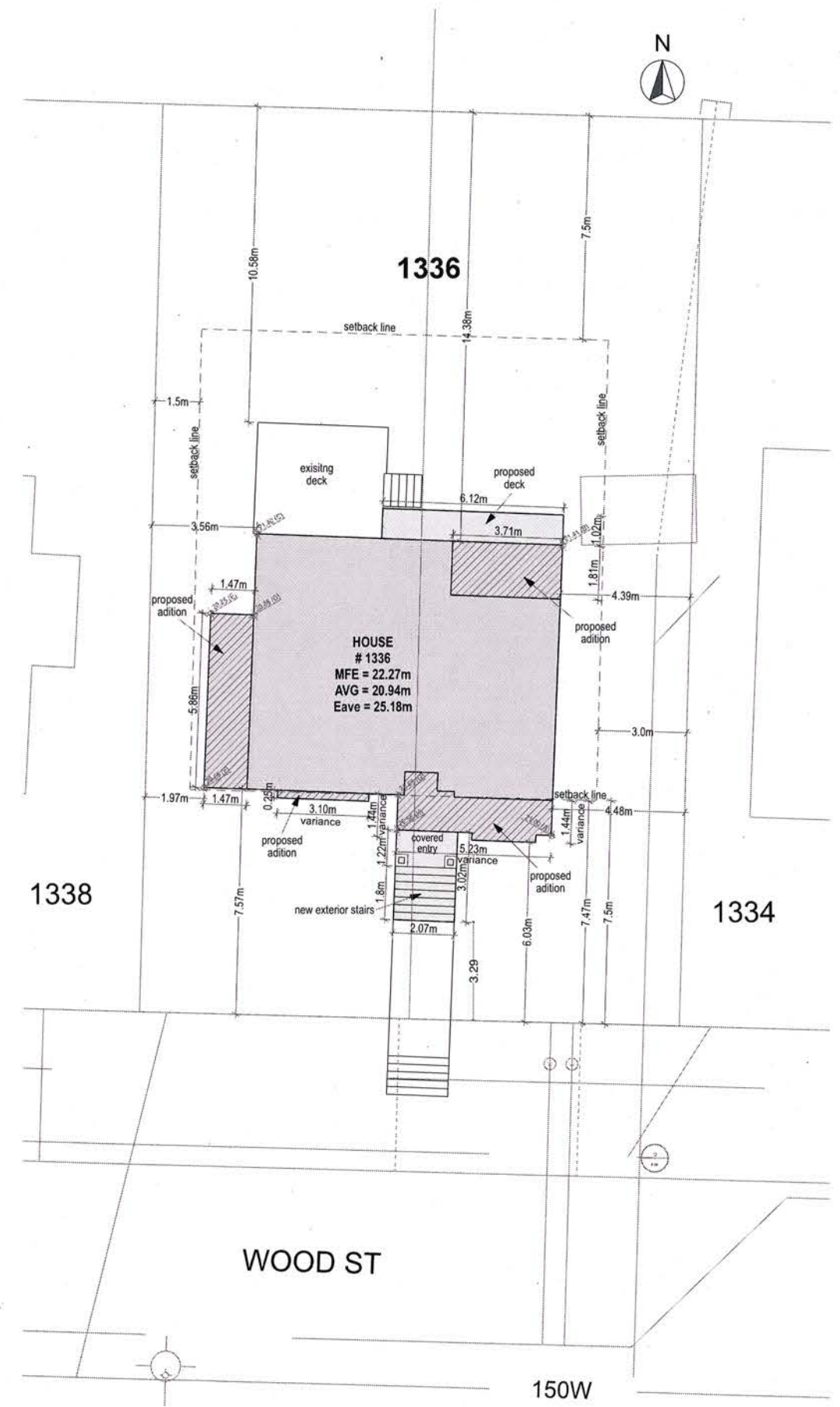
ISSUED:

General Notes &
Perspective

A001



1 EXISTING SITE PLAN
SCALE: 1/4" = 1'-0"



2 PROPOSED SITE PLAN
SCALE: 1/4" = 1'-0"

AVERAGE GRADE

A = 21.00m
B = 21.41m
C = 21.40m
D = 20.86m
E = 20.85m
F = 20.68m
G = 20.68m
H = 20.66m
167.54m/8 = 20.94m



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Property Information

Project Type: Remodel & Addition
Owners: Jake Wenaus
Address: 1336 Wood St, Esquimalt, BC

Legal Description:

Zoning: RS-1

	Zoning	Proposed	Existing
Setbacks:			
Rear	7.5m	10.58m	10.58m
Side	1.5m/3.0m	1.97m/4.39m	3.44m/4.39m
Front	7.5m	3.29m*	7.47m
		*Variance is required	
Eaves	0.6m	0.3m	0.3m
Height	7.3m	5.0m	
Floor Area:			
Lower		900.71 SF	887.54 SF
Main		1125.81 SF	875.49 SF
Total		2026.52 SF	1763.03 SF
Lot Area:		5996.32 SF	
Building Footprint:		1396.27 SF	887.54 SF
Lot Coverage	30%	23.28%	14.80%
Floor Area Ratio	0.35	0.34	0.29
Main Floor Elevation		22.27m	
Average Grade		20.94m	

Applicable Codes

-BC Building Code Current Edition (2012)

Energy

Compliance path: BCBC 9.36
Requirements applicable to this project: Prescriptive Path

Ventilation

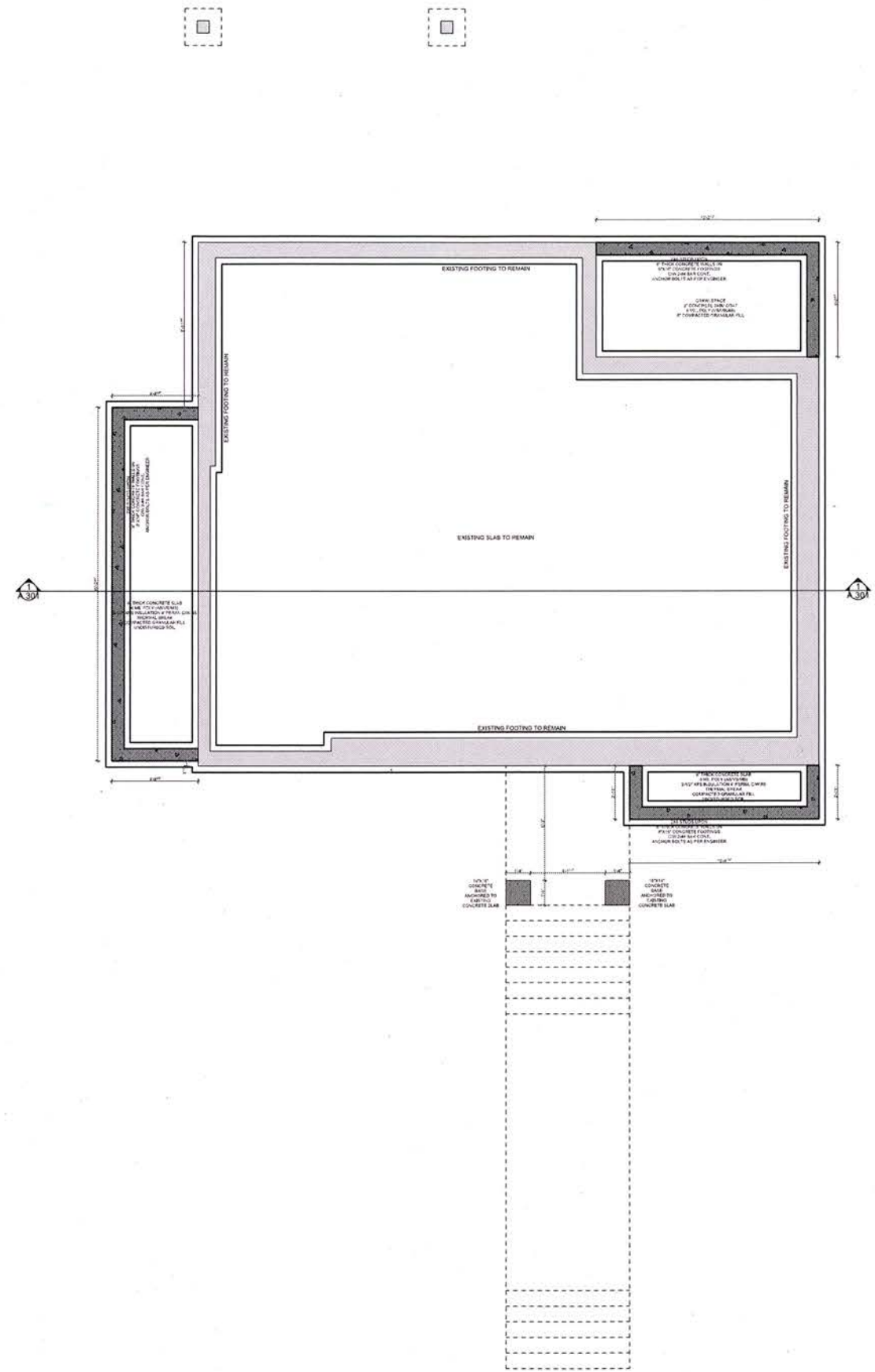
BCBC 9.32

ISSUED FOR PERMIT

ISSUED:

Cover Sheet and Site Plan

A002



PLAN LEGEND

- NEW EXTERIOR WALL
- NEW PARTITION WALL
- EXISTING WALL TO BE REMOVED
- EXISTING WALL TO REMAIN
- NEW FOUNDATION WALL
- EXISTING FOUNDATION WALL TO REMAIN
- BEAM/COLUMN/FLOOR DRAIN
- DIMENSION PLACEMENT
- ROOM SIZES ARE INTERIOR DIMENSIONS WIDTH X DEPTH
- HARDWIRED INTERCONNECTED CO DETECTOR
- HARDWIRED INTERCONNECTED IONIC SMOKE DETECTOR
- HARDWIRED PHOTOELECTRIC SMOKE DETECTOR
- BATH FAN: VENTILATION RATE 25 L/S INTERMITTENT
- KITCHEN FAN: VENTILATION RATE 47 L/S INTERMITTENT
- CONTINUOUS PRINCIPAL EXHAUST FAN: VENTILATION RATE AS PER 19.32.3.4 - 9.32.3.5
- FRESH AIR SUPPLY
- PASSIVE AIR INLET



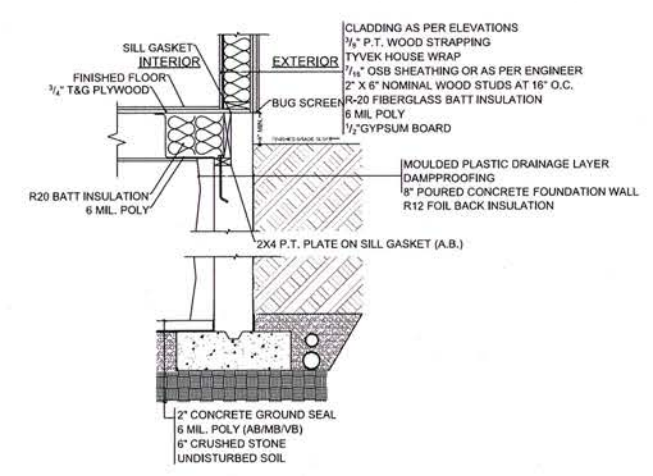
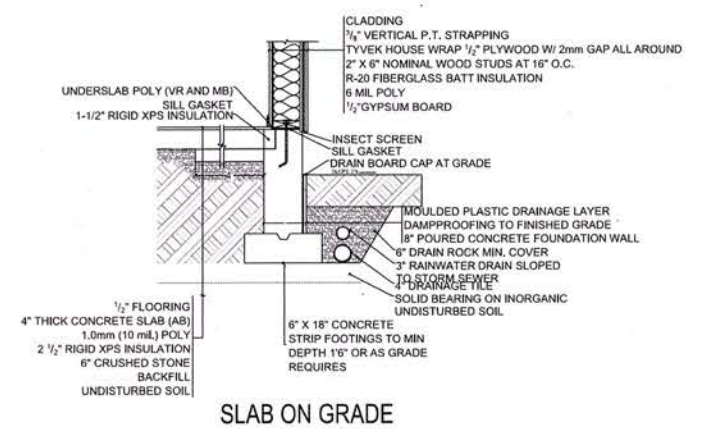
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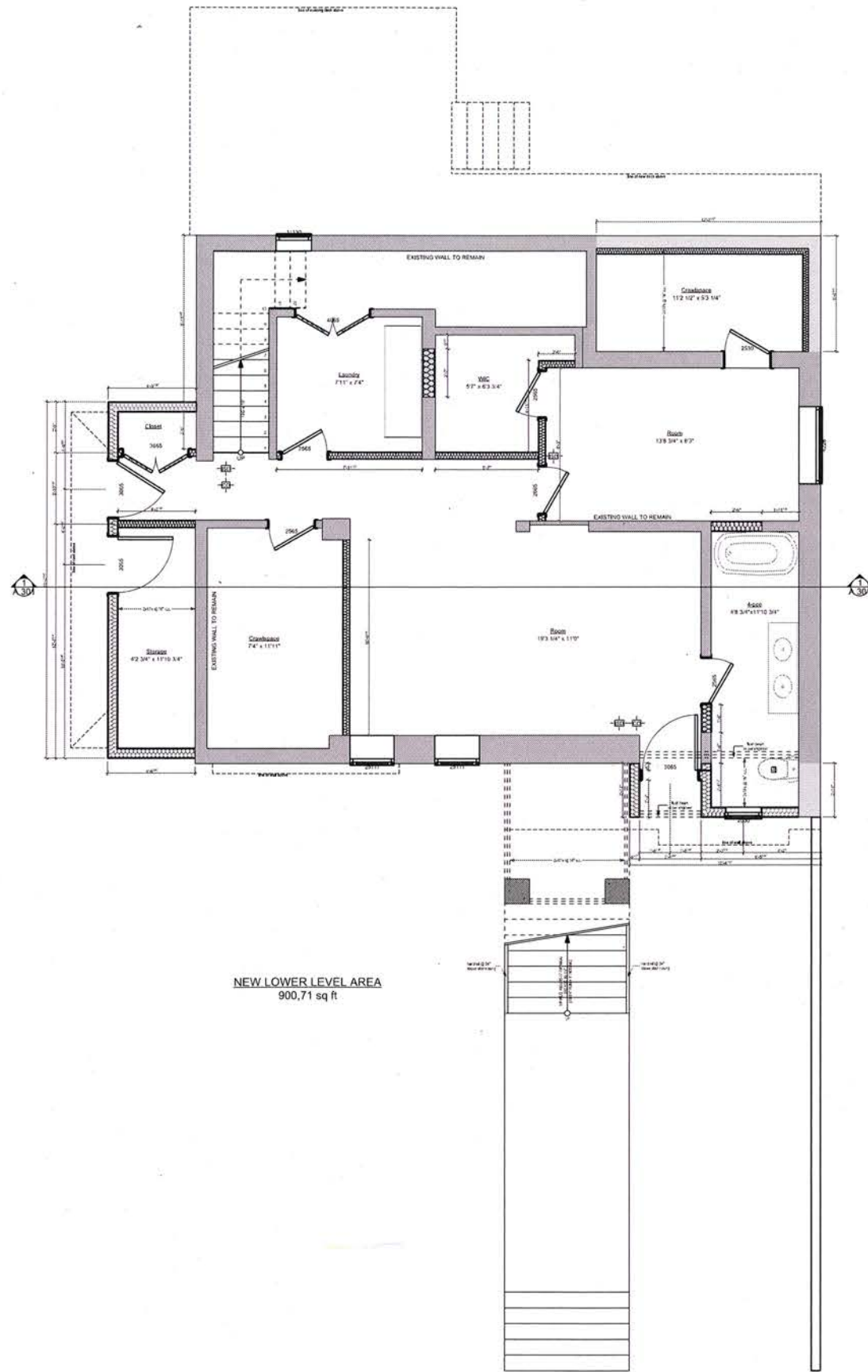
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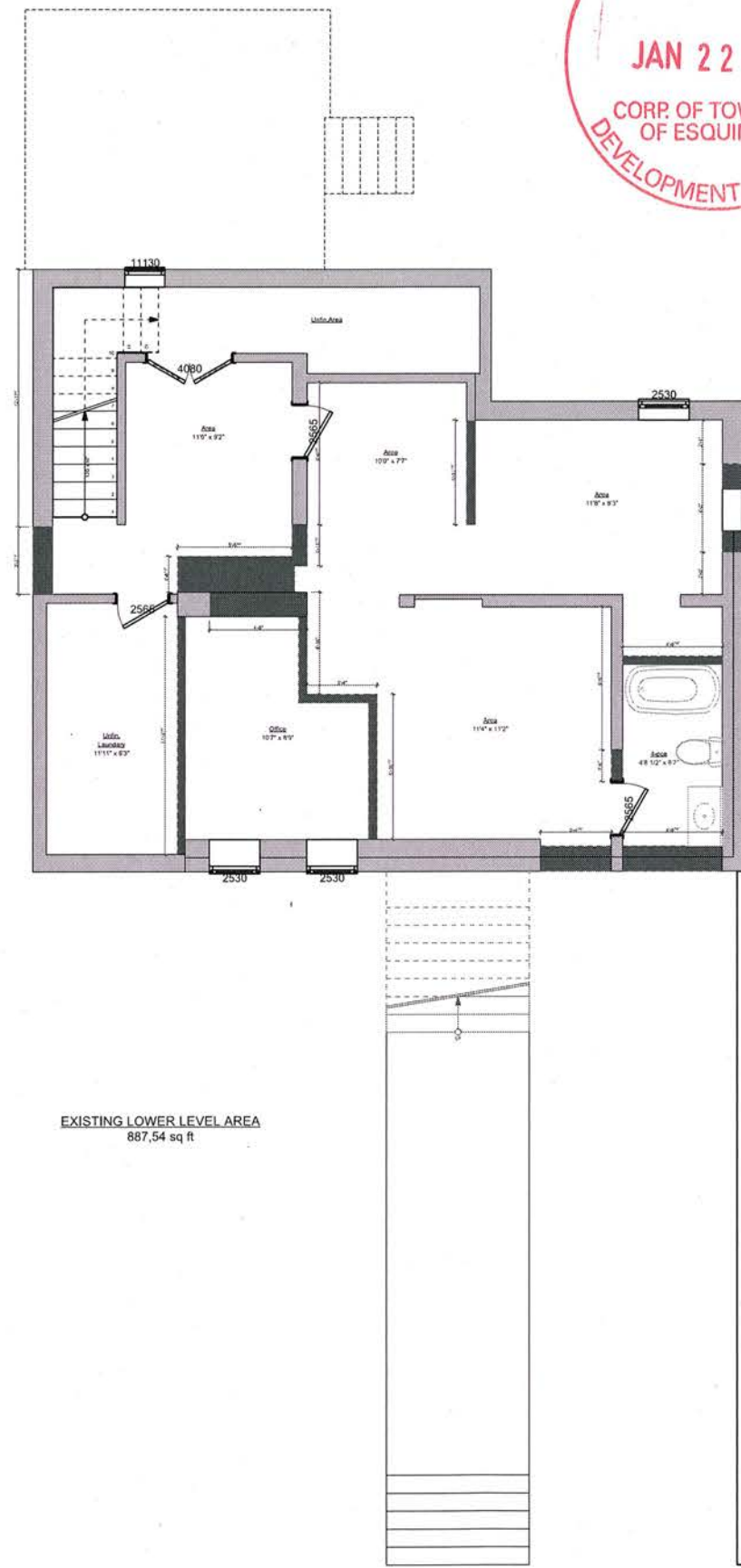
ISSUED:

Foundation Plan



NEW LOWER LEVEL AREA
900.71 sq ft

1 LOWER LEVEL - NEW PLAN
SCALE: 1/4" = 1'-0"



EXISTING LOWER LEVEL AREA
887.54 sq ft

2 LOWER LEVEL - EXISTING-DEMO PLAN
SCALE: 1/4" = 1'-0"



PLAN LEGEND

- NEW EXTERIOR WALL
- NEW PARTITION WALL
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










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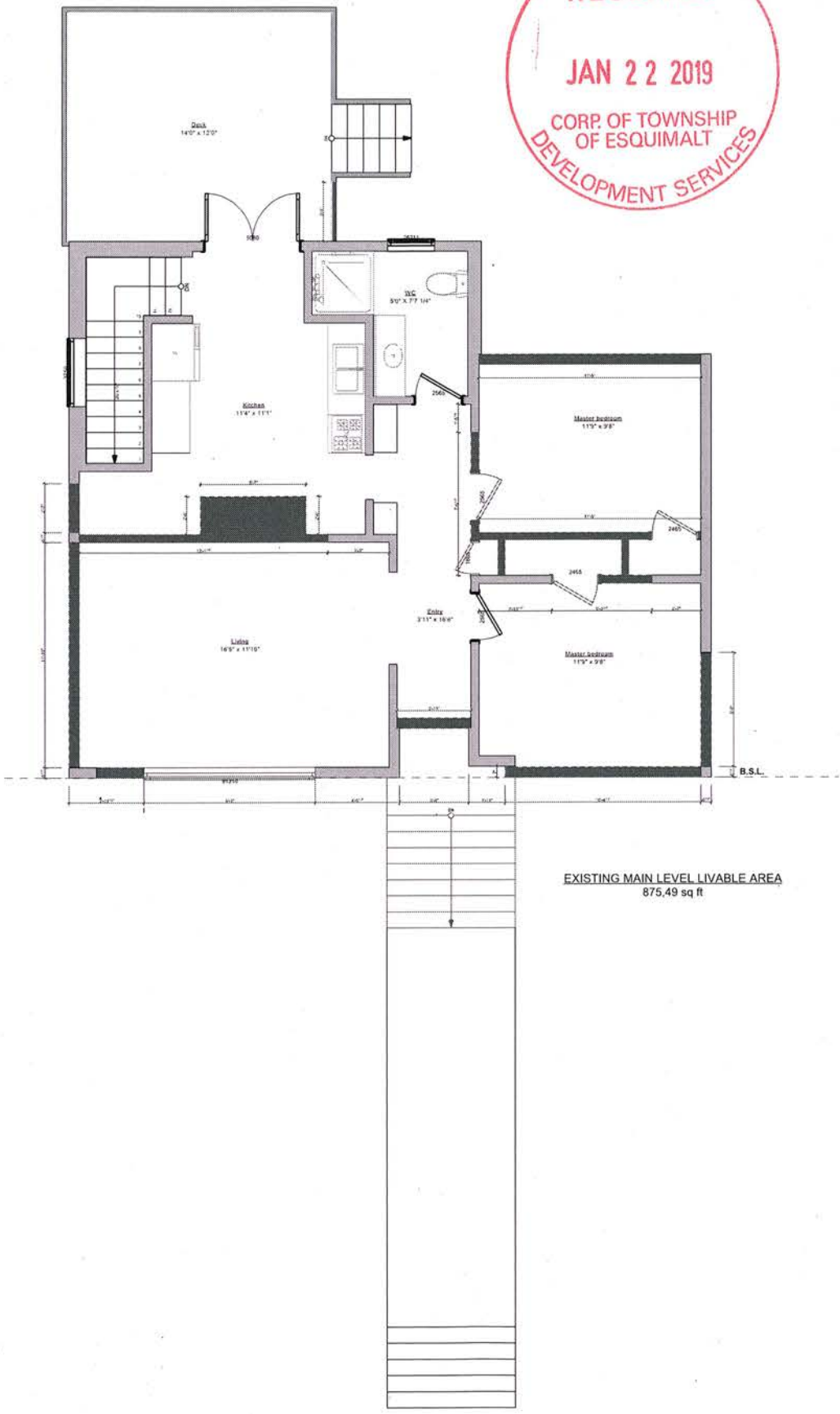
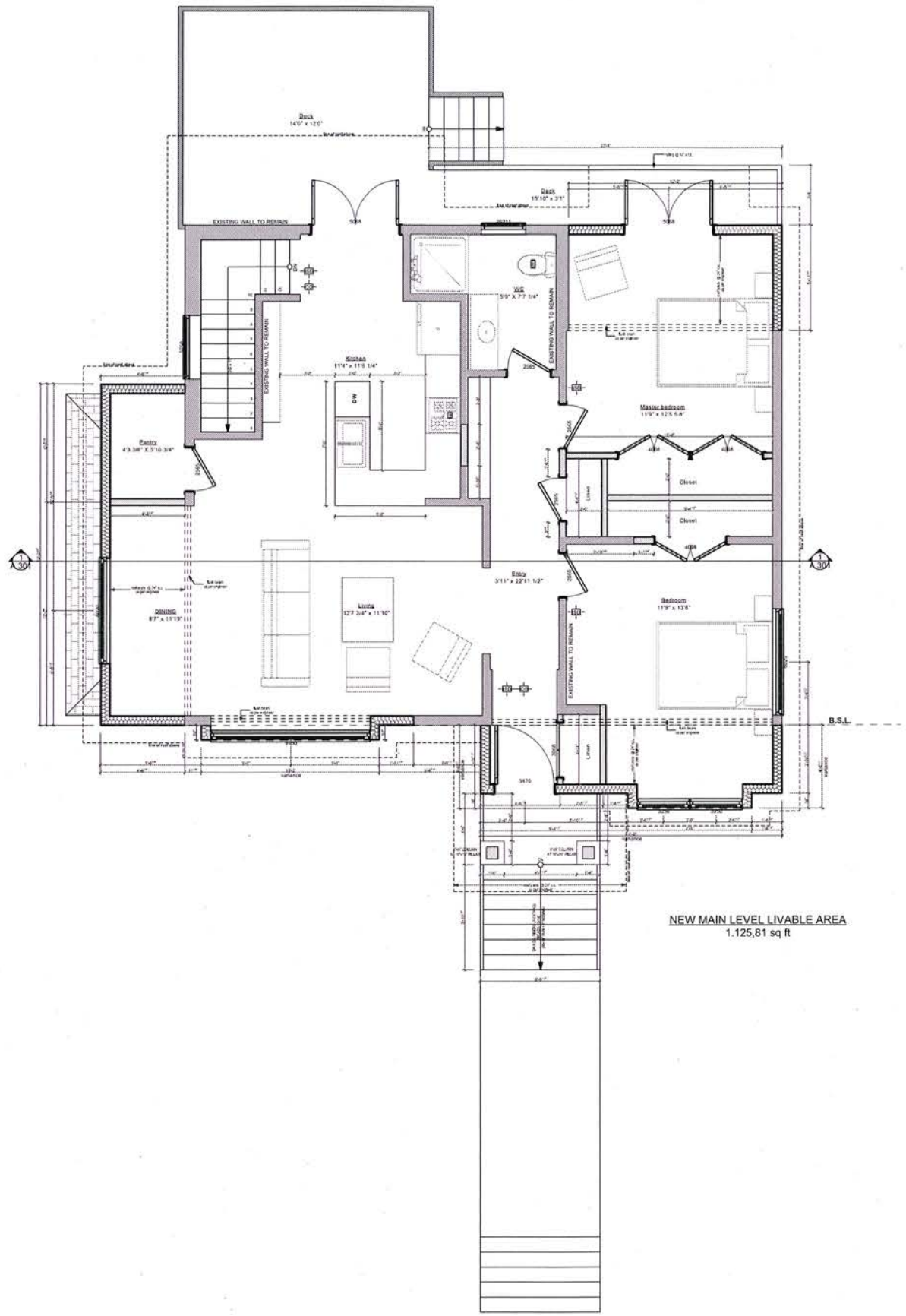
Lower level

A102

PLAN LEGEND

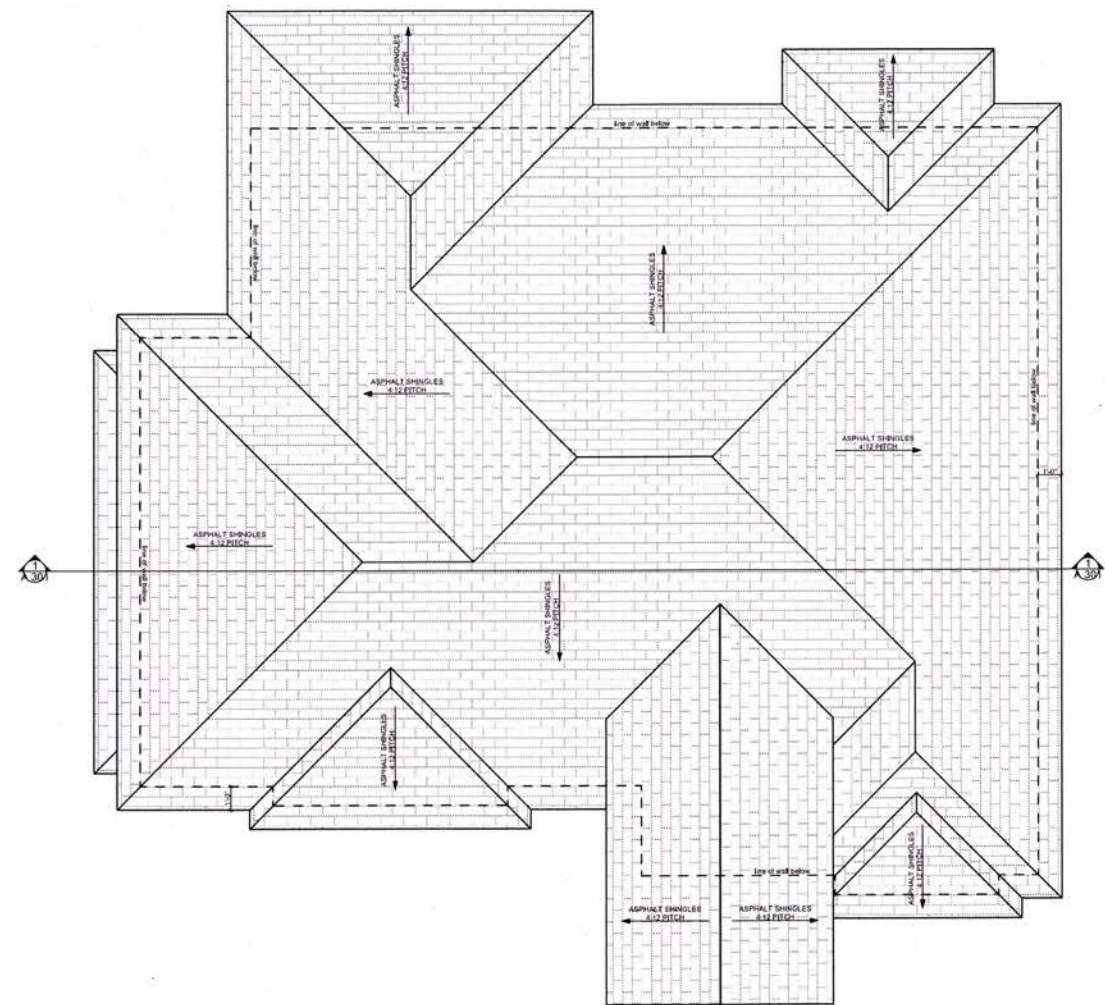
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JAN 22 2019
 CORP. OF TOWNSHIP
 OF ESQUIMALT
 DEVELOPMENT SERVICES

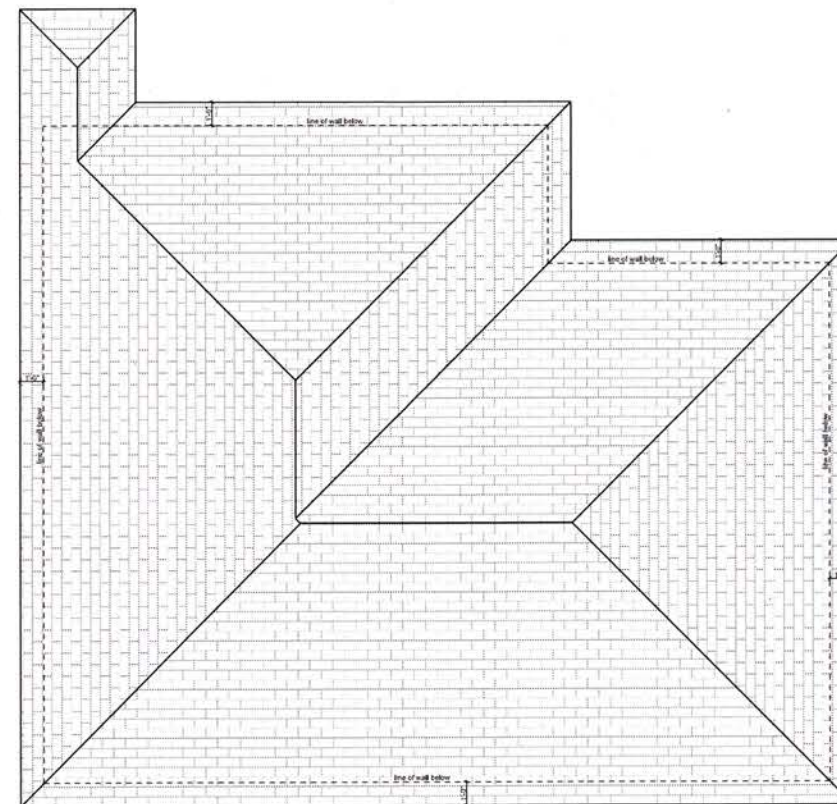


1 MAIN FLOOR - NEW PLAN
SCALE: 1/4" = 1'-0"

2 MAIN LEVEL- EXISTING-DEMO PLAN
SCALE: 1/4" = 1'-0"



1
A104 NEW ROOF PLAN
SCALE: 1/4" = 1'-0"



2
A104 EXISTING ROOF PLAN
SCALE: 1/4" = 1'-0"

ISSUED FOR
PERMIT

ISSUED:

Roof Plan

RECEIVED

JAN 22 2019

**CORP. OF TOWNSHIP
OF ESQUIMALT
DEVELOPMENT SERVICES**

EXTERIOR CLADDING LEGEND	
1	HARDIESHINGLE SIDING PAINTED
2	CEMENT BOARD LAP SIDING
3	ASPHALT ROOFING SHINGLES
4	STONE VENEER BY OWNER

ADDITIONAL EXTERIOR FINISHINGS	
OUTTERS	5" CONTINUOUS ALUMINUM (PREFINISHED)
SOFFIT	C/W 4"X7" ALUMINUM DOWNPOUT (PREFINISHED)
FACSA	VENTED ALUMINUM (PREFINISHED)
BELLY BAND	CLEAR CEDAR SIDING (PAINTED)
WINDOW TRIM	2X4 COMB FACED SPF (PAINTED)
DOOR TRIM	2X4 COMB FACED SPF TOP AND SIDES C/W 2X4 SLOPED S/L AND 2X4 SUBRILL (PAINTED)
CORNER BOARDS	2X4 COMB FACED SPF (PAINTED)
	1X4 COMB FACED SPF (PAINTED)

NOTE
WINDOW OPERATION SHALL BE AS PER OWNERS DIRECTION AND CONFORM TO BCBC EGRESS REQUIREMENTS. CONTRACTOR TO VERIFY ALL S.D. PRIOR TO CONSIDERING WORKS
FLASH OVER ALL MATERIAL TRANSITIONS. DOOR AND WINDOW HEADERS
ALL COLOURS AS PER OWNER

ADAPT DESIGN

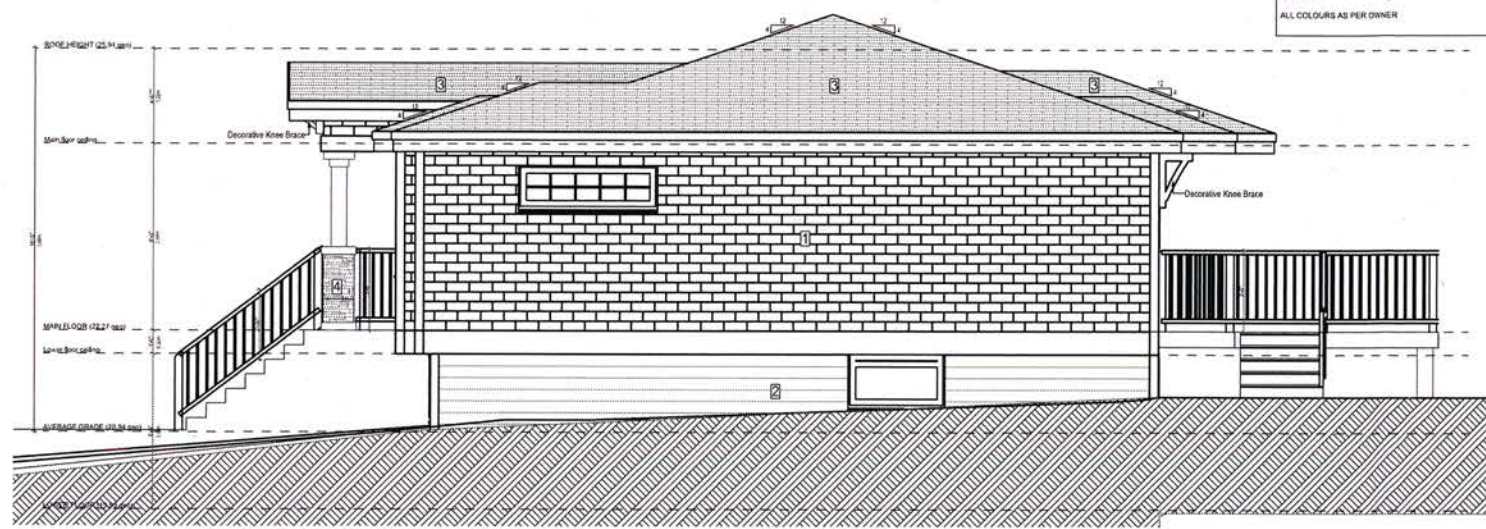
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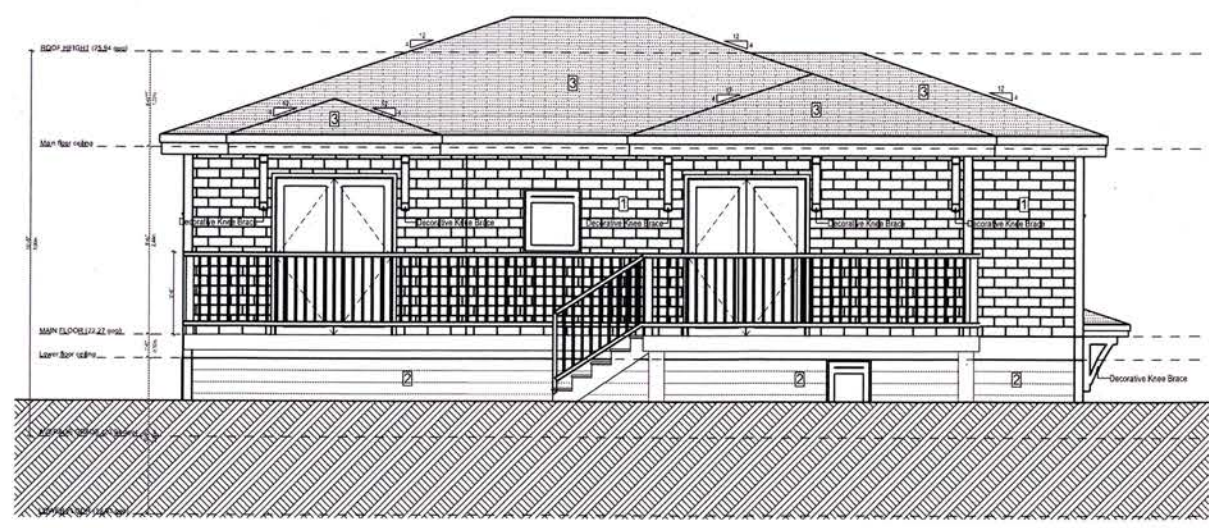
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1 FRONT ELEVATION
SCALE: 1/4" = 1'-0"



2 RIGHT ELEVATION
SCALE: 1/4" = 1'-0"



3 REAR ELEVATION
SCALE: 1/4" = 1'-0"



4 LEFT ELEVATION
SCALE: 1/4" = 1'-0"

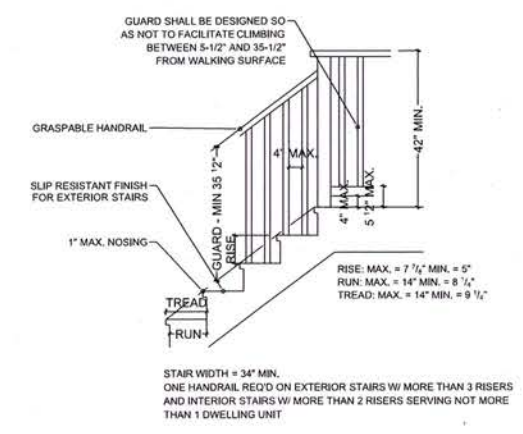
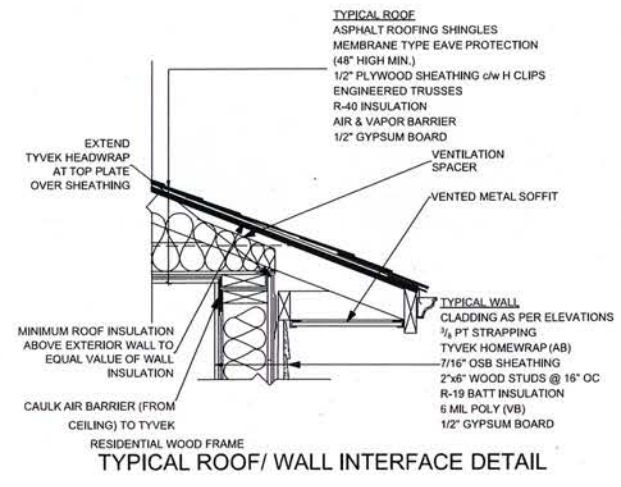
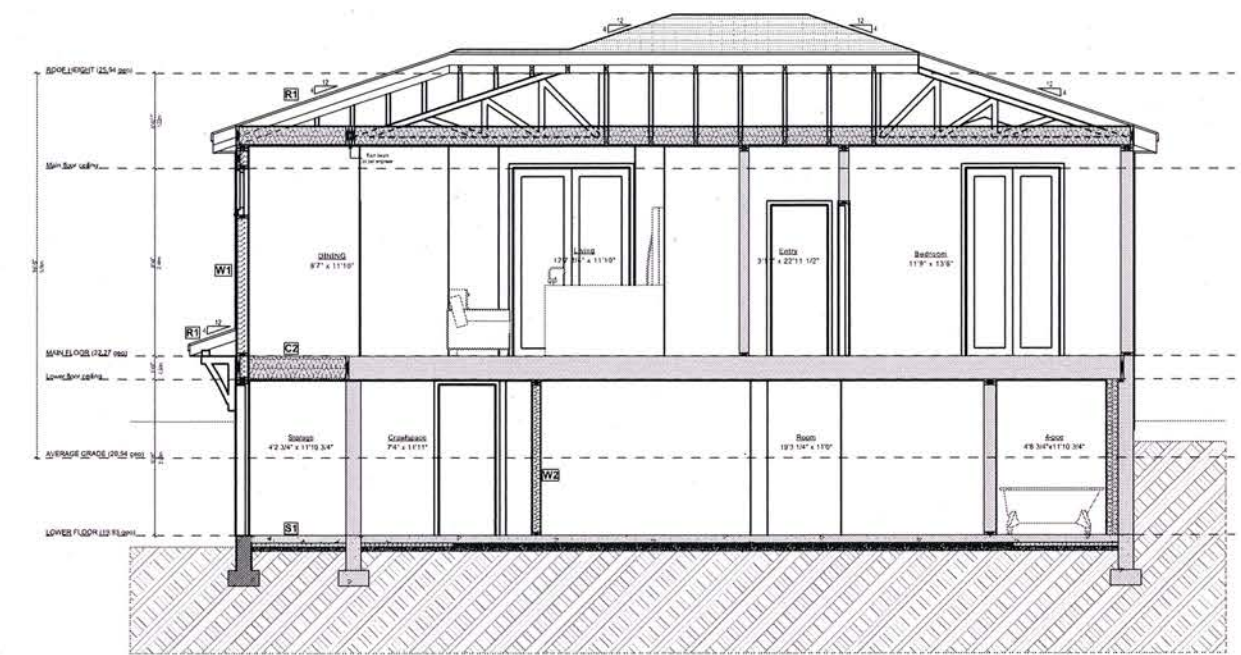
LIMITING DISTANCE BCBC 9.10.15.4
LIMITING DISTANCE = 1.97m
EXPOSING BUILDING FACE = 26.95m²
TOTAL PROPOSED AREA OF OPENINGS = 1.06m²
PROPOSED % OPENINGS = 3.93%
BCBC T9.10.15.4
MAX. ALLOWABLE AREA OF GLAZED OPENINGS = 8%

ISSUED FOR
PERMIT

ISSUED:

Elevations

A201



1 SECTION
A301 SCALE: 1/4" = 1'-0"

SECTION LEGEND

CEILING TYPES

C1 - FLOOR ABOVE CRAWLSPACE (NOT SHOWN)
FINISHED FLOORING
1/2" TAG PLYWOOD
FLOOR JOISTS AS PER ENGINEER
CROSS BRIDGING

C2 - INTERIOR FLOOR
FINISHED FLOORING
1/2" TAG PLYWOOD
FLOOR JOISTS AS PER ENGINEER
CROSS BRIDGING
1/2" GYPSUM BOARD PAINTED

ROOF TYPES

R1 - JOIST ROOF
ASPHALT ROOFING SHINGLES
1/2" PLYWOOD CW/H CLIPS
JOISTS AS PER ENGINEER
R20 BATT INSULATION
6 MIL POLY (VB)
1/2" GYPSUM BOARD PAINTED
ROOF VENTED 1:150

SLAB TYPES

S1 - BASEMENT SLAB
4" THICK CONCRETE SLAB
6 MIL POLY
2-1/2" XPS RIGID INSULATION 4' PERIMETER
2" THERMAL BREAK @ SLAB EDGE
COMPACTED 1/2" MIN. U.S.
UNDISTURBED SOIL

S2 - GROUND SEAL (NOT SHOWN)
2" CONCRETE GROUND SEAL
6 MIL POLY
UNDISTURBED SOIL

WALL TYPES

W1 - EXTERIOR WALL
CLADDING AS PER ELEVATIONS
1/2" P.T. STRAPPING FASTENED TO FRAMING
TYVEK HOMEWRAP (AB)
1/2" OSB SHEATHING OR AS PER ENGINEER
2X4 STUDS @ 16" O.C.
R-19 (COMPRESSED) BATT INSULATION
6 MIL POLY (VB)
1/2" GYPSUM BOARD PAINTED

W2 - INTERIOR WALL
1/2" GYPSUM BOARD PAINTED
2X4 STUDS @ 24" O.C.
1/2" GYPSUM BOARD PAINTED

W3 - CRAWLSPACE WALL (NOT SHOWN)
R12 FOIL BACK INSULATION
8" THICK CONCRETE WALL
DAMP PROOFING
BACKFILL

NEW WALL
EXISTING WALL TO REMAIN
NEW FOUNDATION WALL
EXISTING FOUNDATION WALL TO REMAIN

RECEIVED
JAN 22 2019
CORP. OF TOWNSHIP
OF ESQUIMALT
DEVELOPMENT SERVICES

**BCBC 9.36
PRESCRIPTIVE PATH
CLIMATE ZONE 4**

ASSEMBLY DESCRIPTION

EFF. RSI	
TRUSS CEILING	6.91 RSI
CATHEDRAL CEILING & FLAT ROOF	4.67 RSI
EXTERIOR WALLS	2.78 RSI
FLOORS OVER GARAGE/UNHEATED SPACE	4.51 RSI
WALL @ GARAGE	2.62 RSI
HEATED CONCRETE SLABS	2.32 RSI
CONCRETE SLABS	1.96 RSI
FOUNDATION WALL BELOW GRADE	1.99 RSI

EXTERIOR WALL EFFECTIVE THERMAL RESISTANCE

INTERIOR AIR FILM	0.12 RSI
GYPSUM BOARD	0.08 RSI
2X6 STUD	1.19 RSI
7/16" OSB SHEATHING	0.11 RSI
AIR SPACE	0.15 RSI
WOOD SIDING	0.18 RSI
OUTSIDE AIR FILM	0.03 RSI
TOTAL EFF. R VALUE =	1.86 RSI @ 23% WALL AREA

INTERIOR WALL EFFECTIVE THERMAL RESISTANCE

INTERIOR AIR FILM	0.12 RSI
GYPSUM BOARD	0.08 RSI
R20 INSULATION	3.52 RSI
7/16" OSB SHEATHING	0.11 RSI
AIR SPACE	0.15 RSI
WOOD SIDING	0.18 RSI
OUTSIDE AIR FILM	0.03 RSI
TOTAL EFF. R VALUE =	4.19 RSI @ 77% WALL AREA

EFFECTIVE THERMAL RESISTANCE = **3.27 RSI**
REQUIRED EFFECTIVE THERMAL RESISTANCE = 2.78 RSI

VAULTED CEILING EFFECTIVE THERMAL RESISTANCE

INTERIOR AIR FILM	0.11 RSI
GYPSUM BOARD	0.08 RSI
2X10 RAFTERS	2.0 RSI
EXTERIOR AIR FILM	0.03 RSI
TOTAL EFF. R VALUE =	2.22 RSI @ 13% CEILING

INTERIOR CEILING EFFECTIVE THERMAL RESISTANCE

INTERIOR AIR FILM	0.11 RSI
GYPSUM BOARD	0.08 RSI
R20 BATT INSULATION	3.52 RSI
R12 BATT INSULATION	2.11 RSI
OUTSIDE AIR FILM	0.03 RSI
TOTAL EFF. R VALUE =	5.85 RSI @ 87% CEILING

EFF. THERMAL RESISTANCE = **4.82 RSI**
REQUIRED EFF. THERMAL RESISTANCE = 4.67 RSI

TRUSS ROOF EFFECTIVE THERMAL RESISTANCE

INTERIOR AIR FILM	0.11 RSI
GYPSUM BOARD	0.08 RSI
3-1/2" BOTTOM CHORD	0.76 RSI
OUTSIDE AIR FILM	0.03 RSI
TOTAL EFF. R VALUE @ 11% =	0.98 RSI

INTERIOR ROOF EFFECTIVE THERMAL RESISTANCE

INTERIOR AIR FILM	0.11 RSI
GYPSUM BOARD	0.08 RSI
3-1/2" BLOWN INSULATION	1.67 RSI
OUTSIDE AIR FILM	0.03 RSI
TOTAL EFF. R VALUE @ 89% =	1.89 RSI

EFFECTIVE THERMAL INSULATION @ CAVITY = 1.71 RSI
12" BLOWN FG ABOVE FRAMING = 5.63 RSI

TOTAL EFF. THERMAL RESISTANCE = **7.34 RSI**
REQUIRED EFF. THERMAL RESISTANCE = 6.91 RSI

FLOOR OVER UNHEATED SPACE EFFECTIVE THERMAL RESISTANCE

INTERIOR AIR FILM	0.11 RSI
FLOORING	0.12 RSI
3/4" SHEATHING	0.16 RSI
2X10 JOISTS	2.0 RSI
EXTERIOR AIR FILM	0.03 RSI
WOOD SOFFIT	0.12 RSI
TOTAL EFF. R VALUE =	2.54 RSI @ 13% FLOOR AREA

INTERIOR FLOOR EFFECTIVE THERMAL RESISTANCE

INTERIOR AIR FILM	0.11 RSI
FLOORING	0.12 RSI
3/4" SHEATHING	0.16 RSI
R28 BATT INSULATION	4.93 RSI
EXTERIOR AIR FILM	0.03 RSI
WOOD SOFFIT	0.12 RSI
TOTAL EFF. R VALUE =	5.47 RSI @ 87% FLOOR AREA

EFF. THERMAL RESISTANCE = **4.75 RSI**
REQUIRED EFF. THERMAL RESISTANCE = 4.67 RSI

FLOORS OVER GARAGE EFFECTIVE THERMAL RESISTANCE

INTERIOR AIR FILM	0.16 RSI
WOOD FLOORING	0.12 RSI
SUB FLOOR	0.16 RSI
R28 INSULATION	4.93 RSI
GYPSUM BOARD	0.08 RSI
INTERIOR AIR FILM	0.11 RSI
TOTAL EFF. R VALUE =	5.56 RSI @ 87%

INTERIOR GARAGE EFFECTIVE THERMAL RESISTANCE

INTERIOR AIR FILM	0.16 RSI
WOOD FLOORING	0.12 RSI
SUB FLOOR	0.16 RSI
2X10 FLOOR JOISTS	1.99 RSI
GYPSUM BOARD	0.08 RSI
INTERIOR AIR FILM	0.03 RSI
TOTAL EFF. R VALUE =	2.46 RSI @ 13%

EFF. THERMAL RESISTANCE = **4.77 RSI**
REQUIRED EFF. THERMAL RESISTANCE = 4.51 RSI

EXTERIOR WALL EFFECTIVE THERMAL RESISTANCE

INTERIOR AIR FILM	0.12 RSI
GYPSUM BOARD	0.08 RSI
2X6 STUD	1.19 RSI
7/16" OSB SHEATHING	0.11 RSI
AIR SPACE	0.15 RSI
WOOD SIDING	0.18 RSI
OUTSIDE AIR FILM	0.03 RSI
TOTAL EFF. R VALUE =	1.86 RSI @ 23% WALL AREA

INTERIOR WALL EFFECTIVE THERMAL RESISTANCE

INTERIOR AIR FILM	0.12 RSI
GYPSUM BOARD	0.08 RSI
R20 INSULATION	3.52 RSI
7/16" OSB SHEATHING	0.11 RSI
AIR SPACE	0.15 RSI
WOOD SIDING	0.18 RSI
OUTSIDE AIR FILM	0.03 RSI
TOTAL EFF. R VALUE =	4.19 RSI @ 77% WALL AREA

EFFECTIVE THERMAL RESISTANCE = **3.27 RSI**
REQUIRED EFFECTIVE THERMAL RESISTANCE = 2.79 RSI

WALL @ GARAGE EFFECTIVE THERMAL RESISTANCE

INTERIOR AIR FILM	0.12 RSI
GYPSUM BOARD	0.08 RSI
POLYETHYLENE	NIL
2X6 STUD	1.19 RSI
GYPSUM BOARD	0.08 RSI
INTERIOR AIR FILM	0.12 RSI
TOTAL EFF. R VALUE =	1.59 RSI @ 23% WALL AREA

INTERIOR WALL EFFECTIVE THERMAL RESISTANCE

INTERIOR AIR FILM	0.12 RSI
GYPSUM BOARD	0.08 RSI
POLYETHYLENE	NIL
R20 INSULATION	3.52 RSI
GYPSUM BOARD	0.08 RSI
INTERIOR AIR FILM	0.12 RSI
TOTAL EFF. R VALUE =	3.92 RSI @ 77% WALL AREA

EFFECTIVE THERMAL RESISTANCE = **2.91 RSI**
REQUIRED EFFECTIVE THERMAL RESISTANCE = 2.62 RSI

BASEMENT SLAB ABOVE FROST LINE EFFECTIVE THERMAL RESISTANCE

INTERIOR AIR FILM (FLOOR)	0.16 RSI
CONCRETE SLAB	0.04 RSI
RADIANT IN FLOOR HEATING	N/A
2-1/2" XPS	2.15 RSI
EFF. THERMAL INSULATION =	2.35 RSI (R13.3)
REQUIRED EFF. THERMAL INSULATION =	1.96 RSI (R13.2)

BASEMENT HEATED FLOOR EFFECTIVE THERMAL RESISTANCE

INTERIOR AIR FILM (FLOOR)	0.16 RSI
CONCRETE SLAB	0.04 RSI
RADIANT IN FLOOR HEATING	N/A
2-1/2" XPS	2.15 RSI
EFF. THERMAL RESISTANCE =	2.35 RSI
REQUIRED EFF. THERMAL RESISTANCE =	2.32 RSI

THERMAL BREAK BETWEEN SLAB AND FOUNDATION WALL EFFECTIVE INSULATION

1-1/2" XPS	1.32 RSI
50% REQUIRED HEATED CONCRETE SLAB	2.35 RSI X 50% = 1.18 RSI REQUIRED
EFF. THERMAL INSULATION =	1.32 RSI
REQUIRED EFF. THERMAL INSULATION =	1.18 RSI

CRAWLSPACE FOUNDATION WALLS EFFECTIVE INSULATION

INTERIOR AIR FILM (FLOOR)	0.16 RSI
R12 FOIL BACK INSULATION	0.04 RSI
8" THICK CONCRETE WALL	2.11 RSI
EFF. THERMAL RESISTANCE =	2.31 RSI
REQUIRED EFF. THERMAL RESISTANCE =	1.99 RSI

FOUNDATION WALL BELOW GRADE INTERIOR FURRING WALL

200mm CONCRETE	0.08 RSI
1/2" AIR SPACE	0.16 RSI
2X4 @ 24" OC FRAMING (13%)	0.76 RSI
R12 FG BATTS (87%)	2.11 RSI
1/2" GYPSUM BOARD	0.08 RSI
INTERIOR AIR FILM	0.12 RSI
ACTUAL EFF. THERMAL INSULATION =	2.22 RSI
REQUIRED EFF. THERMAL INSULATION MIN. =	1.99 RSI

FOUNDATION WALL BELOW GRADE EXTERIOR INSULATION

200mm CONCRETE	0.08 RSI
2-1/2" XPS CONTINUOUS INSULATION	2.15 RSI
INTERIOR AIR FILM	0.12 RSI
ACTUAL EFF. THERMAL RESISTANCE =	2.35 RSI
REQUIRED EFF. THERMAL RESISTANCE MIN. =	1.99 RSI



1500 Shorncliffe Road Victoria BC V8P 2T4
250.893.8127
www.adaptdesign.ca

1336 Wood St,
Esquimalt, BC

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The General Contractor is responsible for confirming and correlating dimensions at the job site. The Designer will not be responsible for construction means, methods, techniques, sequences, or procedures, or for safety precautions and programs in connection with the project.

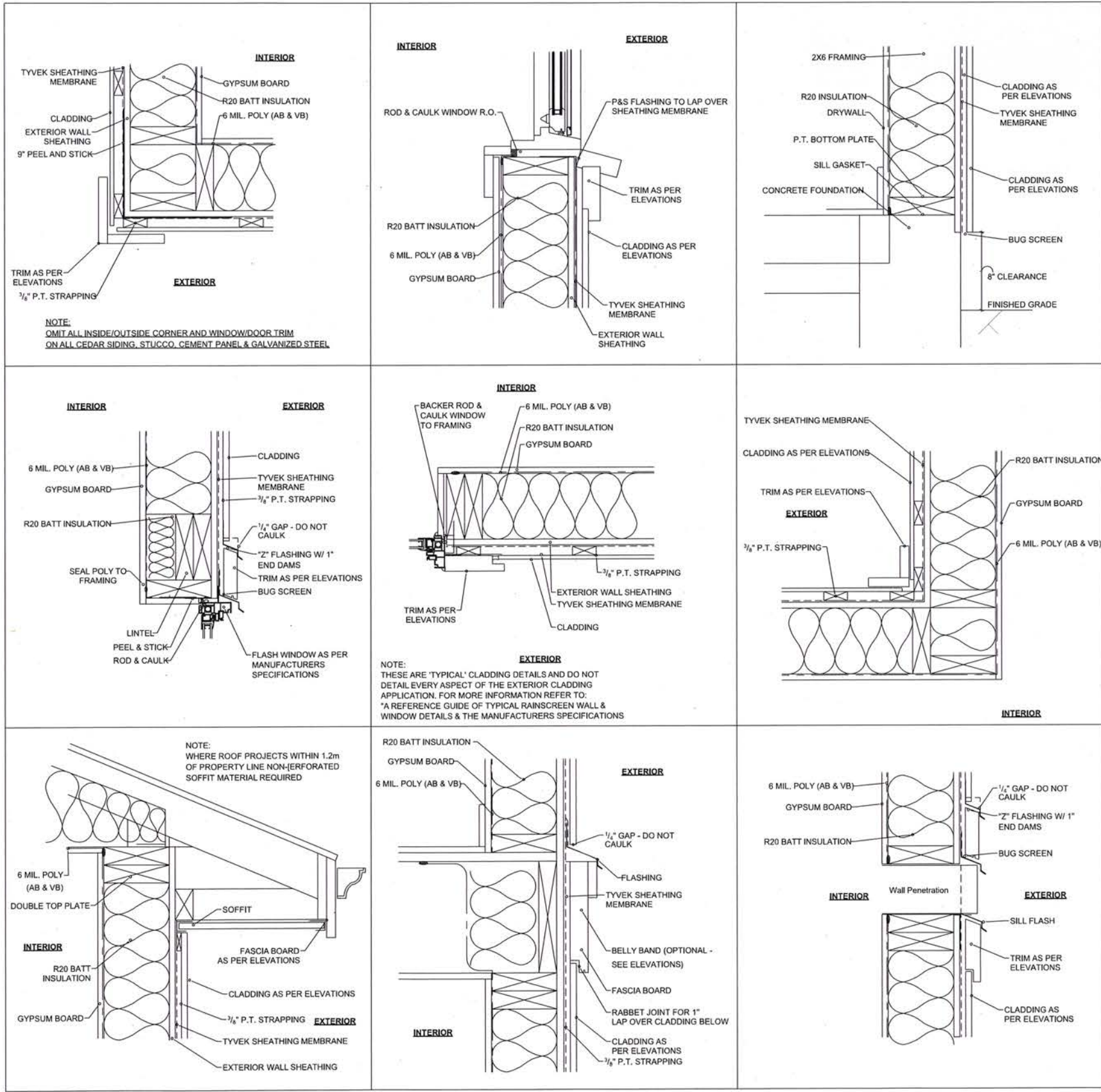
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ISSUED FOR PERMIT

ISSUED:

Section

A301



1 CLADDING DETAILS
A401 NOT TO SCALE

ISSUED FOR PERMIT

ISSUED:

Site Plan Of:
Lots 59 And 58, Suburban Lot 37,
Esquimalt District, Plan 2854.
P.I.D. 006-375-294
P.I.D. 006-375-324



Scale = 1:250

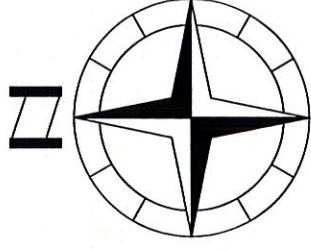
Dated this 22th day of July, 2018.

Distances and elevations shown are in metres.

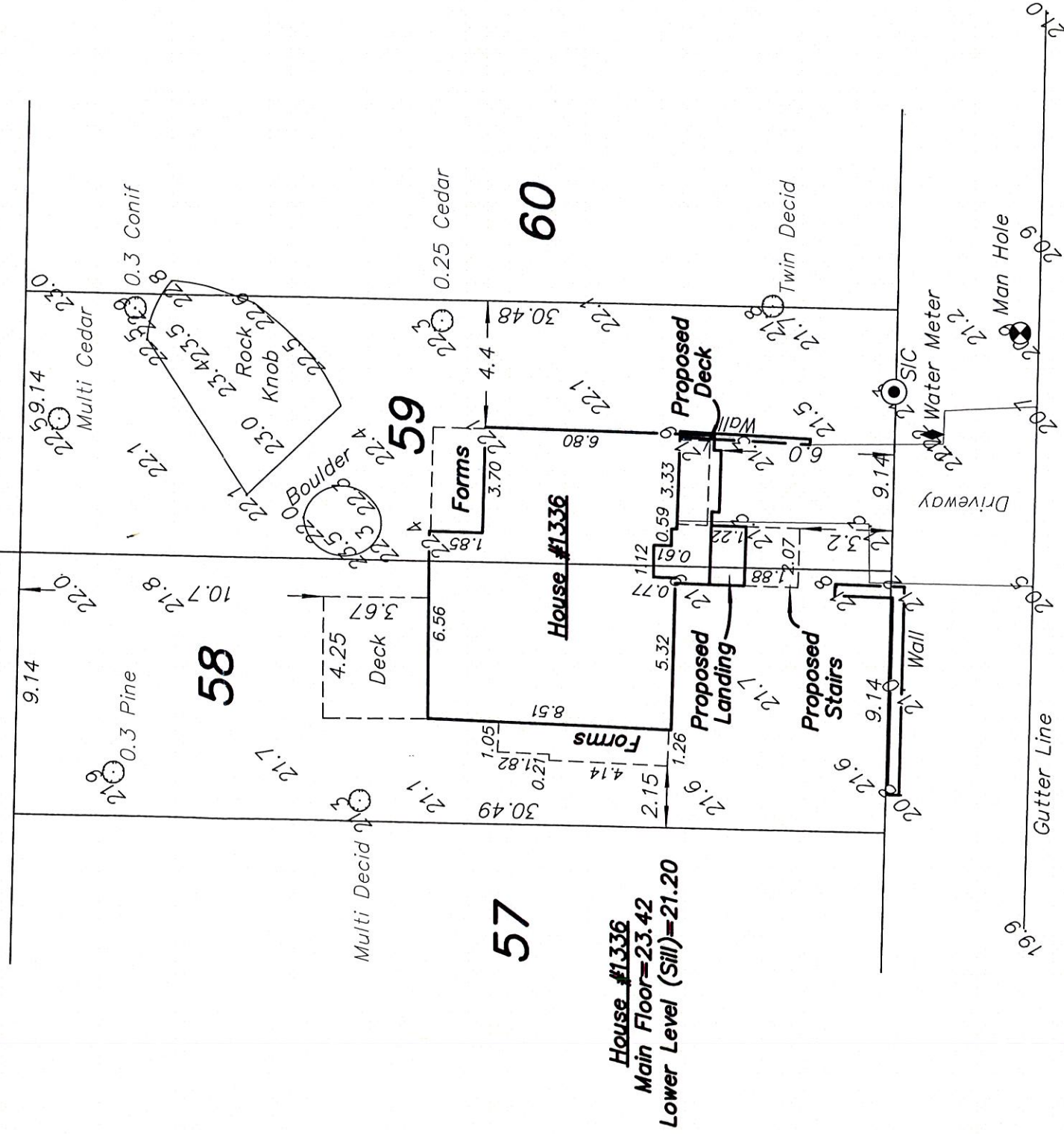
Elevations are based on geodetic datum CVD28BC and derived from OCM 84H0153.

This site plan is for building and design purposes and is for the exclusive use of our client.

This document shows the relative location of the surveyed structures and features with respect to the boundaries of the parcel described above. This document shall not be used to define property lines or property corners.



A
Plan 29057
Strata
Plan VIS3741



Wood Street



Wey Mayenburg Land Surveying Inc.
www.weysurveys.com
 #4-2227 James White Boulevard
 Sidney, BC V8L 1Z5
 Telephone (250) 656-5155
 File: 180218\SIT\GH



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

APC Meeting: March 19, 2019

STAFF REPORT

DATE: March 15, 2019

TO: Chair and Members of the Advisory Planning Commission

FROM: Alex Tang, Planner
Bill Brown, Director of Development Services

SUBJECT: Official Community Plan Amendment and Rezoning Application
616 Constance Avenue
[PID 000-713-465 Lot 95, Suburban Lot 44, Esquimalt District, Plan 2854]
620 Constance Avenue
[PID 000-819-832 Lot 4, Suburban Lot 44, Esquimalt District, Plan 3135]
619 Nelson Street
[PID 006-393-608 Lot 84, Suburban Lots 44 and 45, Esquimalt District,
Plan 2854]
623 Nelson Street
[PID 006-278-647 Lot 3, Suburban Lot 44, Esquimalt District, Plan 3135]
and 1326 Miles Street
[PID 006-375-723 Lot 96, Suburban Lot 44, Esquimalt District, Plan 2854]
[PID 006-375-693 Lot 85, Suburban Lot 44, Esquimalt District, Plan 2854]

RECOMMENDATION:

That the Esquimalt Advisory Planning Commission recommends that the application for an Official Community Plan Amendment and Rezoning, authorizing a 45 metre [12 storeys], 109 unit, commercial mixed use, multiple family residential building, sited in accordance with the BCLS Site Plan provided by McElhanney Consulting Services, Ltd., stamped "Received March 12, 2019", and incorporating height and massing consistent with the architectural plans provided by DIALOG, stamped "Received March 12, 2019", detailing the development proposed to be located at 616 Constance Avenue [PID 000-713-465 Lot 95, Suburban Lot 44, Esquimalt District, Plan 2854], 620 Constance Avenue [PID 000-819-832 Lot 4, Suburban Lot 44, Esquimalt District, Plan 3135], 619 Nelson Street [PID 006-393-608 Lot 84, Suburban Lots 44 and 45, Esquimalt District, Plan 2854], 623 Nelson Street [PID 006-278-647 Lot 3, Suburban Lot 44, Esquimalt District, Plan 3135], and 1326 Miles Street [PID 006-375-723 Lot 96, Suburban Lot 44, Esquimalt District, Plan 2854] + [PID 006-375-693 Lot 85, Suburban Lot 44, Esquimalt District, Plan 2854] be forwarded to Council with a recommendation **to either approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

BACKGROUND:

Purpose of the Application:

The applicant is requesting a change in Official Community Plan Proposed Land Use Designation from the current designation of High Density Residential to Commercial/Commercial Mixed-Use, and a change in zoning from the current RM-1 [Multiple Family Residential] to a Comprehensive Development District zone [CD]. The change in Official Community Plan Proposed Land Use Designation is required to accommodate the commercial uses proposed. The change in zoning is required to accommodate the proposed 12 storey, commercial mixed use, multiple family residential building including 109 residential units and a 111 space parking garage.

Evaluation of this application should focus on issues related to zoning such as the proposed height, density, massing, proposed unit sizes, siting, setbacks, lot coverage, usable open space, parking, uses, fit with the neighbourhood, and consistency with the overall direction contained within the Official Community Plan.

This site is located within Development Permit Area No. 1 - Natural Environment, No. 6 - Multi-Family Residential, No. 7 - Energy Conservation and Greenhouse Gas Reduction and No. 8 - Water Conservation of the Township's Official Community Plan. Should the rezoning be approved, the form and character of the buildings, landscaping, and consistency with guidelines relating to natural environment protection, energy conservation, greenhouse gas reduction, and water conservation would be controlled by a Development Permit that would be considered by Council at a future date.

Context

Applicant: Sierra Land Construction Inc. [Troy Grant]

Owners: 1174489 B.C. LTD.,INC.NO. BC01174489
[616 Constance Ave, 620 Constance Ave, 619 Nelson St, and 623 Nelson St]
Rashpal Singh Basi and Jasbir Basi [1326 Miles St]

Property Size: Metric: 2627 m² Imperial: 28277 ft²

Existing Land Use: Single Family Residential
[616 Constance Ave, 620 Constance Ave, 619 Nelson St,
and 623 Nelson St]
Four-Unit Residential
[1326 Miles St]

Surrounding Land Uses:

North: Single Family Residential
South: Multiple Family Residential [2 storeys]
Multiple Family Residential Townhouses [3 storeys]
West: Single Family Residential
Multiple Family Residential [1 storey]
East: Multiple Family Residential Apartment [3 storeys]

Current OCP Proposed Land Use Designation: High Density Residential
Amended OCP Proposed Land Use Designation: Commercial/Commercial Mixed-Use

Existing Zoning: RM-1 [Multiple Family Residential]
Proposed Zoning: CD [Comprehensive Development District]

Official Community Plan

The applicant is applying to amend the Proposed Land Use Designation from 'High Density Residential' to 'Commercial/Commercial Mixed-Use' in order to accommodate the proposed commercial uses. In commercial mixed-use areas, buildings with a floor area ratio of up to 3.0 for the residential portion of the building may be acceptable. The proposed development consists of a total Floor Area Ratio of 3.37 with the residential portion accounting for 3.21 in Floor Area Ratio. Hence, this proposal is inconsistent with the acceptable density prescribed in the Official Community Plan. In order to achieve consistency with the Official Community Plan, the applicant is proposing to provide amenities through density bonusing for the benefit of the community. These amenities have not yet been detailed.

OCP Section 3.3 Housing and Community identifies land lying north of Esquimalt Road between Admirals Road and CFB Esquimalt as an area for residential densification. The Plan also supports the provision of a variety of housing types and tenures delivered by both market and non-market developers to support the housing needs of a diverse demographic.

OCP Section 5.1 states a policy to 'support the development of a variety of housing types and designs to meet the anticipated housing needs of residents. This may include non-market and market housing options that are designed to accommodate young and multi-generational families, the local workforce, as well as middle and high income households.'

OCP Section 5.3 Medium and High Density Residential Development states an objective to support compact, efficient medium density and high density residential development that integrates with existing proposed adjacent uses.

Supporting policies in this section consistent with the proposed development include:

- Encourage new medium density and high density residential development with high quality design standards for building and landscaping and which enhance existing neighbourhoods.
- Prioritize medium density and high density residential development in proposed land use designated areas that:
 1. reduce single occupancy vehicle use;
 2. support transit service;
 3. are located within close proximity to employment centres; and
 4. accommodate young families.
- A mix of dwelling unit sizes should be provided in medium density and high density residential land use designated areas in order to meet the varying housing needs of Esquimalt residents.
- Encourage the incorporation of spaces designed to foster social interaction.
- Encourage the installation of electric vehicle charging infrastructure in medium and high density residential developments.

Section 5.4 of the Official Community Plan states an objective to encourage a range of housing by type, tenure and price so that people of all ages, household types, abilities and incomes have a diversity of housing choice in Esquimalt. Through the provision of affordable, special needs or seniors housing, the proposed development would be consistent with the following policies in this section:

- Encourage the placement of new rental, affordable, special needs, and seniors housing

in accordance with designated residential land use areas as they are integral components of inclusive neighbourhoods.

- Consider bonus density, parking relaxations or other development variances where a development proposal includes affordable, special needs or seniors housing. This may apply to both market and non-market housing, and mixed-use proposals. A housing agreement may be entered into between the Township and the owner.

Currently, the applicant has not detailed the amenities provided for the bonus density. Nonetheless, the applicant is contemplating the provision of affordable, special needs or seniors housing.

Section 5.6 Family and Child-friendly Housing states an objective to address the shortage of family and child friendly housing in Esquimalt. The proposed development has a mixture of dwelling unit sizes, including 11 3-bedroom apartment dwelling units and 6 3-bedroom townhouse dwelling units, along with a commercial space labelled 'Daycare' proposed for Group Children's Day Care Centre Use which would be consistent with the following policy:

- Encourage the provision of medium and high density commercial mixed-use developments designed for families with children.

Section 6.1 Commercial & Commercial Mixed-Use Land Use states an objective to establish Esquimalt as a complete community where commercial enterprises serve the needs of area residents, local businesses, and visitors.

As the proposed development has a floor area ratio of 3.21 for the residential portion of the development, it is inconsistent with the following policy:

- In commercial mixed-use areas, buildings with a floor area ratio of up to 3.0 for the residential portion of the building may be acceptable.

However, the following policies address the use of density bonus for this proposed development in order to achieve consistency with the Official Community Plan:

- Consider, where appropriate, development proposals with densities greater than those set out in the OCP through density bonus of floor-space provided that the additional density results in the provision of community amenities deemed appropriate by Council for the benefit of the community.
- Recognize, for the purposes of density bonuses, "amenities" may include but are not limited to:
 1. Privately-owned, publicly-accessible open space;
 2. Public art;
 3. Contributions towards the enhancement of public recreation facilities;
 4. Contributions towards street and boulevard enhancements, including street furniture and decorative lighting;
 5. Building to a higher step of the BC Energy Step Code than required under the Building Bylaw;
 6. Group daycare and respite for children and adults;
 7. Preservation of heritage structures, features or assets;
 8. Affordable housing units;
 9. Special needs housing units;
 10. Community gardens;
 11. Enhanced green family play space for residents;
 12. Public space improvements supporting and surrounding transit stations; and
 13. Other as may be appropriate to the development proposal or surrounding community as deemed appropriate by Council.

The proposed development is also generally consistent with the following policies of this section:

- Encourage a mix of uses in the commercial/commercial mixed-use areas.
- The majority of the frontage for commercial mixed-use buildings at the ground floor, should be commercial
- On corner sites, the ground floor commercial use in mixed-use buildings should wrap around the corner.
- In commercial/commercial mixed-use developments, parking should be located behind or under the building.
- The installation of electric vehicle charging infrastructure in commercial/commercial mixed-use developments is encouraged.
- Encourage the provision of amenities such as mini parks/plazas, street furniture, public art and decorative lighting on private lands in all commercial areas.
- To encourage the use of bicycles, provisions should be made in all commercial/commercial mixed-used developments for bicycle parking for employees and visitors.

Section 11.3.1 Public Cycling Infrastructure states the following policy:

- Encourage end-of-trip facilities including secure lockup and shower facilities

Section 11.3.2 New Development states the following policy:

- Encourage developers to provide a variety of end of trip facilities for active transportation.
- Encourage bike lockers in multi-unit residential and commercial/commercial mixed-use developments.

Section 13.3.3 Building Energy Efficiency states the following policy:

- Adopt best practices based on evolving building technologies and materials.
- Encourage the adoption of passive, efficient, and renewable energy systems in new buildings and during building retrofits
- Investigate options for encouraging developers to achieve high energy performance in new developments through such tools as density bonusing, expedited permit approval process, rebate of development fees, revitalization tax exemption, and other incentives.
- Pursue higher energy-efficiency performance in new developments, through the achievement of higher steps in the BC Energy Step Code as an amenity associated with rezoning.

Under Section 13.3.6 Passenger Vehicle Alternatives, the following policies are listed:

- Encourage the installation of electric vehicle charging infrastructure in all new multi-unit developments.
- Pursue the installation of electric vehicle charging capacity in new developments during the rezoning process.
- Encourage the inclusion of car share in new multi-unit residential developments.

Relevant Development Permit Area Guidelines to consider as it relates to the rezoning application include:

- Avoid disturbing, compacting and removing areas of natural soil as this can lead to invasion by unwanted plant species, poor water absorption and poor establishment of new plantings. Use of local natural soil in disturbed and restored areas will support re-establishment of ecosystem functions.

- Buildings should be designed and sited to minimize the creation of shadows on public spaces.
- Off-street parking areas should be located either at the rear of commercial buildings or underground. Surface parking should be screened with landscaping. Large parking areas should contain additional islands of landscaping.
- The size and siting of buildings that abut existing single- and two-unit and townhouse dwelling should reflect the size and scale of adjacent development and complement the surround uses. To achieve this, height and setback restrictions may be imposed as a condition of the development permit.
- New buildings should be designed and sited to minimize visual intrusion on to the privacy of surround homes and minimize the casting of shadows on to the private outdoor space of adjacent residential units.
- High-density multi-unit residential buildings or mixed commercial/residential buildings in commercial areas should be designed so that the upper storeys are stepped back from the building footprint, with lower building heights along the street front to address human scale, public space, and maximum light penetration at street level.
- Underground parking should be encouraged for any multi-unit residential buildings exceeding four storeys.
- Orient buildings to take advantage of site specific climate conditions, in terms of solar access and wind flow; design massing and solar orientation for optimum passive performance.
- Build new developments compactly, considering the solar penetration and passive performance provided for neighbouring sites, and avoid shading adjacent to usable outdoor open spaces.
- In commercial, residential or commercial mixed-use designated areas with taller developments, vary building heights to strategically reduce the shading on to adjacent buildings.

Zoning

Density, Lot Coverage, Height and Setbacks: The following chart lists the floor area ratios, lot coverage, setbacks, height, parking, and usable open space of this proposal. Zoning Bylaw, 1992, No. 2050 does not currently contain a zone that can accommodate this proposed development.

	Proposed Comprehensive Development Zone
Residential Units	109
Residential Floor Area Ratio	3.21
Total Floor Area Ratio (including Commercial Area)	3.37
Lot Coverage	81.1%
Lot Coverage at or above the First Storey	56.5%
Setbacks	
• Front [West]	3.0 m
• Rear [East]	6.0 m
• Interior Side [North]	6.0 m
• Interior Side [South]	6.0 m

Building Height	45 m [12 storeys]
Off Street Parking	111 spaces
Usable Open Space	256 m ² [9.7%]
Bicycle Parking	129 resident + 10 visitor

Floor Area Ratio: The FAR of this proposal is 3.37 where the residential portion of the building has a FAR of 3.21. This is greater than the acceptable amount of 3.0 for the residential portion of a building in a commercial mixed-use area.

Lot Coverage: The lot coverage at or above the First Storey of 56.5% is a significant change and increase from the currently allowed 40% within a RM-1 zone that accommodates low density townhouse development and the allowed 30% for RM-4/RM-5 zones that accommodate medium density apartment developments. Staff has concerns with the substantial lot coverage as it relates to the development's massing.

Usable Open Space: Our zones that accommodate apartment developments generally require usable open space in the amount of not less than 7.5% of the area of the parcel. This development allows for an usable open space north of the Principal Building in the amount of 256 m² [9.7% of the consolidated parcels].

Parking: Parking Bylaw, 1992, No. 2011 requires 1.3 parking spaces per unit to be provided for multiple family developments. Parking areas are required to be constructed to meet the standards for manoeuvring aisle dimensions and associated parking stall dimensions detailed in Part 14, Table 2, of the Bylaw.

This proposal incorporates 111 parking spaces to serve 109 residential dwelling units, in addition to 4 commercial units with an area of 439 square metres. In mixed use buildings with a commercial and residential component, there is a separate parking requirement for each component. The parking requirement for the commercial component is based on the type of commercial use. For 'Retail Sales of goods and services', 1 parking space is required for every 25 square metres. For 'Business and Professional Offices', 1 parking space is required for every 30 square metres. Hence, the proposed 439 square metres of commercial space will require 14-18 parking spaces according to the parking bylaw.

In addition to varying the required parking spaces, the following variances are required to the Parking Bylaw No. 2011 based on the current parking plan:

- Reduction of the width of a two way manoeuvring aisle accessing two banks of parking from 7.6 metres to 6.5 metres
- Reduction of the width of a two way manoeuvring aisle accessing one bank of parking from 6.75 metres to 6.4 metres
- Reduction of the width of a 90 degree small car parking stall from 2.6 metres to 2.3 metres
- Reduction of the requirement where any parking space abuts any portion of a fence or structure, the minimum stall width shall be increased by 0.3 metres from 0.3 metres to 0.0 metres.

Currently, 9 of the parking spaces that abut a wall have not been increased by the required 0.3 metres.

The current parking layout does not have disabled nor visitor parking spaces labelled. The parking bylaw requires 1 disabled persons' parking stall for every 50 and 1 visitor's parking stall for every 4.

As there is an application for an OCP Amendment to allow for the commercial units, Parking Bylaw 1992, No. 2011 13(5) states that 'Where all of the following criteria are met in a commercial or industrial building, the off-street requirement of Section 13(1) may be reduced by a maximum of two spaces:

- 2 or more secure bicycle parking spaces are provided on-site
- Shower and change rooms are provided within the building
- 6 visitor parking spaces are provided on-site
- The building is located within 200 metres of a regional bus route.

Green Building Features

The applicant has completed the Esquimalt Green Building Checklist [attached].

Comments From the Design Review Committee

This application was considered at the regular meeting of the Design Review Committee held on January 9, 2019.

Members had concerns with the height and massing as it is different from the rest of the neighbourhood. Members had questions regarding plans for the density bonus amenities and the applicant responded that they are still undecided and seeking input from the community. The Committee had concerns with the parking for the commercial component and the parking relaxations in general. The Committee also had concerns with the limited open green space and landscaping.

The Design Review Committee resolved that the application be forwarded to Council with a recommendation of denial because it does not fit within the context of the neighbourhood. (4 for and 2 opposed)

Comments from Other Departments

The plans for this proposal were circulated to other departments and the following comments were received:

Community Safety Services: Building to be constructed to requirements of BC Building Code 2012 and Municipal Building Code Bylaw. Plans will be reviewed for compliance with BC Building Code upon submission of a Building Permit application.

Engineering Services: Engineering staff has completed a preliminary evaluation of Works and Services that would be required for the proposed 109 unit multiple family residential building. Staff confirms that the design appears achievable on the site and that appropriate works and services are available in the immediate area. If approved, the development must be serviced in accordance with bylaw requirements including, but not limited to, new sewer and drain connections, underground hydro, telephone and cable services and new road works may be required up to the centre line of Constance Avenue and Nelson Street. Should the application be approved, additional comments will be provided when detailed civil engineering drawings are submitted as part of a Building Permit application.

Parks Services: Parks staff has completed a preliminary review of the proposed on-site and off-site landscaping and commented that a tree cutting permit application is required for all trees to be removed. All trees that are to be retained, including boulevard trees, must have tree protection fencing erected at the drip line.

Fire Services: Fire Services staff has completed a preliminary review of the proposed plans and recommended that:

- Hydro lines from Esquimalt Road to northern portion of the proposed development property line along Nelson Street to be buried.
- Hydro lines from Esquimalt Road to northern portion of the proposed development property line along Constance Avenue to be buried.
- Fire flow calculations must be done to determine if sufficient water supply is available in the area before a building permit application is submitted. If it is determined that the area lacks sufficient water supply to address fire flow requirements, it is expected that the water mains along both Nelson Street and Constance Avenue be upgraded from Esquimalt Road to the most northern portion of the development property line before any construction of wood framing above grade commences, with additional hydrants if required. The method for increasing water supply for firefighting purposes must be completed in consultation with Victoria Water Works Department, the developer's fire protection system engineer and the Esquimalt Fire Department.
- A construction fire safety plan must be submitted at the same time as the building permit application.
- Tree planting along all building frontage must be designed not to impede fire truck aerial use and operations. The developer must consult with the Township of Esquimalt Parks Services and Fire Services to determine appropriate species at full growth.
- If heavy timber concept is the method of construction to be used and is not permitted within the current BC Building Code for the size of the building, the developer must obtain approvals (SSR) from BC Building and Safety Standards Branch.
- Fire department to be consulted on building and unit addressing.

Public Notification

As this is an Official Community Plan Amendment and Rezoning application, should it proceed to a Public Hearing, notices would be mailed to tenants and owners of properties within 100m (328ft) of the subject property. In order to satisfy the requirements of the *Local Government Act*, a notice to relevant government and institutional stakeholders within the Capital Region would be required. Three signs indicating that the property is under consideration for a change in Zoning have been installed on the Constance Avenue frontage. In addition, three signs have been installed on the Nelson Street frontage and one sign has been installed on the Miles Street frontage. These signs would be updated to include the date, time, and location of the Public Hearing.

ALTERNATIVES:

1. Forward the application for Official Community Plan Amendment and Rezoning to Council with a **recommendation of approval including reasons for the recommendation.**
2. Forward the application for Official Community Plan Amendment and Rezoning to Council with a **recommendation of approval including specific conditions and including reasons for the recommendation.**
3. Forward the application for Official Community Plan Amendment and Rezoning to Council with a **recommendation of denial including reasons for the recommendation.**



Sussex St

Nelson St

Miles St

Constance Ave

633

626

625

627

622

623

620

621

1319

618

619

616

611

612

1326

608

4-601

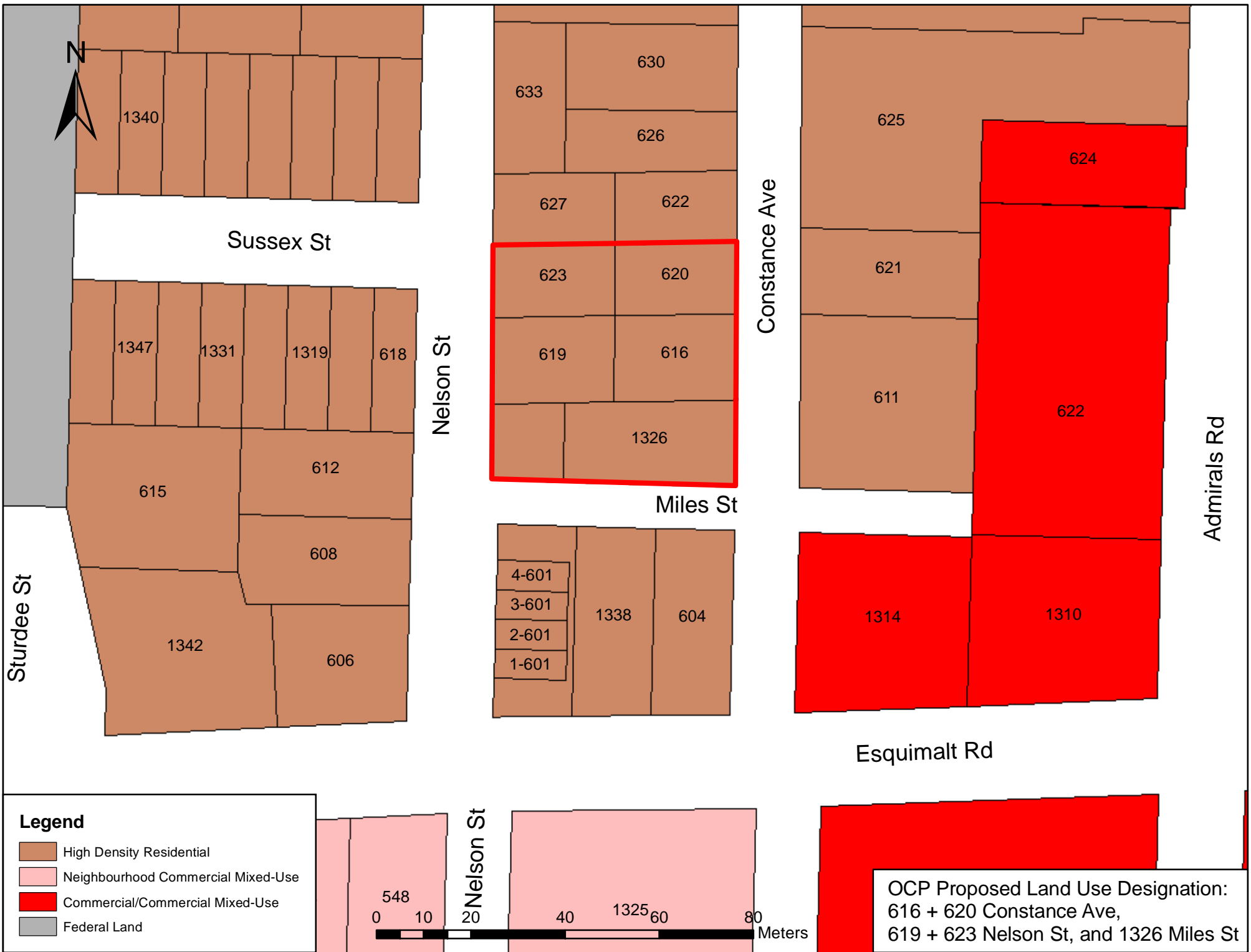
1338

604

3-601

Subject Property Map:
616 + 620 Constance Ave,
619 + 623 Nelson St, and 1326 Miles St





1340

Sussex St

1347

1331

1319

618

615

612

608

1342

606

633

630

626

627

622

623

620

619

616

1326

Constance Ave

625

624

621

611

622

Miles St

4-601

3-601

2-601

1-601

1338

604

Admirals Rd

Esquimalt Rd

548

0

10

20

40

1325

60

80

Meters

Nelson St



RM-4

1340

P-1

Sussex St

1347

1331

1319

618

Nelson St

RM-1

615

612

608

RM-4

1342

606

Sturdee St

633

630

626

627

622

RM-1

623

620

619

616

1326

Constance Ave

RM-4

625

C-3

624

621

611

CD-82

622

Miles St

4-601

3-601

2-601

1-601

1338

604

RM-4

C-3

1314

1310

Admirals Rd

Esquimalt Rd

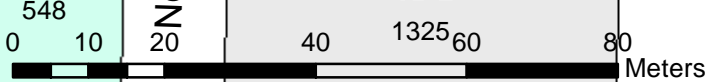
RM-1

548

Nelson St

CD-2

1325



Meters

5

HOUSING & RESIDENTIAL LAND USE



5.1 General: Anticipated Housing Needs in the Next Five Years

Table 1 and Figure 7 show the housing statistics for Esquimalt based on the 2011 and 2016 Federal Censuses. Note that there are significant differences in the trends for each housing type. Single detached houses decreased by 1.7 percent while apartments in buildings that have five or more stories increased by 23.1 percent. Table 1 clearly shows a trend toward multi-family dwellings and away from single detached dwellings.

TABLE 1. PERCENT CHANGE IN HOUSING TYPES BETWEEN 2011 AND 2016.

Dwelling Type	Number of Dwelling Units - 2011 Census	Number of Dwelling Units - 2016 Census	Percent Change 2011 to 2016
Single Detached House	2030	1995	-1.7
Apartment in building that has five or more stories	455	560	23.1
Semi-detached house	665	690	3.8
Row house	385	405	5.2
Apartment or flat in duplex	795	910	14.5
Apartment in a building that has fewer than five storeys	3695	3910	5.8
Other single - attached house	15	15	0.0
Total	8040	8485	5.5

Table 2 contains simple arithmetic population projection estimates based on various growth rate scenarios. The net increase in population under each scenario is then used to estimate the number of new housing units that will be required in the next five years based on an average household size of 2.0 as per the 2016 Federal Census.

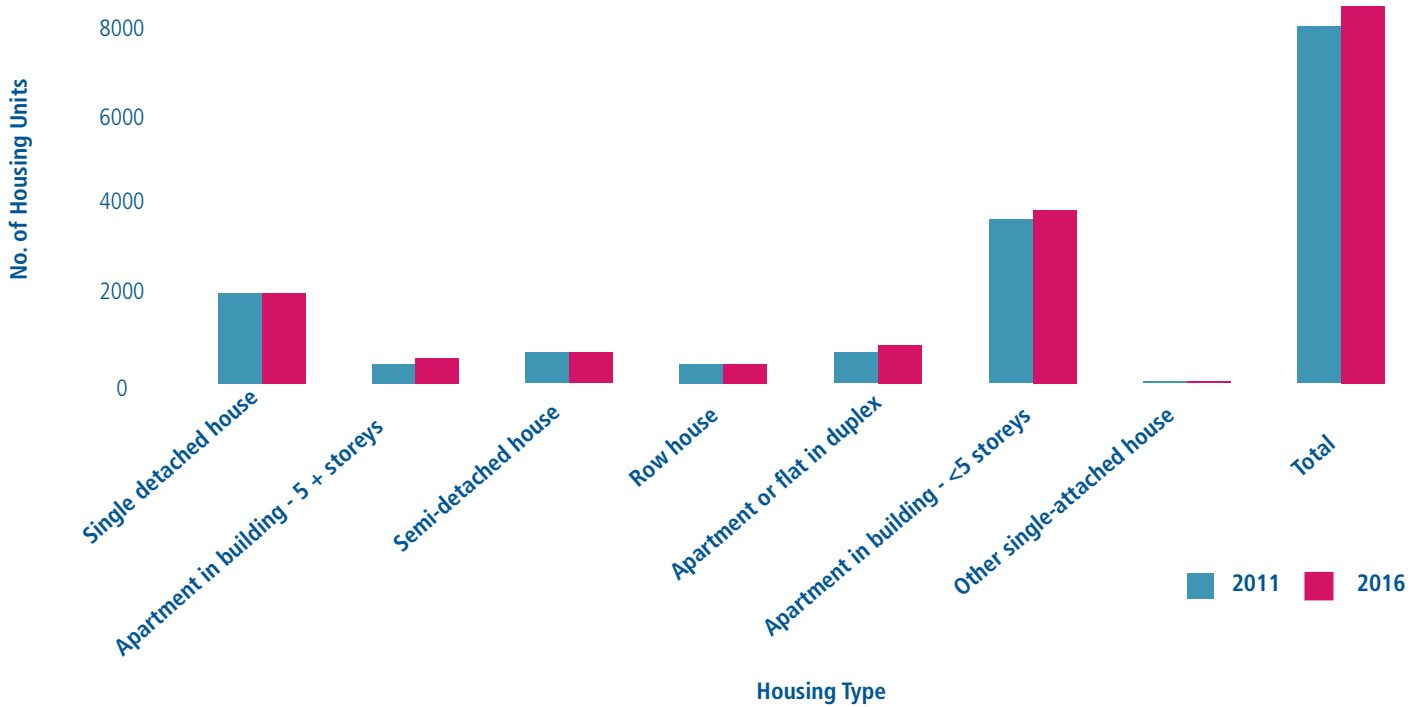


Figure 7. Percent change in housing types between 2011 and 2016

TABLE 2 POPULATION GROWTH SCENARIOS FOR THE NEXT FIVE YEARS AND ESTIMATED NUMBER OF NEW HOUSING UNITS REQUIRED.

2016 Population	Rate of Growth Over Five Years (%)	Estimated 2021 Population	Increase in Population	Estimated Number of New Housing Units Required at an Average of 2.0 People Per Unit
17,665	2.5	18107	442	221
17,665	5	18548	883	442
17,665	7.5	18990	1325	662
17,665	10	19432	1767	883

Table 3 summarizes the number of units in all developments where Council has approved the zoning but the developments have not received an occupancy permit. The data in the table indicates that 664 housing units of various forms and tenures have been approved but have not yet receive an occupancy permit. The current approvals would be sufficient to accommodate a population increase of approximately 7.5 percent over the next five years based on an average household size of 2.0. It should be noted that in order to maintain a balanced housing market the supply of housing should exceed the demand for housing. Given the amount of land designated for multi-unit development as indicated in the Future Land Use Designation Map that is not yet yielding it maximum potential number of dwelling units, the Township has more than adequate land to accommodate its housing needs for the next five years at growth rates beyond 10 percent, assuming that zoning approvals coincide with the maximum development potential allowed by the OCP.

TABLE 3. ESTIMATED NUMBER OF HOUSING UNITS THAT HAVE RECEIVED ZONING APPROVAL BUT NOT AN OCCUPANCY PERMIT.

Dwelling Type	Location	Number of Proposed Units - Rental	Number of Proposed Units - Owner	Density (units per net ha.)	Status
Single Detached House (Infill)	Various (infill)		7	21.6	
Apartment in building that has five or more stories	Esquimalt Town Square	34	68	130	Development permit approved
	Legion	152		415	Zoning approved
	English Inn		173	165	Development permit approved
	856 Esquimalt Road		168	230	Zoning approved
Semi-detached house	Various		4	29.5	Ongoing
	English Inn		6	40	Development permit approved
Apartment or flat in duplex	Various				
Apartment in a building that has fewer than five storeys	615 Fernhill		10	130	Zoning approved
	Triangle Lands			160	Zoning approved
	826 Esquimalt Road		30	330	Under construction
Other single - attached house	West Bay				
Total		186	478		

OBJECTIVE: Support expansion of housing types within Esquimalt while addressing concerns such as tree protection, parking, traffic, noise, effects on neighbouring properties, and neighbourhood character.

Policy

Support the inclusion of secondary suites within present and proposed low density residential land use designated areas.

Policy

Support the inclusion of detached accessory dwelling units on appropriate low density residential land use designated areas where only one principal dwelling unit exists.

Policy

Consider new townhouse residential proposals with a Floor Area Ratio of up to 0.70, and up to three storeys in height, in areas designated Townhouse Residential on the “Proposed Land Use Designation Map,” provided the design responds effectively to both its site and surrounding land uses.

Policy

Recognize that float homes provide an alternative housing choice, and support the continued existence of Esquimalt’s only float home community located in West Bay, as designated on the “Present Land Use Designation Map” and “Proposed Land Use Designation Map.”

Policy

Support the inclusion of live/work units on residential parcels located outside of areas designated commercial on the “Proposed Land Use Designation Map” and on Major Roads as designated on the “Road Network Map” (Schedule “C”).

Policy

Recognize short-term accommodation use as appropriate within designated residential land use areas only where appropriate zoning has been sought and secured.

Policy

Support the development of a variety of housing types and designs to meet the anticipated housing needs of residents. This may include non-market and market housing options that are designed to accommodate young and multi-generational families, the local workforce, as well as middle and high income households.

Policy

Encourage the development of rental accommodation designed for a variety of demographic household types, including young families.

5.2 Low Density Residential Redevelopment

OBJECTIVE: Strive for redevelopment and infill development that improves and enhances the appearance and livability of neighbourhoods and the community as a whole.

Policy

Proposed redevelopment or infill within present low density residential land use designated areas should be built to high quality design and landscaping standards and respond sensitively to existing neighbourhood amenities.

Policy

Consider the inclusion of secondary suites in infill developments where it is demonstrated that neighbourhood impacts can be mitigated.

Policy

Discourage new applications for infill housing, including rezoning and subdivision, for panhandle lots in the 1100 and 1200 blocks of Old Esquimalt Road and the 600 block of Fernhill Road.

5.3 Medium and High Density Residential Development

OBJECTIVE: Support compact, efficient medium density and high density residential development that integrates with existing and proposed adjacent uses.

Policy

Encourage new medium density and high density residential development with high quality design standards for building and landscaping and which enhance existing neighbourhoods.

Policy

Prioritize medium density and high density residential development in proposed land use designated areas that:

1. reduce single occupancy vehicle use;
2. support transit service;
3. are located within close proximity to employment centres; and
4. accommodate young families.

Policy

Consider new medium density residential development proposals with a Floor Area Ratio of up to 2.0, and up to six storeys in height, in areas designated on the “Proposed Land Use Designation Map.”

Policy

Consider new high density residential development proposals with a Floor Area Ratio of up to 3.0, and up to 12 storeys in height, in areas designated on the “Proposed Land Use Designation Map.”

Policy

Notwithstanding other policies set out in this OCP, maximum heights in medium density and high density residential land use designated areas in West Bay are limited to those parcel heights identified in the West Bay Development Permit Area.

Policy

Consider, where appropriate, development proposals with densities greater than those set out in the OCP through density bonus of floor-space provided that the additional density results in the provision of community amenities deemed appropriate by Council for the benefit of the community.

Policy

Recognize, for the purposes of density bonuses, “amenities” may include but are not limited to:

1. Privately-owned, publicly-accessible open space;
2. Public art;
3. Contributions towards the enhancement of public recreation facilities;
4. Contributions towards street and boulevard enhancements, including street furniture and decorative lighting;
5. Building to a higher step of the BC Energy Step Code than required under the Building Bylaw;
6. Group daycare and respite facilities for children and adults;
7. Preservation of heritage structures, features or assets;
8. Affordable housing units;
9. Special needs housing units;
10. Community gardens;
11. Enhanced green family play space for residents;
12. Public space improvement supporting and surrounding transit stations; and
13. Other as may be appropriate to the development proposal or surrounding community as deemed appropriate by Council.

Policy

A mix of dwelling unit sizes should be provided in medium density and high-density residential land use designated areas in order to meet the varying housing needs of Esquimalt residents.

Policy

Buildings with shallow setbacks should step down to no more than three storeys at street level in order to provide an appropriate human scale along the sidewalk.

Policy

Encourage the incorporation of spaces designed to foster social interaction.

Policy

Encourage the installation of electric vehicle charging infrastructure in medium and high density residential developments.

5.4 Affordable Housing

OBJECTIVE: To encourage a range of housing by type, tenure and price so that people of all ages, household types, abilities and incomes have a diversity of housing choice in Esquimalt.

Policy

Encourage the provision of affordable housing by the private market and the non-profit housing sector. Partnerships between private, public or non-market housing providers may be supported. These might include innovative approaches such as limited equity, rent-to-own, co-op, mixed market and non-market projects.

Policy

Encourage the placement of new rental, affordable, special needs, and seniors housing in accordance with designated residential land use areas as they are integral components of inclusive neighbourhoods.

Policy

Avoid the spatial concentration of affordable and special needs housing in neighbourhoods.

Policy

Promote housing agreements to ensure that all strata units have the opportunity to be used as long-term residential rentals within strata buildings.

Policy

Consider offering a Revitalization Tax Exemption to all new or renovated purpose-built multi-unit rental buildings within the Township.

Policy

Consider bonus density, parking relaxations or other development variances where a development proposal includes affordable, special needs or seniors housing. This may apply to both market and non-market housing, and mixed-use proposals. A housing agreement may be entered into between the Township and the owner.

Policy

Work in co-operation with the provincial and federal governments, the real estate community, social service agencies, faith-based organizations, service clubs and other community resources to provide affordable housing and support services for seniors, the frail elderly and other special needs groups.

Policy

Encourage the provision of missing middle housing types such as two-unit dwellings (duplexes), townhouses and small lot infill as one avenue to address housing affordability.

5.5 Age-friendly Housing

OBJECTIVE: To expand and protect seniors housing in Esquimalt to enable citizens to “age in place.”

Policy

Support and facilitate development of multi-generational housing, including in medium and high density residential developments.

Policy

Encourage child friendly developments that provide appropriate amenities such as outdoor play areas for young children that are well-separated from traffic circulation and parking areas.

Policy

Encourage adaptable design for all dwellings created through rezoning.

Policy

Encourage more accessible housing for people with mobility limitations on the ground floor of medium and high density residential buildings.

Policy

Support aging-at-home through means such as low or no fee permits for modifications.

Policy

Encourage development of more assisted living residences.

Policy

Promote the development of a complex care facility within Esquimalt.

Policy

Promote modifying homes to enable shared housing for seniors.

Policy

Encourage the development of seniors housing that is within close proximity and accessible to services and amenities.

Policy

Recognize that the provision of bicycle storage may be waived or varied if, in the opinion of Council, there is no demonstrated need, such as in a congregate care facility.

5.6 Family and Child-friendly Housing

Objective: To address the shortage of family and child friendly housing in Esquimalt.

Policy

Encourage the provision of medium and high density commercial mixed-use developments designed for families with children

6

COMMERCIAL & COMMERCIAL MIXED-USE LAND USE



6.1 General

The neighbourhood commercial mixed use and Commercial/Commercial Mixed-use Areas are designated on the “Present and Proposed Land Use Designation Maps” (Schedules “A” and “B”).

OBJECTIVE: Esquimalt is a complete community where commercial enterprises serve the needs of area residents, local businesses, and visitors.

Policy

Encourage a mix of uses in the commercial/commercial mixed-use areas.

Policy

Exclusive commercial development is appropriate in commercial/commercial mixed-use areas.

Policy

All commercial/commercial mixed-use areas are designated Development Permit Areas, in order to ensure that future development and infill contributes positively to the visual and aesthetic character of its site, setting and surrounding properties.

Policy

Lands outside the Commercial/Commercial Mixed-use designation on the “Proposed Land Use Designation Map” should not be considered for commercial zoning unless the following criteria are met:

1. The project is desirable to the community and cannot be appropriately located within established commercial areas;
2. The density and scale of the project is sensitive to the prevailing character of surrounding lands;
3. The project, through its exterior finishes enhances the aesthetics of the neighbourhood;
4. The project’s parking requirements can be satisfied on-site; and
5. The project’s parking requirements should not unduly affect neighbouring land uses.

Policy

In commercial mixed-use areas, buildings with a floor area ratio of up to 3.0 for the residential portion of the building may be acceptable.

Policy:

Consider, where appropriate, development proposals with densities greater than those set out in the OCP through density bonus of floor-space provided that the additional density results in the provision of community amenities deemed appropriate by Council for the benefit of the community.

Policy

Recognize, for the purposes of density bonuses, “amenities” may include but are not limited to:

1. Privately-owned, publicly-accessible open space;
2. Public art;
3. Contributions towards the enhancement of public recreation facilities;
4. Contributions towards street and boulevard enhancements, including street furniture and decorative lighting;
5. Building to a higher step of the BC Energy Step Code than required under the Building Bylaw;
6. Group daycare and respite facilities for children and adults;
7. Preservation of heritage structures, features or assets;
8. Affordable housing units;
9. Special needs housing units;
10. Community gardens;
11. Enhanced green family play space for residents;
12. Public space improvements supporting and surrounding transit stations; and
13. Other as may be appropriate to the development proposal or surrounding community as deemed appropriate by Council.

Policy

The majority of the frontage for commercial mixed-use buildings at the ground floor, should be commercial.

Policy

On corner sites, the ground floor commercial use in mixed-use buildings should wrap around the corner.

Policy

In commercial/commercial mixed-use developments, parking should be located behind or under the building.

Policy

The installation of electric vehicle charging infrastructure in commercial/commercial mixed-use developments is encouraged.

Policy

Encourage the provision of amenities such as mini parks/plazas, street furniture, public art and decorative lighting on private lands in all commercial areas.

Policy

To encourage the use of bicycles, provision should be made in all commercial/commercial mixed-use developments for bicycle parking for employees and visitors.

6.2 Revitalization

OBJECTIVE: To encourage growth through revitalization and redevelopment of commercial areas.

Policy

Encourage public and private sector initiatives to improve streetscapes and accessibility for pedestrians and cyclists to all commercial areas.

Policy

Design and install entrance signage at key street locations, including entrances to Esquimalt.

6.3 Esquimalt Town Centre

OBJECTIVE: To foster the creation of an Esquimalt Town Centre that successfully integrates commercial, institutional and residential activity.

Policy

Esquimalt Town Centre is located along Esquimalt Road, between Constance Avenue and Joffre Street.

Policy

Esquimalt Town Centre will continue to be the principal commercial area within Esquimalt.

Policy

Encourage a range of retail stores, services, professional offices, community services, recreation, institutional, educational, and arts / cultural uses that encourage activity throughout the day and evening.

Policy

Encourage high quality development that supports and reinforces Esquimalt Town Centre as the main commercial, civic and recreational service centre. Designs are encouraged which incorporate a pedestrian orientation and include such features as sidewalk cafes, attractive landscaping, public art and other amenities.

Policy

Encourage high density mixed-use commercial/residential complexes in Esquimalt Town Centre.

1100, 2 Bloor Street East
Toronto, Ontario M4W 1A8
TEL 416 966 0220

100, 10237-104th Street
Edmonton, Alberta T5J 1B1
TEL 780 429 1580

300, 134-11th Avenue SE
Calgary, Alberta T2G 0X5
TEL 403 245 5501

406, 611 Alexander Street
Vancouver, BC V6A 1E1
TEL 604 255 1169

500 Sansome Street - Suite 370
San Francisco, CA 94111-3215
TEL 628 444 6130



23 November 2018

Alex Tang, Planning Officer
Township of Esquimalt
Development Services
1229 Esquimalt Road
Esquimalt BC
V9A 3P1



Re: 'Shoaling Heights'; Rezoning Application for 616-620 Constance, 619-623 Nelson and 1326 Miles

Dear Alex,

We are pleased to submit the enclosed initial Rezoning application for the above noted site(s). This application represents a request to amend the current RM-1 zoning to a CD Zone for the purposes of establishing a twelve-storey mixed-use residential development complete with one hundred and three apartment units (ten of which will address supportive housing needs), six family-sized walk-up townhomes, daycare space and professional office/commercial space.

The site's proximity to the junction of Esquimalt Road and Admirals Road, its nearness to CFB Esquimalt, and its immediate access to multiple transit modes, retail and natural amenities establishes it as an important gateway into the Township's envisioned 'High Density Residential' zone. The assembly of five smaller lots collectively bounded by Nelson, Miles and Constance afforded an opportunity to create a well-considered urban project that both effectively anchors the block and enlivens the street edge(s). Key design drivers to this end included a respect for topography, a keen consideration of views to and from the project, a passive responsiveness to solar aspect/shading, a desire to temper how the building meets the property line, and an approach to designing the ground plane that anticipates incorporation of a culturally significant naval monument.

Shoaling Heights is targeted at middle-income earning families/individuals in Esquimalt with a mix of unit sizes ranging from studios to three-bedroom and den layouts. While these units have been distributed around a central elevator core, the floorplates have been staggered to break down the building massing. This latter move has the added advantage of maximizing the number of corner units and number of units with views to the northwest, creating distinct and separate podium rooftop amenity spaces, emitting natural light into the common corridors, and reducing the overall building's exposure to late afternoon heat. The structure itself is being designed as mass-timber and will contribute to the growing acceptance of sustainable and regional building practices in the residential development industry.

We are excited by how this project might contribute to the Township of Esquimalt's vision as captured in your Official Community Plan and look forward to engaging with you over the coming months.

Yours truly,

DIALOG® BC Architecture Engineering Interior Design Planning Inc.

Per:

Vance Harris Architect AIBC AAA MRAIC LEED® AP
Principal

PRINCIPALS

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CRAIG APPLEGATH, AIBC
NARESH ARORA, P. ENG.
JOOST BAKKER, AIBC
ALAN BONIFACE, AIBC
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GERALD CARSON, P. ENG.

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DIEGO MORETTIN, AIBC
MARTIN NIELSON, AIBC
NEIL ROBSON, P. ENG.
MICHELE SIGURDSON, IDIBC

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CAMERON VERES, AIBC
LYNN WEBSTER, AIBC
THOMAS WU, P. ENG.
ROD YEOH, P. ENG.

: DENOTES ARCHITECT AIBC



Lorvatto Mixed-use Development:
*616 & 620 Constance Avenue,
619 & 623 Nelson Street and
1326 Miles Street*
Transportation Impact Assessment
Draft Report

Prepared for
K&M Group

Date
October 12, 2018

Project No.
04-18-0379

October 12, 2018
04-18-0379

Troy Grant
President
K&M Group
#102, 28 Mission Avenue
St. Albert, Alberta
T8N 1H4

Dear Mr. Grant:

**Re: Lorvatto: 616 Constance Avenue
Draft TIA Report**

K&M Group is proposing to redevelop the properties at 616 and 620 Constance Avenue, 619 and 623 Nelson Street, and 1326 Miles Street in Esquimalt, BC. The development is planning on providing approximately 110 residential units and 5,000 square feet of commercial space. The development requires a Transportation Impact Assessment (TIA) to support the proposed parking requirements and to confirm the development would not worsen the surrounding transportation network.

As requested, Bunt has prepared a draft TIA report covering the development plan review, a traffic & safety review and recommendations.

We trust this information will be helpful for your application approval. Please let us know if you have any questions or comments on the enclosed report.

Yours truly,
Bunt & Associates

Simon Button, P.Eng.
Transportation Engineer

Kate Worthy, Co-op Student
Transportation Analyst

CORPORATE AUTHORIZATION

Prepared By: Simon Button, P.Eng.

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TABLE OF CONTENTS

EXECUTIVE SUMMARY	1
1. INTRODUCTION	1
1.1 Study Scope and Objectives	1
1.2 Development Details	1
2. LOCAL CONTEXT	3
2.1 Land Use.....	3
2.2 Street Network	3
2.3 Walking and Cycling	3
2.4 Transit	3
3. DEVELOPMENT PLAN REVIEW	4
3.1 Vehicle Parking	4
3.1.1 Bylaw Requirement.....	4
3.1.2 Proposed Supply	4
3.2 Bicycle Parking	6
3.3 Access	6
4. TRAFFIC OPERATIONS REVIEW	7
4.1 Traffic Operations Assessment Methodology	7
4.2 Existing Conditions	7
4.3 Future Conditions	8
4.3.1 Background Traffic.....	8
4.3.2 Development Generated Traffic	8
4.3.3 Traffic Operations Results	9
5. SUMMARY AND RECOMMENDATIONS	15
5.1 Summary	15
5.2 Recommendations	15

EXHIBITS

Exhibit 1.1: Study Area	2
Exhibit 4.1: Existing Vehicle Volumes - PM Peak Hour	10
Exhibit 4.2: Existing Traffic Operations - PM Peak Hour	11
Exhibit 4.3: Site Volumes - PM Peak Hour	12
Exhibit 4.4: 2030 Total Traffic Forecast - PM Peak Hour	13
Exhibit 4.5: 2030 Future Traffic Operations - PM Peak Hour	14

TABLES

Table 4.1: PM Peak Hour Vehicle Trip Generation	8
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EXECUTIVE SUMMARY

K&M Group (the developer) is proposing to redevelop the properties at 616 and 620 Constance Avenue, 619 and 623 Nelson Street, and 1326 Miles Street in Esquimalt, BC. The development is planning on providing approximately 110 residential units and 5,000 square feet of commercial space.

The development is located on Constance Avenue, north of Miles Street, which is less than 100 metres northwest of Esquimalt Village which is one of the four mixed-use commercial clusters identified in the Township of Esquimalt's Official Community Plan. The site is well serviced with transit, quality cycling infrastructure and has a variety of commercial and service amenities within walking distance.

The development has shown initiative to encourage non-vehicle travel. The development will be providing at least one secure bicycle storage space per dwelling and is considering a variety of other transportation demand management measures related to cycling and transit.

The Esquimalt Parking Bylaw (Bylaw 2011) requires the proposed building to provide 1.3 vehicle parking spaces per multi-family dwelling unit for a total of 143 parking spaces for the residential component and 155 parking spaces for the entire mixed-use development (including 12 spaces for commercial uses). The development is seeking a parking variance to provide 111 spaces (a reduction of 44 spaces) given the Township's high Bylaw requirement for residential visitor parking and the development's ability to share parking between the multiple land uses. This is a reasonable parking supply given the large number of key destinations that can be reached from the development site by walking, cycling and transit and the development is committed to providing a variety of transportation demand management measures to reduce parking demand and traffic generation.

The intersections of Esquimalt Road with Admirals Road and Nelson Street intersection currently operate within acceptable capacity thresholds during the PM peak hour. The intersections are forecasted to continue operating well for the 2030 horizon year including increases in background traffic and traffic generated by the proposed development.

1. INTRODUCTION

1.1 Study Scope and Objectives

K&M Group (the developer) is proposing to redevelop the properties at 616 and 620 Constance Avenue, 619 and 623 Nelson Street, and 1326 Miles Street in Esquimalt, BC. The site location is shown in **Exhibit 1.1**.

The development is located approximately less than 100 metres northwest of the Esquimalt Village which is one of four commercial mixed-use clusters in Esquimalt. The existing properties are zoned as multi-unit residential. Although still early in the development process, it is currently envisioned to have:

- 100 market residential units;
- 10 non-market residential units; and,
- 5,000 square feet of commercial space with potential tenants being a daycare and/or medical/dental offices.

The purpose of this study is to:

- Review the development's parking strategy and determine its suitability; and,
- Evaluate the transportation impacts the proposed development has on the nearby road network;

1.2 Development Details

The development proposes to have 110 residential units with 40% of the units containing one-bedroom (44 units), 50% containing 2 bedrooms (55 units) and 10% containing 3 bedrooms (11 units). Approximately 10 of the units will be for people with special needs and/or offered at below-market rates.

The driveway to the underground parkade will be located on Nelson Street. In an effort to minimize traffic impacts on Constance Avenue, the development is proposing to convert Miles Street into a one-way street westbound between Constance Avenue and Nelson Street.

The development is committed to limit vehicle use and encourage residents to walk, cycle and use transit. The development plans on providing a car share opportunities to residents, enhanced bicycle parking facilities and/or bus passes. Electric car charging stations will also be provided to enable more sustainable vehicle use.



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**Exhibit 1.1
Site Location**

Lorvatto (616 Constance Avenue)
04-18-0379 October 2018



2. LOCAL CONTEXT

2.1 Land Use

The site is located near the southern end of Constance Avenue which is approximately 85 metres northwest of Esquimalt Village. Esquimalt Village has a series of commercial buildings with variety of retail stores and restaurants, and other daily services. Increased densification has occurred in Esquimalt Village recently with a number of ongoing development projects helping to make it a more compact, walkable community.

2.2 Street Network

Constance Avenue and Nelson Street are classified as local streets whereas the nearby Admirals Road and Esquimalt Road are classified as Major Roads. Since Nelson Street terminates with a cul-de-sac, it primarily serves local residents. Admirals Road and Esquimalt Road provide north/south and east/west connectivity across Esquimalt.

2.3 Walking and Cycling

The majority of nearby local streets do not have sidewalks on either side whereas the major roads (Admirals Road and Esquimalt Road) have sidewalks on both sides. Crosswalks are provided on all four legs of the Admirals Road & Esquimalt Road intersection. Admirals Road has limited pedestrian crossing opportunities however Esquimalt Road has crosswalks every 100 to 200 metres in Esquimalt Village in addition to the pedestrian crossing at Constance Avenue for increased pedestrian permeability.

Admirals Road has painted bike lanes in both directions in the vicinity of the development site. Esquimalt Road has painted bike lanes in both directions beginning 200 metres east of Admirals Road, continuing eastwards to the Johnson Street Bridge in the City of Victoria and westwards approximately 3.5 kilometres.

The site is approximately 750 metres from the E&N Regional Trail, which currently extends from Esquimalt Road in the east to the Old Island Highway at the junction joining with the Galloping Goose Regional Trail.

2.4 Transit

BC Transit route 25 (Maplewood/Admirals Walk) services the site with northbound and southbound stops on Admirals Road at the Esquimalt Road intersection. Route 25 connects Esquimalt with Victoria-West and Downtown Victoria. Bus shelters are not provided at either northbound or southbound bus stops at Esquimalt Road.

BC Transit route 15 (Esquimalt/UVic) and 26 (Dockyard/UVic) services the site with westbound and eastbound stops on Esquimalt Road at Admirals Road and Constance Avenue. Route 15 connects Esquimalt with Victoria-West, Downtown Victoria and the University of Victoria. Route 26 connects Esquimalt with Gorge-Tillicum Area, Uptown Mall, Lake Hill Area and the University of Victoria. Bus shelters are not provided at either bus stop.

3. DEVELOPMENT PLAN REVIEW

3.1 Vehicle Parking

3.1.1 Bylaw Requirement

The total required parking supply based on the Esquimalt Parking (Bylaw 2011) is shown in **Table 3.1**. The Bylaw requires 1.3 parking spaces per dwelling unit in medium and high density buildings such as the proposed development. The Parking Bylaw also stipulates that 25% of the required parking spaces should be for visitors. The commercial vehicle parking requirement calculation is based on the assumed tenants at this time.

Table 3.1: Bylaw Vehicle Parking Requirement

LAND USE	QUANTITY	BYLAW RATE	BYLAW REQUIREMENT
Residential - Residents	110 units	$1.3 \times 0.75 = 0.98$ per unit	107 spaces
Residential - Visitors	110 units	$1.3 \times 0.25 = 0.32$ per unit	36 spaces
Daycare	2,000 ft ² (assumed 2 classrooms)	1.5 spaces per classroom	3 spaces
Medical Office	3,000 ft ²	1 space per 323 ft ²	9 spaces
			155 SPACES

As shown, the development would require a total of 155 parking spaces including 143 for residents and residential visitors, and 12 spaces for commercial uses.

3.1.2 Proposed Supply

The development intends to apply for a parking variance to provide 0.8 spaces per residential unit in order to minimize traffic generated by the site and to facilitate more sustainable modes of transportation. This is supportable due to the following rationale:

- The Township of Esquimalt Parking Bylaw requires a high level of residential visitor parking at 0.32 spaces per unit for multi-unit residential uses. However, based on Bunt's previous experience for similar village centres in municipalities across Greater Victoria and Metro Vancouver, a visitor parking supply rate of 0.05 to 0.10 spaces per unit is more appropriate for the proposed development.

This recommendation stems from the Metro Vancouver Residential Apartment Parking Study¹ which found that visitor parking demand never exceeded 0.06 vehicles per dwelling unit during the study period. These rates have been further substantiated by previous Bunt studies for similar projects.

- The development site is in a walkable location near a variety of shops, services and other amenities and is in close proximity to cycling and transit routes.
- There is an opportunity for the daycare and medical office land uses to share parking spaces with the residential visitors since daycares and medical offices have their highest parking demands during the daytime on weekdays whereas demand for residential visitor parking peaks during evenings and weekends which are offset from one another.
- The development is committed to provide a range of transportation demand management measures to facilitate more sustainable modes of transportation, potentially including:
 - A car share vehicle for use by residents and the general public;
 - Car share memberships to residents;
 - Transit pass subsidies for residents;
 - High quality bicycle parking facilities and maintenance tools; and,
 - Electric vehicle charging.

A summary of the Bylaw required parking compared with the proposed parking supply is shown in **Table 3.2**. The recommended parking supply is a total of 111 spaces, of which 88 spaces are reserved for residents and 12 spaces are for the daycare and medical office as well as up to 11 spaces for residential visitors (residential visitor and commercial parking to be shared). In addition, if a daycare is selected as a tenant, preferential locations for drop-off and pick-up should be identified which could either be on-site or on-street (such as on Miles Street).

Table 3.2: Bylaw Vehicle Parking Requirement

LAND USE	BYLAW		PROPOSED	
	RATE	REQUIREMENT	RATE	SUPPLY
Residential – Residents	0.98 per unit	107 spaces	0.8 per unit	88 spaces
Residential – Visitors	0.32 per unit	36 spaces	0.1 per unit	6 to 11 spaces (shared with daycare and medical office)
Daycare	1.5 spaces per classroom	3 spaces	1.5 spaces per classroom	3 spaces
Medical Office	1 space per 323 ft ²	9 spaces	1 space per 323 ft ²	9 spaces

¹ The visitor parking demand results from the Metro Vancouver Residential Parking Study was obtained from suburban sites in Burnaby, Port Coquitlam and Richmond which had varying levels of transit service. The visitor parking demand was not correlated with proximity to the Frequent Transit Network; in fact the site with the worst transit service had the lowest peak visitor parking demand of 0.02 visitor vehicles per dwelling. Therefore the results from the Metro Vancouver Residential Parking Study are seen as applicable to the proposed development.

LAND USE	BYLAW		PROPOSED	
	RATE	REQUIREMENT	RATE	SUPPLY
		155 SPACES	-	111 SPACES
	TOTALS			

The Parking Bylaw requires that one disabled persons' parking space be provided for every 50 required parking spaces or part thereof. Therefore, four of the parking spaces should be able to accommodate disabled persons' to comply with this requirement.

3.2 Bicycle Parking

The Parking Bylaw does not have any requirements for bicycle parking for multi-family dwellings. However, the development is planning on providing secure bicycle parking in the parkade and potentially providing a bicycle maintenance stand with tools. Bunt recommends that at least 1.0 secure bicycle parking stall is provided per dwelling. Bicycle racks for visitors should be installed near building entries with weather protection if possible.

3.3 Access

Vehicle and bicycle access to the underground parkade will be located on Nelson Street. Bunt recommends that pedestrian entrances to the building be located on Nelson Street and Constance Avenue to improve the pedestrian accessibility of the building.

4. TRAFFIC OPERATIONS REVIEW

4.1 Traffic Operations Assessment Methodology

The traffic operations were assessed at the Admirals Road / Esquimalt Road and the Esquimalt Road / Nelson Street intersections for the PM peak hour. The analysis was completed for the existing conditions (2018) and for the 2030 horizon year (ten years after development completion). The 2030 analysis includes the vehicle trips generated by the proposed development and background traffic (i.e. existing traffic plus growth on the network).

The operation of study intersection was assessed using the methods outlined in the 2000 Highway Capacity Manual (HCM), using the Synchro 9 analysis software. The traffic operations were assessed using the performance measures of Level of Service (LOS) and volume-to-capacity (V/C) ratio.

The LOS rating is based on average vehicle delay and ranges from "A" to "F" based on the quality of operation at the intersection. LOS "A" represents minimal queuing time conditions while a LOS "F" represents an over-capacity condition with considerable congestion and/or queuing time. A queuing time of less than 10 seconds receive a LOS A whereas queuing times greater than 50 seconds receive a LOS F. In downtown and Town Centre contexts, during peak demand periods, queuing times greater than 50 seconds (LOS F) are common.

The volume to capacity (V/C) ratio of an intersection represents the ratio between the demand volume and the available capacity. A V/C ratio less than 0.85 indicates that there is sufficient capacity to accommodate demands and generally represents reasonable traffic conditions in suburban settings. A V/C value between 0.85 and 0.95 indicates an intersection is approaching practical capacity; a V/C ratio over 0.95 indicates that traffic demands are close to exceeding the available capacity, resulting in saturated conditions. A V/C ratio over 1.0 indicates a congested intersection where drivers may have to wait through multiple signal cycles. In urban downtown and town centre contexts, during peak demand periods, V/C ratios over 0.90 and even 1.0 are common.

4.2 Existing Conditions

Bunt conducted PM peak period traffic counts at the Esquimalt Road / Nelson Street intersection on Tuesday October 2, 2018. During this time period, 3:30 to 4:30 pm was identified as the peak hour. Vehicle volumes for the Esquimalt Road / Admirals Road intersection were obtained from the Esquimalt Town Centre Traffic Impact Assessment from 2016. The vehicle volumes for these two intersections are shown in **Exhibit 4.1**.

Bunt observed approximately 900 vehicles (two-way) on Admirals Road during the weekday PM peak hour with approximately three quarters travelling to the east and one quarter to the west which is consistent with the travel patterns caused by CFB Esquimalt. Minimal vehicles were observed turning to/from Nelson Street at the intersection.

As shown in **Exhibit 4.2**, there are no traffic operations concerns with the existing conditions. All movements operate within their capacity and have reasonable queuing times.

4.3 Future Conditions

4.3.1 Background Traffic

Background traffic is the traffic that would exist without the proposed development and taking into account any increase in traffic due to other developments in the area that would add to the road network. Background traffic was estimated by growing the existing vehicle volumes by 1% per year (consistent with the Esquimalt Town Centre TIA) as well as adding the traffic forecasts from the Esquimalt Town Centre project.

These assumptions likely overestimate the amount of traffic growth as the number of vehicles on Esquimalt Road, west of Admirals Road has not changed considerably over the last decade. The daily vehicle volumes on Esquimalt Road west of Admirals Road slightly decreased from 8,460 to 8,310 between 2007 and 2017.

4.3.2 Development Generated Traffic

The Institute of Transportation Engineers (ITE) Trip Generation Manual (10th Edition) was used to estimate the number of vehicle trips generated from the proposed building. The vehicle trips rates as per the ITE Trip Generation Manual and the resulting trip generation is presented in **Table 4.1**.

Table 4.1: PM Peak Hour Vehicle Trip Generation

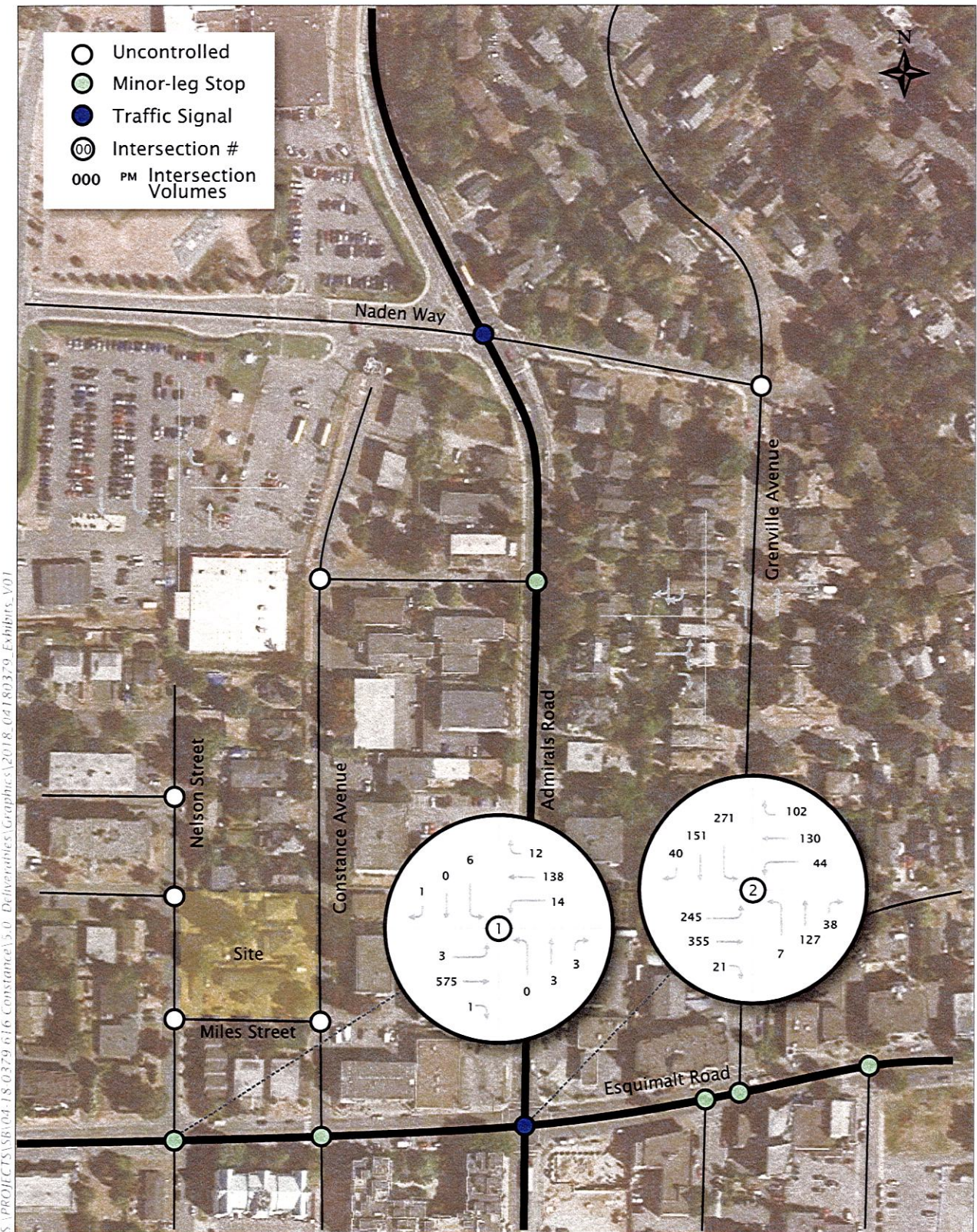
LAND USE				TRIP RATES			TRIP GENERATION		
ITE LAND USE CODE	TITLE	SIZE	VARIABLE	IN	OUT	TOTAL	IN	OUT	TOTAL
222	High-rise Apartment	100	Units	61%	39%	0.36	22	14	36
565	Daycare	2	1000 ft ²	47%	53%	11.82	11	13	24
720	Medical/Dental Office	3	1000 ft ²	28%	72%	3.46	3	7	10
TOTALS							36	34	70

As shown in Table 4.1, the ITE trip rate results in 70 vehicle trips (36 in and 37 out) during a typical PM peak hour period which is equivalent to just over one vehicle per minute. The ITE trip rates are obtained from suburban locations with almost all travel completed by vehicle. It is anticipated that a number of residents and visitors of the proposed development will walk, bike and use transit. Thus the trip volumes shown in Table 4.1 are likely an overestimation of the actual vehicle trips generated by the proposed development.

The new vehicle trips were assigned travel directions based on the existing travel patterns. **Exhibit 4.2** shows the assumed travel pattern for the 70 new vehicle trips minus the estimated 6 vehicle trips per PM peak hour associated with the existing residences on the development site. It was assumed that all vehicles would access the development site from the Esquimalt Road & Nelson Street intersection. This assumption is consistent with the potential conversion of Miles Street into a one-way street westbound.

4.3.3 Traffic Operations Results

Exhibit 4.3 demonstrates the traffic operation results for the year 2030 with growth in background traffic and the traffic generated by the proposed development. As **Exhibit 4.3** demonstrates, there are no traffic operational concerns either of the two study intersections with all movements operating within their capacity. The queuing time for vehicles turning left from Nelson Street onto Esquimalt Road is anticipated to stay within 15 seconds (LOS C). At the Esquimalt Road / Admirals Road intersection, the most congested movement is the southbound through/right movement which operates at LOS D during the PM peak hour which is still considered reasonable.



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Exhibit 4.1
Existing Vehicle Volumes
PM Peak Hour

Lorvatto (616 Constance Avenue)
 04-18-0379 October 2018



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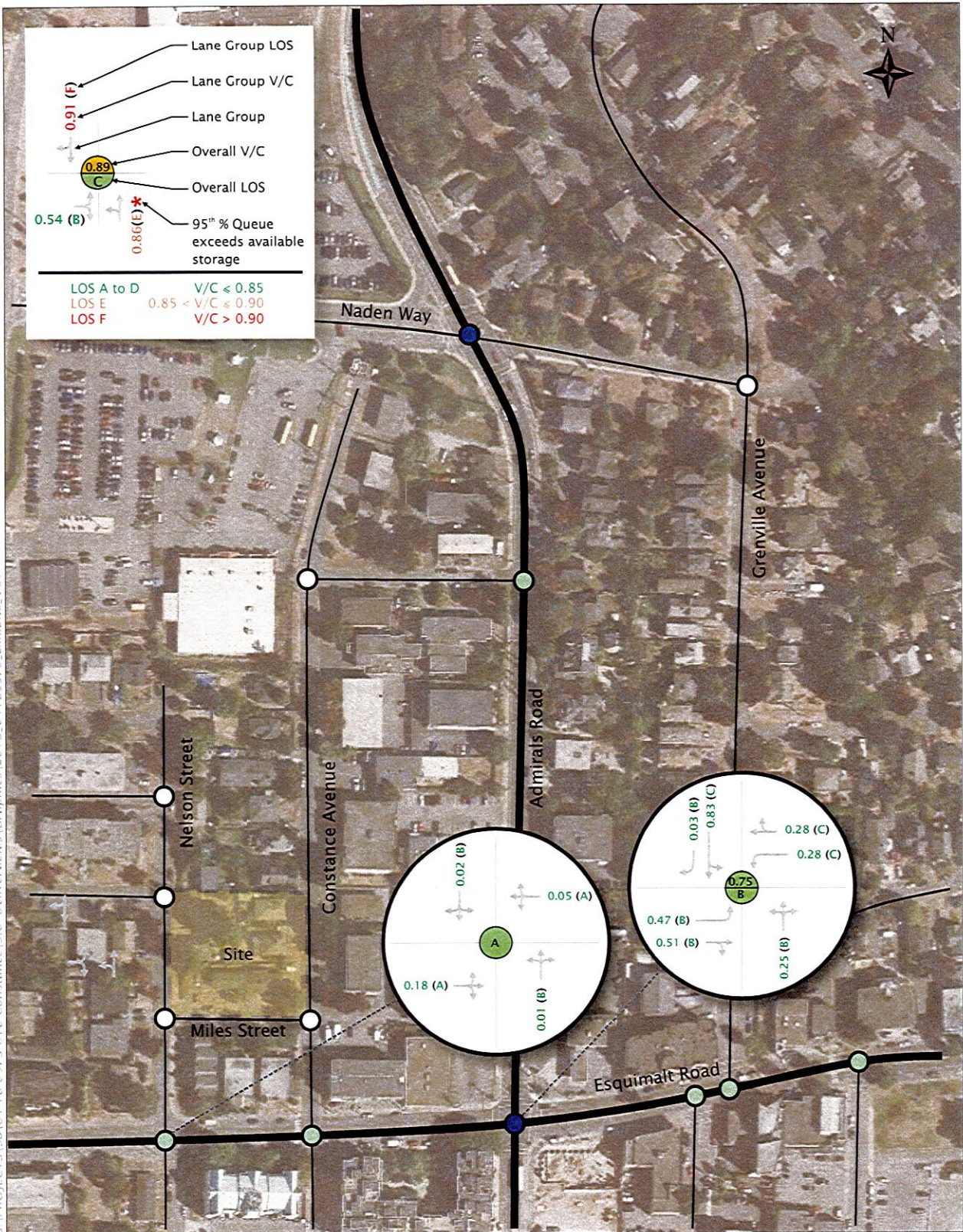
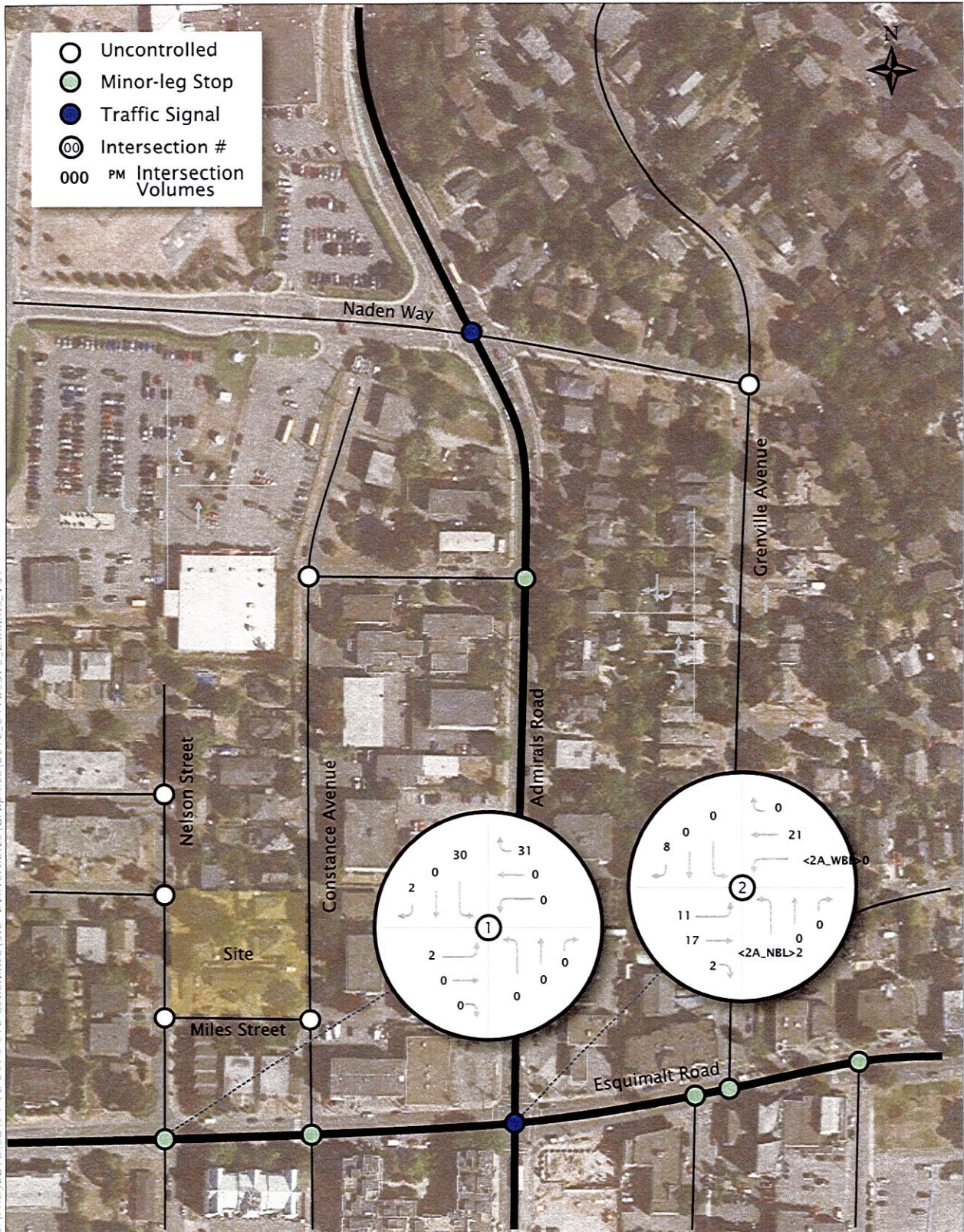


Exhibit 4.2 Existing Traffic Operations PM Peak Hour

Lorvatto (616 Constance Avenue)
04-18-0379 October 2018



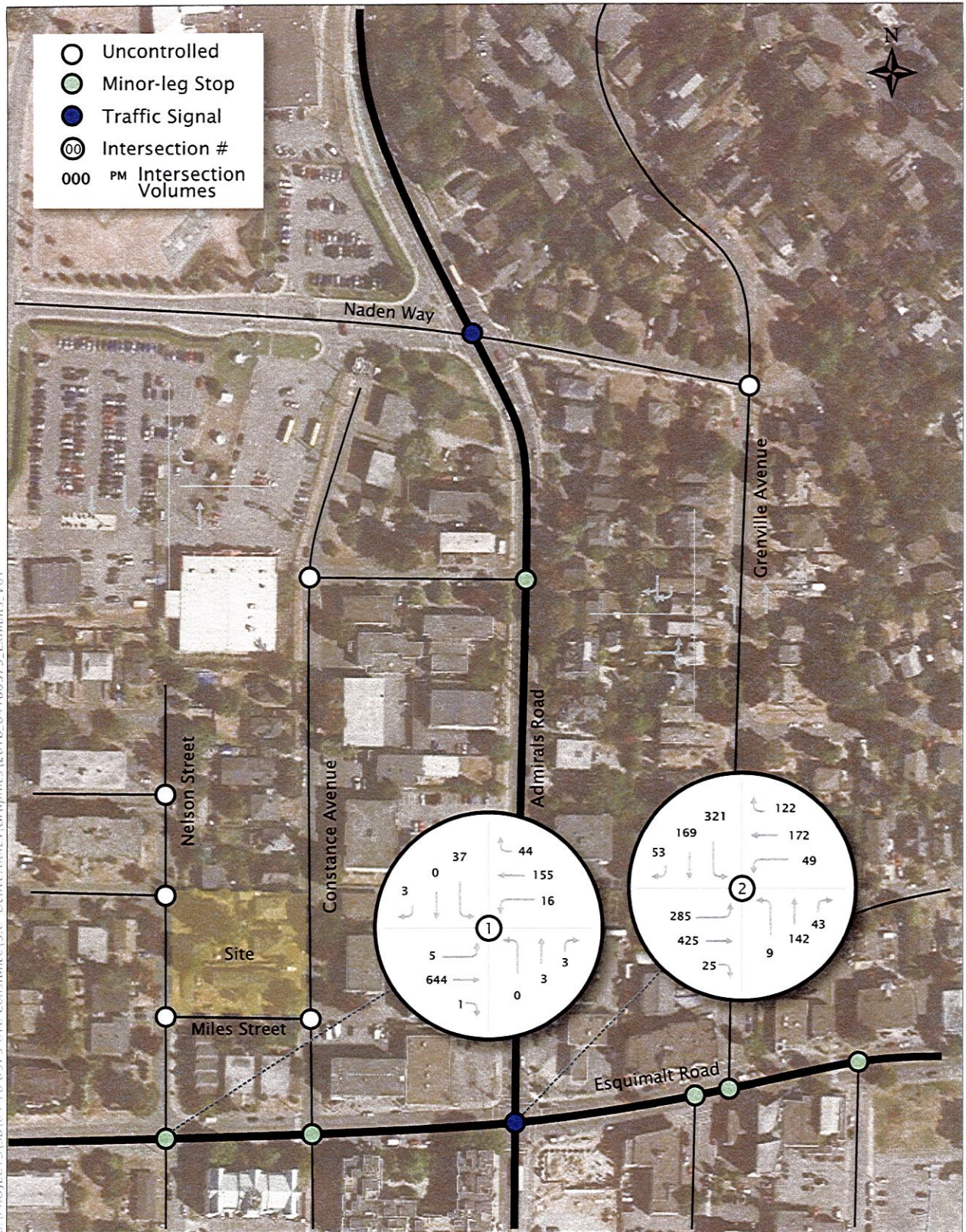


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Exhibit 4.3
Site Volumes
PM Peak Hour

Lorvatto (616 Constance Avenue)
 04-18-0379 October 2018





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Exhibit 4.4
2030 Total Traffic Forecast
PM Peak Hour

Lorvatto (616 Constance Avenue)
 04-18-0379 October 2018



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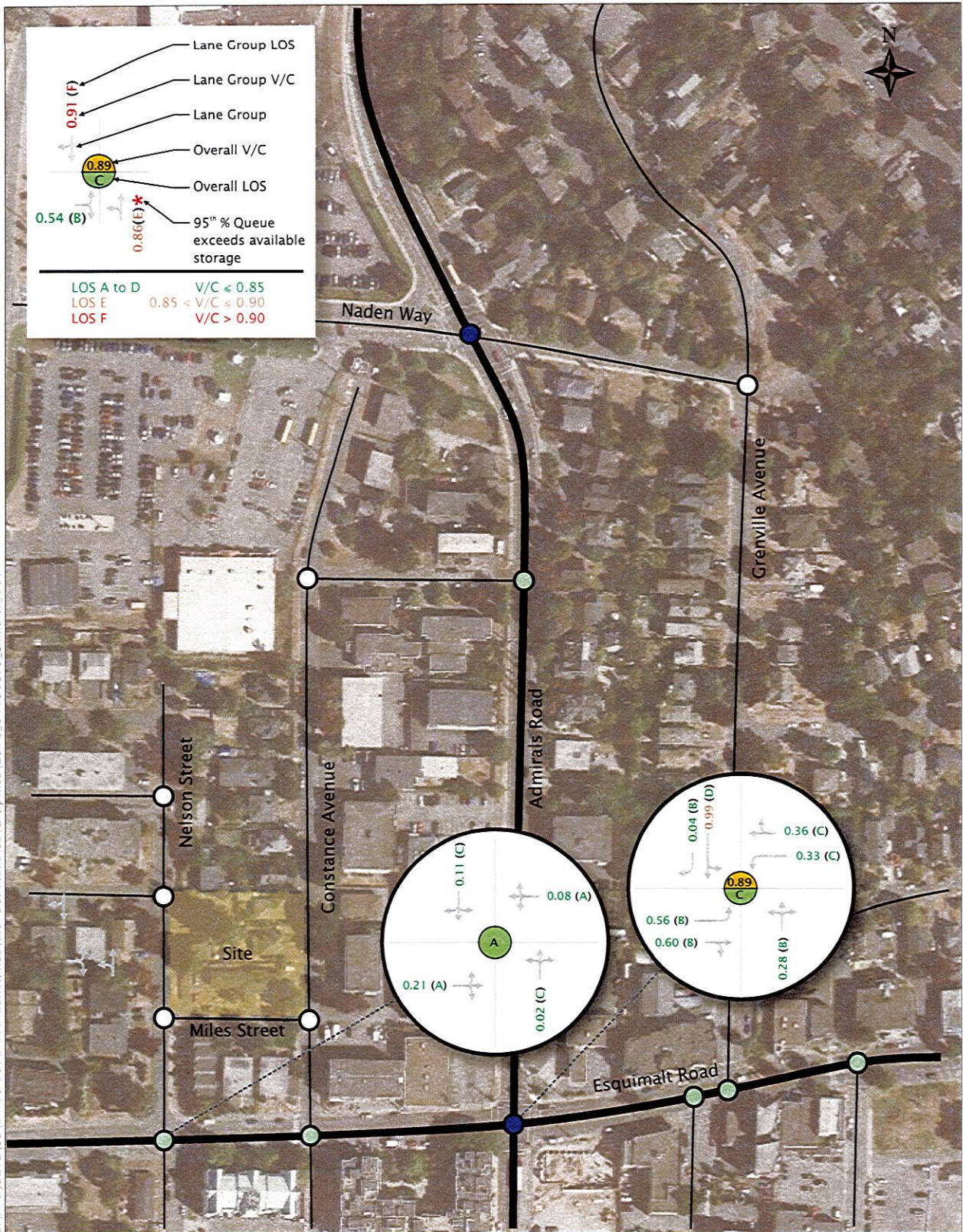


Exhibit 4.5 2030 Future Traffic Operations PM Peak Hour

Lorvatto (616 Constance Avenue)
04-18-0379 October 2018



5. SUMMARY AND RECOMMENDATIONS

5.1 Summary

- The proposed mixed-use development is anticipated to have approximately 100 market residential units, 10 non-market residential units and a 5,000 square foot commercial space which may include a daycare and/or medical offices. Vehicle access will be from Nelson Street.
- The development is proposing that Miles Street become a one-way street in order to minimize traffic impacts on Constance Avenue. The development also plans to provide a suite of transportation demand management measures (i.e. car share memberships for residents) to reduce parking demand and traffic generation.
- The site is well serviced with transit and is within walking range to a wide variety of commercial and service amenities.
- The Parking Bylaw requires 1.3 parking spaces per residential unit. The development is proposing 0.8 spaces per unit for residents and 0.1 spaces per unit for residential visitors. The majority of the parking variance being requested is for visitor parking which the Parking Bylaw requires significantly more than needed to accommodate the anticipated demand. The development plans on having a shared parking pool for residential visitors and the commercial land uses.
- The site is expected to generate approximately 70 total vehicle trips per weekday PM peak hour. This is considered to be a nominal amount of traffic considering Esquimalt Road currently has approximately 9,000 vehicles during the PM peak hour.
- The intersections of Esquimalt Road with Nelson Street and Admirals Road currently operate within capacity and are forecasted to continue operating within their capacity with the proposed development and background traffic growth.

5.2 Recommendations

- Converting Miles Street into a one-way street will minimize traffic impacts on Constance Avenue.
- 0.8 parking spaces per dwelling should be provided. The residential visitor parking should be shared with the commercial land uses to take advantage of their differing time-of-day parking demands. Based on the current development statistics, a total of 111 parking spaces should be provided, of which 88 are reserved for residents.
- Should a daycare tenant be secured, additional consideration should be given to drop-off and pick-up activity.
- The development should have pedestrian access from Constance Avenue and Nelson Street.
- Short-term bicycle parking should be provided near building entrances with weather protection.



GREEN BUILDING CHECKLIST

The purpose of this Checklist is to make property owners and developers aware of specific green features that can be included in new developments to reduce their carbon footprints to help create a more sustainable community.

Creating walkable neighbourhoods, fostering green building technologies, making better use of our limited land base and ensuring that new development is located close to services, shops and transit are some of the means of achieving sustainability.

The Checklist which follows focuses on the use of **Green Technologies** in new buildings and major renovations. The Checklist is not a report card, it is a tool to help identify how your project can become 'greener' and to demonstrate to Council how your project will help the Township of Esquimalt meet its sustainability goals. It is not expected that each development will include all of the ideas set out in this list but Council is looking for a strong commitment to green development.

There are numerous green design standards, for example, Built Green BC; LEED ND; Living Building Challenge; Green Shores; Sustainable Sites Initiative. Esquimalt is not directing you to follow any particular standard, however, you are strongly encouraged to incorporate as many green features as possible into the design of your project .

As you review this checklist, if you have any questions please contact **Development Services** at 250.414.7108 for clarification.

**New development is essential to Esquimalt.
We look forward to working with you
to ensure that development is
as green and sustainable as possible.**

Other documents containing references to building and site design and sustainability, which you are advised to review, include:

- Esquimalt's Official Community Plan
- Development Protocol Policy
- Esquimalt's Pedestrian Charter
- Tree Protection Bylaw No. 2664
- A Sustainable Development Strategic Plan for the Township of Esquimalt

Adopted on January 10th, 2011



“One-third of Canada’s energy use goes to running our homes, offices and other buildings. The federal government’s Office of Energy Efficiency (Natural Resources Canada) reports that a corresponding one-third of our current greenhouse gas (GHG) emissions come from the built environment.”
 [Green Building and Development as a Public Good, Michael Buzzelli, CPRN Research Report June 2009]

Please answer the following questions and describe the green and innovative features of your proposed development. Depending on the size and scope of your project, some of the following points may not be applicable.

Green Building Standards

Both energy use and emissions can be reduced by changing or modifying the way we build and equip our buildings.

1	Are you building to a recognized green building standard? If yes, to what program and level?	Yes	<input checked="" type="radio"/> No
2	If not, have you consulted a Green Building or LEED consultant to discuss the inclusion of green features?	<input checked="" type="radio"/> Yes	No
3	Will you be using high-performance building envelope materials, rainscreen siding, durable interior finish materials or safe to re-use materials in this project? If so, please describe them.	<input checked="" type="radio"/> Yes	No
4	What percentage of the existing building[s], if any, will be incorporated into the new building?	N/A	%
5	Are you using any locally manufactured wood or stone products to reduce energy used in the transportation of construction materials? Please list any that are being used in this project. The intent of the project is to use Mass Timber, we will use local everywhere it is available.		
6	Have you considered advanced framing techniques to help reduce construction costs and increase energy savings?	<input checked="" type="radio"/> Yes	No
7	Will any wood used in this project be eco-certified or produced from sustainably managed forests? If so, by which organization? <u>FSC (Forest Stewardship Council)</u> For which parts of the building (e.g. framing, roof, sheathing etc.)? <u>CLT (Cross Laminated Timber), Glulam, sheathing</u>		-
8	Can alternatives to Chlorofluorocarbon’s and Hydro-chlorofluorocarbons which are often used in air conditioning, packaging, insulation, or solvents] be used in this project? If so, please describe these. _____	Yes	No <input checked="" type="checkbox"/>
9	List any products you are proposing that are produced using lower energy levels in manufacturing. <u>mass timber structural components</u>		
10	Are you using materials which have a recycled content [e.g. roofing materials, interior doors, ceramic tiles or carpets]?	<input checked="" type="radio"/> Yes	No
11	Will any interior products [e.g. cabinets, insulation or floor sheathing] contain formaldehyde?	Yes	<input checked="" type="radio"/> No

Water Management

The intent of the following features is to promote water conservation, re-use water on site, and reduce storm water run-off.

Indoor Water Fixtures

- 12 Does your project exceed the BC Building Code requirements for public lavatory faucets and have automatic shut offs? Yes No
- 13 For commercial buildings, do flushes for urinals exceed BC Building Code requirements? *applicable to podium level* Yes No
- 14 Does your project use dual flush toilets and do these exceed the BC Building Code requirements? Yes No
- 15 Does your project exceed the BC Building Code requirements for maximum flow rates for private showers? Yes No
- 16 Does your project exceed the BC Building Code requirements for flow rates for kitchen and bathroom faucets? Yes No

Storm Water

- 17 If your property has water frontage, are you planning to protect trees and vegetation within 60 metres of the high water mark? [Note: For properties located on the Gorge Waterway, please consult Sections 7.1.2.1 and 9.6 of the Esquimalt Official Community Plan.] Yes No N/A
- 18 Will this project eliminate or reduce inflow and infiltration between storm water and sewer pipes from this property? Yes No N/A
We will be separating storm water from sanitary on site through separate systems.
- 19 Will storm water run-off be collected and managed on site (rain gardens, wetlands, or ponds) or used for irrigation or re-circulating outdoor water features? If so, please describe. *reflective water terrace and all storm water will be managed on site* Yes No N/A
- 20 Have you considered storing rain water on site (rain barrels or cisterns) for future irrigation uses? *irrigation storage tank on parkade level* Yes No N/A
- 21 Will surface pollution into storm drains will be mitigated (oil interceptors, bio-swales)? If so, please describe. _____
Detailed engineering will best describe the systems in place. Yes No N/A
- 22 Will this project have an engineered green roof system or has the structure been designed for a future green roof installation? Yes No N/A
- 23 What percentage of the site will be maintained as naturally permeable surfaces? Please refer to the landscape plans _____%

Waste water

- 24 For larger projects, has Integrated Resource Management (IRM) been considered (e.g. heat recovery from waste water or onsite waste water treatment)? If so, please describe these. _____ Yes No N/A

Natural Features/Landscaping

The way we manage the landscape can reduce water use, protect our urban forest, restore natural vegetation and help to protect the watershed and receiving bodies of water.

- 25 Are any healthy trees being removed? If so, how many and what species? Yes No N/A
Consultations will proceed with the Township prior to removal and replacement
Could your site design be altered to save these trees? *no*
Have you consulted with our Parks Department regarding their removal? *no*

- 26 Will this project add new trees to the site and increase our urban forest?
If so, how many and what species? Please refer to the landscape plan for full details Yes No N/A
- 27 Are trees [existing or new] being used to provide shade in summer or to buffer winds? Yes No N/A
- 28 Will any existing native vegetation on this site be protected?
If so, please describe where and how. _____ Yes No N/A
- 29 Will new landscaped areas incorporate any plant species native to southern Vancouver Island? Yes No N/A
- 30 Will xeriscaping (i.e. the use of drought tolerant plants) be utilized in dry areas? Yes No N/A
- 31 Will high efficiency irrigation systems be installed (e.g. drip irrigation; 'smart' controls)? Yes No N/A
Where in use we will incorporate high efficiency irrigation systems
- 32 Have you planned to control invasive species such as Scotch broom, English ivy, Himalayan and evergreen blackberry growing on the property? Yes No N/A
- 33 Will topsoil will be protected and reused on the site? Yes No N/A

Energy Efficiency

Improvements in building technology will reduce energy consumption and in turn lower greenhouse gas [GHG] emissions. These improvements will also reduce future operating costs for building occupants.

- 34 Will the building design be certified by an independent energy auditor/analyst?
If so, what will the rating be? _____ Yes No N/A
- 35 Have you considered passive solar design principles for space heating and cooling or planned for natural day lighting? Yes No N/A
- 36 Does the design and siting of buildings maximize exposure to natural light?
What percentage of interior spaces will be illuminated by sunlight? _____ % Yes No N/A
- 37 Will heating and cooling systems be of enhanced energy efficiency (ie. geothermal, air source heat pump, solar hot water, solar air exchange, etc.).
If so, please describe. _____
If you are considering a heat pump, what measures will you take to mitigate any noise associated with the pump? _____ Yes No N/A
- 38 Has the building been designed to be solar ready? Yes No N/A
- 39 Have you considered using roof mounted photovoltaic panels to convert solar energy to electricity? Yes No N/A
- 40 Do windows exceed the BC Building Code heat transfer coefficient standards? Yes No N/A
- 41 Are energy efficient appliances being installed in this project?
If so, please describe. Energy Star Yes No N/A
- 42 Will high efficiency light fixtures be used in this project?
If so, please describe. LED Yes No N/A
- 43 Will building occupants have control over thermal, ventilation and light levels? Yes No N/A
- 44 Will outdoor areas have automatic lighting [i.e. motion sensors or time set]? Yes No N/A
- 45 Will underground parking areas have automatic lighting? Yes No N/A

Air Quality

The following items are intended to ensure optimal air quality for building occupants by reducing the use of products which give off gases and odours and allowing occupants control over ventilation.

46	Will ventilation systems be protected from contamination during construction and certified clean post construction?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
47	Are you using any natural, non-toxic, water soluble or low-VOC [volatile organic compound] paints, finishes or other products? If so, please describe. _____	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
48	Will the building have windows that occupants can open?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
49	Will hard floor surface materials cover more than 75% of the liveable floor area?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
50	Will fresh air intakes be located away from air pollution sources?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A

Solid Waste

Reuse and recycling of material reduces the impact on our landfills, lowers transportation costs, extends the life-cycle of products, and reduces the amount of natural resources used to manufacture new products.

51	Will materials be recycled during demolition of existing buildings and structures? If so, please describe. _____ Any material that can be recovered through recycling will be appropriately disposed of	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
52	Will materials be recycled during the construction phase? If so, please describe. _____ We are using an efficient off-site fabrication to minimize waste	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
53	Does your project provide enhanced waste diversion facilities i.e. on-site recycling for cardboard, bottles, cans and or recyclables or on-site composting?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
54	For new commercial development, are you providing waste and recycling receptacles for customers?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A

Green Mobility

The intent is to encourage the use of sustainable transportation modes and walking to reduce our reliance on personal vehicles that burn fossil fuels which contributes to poor air quality.

55	Is pedestrian lighting provided in the pathways through parking and landscaped areas and at the entrances to your building[s]?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
56	For commercial developments, are pedestrians provided with a safe path[s] through the parking areas and across vehicles accesses?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
57	Is access provided for those with assisted mobility devices?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
58	Are accessible bike racks provided for visitors?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
59	Are secure covered bicycle parking and dedicated lockers provided for residents or employees?	<input checked="" type="radio"/> Yes	<input type="radio"/> No	N/A
60	Does your development provide residents or employees with any of the following features to reduce personal automobile use [check all that apply]: <input checked="" type="checkbox"/> transit passes <input checked="" type="checkbox"/> car share memberships <input checked="" type="checkbox"/> shared bicycles for short term use We are engaging with a local bike renter <input type="checkbox"/> weather protected bus shelters There is no bus route next to our project <input checked="" type="checkbox"/> plug-ins for electric vehicles			

Is there something unique or innovative about your project that has not been addressed by this Checklist? If so, please add extra pages to describe it.



DRAWINGS LIST - ARCHITECTURAL	
Sheet Number	Sheet Name
.A0.00	COVER SHEET
.A0.01	DIAGRAM SECTION
.A0.02	DIAGRAM SECTION
.A0.03	VIGNETTE
.A0.04	3D
18-061-BASE	EXISTING UTILITIES DRAWING
03680-V-2SP	TOPOGRAPHIC SURVEY PLAN
A1.01	SITE PLAN
A1.02	PERSPECTIVE VIEWS
A1.03	PERSPECTIVE VIEWS
A1.04	PERSPECTIVE VIEWS
A1.05	PERSPECTIVE VIEWS
A1.06	PERSPECTIVE VIEWS
A1.07	SUN AND SHADOW STUDY
A2.01	OVERALL FLOOR PLANS - LEVEL B3
A2.02	FLOOR PLANS - B2 LEVEL
A2.03	OVERALL FLOOR PLANS - B1 LEVEL
A2.10	TYPICAL UNIT TYPES
A2.11	FLOOR PLANS - GROUND LEVEL
A2.12	FLOOR PLANS - 2ND LEVEL - TOWNHOME
A2.13	FLOOR PLANS - 2ND LEVEL - AMENITY
A2.14	FLOOR PLANS - 3RD LEVEL
A2.15	FLOOR PLANS - 4TH TO 11TH LEVEL TYPICAL
A2.16	FLOOR PLANS - 12TH LEVEL
A2.17	ROOF PLAN
A4.01	EXTERIOR ELEVATIONS
A4.02	EXTERIOR ELEVATIONS
A4.03	EXTERIOR ELEVATIONS
A4.04	EXTERIOR ELEVATIONS
A4.11	BUILDING SECTIONS
A4.12	BUILDING SECTIONS
A4.13	BUILDING SECTIONS
A4.14	BUILDING SECTIONS
L1.01	LANDSCAPE PLAN
L1.02	LANDSCAPE PLAN 2ND LEVEL
L1.03	LANDSCAPE SECTIONS A
L1.04	LANDSCAPE SECTIONS B
L1.05	LANDSCAPE VIEW



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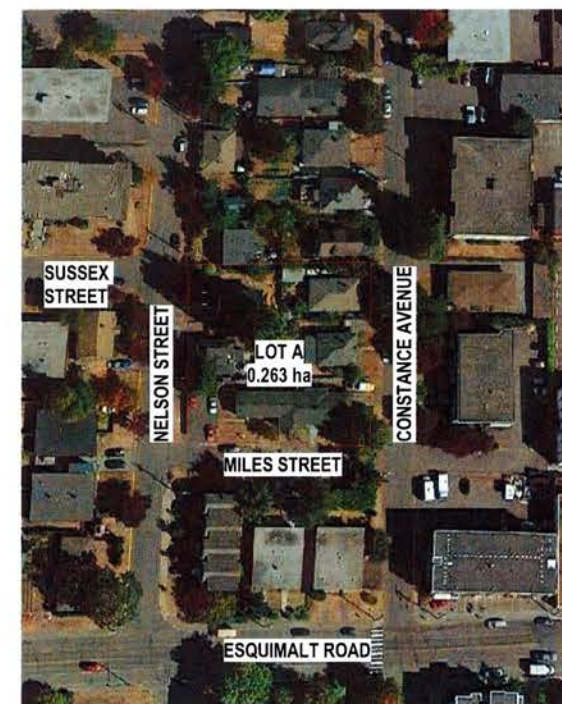
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COVER SHEET

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CONTEXT PLAN

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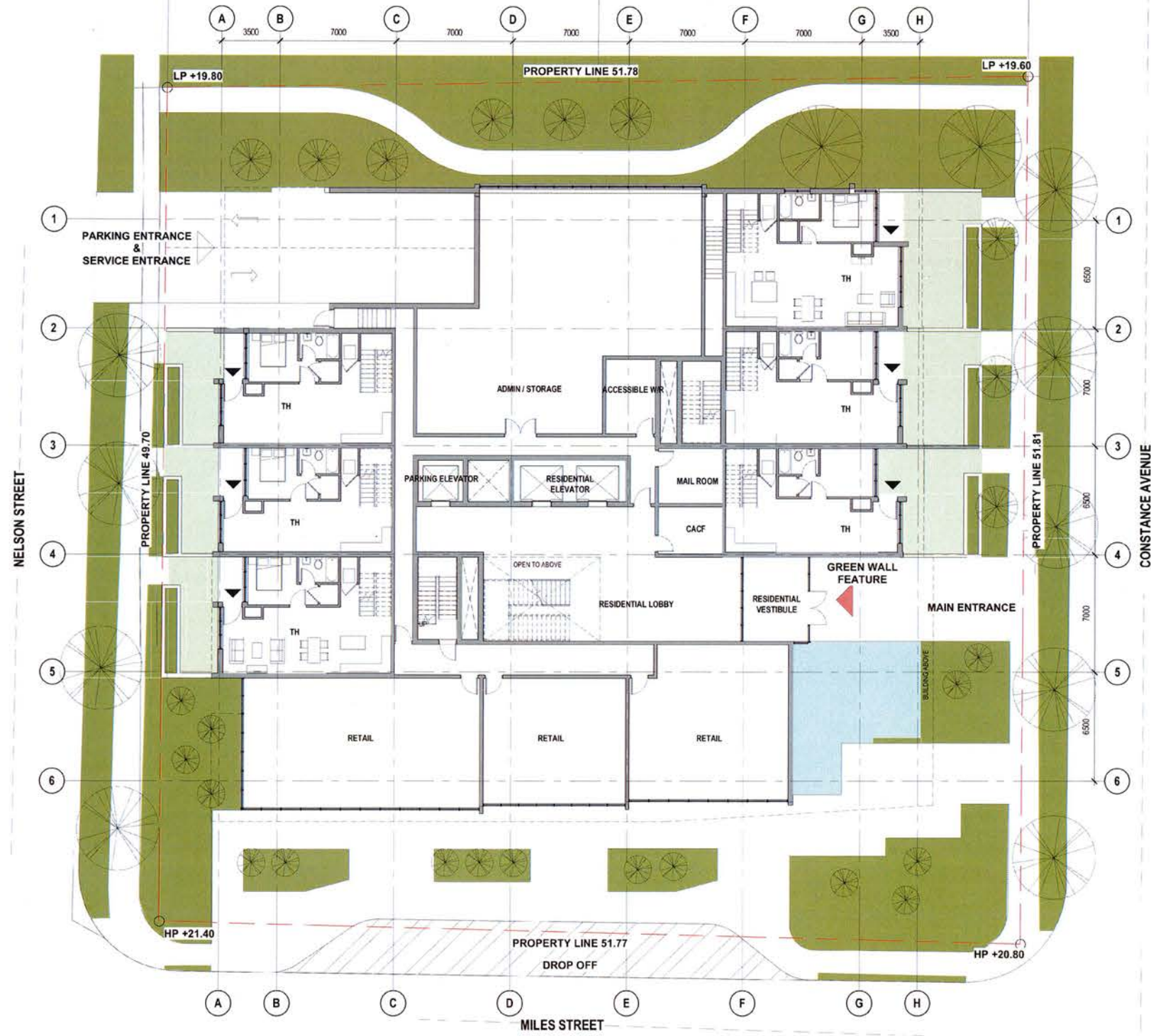


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SITE PLAN LEGEND

- PROPERTY LINE
- BUILDING / STRUCTURE ABOVE
- ▼ MAIN ENTRANCE TO RESIDENCE
- ▽ PARKING / LOADING ENTRANCE
- ▼ TOWNHOME ENTRANCE
- HP DENOTES HIGH POINT AND SPOT ELEVATION
- LP DENOTES LOW POINT AND SPOT ELEVATION

SUSSEX STREET



2019-03-08
For Rezoning - Shoaling Heights
All Dimensions: SQM (unless noted otherwise)

SHOALING HEIGHTS RESIDENTIAL DEVELOPMENT

FLOOR	FSR AREA (sqm)	GROSS AREA	COMMON AREA
12	515	598	83
11	734	840	105
10	734	840	105
9	734	840	105
8	734	840	105
7	734	840	105
6	734	840	105
5	734	840	105
4	734	840	105
3	734	840	105
TH 2	435	435	107
2	627	835	208
GROUND	956	1,236	283
Total	9,135	10,661	1,526

Parking Area	FSR AREA (sqm)	GROSS AREA	COMMON AREA
83		1,258	
82		2,128	
81		2,128	
Total		5,514	

Parking Spaces (0.80/unit)	Count
Regular	88
Visitor	11
Daycare	3
Medical/Retail	9
Total	111

Bike Storage Spaces (1.25/unit)	Count
Bike Stalls	129
At Grade Bike Stalls	10
Total	139

Site Area	Count
FSR	2,627
Building Coverage area (Enclosed)	3,48
Total	6,115

Building Coverage area (not including walkways and stairs)	Count
	54.5%

Building Height (above South Setback Line)(m)	Count
	44M
Building Height (above North Setback Line)(m)	Count
	46M

Setback (m)	Count
North	6
East	6
South	6
West	3

Net Floor Area	Count
	276
Retail Area (to mid wall)	456
Amenity	864
Townhome	181
Daycare	181
Total	109

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SHOALING HEIGHTS RESIDENTIAL DEVELOPMENT

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SITE PLAN

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1 PERSPECTIVE VIEW ALONG MILES STREET
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1 PERSPECTIVE VIEW FROM SOUTH EAST AT CONSTANCE AVENUE AND MILES STREET
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1 PERSPECTIVE VIEW FROM SOUTH WEST AT NELSON STREET AND MILES STREET
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1 PERSPECTIVE VIEW ALONG NELSON STREET
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1 PERSPECTIVE AERIAL VIEW
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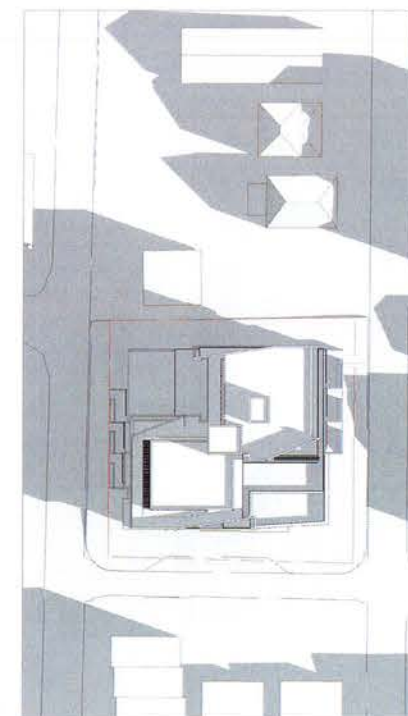
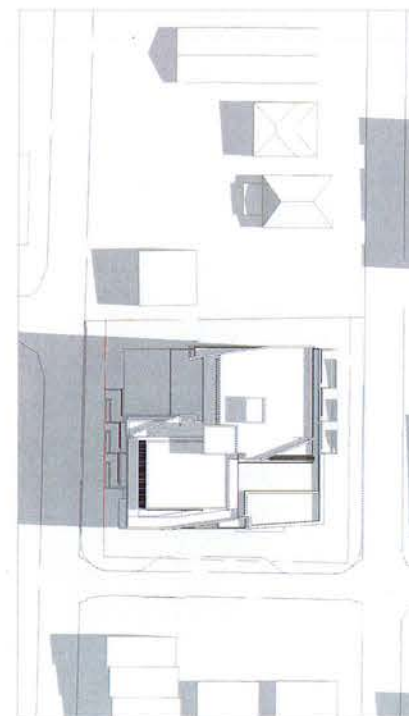
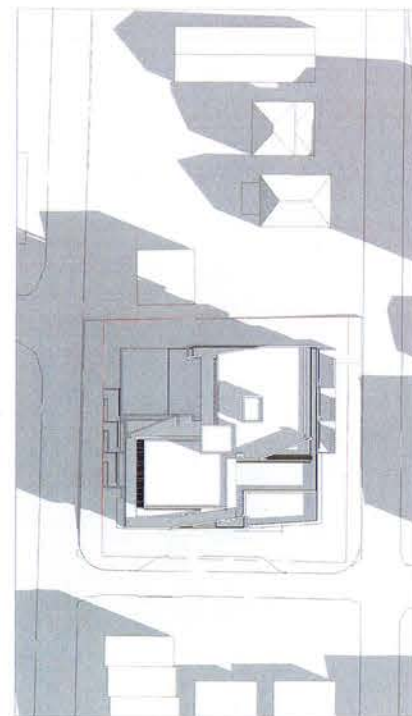
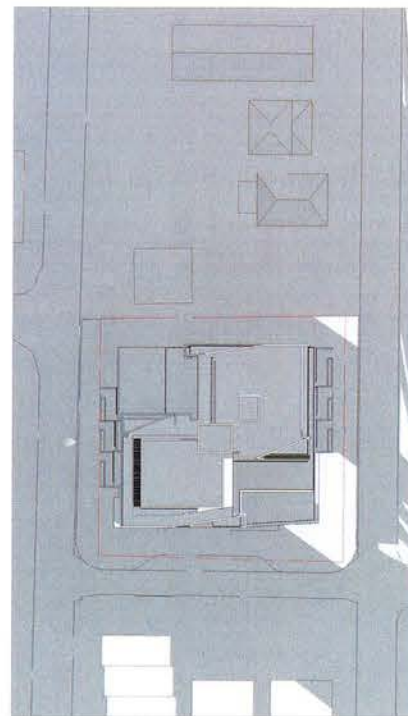
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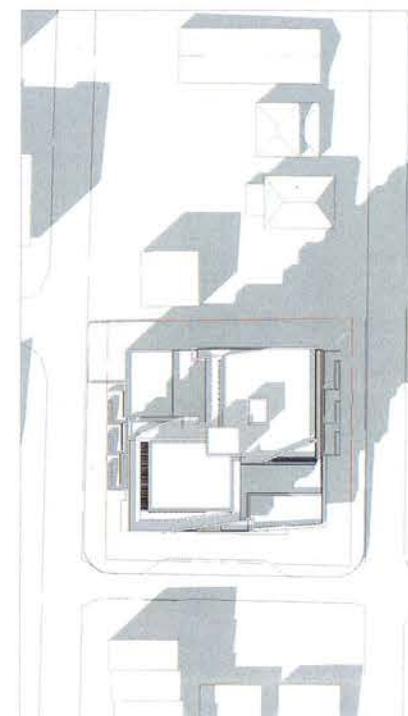
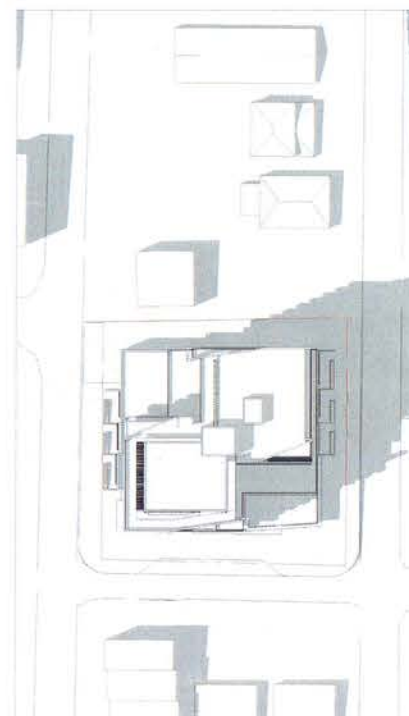
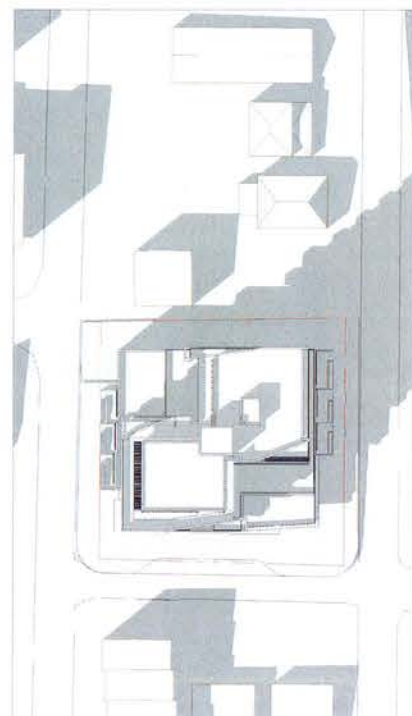
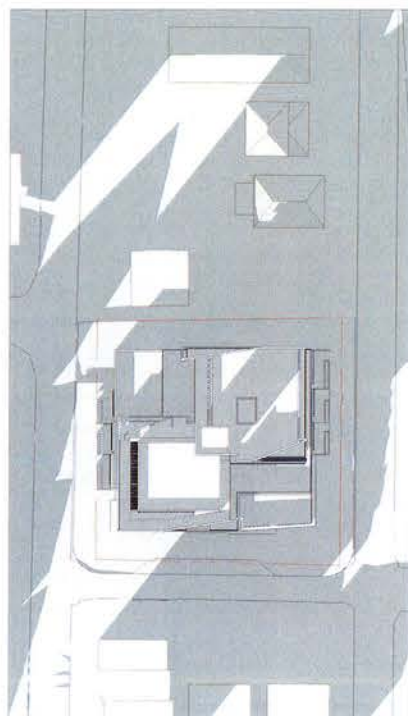
SEPTEMBER 21

JUNE 21

MARCH 21



9:00 AM



4:00 PM

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SEAL



SHOALING HEIGHTS RESIDENTIAL DEVELOPMENT
 CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT, BC

SUN AND SHADOW STUDY

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 PLOT DATE: 08/03/2019 5:43:26 PM

A1.07



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 1 2018-11-23 REZONING
 2 2019-02-15 REZONING
 3 2019-03-08 REZONING

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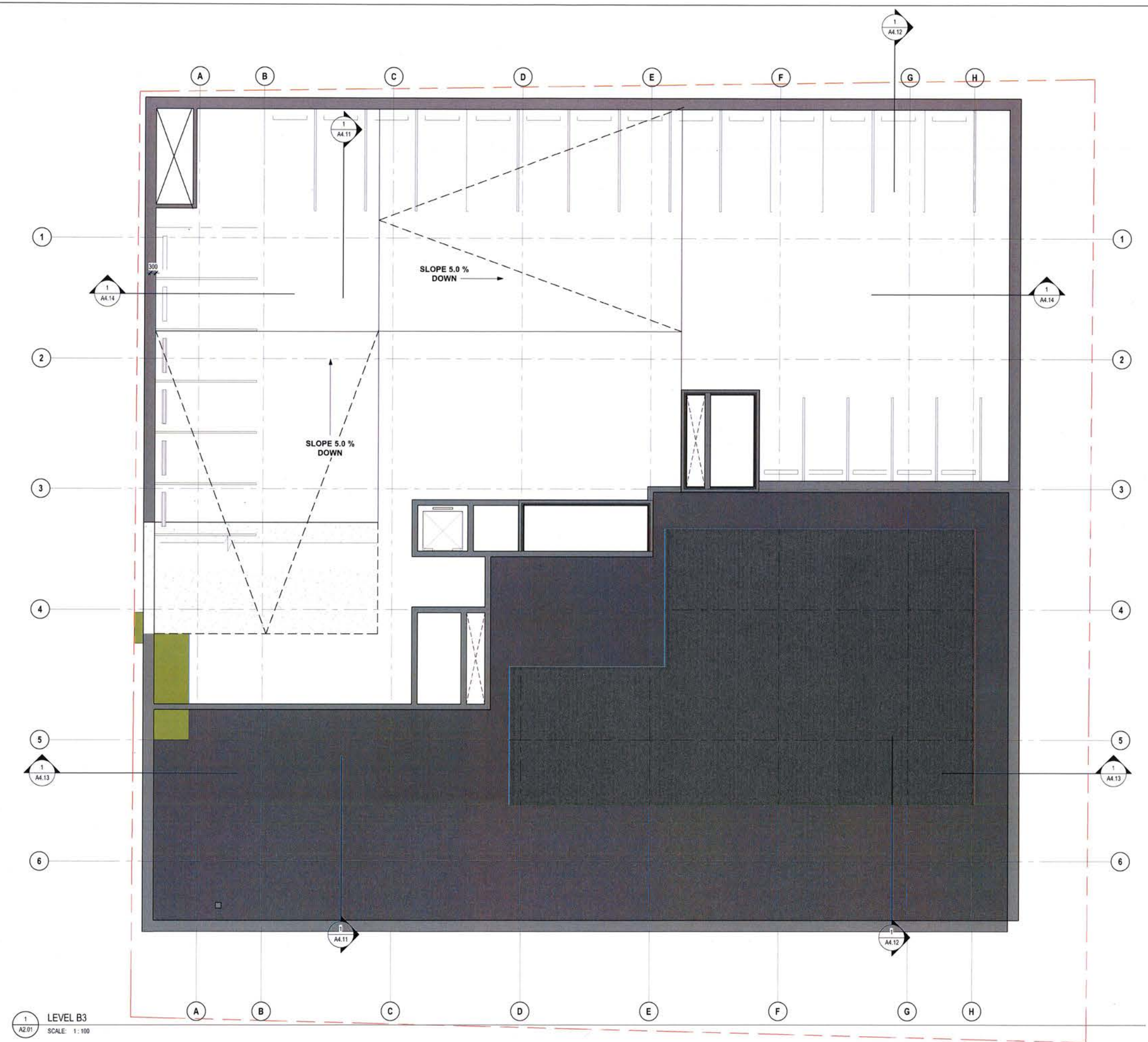
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**SHOALING HEIGHTS
 RESIDENTIAL
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 CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT,
 BC

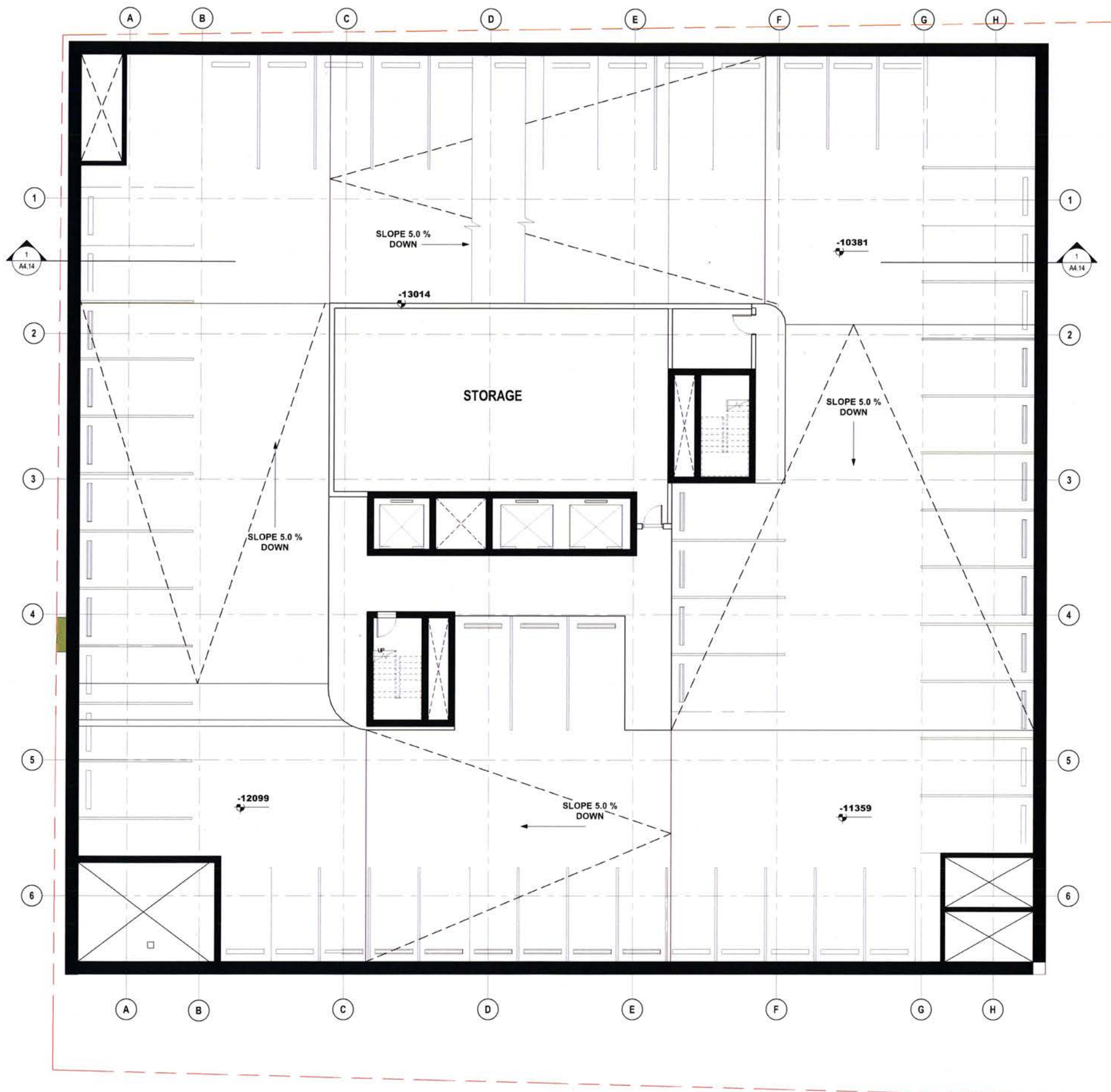
**OVERALL FLOOR
 PLANS - LEVEL B3**

DRAWN: Author CHECKER: Checker
 PLOT DATE: 09/03/2019 5:44:23 PM

A2.01



1 LEVEL B3
 A2.01 SCALE: 1:100



1 LEVEL B2
A2.02 SCALE: 1:100

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CORP OF TOWNSHIP
OF ESQUIMALT



NORTH

ISSUED FOR

- 1 2018-11-23 REZONING
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**SHOALING HEIGHTS
RESIDENTIAL
DEVELOPMENT**

CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT, BC

**FLOOR PLANS - B2
LEVEL**

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PLOT DATE: 08/03/2019 5:44:46 PM

A2.02

04849V



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SHOALING HEIGHTS RESIDENTIAL DEVELOPMENT
 CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT, BC

OVERALL FLOOR PLANS - B1 LEVEL

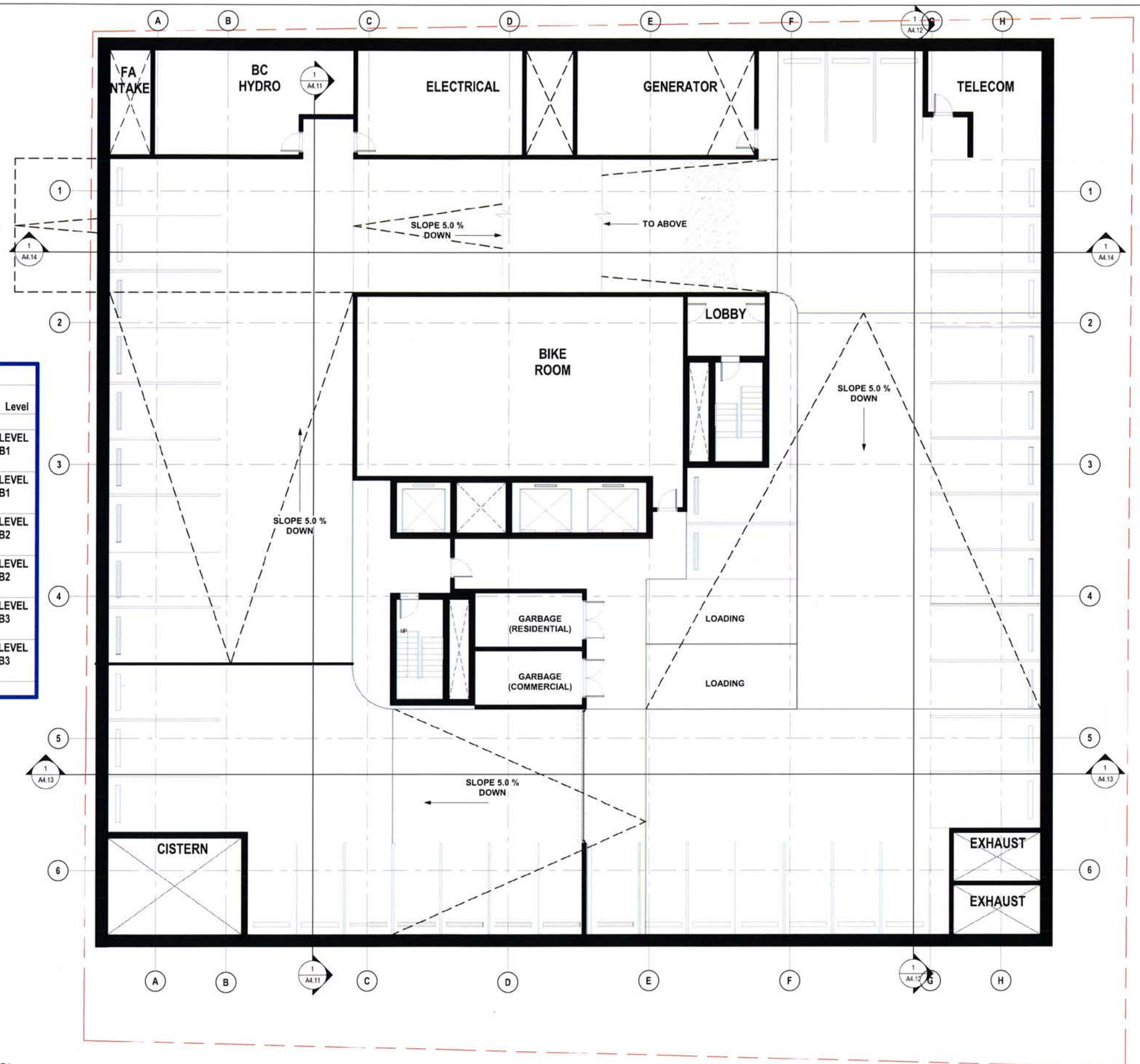
DRAWN: Author CHECKED: Checker
 PLOT DATE: 3/8/2019 5:48:37 PM

A2.03

Parking Schedule		
Count	Family and Type	Level
17	PRK_Parking Stall_Vehicle_UH_3D: 4500 x 2400	LEVEL B1
26	PRK_Parking Stall_Vehicle_UH_3D: 5500 x 2750	LEVEL B1
17	PRK_Parking Stall_Vehicle_UH_3D: 4500 x 2400	LEVEL B2
32	PRK_Parking Stall_Vehicle_UH_3D: 5500 x 2750	LEVEL B2
5	PRK_Parking Stall_Vehicle_UH_3D: 4500 x 2400	LEVEL B3
14	PRK_Parking Stall_Vehicle_UH_3D: 5500 x 2750	LEVEL B3
111		

PARKING ENTRANCE ABOVE

1 LEVEL B1
 A2.03 SCALE: 1:100

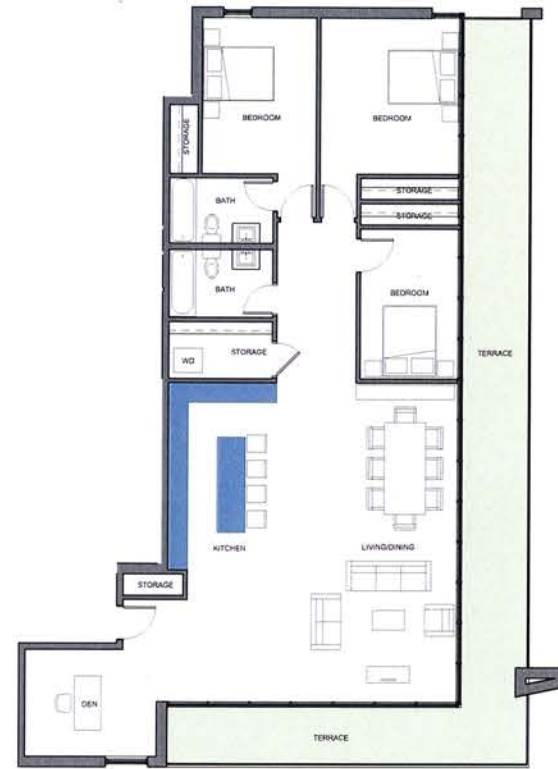




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UNIT TYPE - 3BR+D - 184 SM
SCALE: 1:100



UNIT TYPE - 3BR TYPE B - 119 SM
SCALE: 1:100



UNIT TYPE - 2BR TYPE D - 98 SM
SCALE: 1:100



UNIT TYPE - 2BR TYPE C - 117 SM
SCALE: 1:100



UNIT TYPE - 2BR TYPE A - 60 SM
SCALE: 1:100



UNIT TYPE - 2BR TYPE B - 78 SM
SCALE: 1:100



UNIT TYPE - 2BR - 91 SM
SCALE: 1:100



UNIT TYPE - 1 BR+D TYPE B - 71 SM
SCALE: 1:100



UNIT TYPE - 3BR TYPE A - 114 SM
SCALE: 1:100



UNIT TYPE - TH TYPE B LEVEL 2
SCALE: 1:100



UNIT TYPE - TH TYPE A LEVEL 2
SCALE: 1:100



UNIT TYPE - STUDIO A - 38 SM
SCALE: 1:100



UNIT TYPE - STUDIO B - 36 SM
SCALE: 1:100



UNIT TYPE - 1 BR - 48 SM
SCALE: 1:100



UNIT TYPE - TH TYPE B LEVEL 1 - 154 SM
SCALE: 1:100



UNIT TYPE - TH TYPE A LEVEL 1 - 166 SM
SCALE: 1:100

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CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT, BC

TYPICAL UNIT TYPES

DRAWN: Author CHECK: checker
PLOT DATE: 08/03/2019 6:16:02 PM

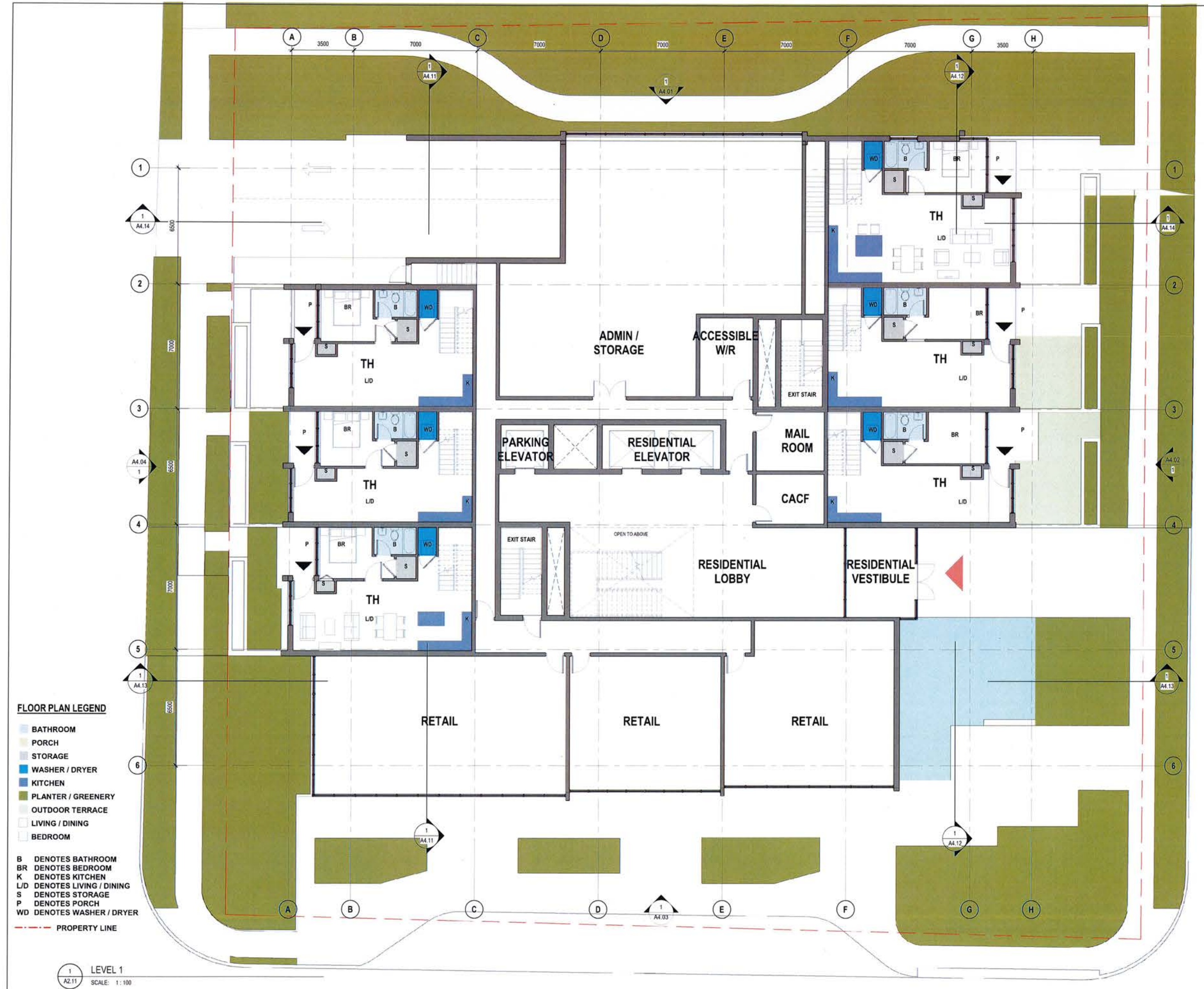
A2.10

04849V

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FLOOR PLAN LEGEND

- BATHROOM
 - PORCH
 - STORAGE
 - WASHER / DRYER
 - KITCHEN
 - PLANTER / GREENERY
 - OUTDOOR TERRACE
 - LIVING / DINING
 - BEDROOM
- B DENOTES BATHROOM
BR DENOTES BEDROOM
K DENOTES KITCHEN
L/D DENOTES LIVING / DINING
S DENOTES STORAGE
P DENOTES PORCH
WD DENOTES WASHER / DRYER
- PROPERTY LINE



- FLOOR PLAN LEGEND**
- BATHROOM
 - PORCH
 - STORAGE
 - WASHER / DRYER
 - KITCHEN
 - PLANTER / GREENERY
 - OUTDOOR TERRACE
 - LIVING / DINING
 - BEDROOM
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 S DENOTES STORAGE
 P DENOTES PORCH
 WD DENOTES WASHER / DRYER
- PROPERTY LINE

1 LEVEL 1
A2.11 SCALE: 1:100

DIALOG
RECEIVED
MAR 12 2019
CORP OF TOWNSHIP OF ESQUIMALT
DEVELOPMENT SERVICES

- ISSUED FOR:
- 1 2018-11-23 REZONING
 - 2 2019-02-15 REZONING
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**SHOALING HEIGHTS
RESIDENTIAL
DEVELOPMENT**
CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT, BC

**FLOOR PLANS -
GROUND LEVEL**

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PLOT DATE: 3/8/2019 5:43:22 PM

A2.11

MAR 12 2019
CORP. OF TOWNSHIP OF ESQUIMALT
DEVELOPMENT SERVICES

NORTH

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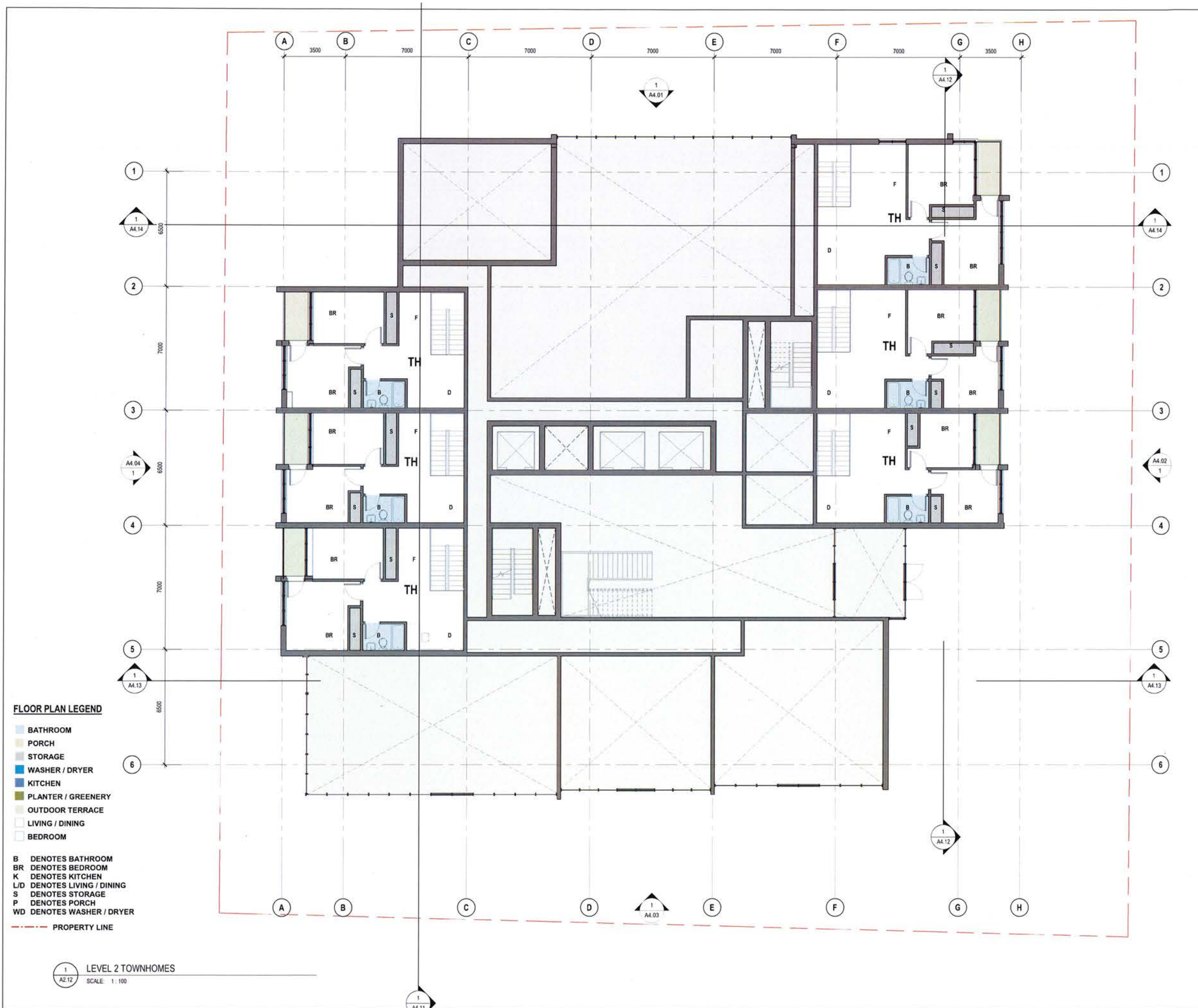
**FLOOR PLANS - 2ND
LEVEL - TOWNHOME**

DRAWN: Author CHECKER: Checker
PLOT DATE: 3/8/2019 5:47:28 PM

A2.12

04849V

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FLOOR PLAN LEGEND

- BATHROOM
- PORCH
- STORAGE
- WASHER / DRYER
- KITCHEN
- PLANTER / GREENERY
- OUTDOOR TERRACE
- LIVING / DINING
- BEDROOM

- B DENOTES BATHROOM
- BR DENOTES BEDROOM
- K DENOTES KITCHEN
- L/D DENOTES LIVING / DINING
- S DENOTES STORAGE
- P DENOTES PORCH
- WD DENOTES WASHER / DRYER

--- PROPERTY LINE

1 LEVEL 2 TOWNHOMES
A2.12 SCALE 1:100



MAR 12 2019

CORP. OF TOWNSHIP OF ESQUIMALT
DEVELOPMENT SERVICES

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SEAL

**SHOALING HEIGHTS
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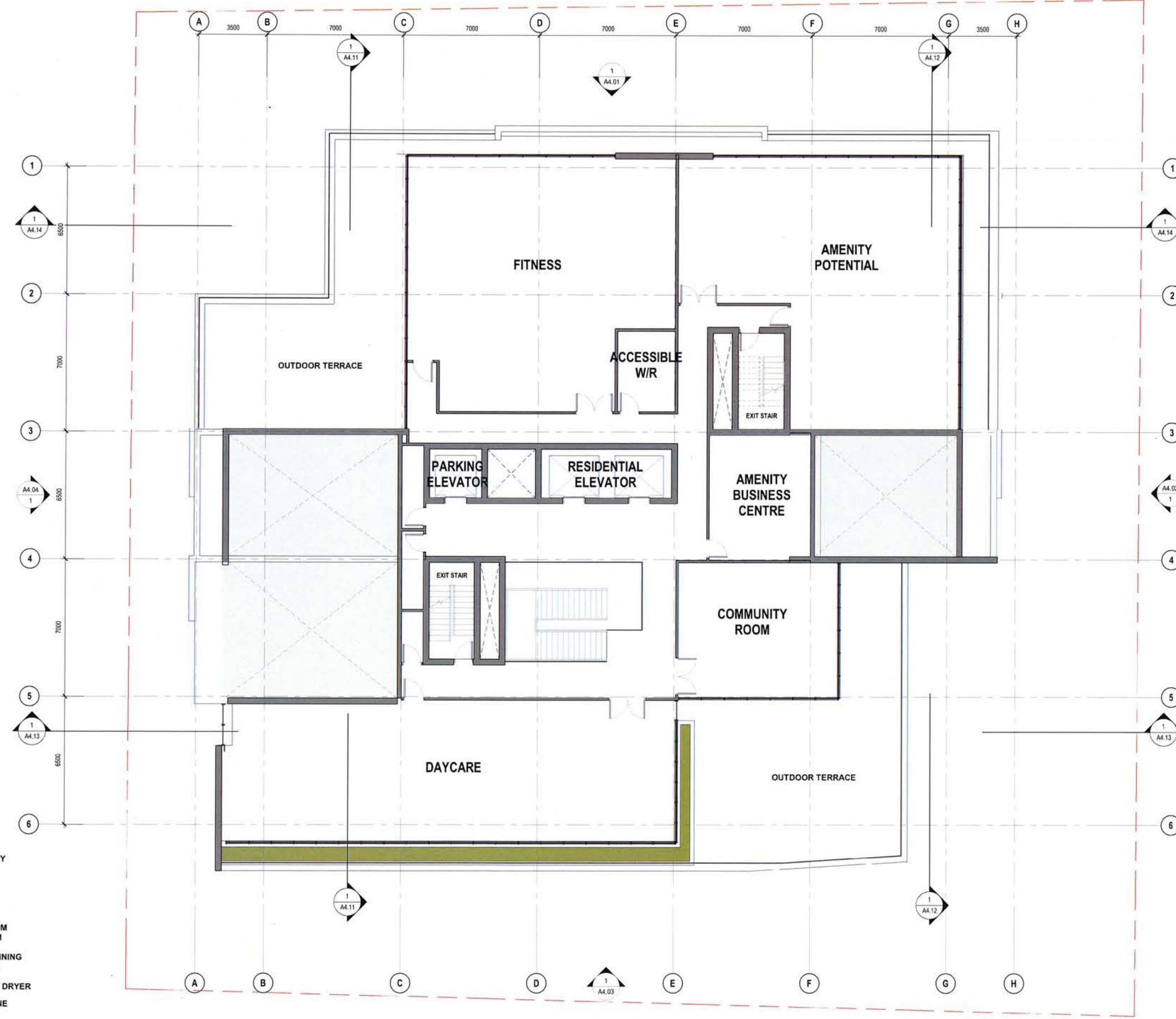
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**FLOOR PLANS - 2ND
LEVEL - AMENITY**

DRAWN: Author CHECKED: Checker
PLOT DATE: 3/8/2019 5:43:55 PM

A2.13

04849V



FLOOR PLAN LEGEND

- BATHROOM
- PORCH
- STORAGE
- WASHER / DRYER
- KITCHEN
- PLANTER / GREENERY
- OUTDOOR TERRACE
- LIVING / DINING
- BEDROOM

- B DENOTES BATHROOM
- BR DENOTES BEDROOM
- K DENOTES KITCHEN
- L/D DENOTES LIVING / DINING
- S DENOTES STORAGE
- P DENOTES PORCH
- WD DENOTES WASHER / DRYER

--- PROPERTY LINE

1 LEVEL 2 AMENITY SPACE
A2.13 SCALE: 1:100



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FLOOR PLANS - 3RD LEVEL

DRAWN: Author CHECKER: Checker
 PLOT DATE: 3/8/2019 5:44:16 PM

A2.14



- FLOOR PLAN LEGEND**
- BATHROOM
 - PORCH
 - STORAGE
 - WASHER / DRYER
 - KITCHEN
 - PLANTER / GREENERY
 - OUTDOOR TERRACE
 - LIVING / DINING
 - BEDROOM
- B DENOTES BATHROOM
 BR DENOTES BEDROOM
 K DENOTES KITCHEN
 L/D DENOTES LIVING / DINING
 S DENOTES STORAGE
 P DENOTES PORCH
 WD DENOTES WASHER / DRYER
- PROPERTY LINE

1 LEVEL 3
 A2.14 SCALE: 1:100



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 CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT, BC

FLOOR PLANS - 4TH TO 11TH LEVEL TYPICAL
 DRAWN: Author CHECK: Checker
 PLOT DATE: 08/03/2019 9:08:51 PM

A2.15

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- FLOOR PLAN LEGEND**
- BATHROOM
 - PORCH
 - STORAGE
 - WASHER / DRYER
 - KITCHEN
 - PLANTER / GREENERY
 - OUTDOOR TERRACE
 - LIVING / DINING
 - BEDROOM
- B** DENOTES BATHROOM
BR DENOTES BEDROOM
K DENOTES KITCHEN
L/D DENOTES LIVING / DINING
S DENOTES STORAGE
P DENOTES PORCH
WD DENOTES WASHER / DRYER
- PROPERTY LINE

1 LEVEL 4 TO 11 TYPICAL
 SCALE: 1:100



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SEAL

SHOALING HEIGHTS RESIDENTIAL DEVELOPMENT

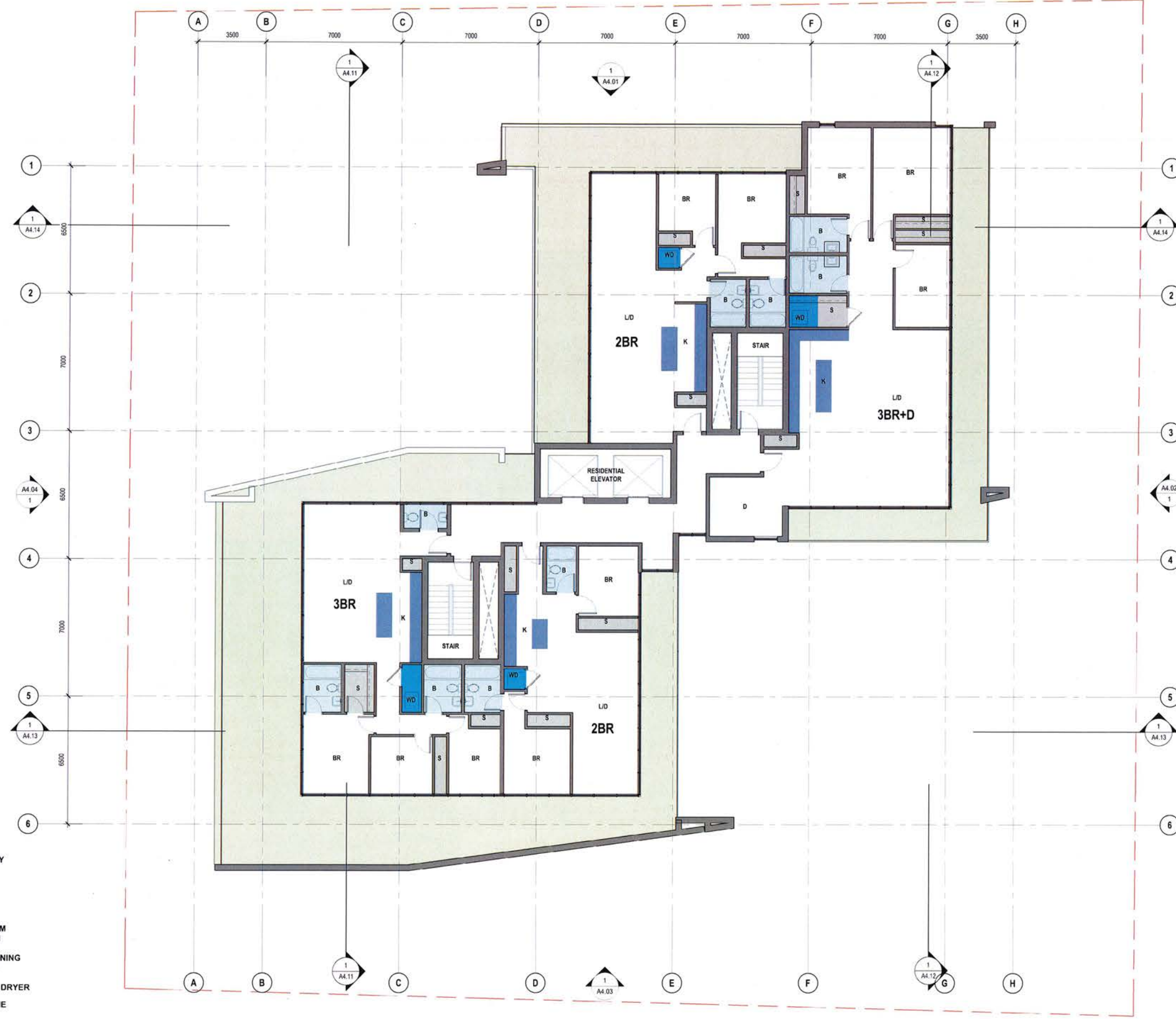
CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT, BC

FLOOR PLANS - 12TH LEVEL

DRAWN: Author CHECKER: Checker
PLOT DATE: 3/8/2019 5:44:53 PM

A2.16

04849V



FLOOR PLAN LEGEND

- B BATHROOM
- PORCH
- STORAGE
- WASHER / DRYER
- KITCHEN
- PLANTER / GREENERY
- OUTDOOR TERRACE
- LIVING / DINING
- BEDROOM

- B DENOTES BATHROOM
- BR DENOTES BEDROOM
- K DENOTES KITCHEN
- L/D DENOTES LIVING / DINING
- S DENOTES STORAGE
- P DENOTES PORCH
- WD DENOTES WASHER / DRYER

--- PROPERTY LINE

1 LEVEL 12
A2.16 SCALE: 1:100



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3	2019-03-08	REZONING

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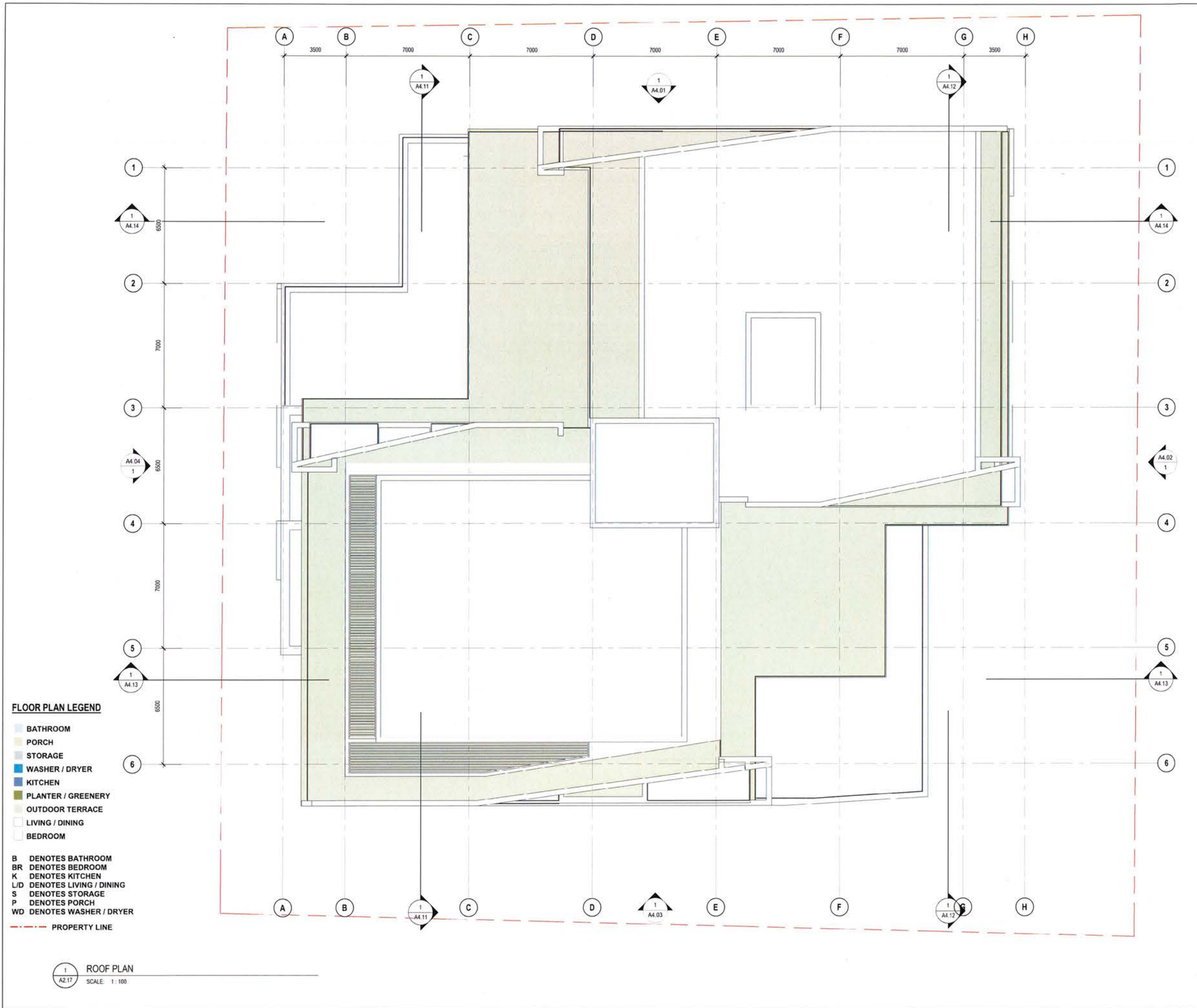
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ROOF PLAN

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 PLOT DATE: 3/8/2019 5:45:11 PM

A2.17

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- FLOOR PLAN LEGEND**
- BATHROOM
 - PORCH
 - STORAGE
 - WASHER / DRYER
 - KITCHEN
 - PLANTER / GREENERY
 - OUTDOOR TERRACE
 - LIVING / DINING
 - BEDROOM
- B DENOTES BATHROOM
 - BR DENOTES BEDROOM
 - K DENOTES KITCHEN
 - L/D DENOTES LIVING / DINING
 - S DENOTES STORAGE
 - P DENOTES PORCH
 - WD DENOTES WASHER / DRYER
- PROPERTY LINE

1 ROOF PLAN
 A2.17 SCALE: 1:100



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 CORP. OF TOWNSHIP
 OF ESQUIMALT
 DEVELOPMENT SERVICES

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**SHOALING HEIGHTS
 RESIDENTIAL
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 CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT,
 BC

**EXTERIOR
 ELEVATIONS**

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A4.01

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1 NORTH ELEVATION
 SCALE 1:100



ISSUED FOR DEVELOPMENT OF TOWNSHIP OF ESQUIMALT

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3	2019-03-08	REZONING

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EXTERIOR ELEVATIONS

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A4.02



1 EAST ELEVATION
 SCALE 1:100



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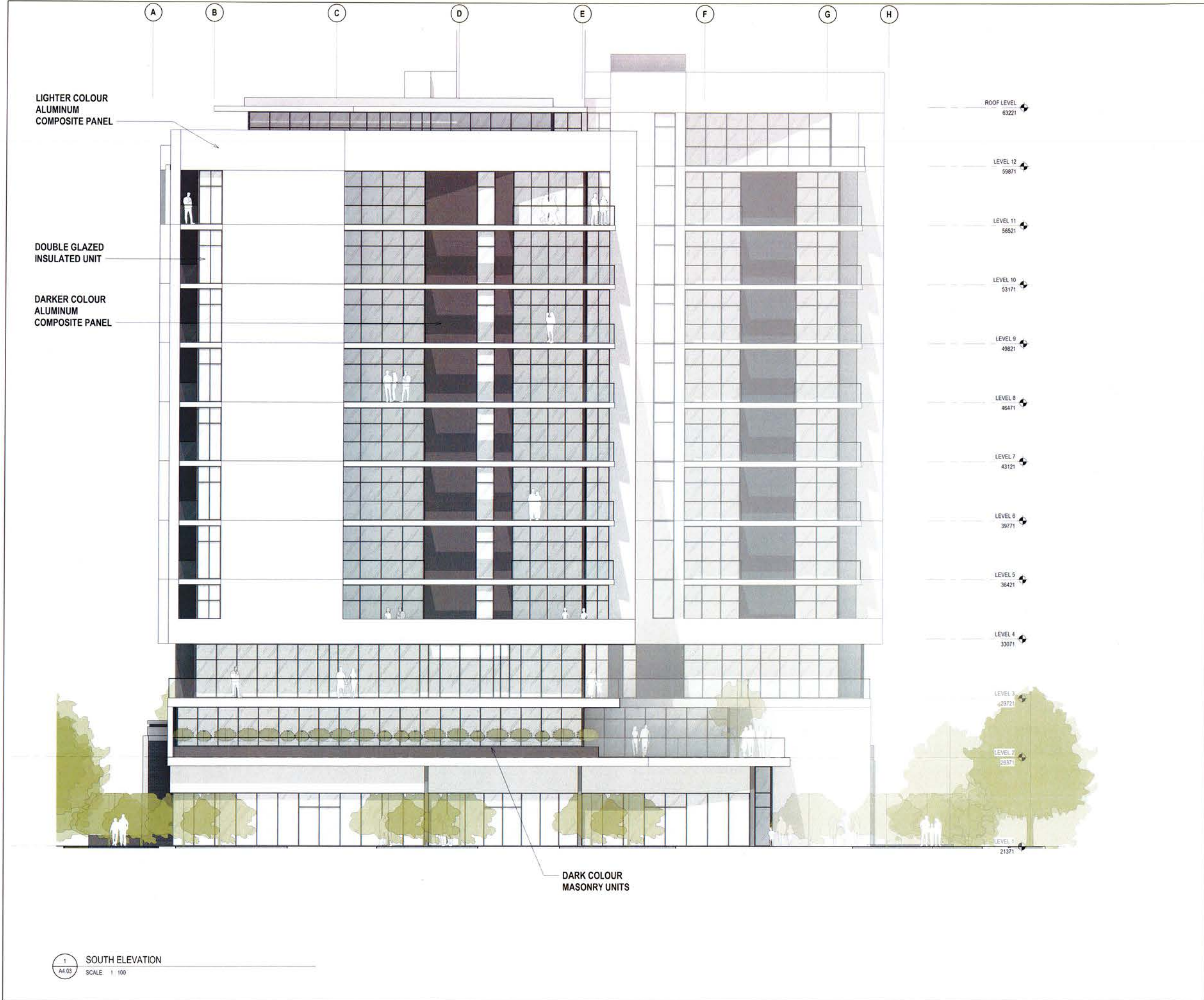
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EXTERIOR ELEVATIONS

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1 SOUTH ELEVATION
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**SHOALING HEIGHTS
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CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT, BC

**EXTERIOR
ELEVATIONS**

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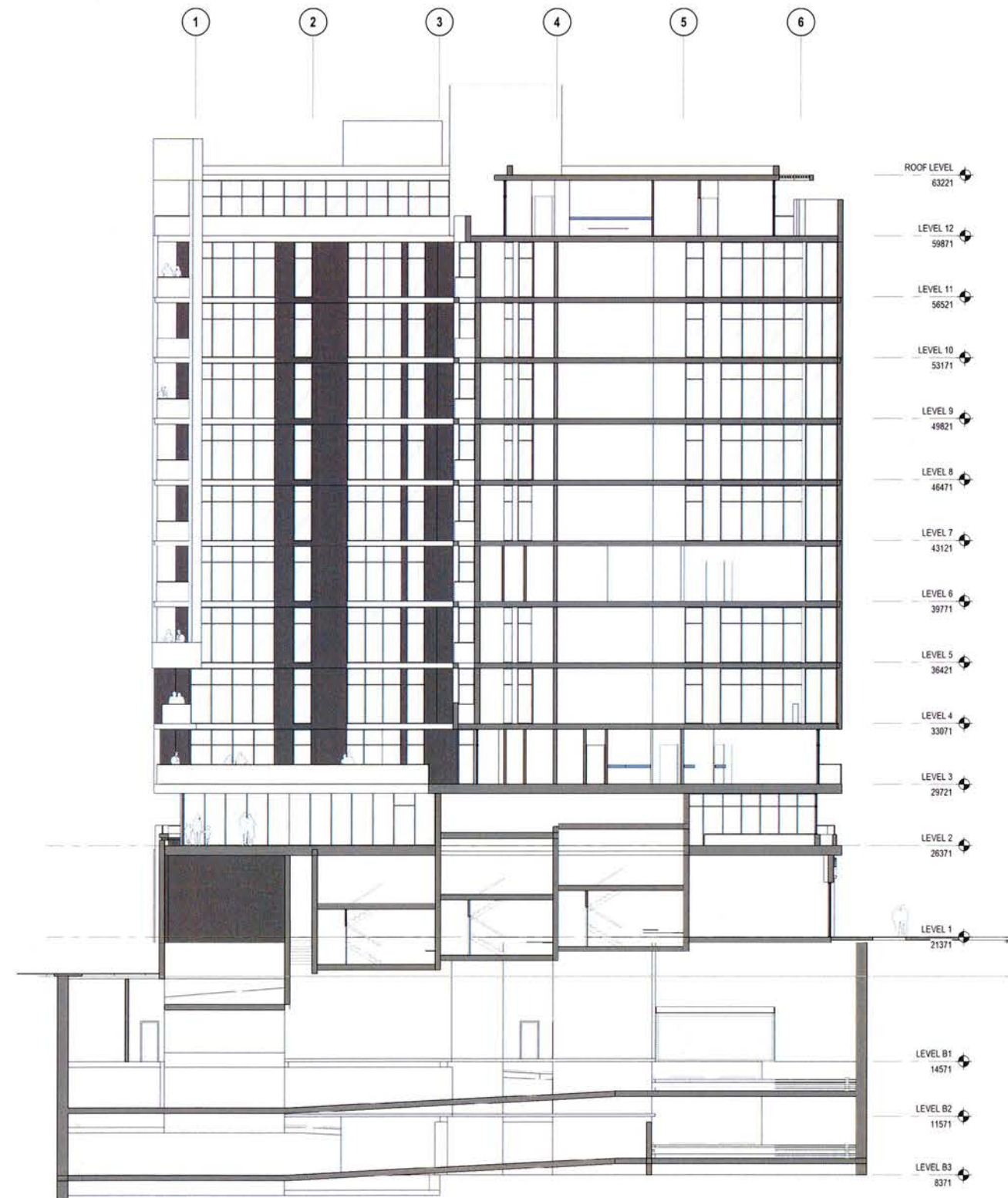
1 WEST ELEVATION
SCALE 1:100



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SEAL

**SHOALING HEIGHTS
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 DEVELOPMENT**
 CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT, BC

BUILDING SECTIONS

DRAWN: Author CHECKER: Checker
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A4.11

1 BUILDING SECTION CUTTING NORTH SOUTH @ NELSON ST
 A4.11 SCALE: 1:150



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**SHOALING HEIGHTS
 RESIDENTIAL
 DEVELOPMENT**

CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT, BC

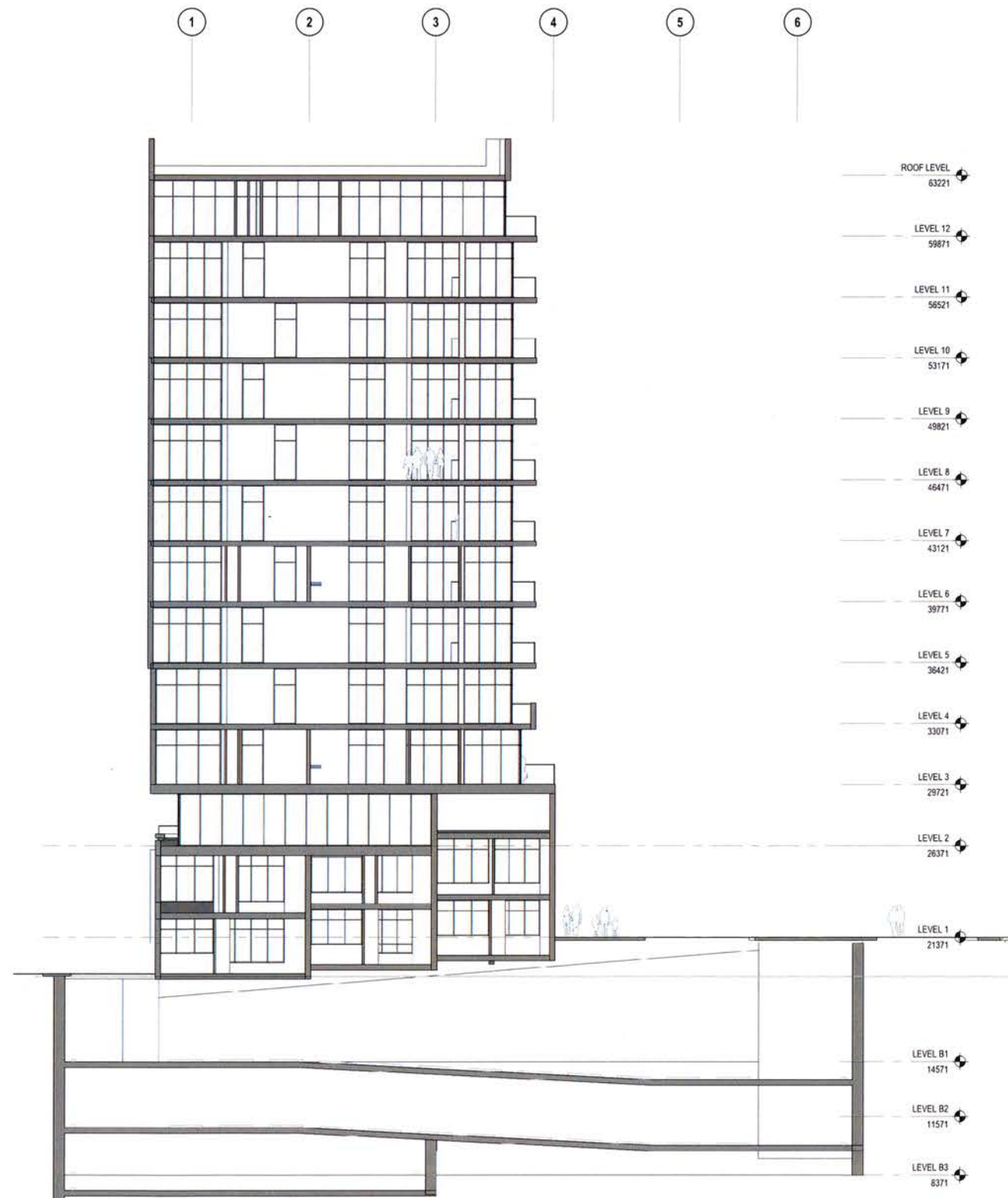
BUILDING SECTIONS

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BUILDING SECTION CUTTING NORTH SOUTH @ CONSTANCE AVE
 SCALE: 1:150



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 RESIDENTIAL
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 CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT, BC

BUILDING SECTIONS

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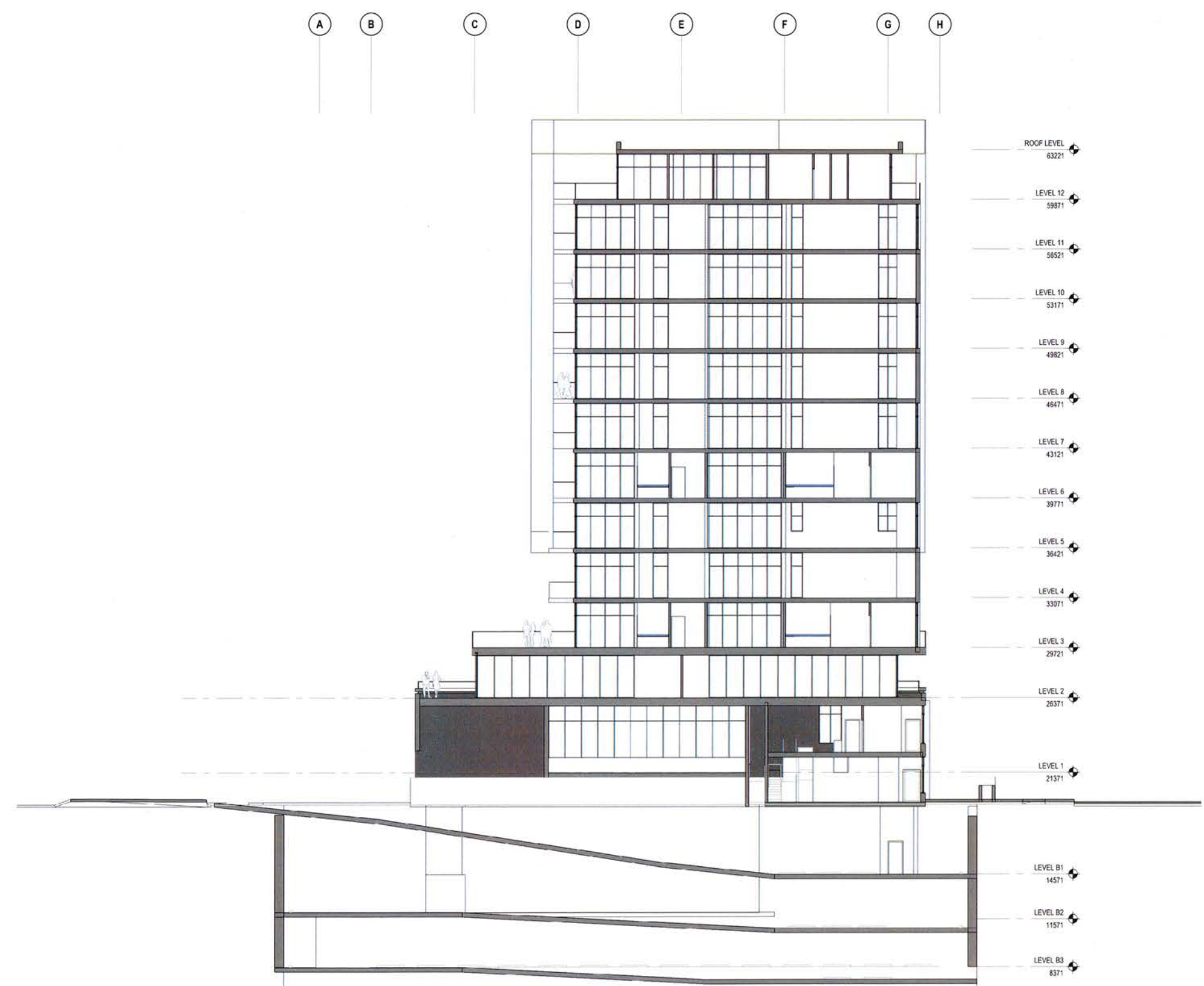
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**SHOALING HEIGHTS
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CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT, BC

BUILDING SECTIONS

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A4.14

1 BUILDING SECTION CUTTING EAST WEST
A4.14 SCALE: 1:150



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**SHOALING HEIGHTS
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 CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT,
 BC
LANDSCAPE PLAN

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**SHOALING HEIGHTS
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**LANDSCAPE PLAN
 2ND LEVEL**

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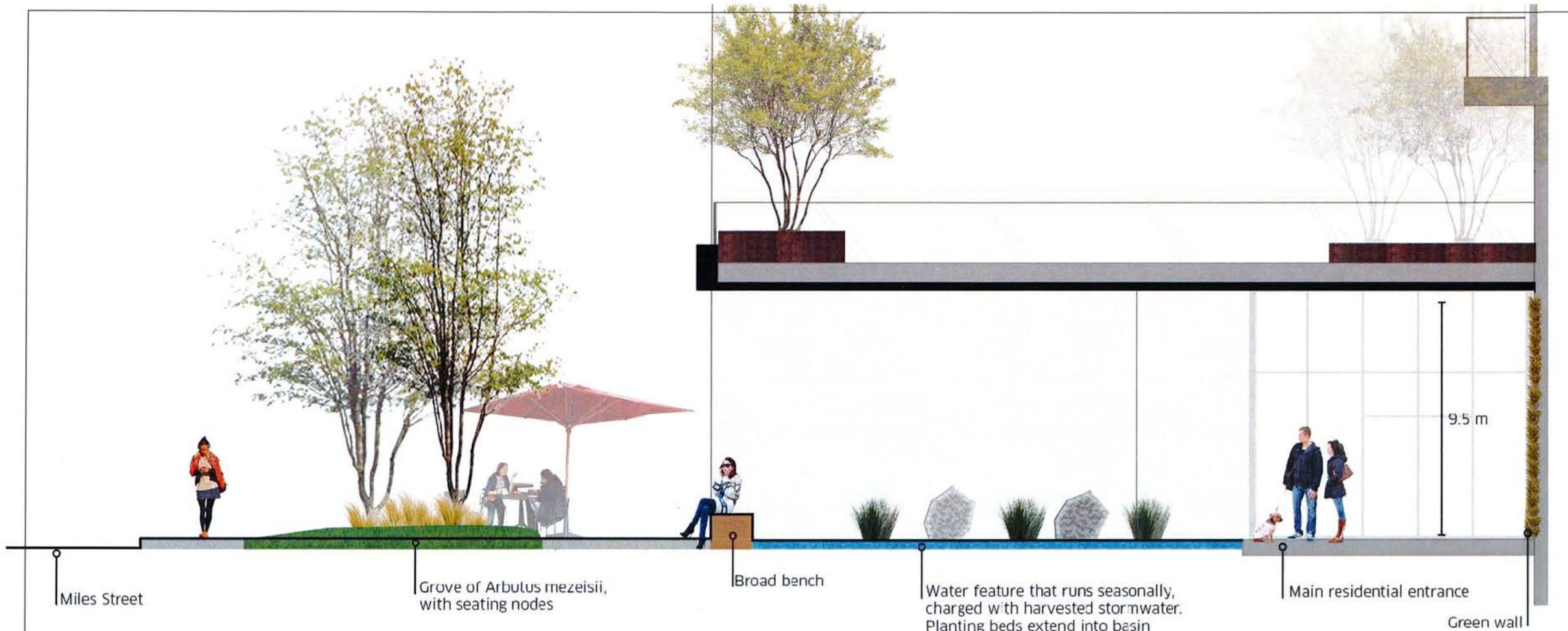
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SHOALING HEIGHTS RESIDENTIAL DEVELOPMENT
 CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT, BC

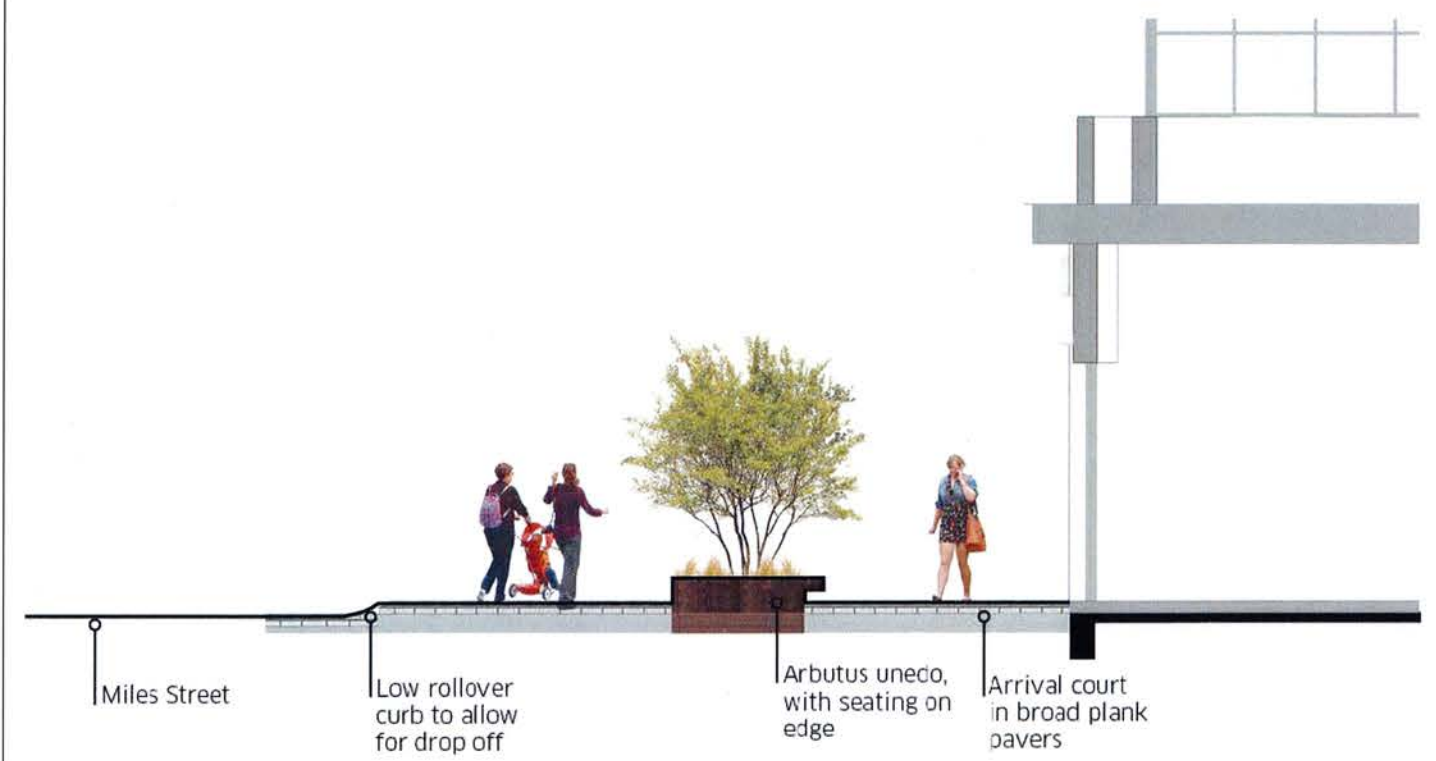
LANDSCAPE SECTIONS A

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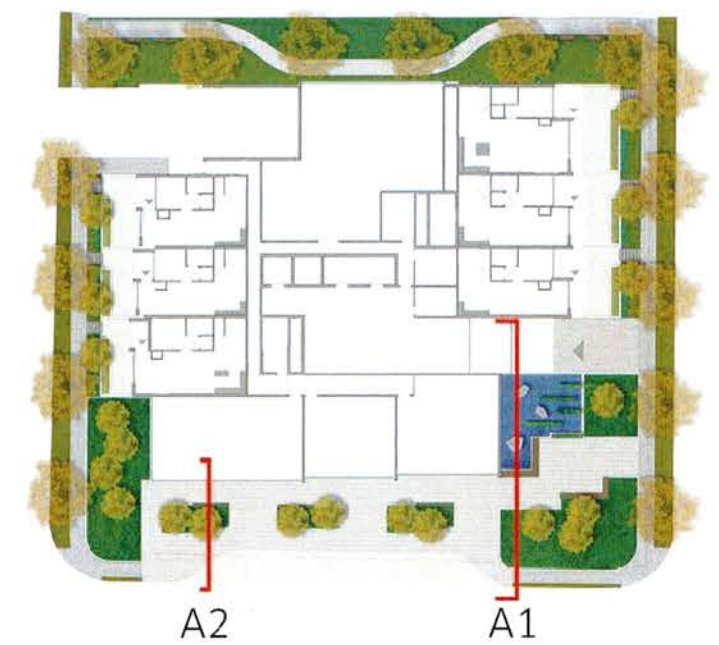
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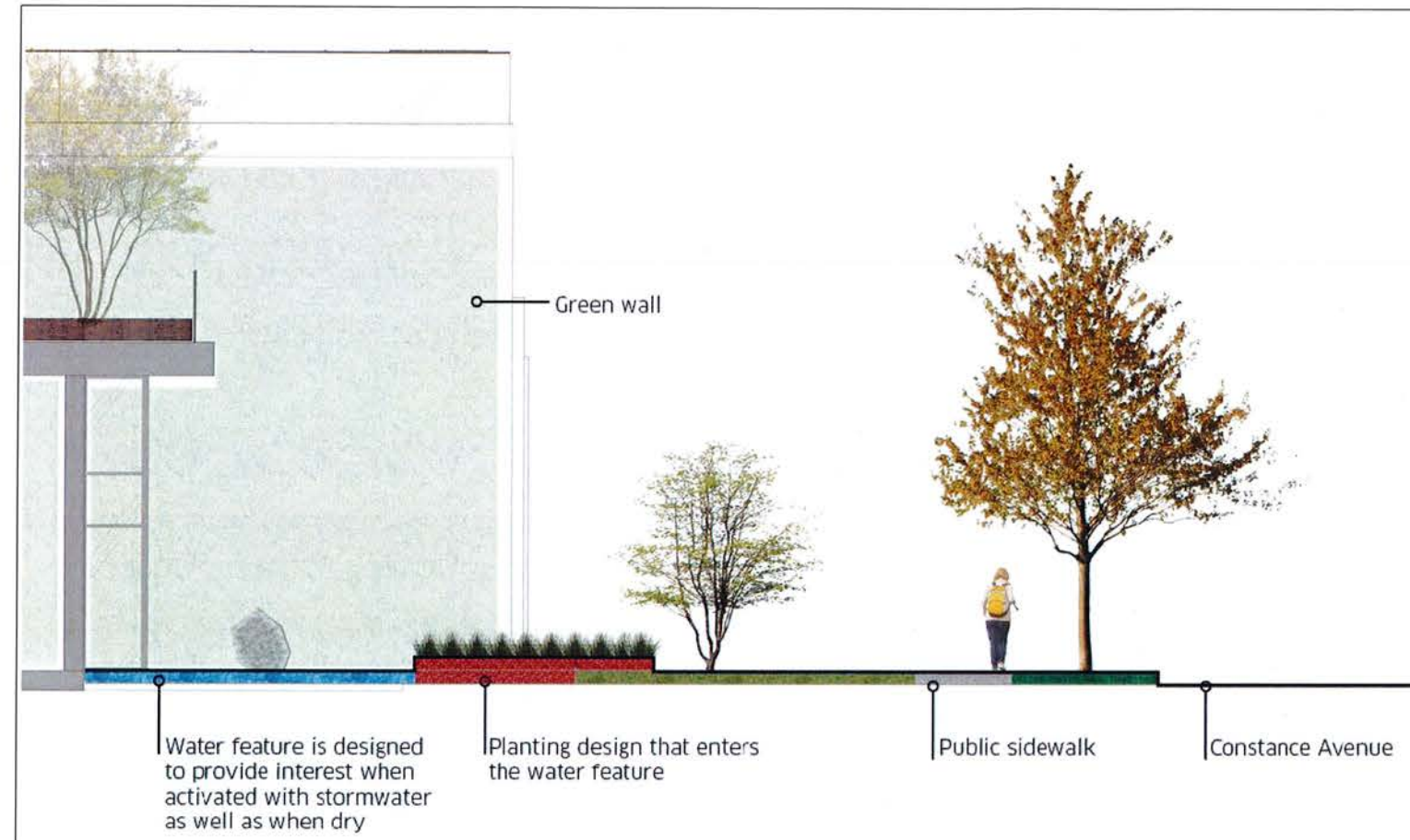


A1 - View West along Miles Street through the Arbutus Corner

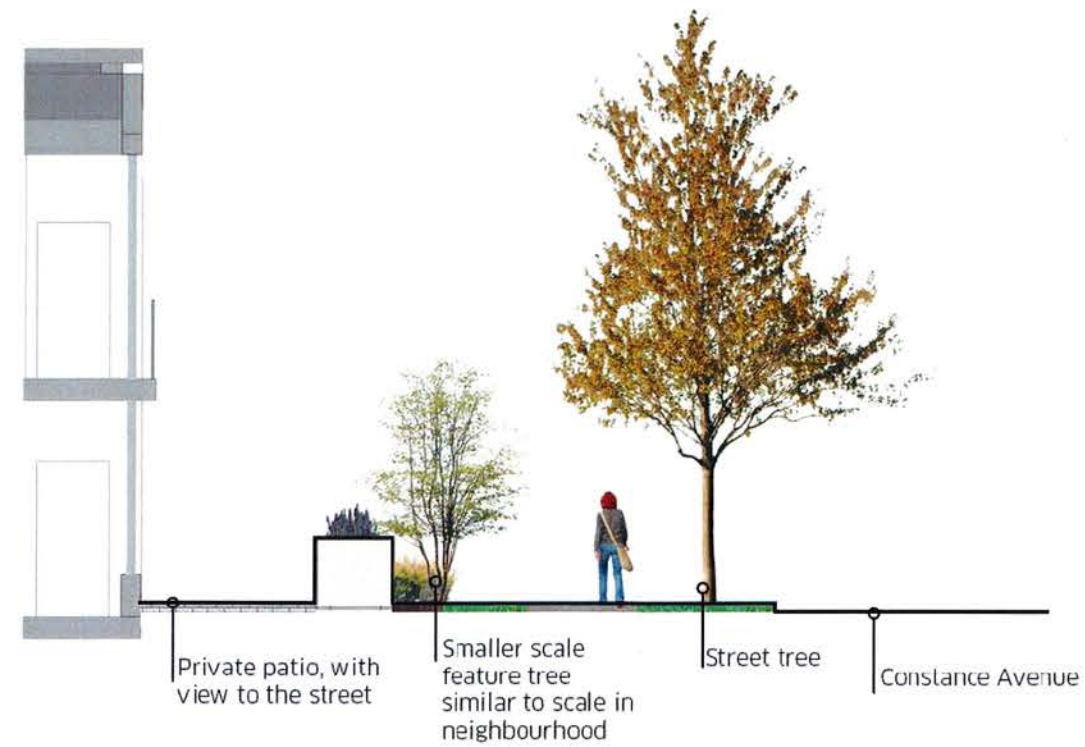


A2 - View West along Miles Street through the Arrival Court



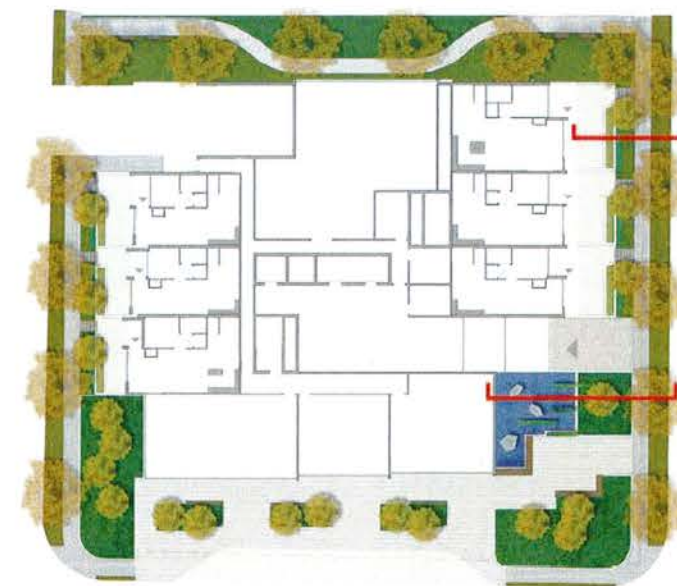


B1 - View North along Constance Avenue at building entrance



B2 - Typical section looking North through townhome on Constance Avenue

sections drawn at 1:50



B2

B1

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CORP. OF TOWNSHIP
OF ESQUIMALT
DEVELOPMENT SERVICES

ISSUED FOR

- 1 2019-02-15 REZONING
- 2 2019-03-08 REZONING

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SEAL

**SHOALING HEIGHTS
RESIDENTIAL
DEVELOPMENT**

CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT,
BC

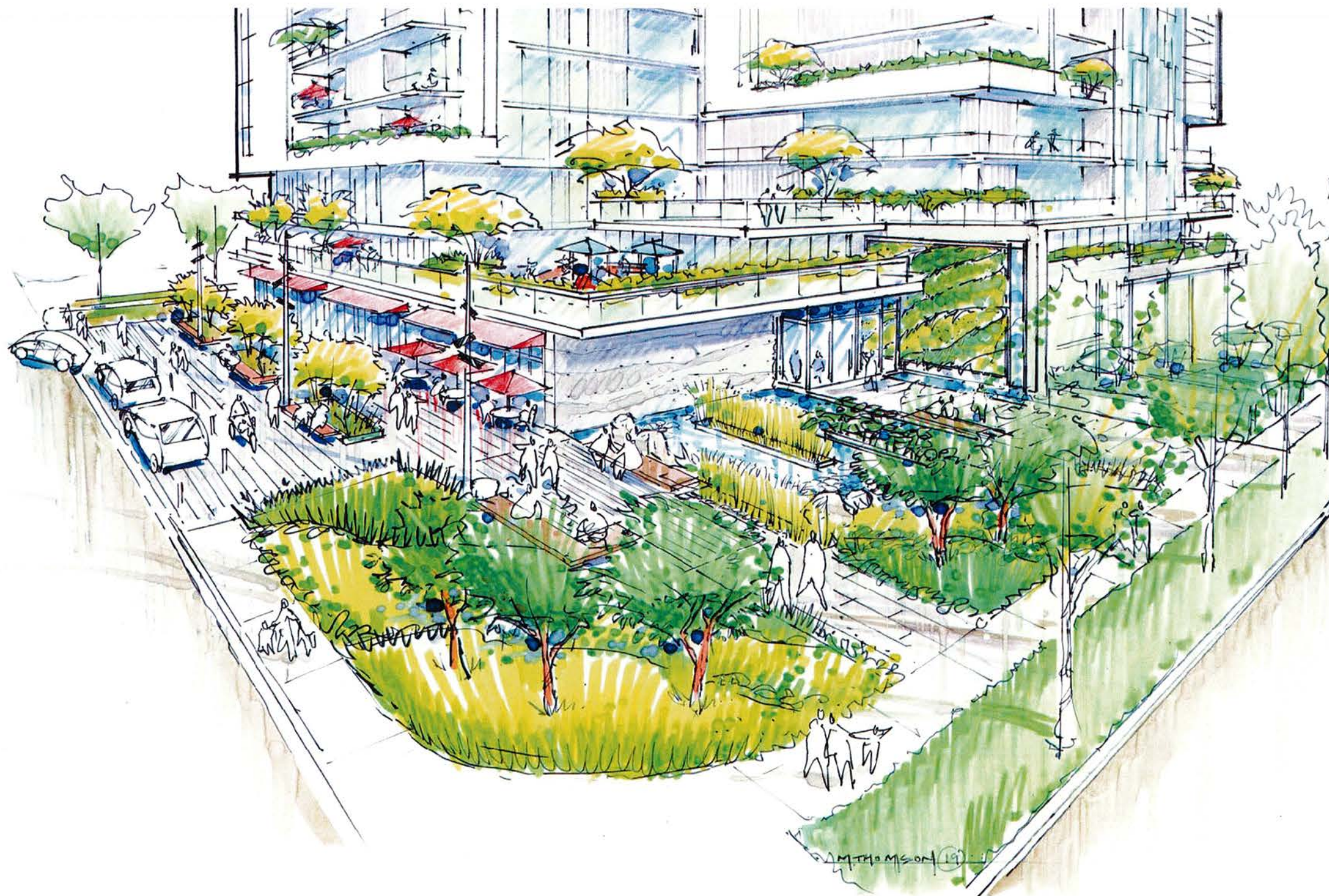
**LANDSCAPE
SECTIONS B**

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A view of Arbutus Corner at Shoaling Heights

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SEAL

**SHOALING HEIGHTS
RESIDENTIAL
DEVELOPMENT**

CONSTANCE AVE/NELSON ST/MILES ST, ESQUIMALT,
BC

LANDSCAPE VIEW

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STREETScape - NELSON STREET
SHOALING HEIGHTS, ESQUIMALT

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ISSUE DATE: 2019 02 15





STREETSCAPE - CONSTANCE STREET
SHOALING HEIGHTS, ESQUIMALT

ISSUED FOR: REZONING
ISSUE DATE: 2019 03 08





Neighborhood Character: Nelson St. off Esquimalt Rd.

- From the public sidewalk, a path to the front door is characterized by a different material than concrete.
- A strip of lawn, with an accent tree, low shrubs delineate the private area



Neighborhood Character: Traveling south on Constance Ave.

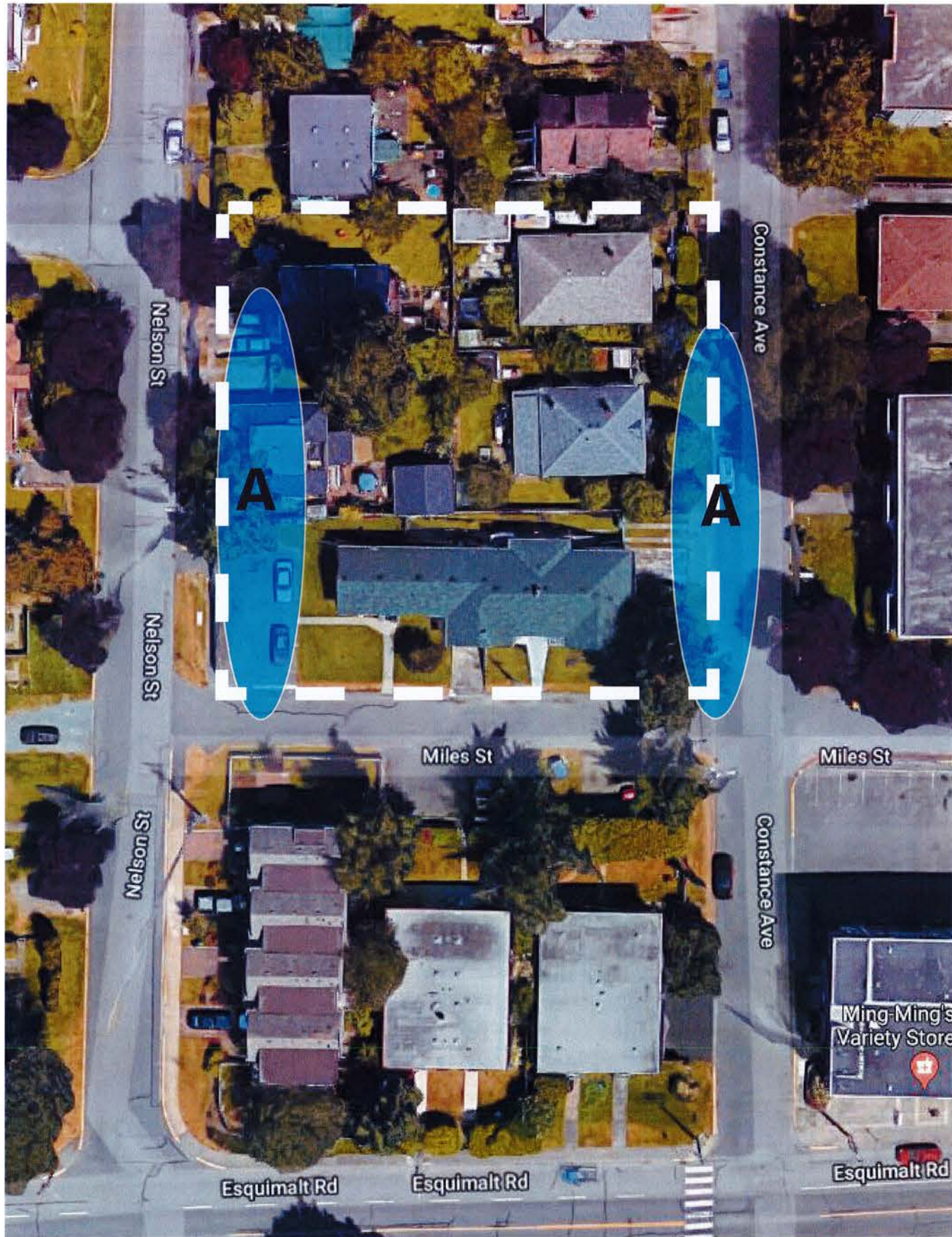
- With no sidewalk, a narrow strip of lawn, followed by a privacy screen shrub
- The trees that are facing the street provide seasonal interest
- Dark colored street trees are numerous



LANDSCAPE CHARACTER - CURRENT
SHOALING HEIGHTS, ESQUIMALT

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A. - Neighborhood Fabric

Maintain the structure and rhythm of a residential neighborhood.

- Low planting adjacent to side walk, maintain sight-lines to front door of town homes
- Border patio with medium size shrubs, offering a buffer to waist height privacy fence
- Plants will be native selections, and will provide seasonal interest either through seasonal color or blooms



Dogwood

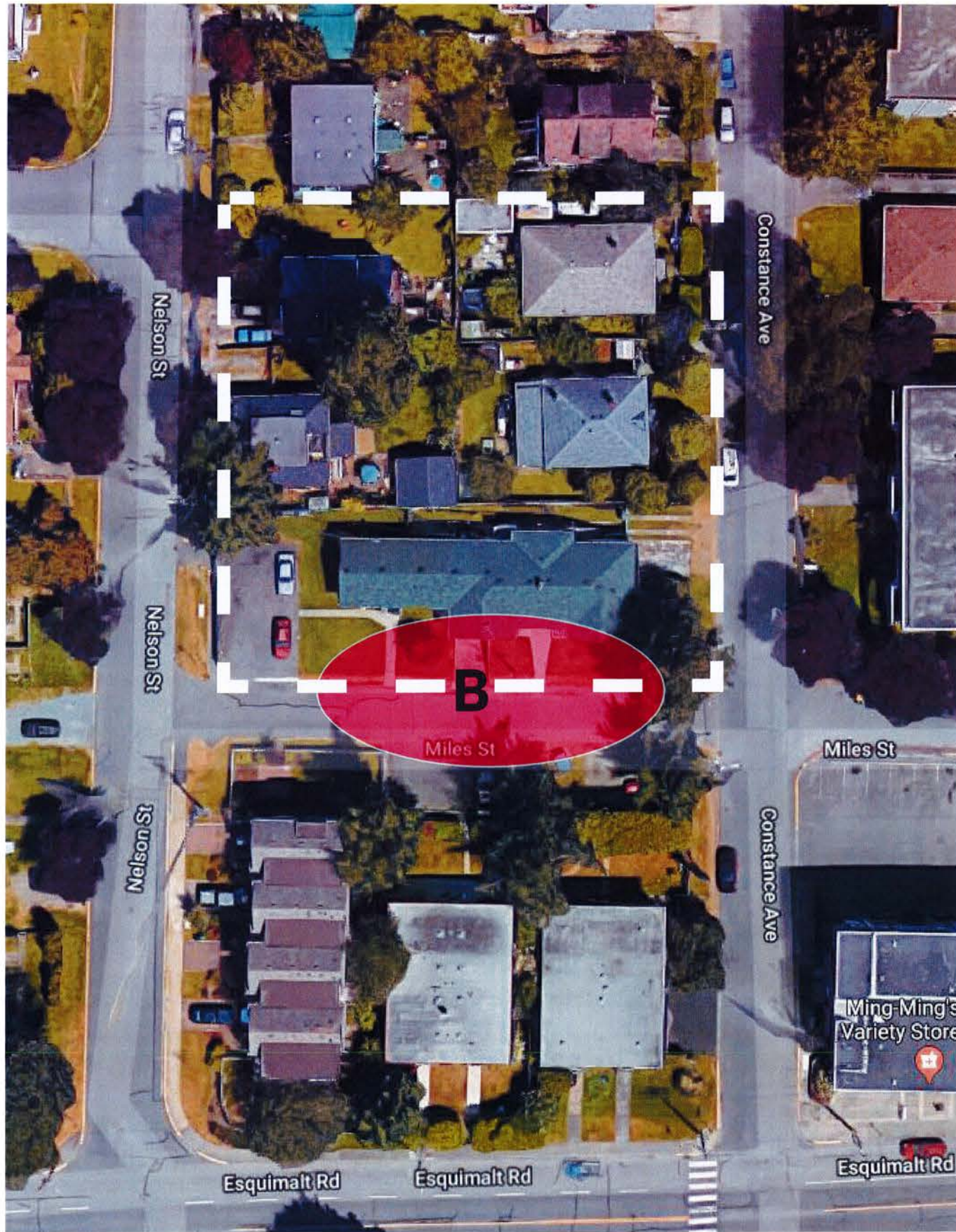


Vine Maple



Witch Hazel





B. - Community Street

- Provide a civic space that allow for people to gather and enjoy amenities
- Stylized hardscape will be unique from surrounding streets, and offer a "welcome mat" to those using the drop off
 - Groupings of medium sized trees will have seating beneath
 - The space between the curb and building will have a generous dimension, reducing narrow encounters.
 - Abrbuts corner will provide a generous seating space with a unique character.



LANDSCAPE STRATEGIES - PROPOSED SHOALING HEIGHTS, ESQUIMALT

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C. - Neighborhood Thruway

A pedestrian thruway that connects the neighborhood network

- A paved accessible path will connect Constance Ave and Nelson St.
- There is an opportunity to have a bocce court, or grand table for community gatherings
- Seating along the path will allow for a moment of rest on a neighborhood walk.
- A single species of tree will provide a bold fall color with a graphic quality (ginkgo tree shown below)





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DATE: 2018-10-23 12:50 FILE: P:\SURVEY\2113\Project\03680-00\616-620\Constance Ave\10.0 DRAWINGS\10.1 Civil 3D\Tce\03680-00\V2.SP.dwg

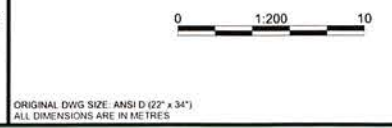


- CATCH BASIN (TOP INLET)
- FIRE HYDRANT
- GUY WIRE
- IRON PIN - FOUND
- IRON PIN - SET
- NON-STANDARD POST - FOUND
- MANHOLE
- MANHOLE - SANITARY
- MANHOLE - STORM
- METER - WATER
- POLE - HYDRO
- POLE - HYDRO/TELEPHONE POLE
- TREE - CONIFEROUS
- TREE - DECIDUOUS
- VALVE - GAS
- VALVE - WATER

- NOTES:
- THIS PLAN SHOWS GROUND MEASURED DISTANCES. TO COMPUTE GRID LEVEL DISTANCES, MULTIPLY GROUND DISTANCES BY A COMBINED FACTOR OF 0.9996108.
 - ELEVATIONS ARE IN METRES AND ARE REFERRED TO GEOODETIC DATUM CV0288C USING MONUMENT B4HD18C. ELEVATION=24.779 METRES.
 - THIS PLAN REPRESENTS FIELD SURVEY CONDUCTED ON AUGUST 08 AND OCTOBER 10, 2018.
 - CONTOUR INTERVAL IS 0.2m.

Rev	Date	Description	Surv	Drawn	App'd
1	2018-10-23	AREA EXPANDED	JC	SA	GQ
0	2018-08-08	ORIGINAL ISSUE	JC	LM	GQ

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Tel 250 370 9221

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JOINT WORKS
5320 89TH ST, EDMONTON, AB

TOPOGRAPHIC SURVEY PLAN OF
616-620 CONSTANCE AVE, 619-623
NELSON ST, AND 1326 MILES ST

ESQUIMALT, BC

Drawing No. **03680-V-2SP**

Project Number 2113-03680-00 Rev. 1

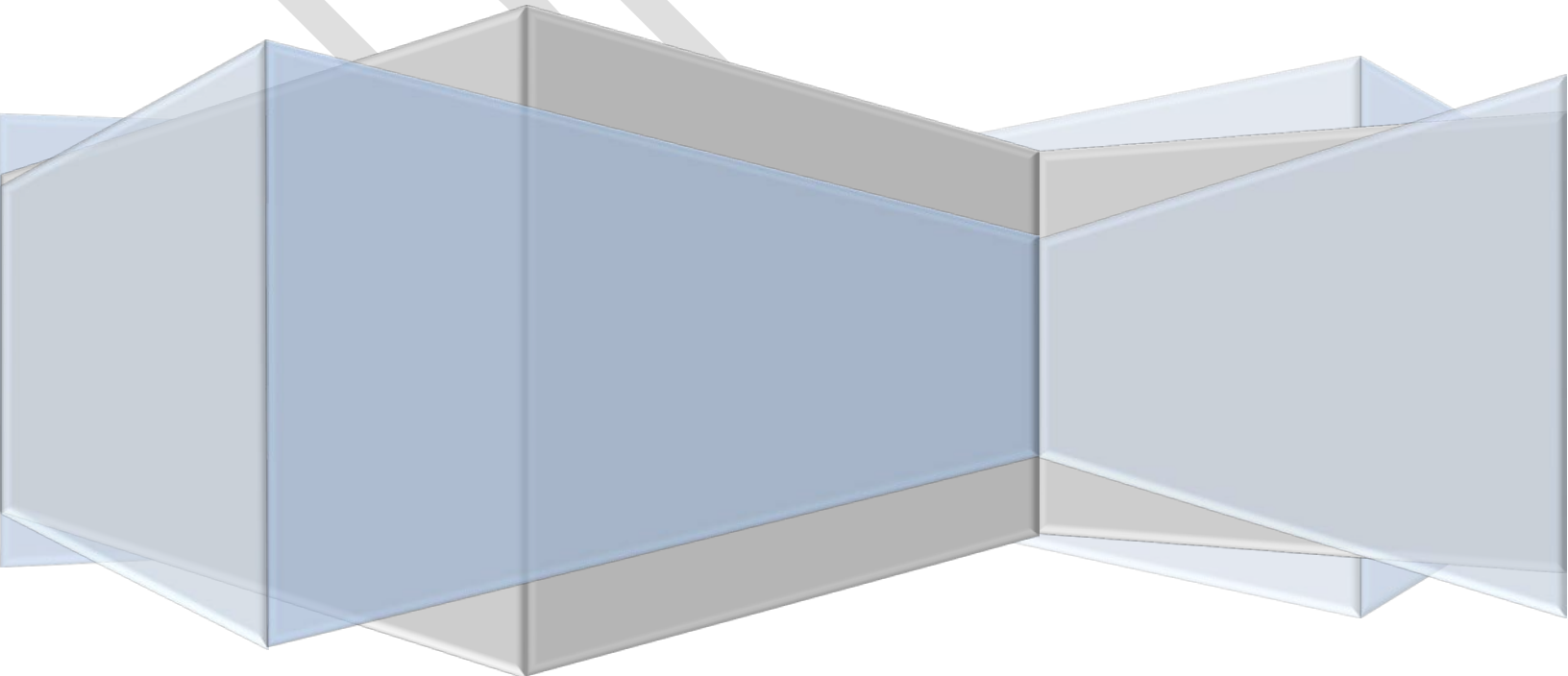
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Township of
ESQUIMALT

**Draft Strategic Priorities
and Goals**

WITH OPERATIONAL STRATEGIES

2019 - 2023



HEALTHY, LIVABLE AND DIVERSE COMMUNITY

Support community growth, housing and development consistent with our Official Community Plan (OCP)

- Develop parking strategy including review and update of parking bylaw
- Facilitate policy discussion for private use of public lands
- Complete housing inventory
- Complete staff report on implications and costs to develop neighborhood design guidelines for the entire community
- Complete staff report on the feasibility and benefits of having a policy in regard to detached accessory dwelling units, carriage houses and below market housing

Support multi-modal transportation strategies that reflect the cumulative impact of business and residential development

- Research and consider best practices for multi-modal traffic initiatives and strategies
- Engage with stakeholders, user and advisory groups to ensure input is considered as development occurs

Support the arts, culture and heritage community

- Develop a Public Art Master Plan
- Provide training for Advisory Planning Commission members on heritage values
- Complete 2 additional statements of significance in accordance with the Heritage Policy
- Utilize Township communication tools to promote and support community events

Enhance opportunities for Parks & Recreation

- Continue with opportunity to replenish Parkland Acquisition Fund
- Develop long range design plans for major parkland areas
- Engage with stakeholders and residents on parks and recreation services

Fully utilize the \$17 million McLoughlin amenity funds to maximum potential

- Identify and facilitate the selection of projects to be funded by McLoughlin amenity funds in conjunction with public consultation

Advance the work of reconciliation with Indigenous People

- Support working group as required as recommendations made to Council

Control deer in partnership with other governments

- Continue with public education program
- Continue with annual deer counts in conjunction with Department of National Defence

HEALTHY, LIVABLE AND DIVERSE COMMUNITY

Develop and implement strategies that reduce impact on the environment consistent with our Climate Action Charter goals

- Evaluate all public buildings for potential energy efficiencies
- Evaluate all equipment purchases consistent with Township sustainable purchasing policies
- Create a Climate Adaptation Plan
- Create a Climate Action Plan
- Participate in the Residential Retrofit Acceleration Project to advance energy efficiency in the built environment

Develop health strategies for our community

- Work with developers and community organizations to support research related to health and wellness
- Work with developers to incorporate health and wellness facilities in new developments
- Develop design guidelines for improving the design of “missing middle” housing (PlanH)
- Continue to develop opportunities that promote healthy and active living

BUILD AND ENHANCE PARTNERSHIPS

Work with Esquimalt and Songhees First Nations to support economic, cultural, social and environmental opportunities

- Organize regular Community to Community forums
- Consult and engage with First Nations on update of Economic Development Strategy
- Consult and engage with First Nations on any Official Community Plan amendments

Work with governments, school districts and stakeholders to enhance relationships and advance issues impacting the community

- Collaborate with CFB Esquimalt, School Districts and other municipalities on efficiencies and partnerships in Parks and Recreation Services
- Promote emergency preparedness through shared initiatives and training
- Utilize Community Safety Working Group on issues impacting the community
- Continue to advocate for improvements to sustainable regional transportation initiatives
- Advocate with other governments for increased childcare opportunities

Work with Esquimalt community groups to advance economic, cultural social and environmental opportunities

- Continue with the Adapting Together Project to assess and prioritize climate risks to Esquimalt
- Create policies and guidelines to improve social health in multi-family housing (Plan H project)
- Continue with consultation for implementation of Checkout Bag Regulation Bylaw

RESILIENT AND DIVERSE ECONOMIC ENVIRONMENT

Develop and seek opportunities for the Municipal Lands within the Esquimalt Town Centre

- Obtain Certificate of Compliance
- Monitor construction of new library space
- Finalize storm and sanitary servicing
- Explore opportunities for Public Safety Building project in conjunction with McLoughlin amenity funds

Support revitalization and beautification initiatives along Esquimalt Road

- Explore further opportunities under the Revitalization Tax Incentive Program
- Enhance intersection safety and explore parking options

Boost investment in the local economy and promote the growth and diversity of businesses

- Review short and long term options for increased focus on economic development including update of Economic Development Strategy, resourcing options and development of marketing strategy
- Consider option to work with South Island Prosperity on an Economic Development Assessment Framework and Business Investment Toolkit
- Work with the Chamber of Commerce to increase engagement with local business
- Facilitate budget discussion to assess readiness for additional business investment and growth
- Utilize relationship with Urban Design Institute and South Vancouver Island Economic Development Association as additional input and resource for economic development
- Provide options to complete a development capacity assessment

LOCAL SERVICES AND INFRASTRUCTURE

Identify infrastructure repair and proactively plan for replacement needs

- Implementation of Inflow and Infiltration management plan and evaluation of underground infrastructure
- Continued implementation of sidewalk master plan
- Continued implementation of roadway master plan
- Continued implementation of street lighting improvements and upgrades
- Explore local initiatives for other waste streams

Identify long term financial requirements for local services and infrastructure

- Complete and update infrastructure and asset inventory
- Development of asset management strategy and plans

Promote opportunities to share services where operationally and financially beneficial

- Develop management plan with the Department of National Defence and Public Works Canada for storm drain spill events

Work to achieve equitable costing and optimization of policing in Esquimalt

- Explore all possibilities to ensure compliance with Victoria and Esquimalt Police Framework Agreement
- Review all alternatives for provision of police services to the community
- Clarify and establish Esquimalt position on the Framework Agreement with Victoria & Esquimalt Police Board

Support ongoing improvements to transportation corridors

- Evaluate transportation corridors for opportunities and options to enhance

Plan for continuity of core local services and infrastructure in the event of an emergency

- Research Opportunities for public alert system
- Review existing plans and update for business continuity and update as necessary

Support Chief Administrative Officer to maintain an organizational culture of collaboration, learning and engagement consistent with the Township's core values

- Ensure hiring practices incorporate core values
- Ensure regular performance evaluations are completed throughout the organization and are reflective of the Core Values

Ensure capacity is adequate to support the goals in the Strategic Plan

- Assess capacity in the organization and develop succession plans
- Work with senior leadership team to develop operational strategies and budgets to achieve strategic goals
- Promote leadership development within the organization

Engage, respect and respond to the needs of our community

- Consider improvement to user services on website to make more accessible
- Explore feasibility of enhanced online services
- Coordinate information and opportunities to improve awareness of financial processes