



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

**ADVISORY PLANNING COMMISSION
AGENDA
TUESDAY MARCH 20, 2018
7:00 P.M.
ESQUIMALT COUNCIL CHAMBERS**

MEMBERS:	Ken Armour, Chair	Amy Higginbotham
	Duncan Cavens	Christina Hamer
	Berdine Jonker	Graeme Dempster
	Nick Kovacs	
STAFF:	Trevor Parkes, Senior Planner, Staff Liaison	
	Pearl Barnard, Recording Secretary	
COUNCIL LIAISON:	Councillor Beth Burton-Krahn	
	Councillor Tim Morrison	

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- I. **CALL TO ORDER**
 - II. **LATE ITEMS**
 - III. **ADOPTION OF AGENDA**
 - IV. **ADOPTION OF MINUTES – February 20, 2018**
 - V. **STAFF REPORTS**

- 1) **DEVELOPMENT VARIANCE PERMIT**
915 Dellwood Road
[PID 001-378-449, Lot B, Section 2, Esquimalt District, Plan 29588]

Purpose of the Application:

The applicant is proposing to build a new rear deck to replace the existing deteriorated one. The proposed rear deck, with its expanded dimensions, encroaches on the rear lot line. The applicant is requesting a variance to allow for a reduction to the minimum setback distance from the Rear Lot Line. A development variance permit is required before a building permit can be issued to authorize this construction.

RECOMMENDATION:

The Esquimalt Advisory Planning Commission recommends that the application for a Development Variance Permit, authorizing construction as shown on plans, stamped "Received February 20, 2018", and sited as detailed on the survey plans prepared by Kenneth Ng, BCLS, stamped "Received February 20, 2018", and including the following relaxations to the Zoning Bylaw, 1992, No. 2050, for the development located at 915 Dellwood Road [PID 001-378-449, Lot B, Section 2, Esquimalt District, Plan 29588], be forwarded to Council with a recommendation **to either approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

Zoning Bylaw, 1992, No. 2050, Section 34. (9)(a)(iii) – Siting Requirements – Principal Building - A 3.12 metres decrease to the requirement that no principal building shall be located within 7.5 metres of a Rear Lot Line, specifically for the

deck located at the rear of the principal building. [from 7.5 metres to 4.38 metres]

2) **REZONING APPLICATION**

638 Constance Avenue

[PID 000-546-437 Lot B (DD 237133I), of Lots 79 and 89, Suburban Lot 44, Esquimalt District, Plan 2854]

640 Constance Avenue

[PID 000-380-911 Amended Lot 88 (DD 208442I), of Suburban Lot 44, Esquimalt District, Plan 2854]

and 637 Nelson Street

[PID 006-386-466 Lot D (DD367731-I), Suburban Lot 44, Esquimalt District, Plan 2854]

Purpose of the Application:

The applicant is requesting a change in Zoning from the current RM-1 [Multiple Family Residential] to a Comprehensive Development District zone [CD]. This change is required to accommodate the proposed 6 storey, 77 unit, multiple family residential building including a 61 space parking garage.

This site is located within Development Permit Area No. 1 – Multi-Unit Residential. Should the rezoning application be approved, the applicant would need to obtain a Development Permit respecting the character of the development, including landscaping, and the siting, form, exterior design and finish of the proposed 6 storey, 77 unit, multiple family residential building, which would be considered by both the Design Review Committee and Council in the future.

Evaluation of this application should focus on the proposed siting, height, mass, density, lot coverage, usable open space, parking, fit with the neighbourhood, and consistency with the overall direction contained within the Official Community Plan.

RECOMMENDATION:

The Esquimalt Advisory Planning Commission recommends that the application for Rezoning, authorizing a 20.5 metre [6 storeys], 77 unit, multiple family residential building sited in accordance with the BCLS Site Plan provided by J.E. Anderson and Associates Surveyors-Engineers, stamped “Received January 18, 2018”, and incorporating height and massing consistent with the architectural plans provided by Praxis Architects Inc., stamped “Received January 16, 2018”, detailing the development proposed to be located at 638 Constance Avenue [PID 000-546-437 Lot B (DD 237133I), of Lots 79 and 89, Suburban Lot 44, Esquimalt District, Plan 2854], 640 Constance Avenue [PID 000-380-911 Amended Lot 88 (DD 208442I), of Suburban Lot 44, Esquimalt District, Plan 2854], and 637 Nelson Street [PID 006-386-466 Lot D (DD367731-I), Suburban Lot 44, Esquimalt District, Plan 2854] be forwarded to Council with a recommendation **to either approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

3) **REVIEW OF THE DRAFT OFFICIAL COMMUNITY PLAN**

Purpose of the Application:

To review the Draft Official Community Plan and provide Council with any recommended changes along with reasons for the proposed changes.

VI. NEXT REGULAR MEETING

Tuesday, April 17, 2018

VII. ADJOURNMENT



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

ADVISORY PLANNING COMMISSION MINUTES OF FEBRUARY 20, 2018 ESQUIMALT COUNCIL CHAMBERS

PRESENT:	Ken Armour, Chair Nick Kovacs Duncan Cavens	Christina Hamer Berdine Jonker
ABSENT:	Amy Higginbotham, Graeme Dempster	
STAFF:	Bill Brown, Director of Development Services, Staff Liaison Karen Hay, Planner Alex Tang, Planner Pearl Barnard, Recording Secretary	
COUNCIL LIAISONS:	Councillor Tim Morrison	

I. CALL TO ORDER

The Chair called the meeting to order at 7:00 p.m.

II. LATE ITEMS

There were no late items.

III. APPROVAL OF THE AGENDA

Moved by Duncan Cavens, seconded by Nick Kovacs: That the agenda be approved as circulated. **CARRIED UNANIMOUSLY.**

IV. ADOPTION OF MINUTES

Moved by Nick Kovacs, seconded by Duncan Cavens: That the minutes of the Advisory Planning Commission held January 16, 2018 be adopted as circulated. **CARRIED UNANIMOUSLY.**

V. STAFF REPORTS

1) REZONING and OFFICIAL COMMUNITY PLAN AMENDMENT 520 Constance Avenue

Staff outlined that the applicant is requesting an amendment to Official Community Plan Bylaw, 2006, No. 2646 to change the property's designation from 'Institutional' to 'Townhouse Residential', and an amendment to Zoning Bylaw, 1992, No. 2050 to change the property's zoning from 'Institutional Day Use' [P-4] to 'Multiple Family Residential' [RM-1] to permit future residential use of the subject property. The current use of the property as 'day services for adults with disabilities' is no longer needed at the subject location as the Victoria Association for Community Living is moving their facility to another building in the neighbourhood, at 520 Comerford Street, and the proposed changes will facilitate sale of the property.

Ellen Tarshis, Executive Director of Community Living Victoria, gave a brief introduction to the work of their organization, and advised that they had chosen the subject property for its residential setting, requested the rezoning to an institutional use to specifically match their needs, and that they had now found a newer, larger facility in the neighbourhood that will better fit their organization's needs.

Commission comments included (*response in italics*):

- was there any demand from other organizations to use this institutional facility? *Ms. Tarshis explained that the zone was created very specific to their needs.*

RECOMMENDATION:

Moved by Nick Kovacs, seconded by Berdine Jonker: That the application for rezoning and an OCP amendment authorizing the change from Institutional use to Residential use without any physical changes to the property at 520 Constance Avenue [PID 006-377-408, Lot 132, Suburban Lot 38, Esquimalt District, Plan 2854] **be forwarded to Council with a recommendation by the Esquimalt Advisory Planning Commission to approve**; as the requested use fits with the uses and character of the neighbourhood. **Carried Unanimously.**

**2) OFFICIAL COMMUNITY PLAN AMENDMENT and REZONING APPLICATION
833 Dunsmuir Road and 835 Dunsmuir Road**

Staff outlined that the applicant is requesting a change in Official Community Plan Land Use Designation and Zoning from the current designation of “Multi-Unit, Low-Rise Residential” to “Multi-Unit, High-Rise Residential” and a change in zoning from the current mix of RD-3 [Two Family/Single Family Residential] zone and RM-4 [Multiple Family Residential] to a Comprehensive Development District zone [CD], to accommodate the proposed 5 storey, 34 unit, multiple family residential building.

Heather Spinney, Praxis Architects Inc., and Jordan Mann & Graham Mann, Owners, were in attendance. Heather Spinney gave a PowerPoint presentation detailing the site plan and a brief overview of the building design, materials, and landscape plan.

Commission comments included (*response in italics*):

- proposal fits within the surrounding area, ideal location for someone who doesn't need a vehicle, as it is close to transit
- how will this building help Esquimalt reach it's target of 38% greenhouse gas reduction by 2020? *Staff advised that it is well documented if density is moved closer to the core where the jobs are, then greenhouse gases associated with transportation will be reduced; this is a great location for higher density as it is close to Navy Base, graving dock and bus routes, and option of using bicycles or walking within pedestrian oriented neighbourhood.*
- vehicle charging stations? *Ms. Spinney advised that they are considering Modo and the installation of charging stations.*
- one and two bedrooms units are desirable
- parking issue with increased density; concern that overflow parking will end up on Garrett Place or Dunsmuir Road impacting local residences
- visitor parking spaces? *Ms. Spinney advised approximately three.*
- step back of development on fifth storey? *Staff clarified that the building must step back beyond the third storey according to the OCP policy Section 2.2.4.4.*
- importance of valuing the context, character, and heritage values of a neighbourhood in assessing a proposed development

RECOMMENDATION:

Moved by Duncan Cavens, seconded by Nick Kovacs: That the application for an Official Community Plan Amendment and Rezoning, authorizing a 18 metre [5 storeys], 34 unit, multiple family residential building sited in accordance with the BCLS Site Plan provided by J.E. Anderson and Associates Surveyors-Engineers, stamped "Received January 18, 2018", and incorporating height and massing consistent with the architectural plans provided by Praxis Architects Inc., stamped "Received February 7, 2018", detailing the development proposed to be located at 833 Dunsmuir Road [PID 005-388-899 Lot 3, Section 11, Esquimalt District, Plan 9759] and 835 Dunsmuir Road [PID 005-388-881 Lot 2, Section 11, Esquimalt District, Plan 9759] **be forwarded to Council with a recommendation by the Esquimalt Advisory Planning Commission for approval** as this is a suitable location for development and it is a relatively sensitive building within the context. **Carried Unanimously.**

**3) OFFICIAL COMMUNITY PLAN AMENDMENT and REZONING APPLICATION
838 Admirals Road and 842 Admirals Road**

Staff outlined that the applicant is requesting a change in Official Community Plan Land Use Designation and Zoning from the current designation of "Townhouse Residential" to "Multi-Unit, Low-Rise Residential" and a change in zoning from the current mix of CD-75 [Comprehensive Development District] zone and RD-3 [Two Family/Single Family Residential] to a Comprehensive Development District zone [CD] to accommodate the proposed 4 storey, 30 unit, multiple family residential building.

Heather Spinney, Praxis Architects Inc. gave a PowerPoint presentation detailing the site plan and a brief overview of the building design, materials and landscape plan.

Commission comments included (*responses in italics*):

- good fit and will be effective in addressing the housing shortage
- ideal location for densification as it is on a busy route where it is no longer desirable to have single family homes
- will owner be managing the rental building? *Ms. Spinney advised that it is her understanding that the owner would be managing the building.*
- there are one and two bedroom units, but why no three bedroom units? *Ms. Spinney advised the direction from their client was for one and two bedroom units, and that three bedroom units can sometimes be tricky to fit in.*
- pleased with the amount of bike parking and suggested that the applicant consider having designated bike stalls for each unit
- consider installing electric vehicle charging stations
- streetscape on Admirals Road is basically a concrete wall with some landscaping. *Ms. Spinney advised that it is not going to be exposed concrete wall as it will be behind plantings and they are still working on this now that the plan is to remove Garry Oak tree.*
- increase in traffic on both Coles Street and Kindersley Road; applicant consider a traffic analysis. *Staff clarified that it is an uncontrolled intersection.*

RECOMMENDATION:

Moved by Berdine Jonker, seconded by Nick Kovacs: That the application for an Official Community Plan Amendment and Rezoning, authorizing a 15.4 metre [4 storeys], 30 unit, multiple family residential building sited in accordance with the BCLS Site Plan provided by

J.E. Anderson and Associates Surveyors-Engineers, stamped “Received January 18, 2018”, and incorporating height and massing consistent with the architectural plans provided by Praxis Architects Inc., stamped “Received February 8, 2018”, detailing the development proposed to be located at 838 Admirals Road [PID 005-074-011 Lot 17, Block 7, Section 10, Esquimalt District, Plan 2546 Except Plan 86845] and 842 Admirals Road [PID 006-324-118 Lot 16, Block 7, Section 10, Esquimalt District, Plan 2546] **be forwarded to Council with a recommendation by the Esquimalt Advisory Planning Commission for approval** on the basis that this is an ideal location for increased density. **Carried Unanimously.**

VI. NEXT REGULAR MEETING

Tuesday, March 20, 2017

VII. ADJOURNMENT

The meeting adjourned at 8:10 p.m.

CERTIFIED CORRECT

CHAIR, ADVISORY PLANNING COMMISSION
THIS 20th DAY OF MARCH, 2018

ANJA NURVO, CORPORATE OFFICER



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

APC Meeting: March 20, 2018

STAFF REPORT

DATE: March 13, 2018

TO: Chair and Members of the Advisory Planning Commission

FROM: Alex Tang, Planner
Bill Brown, Director of Development Services

**SUBJECT: DEVELOPMENT VARIANCE PERMIT
915 Dellwood Road
[PID 001-378-449, Lot B, Section 2, Esquimalt District, Plan 29588]**

RECOMMENDATION:

The Esquimalt Advisory Planning Commission recommends that the application for a Development Variance Permit, authorizing construction as shown on plans, stamped "Received February 20, 2018", and sited as detailed on the survey plans prepared by Kenneth Ng, BCLS, stamped "Received February 20, 2018", and including the following relaxations to the Zoning Bylaw, 1992, No. 2050, for the development located at 915 Dellwood Road [PID 001-378-449, Lot B, Section 2, Esquimalt District, Plan 29588], be forwarded to Council with a recommendation **to either approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

Zoning Bylaw, 1992, No. 2050, Section 34. (9)(a)(iii) – Siting Requirements – Principal Building - A 3.12 metres decrease to the requirement that no principal building shall be located within 7.5 metres of a Rear Lot Line, specifically for the deck located at the rear of the principal building. [from 7.5 metres to 4.38 metres]

BACKGROUND:

Purpose of the Application:

The applicant is proposing to build a new rear deck to replace the existing deteriorated one. The proposed rear deck, with its expanded dimensions, encroaches on the rear lot line. The applicant is requesting a variance to allow for a reduction to the minimum setback distance from the Rear Lot Line. A development variance permit is required before a building permit can be issued to authorize this construction.

Context

Applicant / Owner: Ricky Mongrain

Property Size: Metric: 696 m² Imperial: 7491 ft²

Existing Land Use: Single Family Residential

Surrounding Land Uses:

North: Single Family Residential
South: Single Family Residential
West: Single Family Residential
East: Single Family Residential

Existing Zoning: RS-1 [Single Family Residential]

Zoning

The siting requirement for a Principal Building in the Single Family Residential [RS-1] zone is at least 7.5 metres from a Rear Lot Line. The proposed deck would encroach into the rear setback and would be 4.38 metres from the Rear Lot Line.

Public Notification

As this is a development variance permit application, should it proceed to Council, notice would be mailed to tenants and owners of properties within 50 metres (164 feet) of the subject property.

ALTERNATIVES:

1. Forward the application for a Development Variance Permit to Council with a **recommendation of approval including reasons for the recommendation.**
2. Forward the application for a Development Variance Permit to Council with a **recommendation of approval including specific conditions and including reasons for the recommendation.**
3. Forward the application for a Development Variance Permit to Council with a **recommendation of denial including reasons for the recommendation.**



Subject Property Map:
915 Dellwood Road



DIVISION 1 - RESIDENTIAL ZONES

34. SINGLE FAMILY RESIDENTIAL [RS-1]

The intent of this Zone is to accommodate Single Family Dwellings on individual Parcels of land.

(1) Permitted Uses

The following Uses and no others are permitted:

- (a) Single Family Residential
- (b) Home Occupation
- (c) Secondary Suite: subject to the requirements of Section 30.6
- (d) Boarding: subject to the requirements of Section 30.3
- (e) Urban Hens: subject to the requirements of Section 30.4 of this bylaw.

(2) Parcel Size

The minimum Parcel Size for Parcels created by subdivision shall be 530.0 square metres.

(3) Minimum Lot Width

The minimum width of Parcels created by subdivision shall be 16 metres measured at the Front Building Line.

(4) Floor Area Ratio

The Floor Area Ratio shall not exceed 0.35.

(5) Floor Area

The minimum Floor Area for the First Storey of a Principal Building shall be 88 square metres.

(6) Building Height

- (a) No Principal Building shall exceed a Height of 7.3 metres.
- (b) No Accessory Building shall exceed a Height of 3.6 metres.

(7) Building Width

The minimum width for any Single Family Dwelling shall be 7 metres.

(8) Lot Coverage

- (a) All Principal Buildings, Accessory Buildings and Structures combined, shall not cover more than 30% of the Area of a Parcel.
- (b) All Accessory Buildings and Structures combined shall not exceed 10% of the Area of Parcel.

(9) **Siting Requirements**

(a) **Principal Building**

- (i) Front Setback: No Principal Building shall be located within 7.5 metres of the Front Lot Line.
- (ii) Side Setback: No Principal Building shall be located within 1.5 metres of an Interior Side Lot Line, with the total Setback of all Side Yards not to be less than 4.5 metres. In the case where a Parcel is not served by a rear lane, one (1) Side Yard shall not be less than 3 metres. In the case of a Corner Lot, no Principal Building shall be located within 3.6 metres of an Exterior Side Lot Line
- (iii) Rear Setback: No Principal Building shall be located within 7.5 metres of a Rear Lot Line.

(b) **Accessory Building**

- (i) Front Setback: No Accessory Building shall be located in front of the front face of the Principal Building.
- (ii) Side Setback: No Accessory Building shall be located within 1.5 metres of an Interior Side Lot Line nor 3.6 metres of an Exterior Side Lot Line.
- (iii) Rear Setback: No Accessory Building shall be located within 1.5 metres of a Rear Lot Line.
- (iv) Building Separation: No Accessory Building shall be located within 2.5 metres of a Principal Building.

(10) **Fencing**

Subject to Section 22, no fence shall exceed a Height of 1.2 metres in front of the front face of the Principal Building and 2 metres behind the front face of the Principal Building.

(11) **Off Street Parking**

Off street parking shall be provided in accordance with the requirements of Parking Bylaw, 1992, No. 2011 (as amended).

ROOF

SOFFIT



STUCCO

1.62m

SOFFIT 3"

1/2"

SOFFIT 3"

(1.5m MAX ALLOWANCE)

HARDIE SIDING

GRADE

8" CONCRETE WALL ABOVE GRADE

DRAINAGE SYSTEM

GRADE

PROPOSED EAST ELEVATION

SCALE 1/2" = 1'0"

PROPOSED
NORTH ELEVATION

SIDING



STUCCO

HARDIE SIDING

EAST GRADE (60")

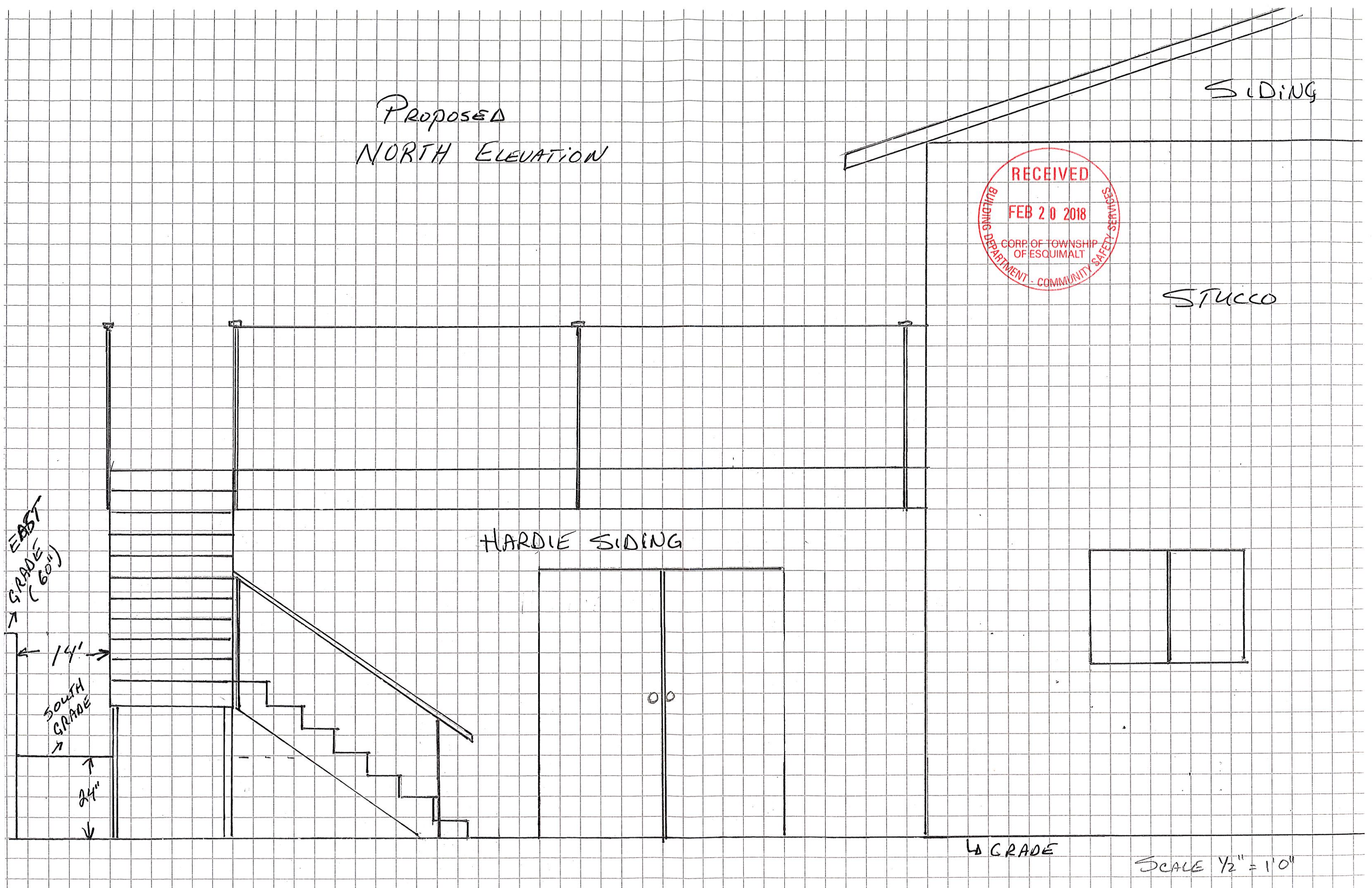
14'

SOUTH GRADE

24"

GRADE

SCALE 1/2" = 1'0"



Explorer

Land Surveying Inc

B. C. Land Surveyor's Certificate of Location for:

LOT B, SECTION 2, ESQUIMALT DISTRICT, PLAN 29588

LEGEND

Elevations are geodetic based on Integrated survey monument 84H0192 in the MUNICIPALITY OF ESQUIMALT at elevation 17.929m.

SCALE:

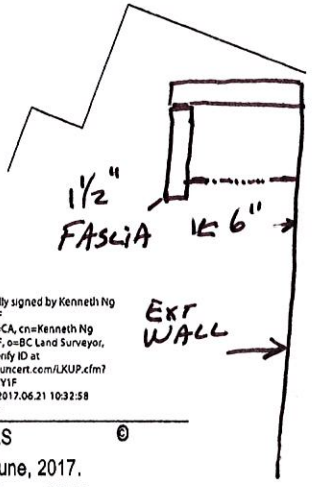
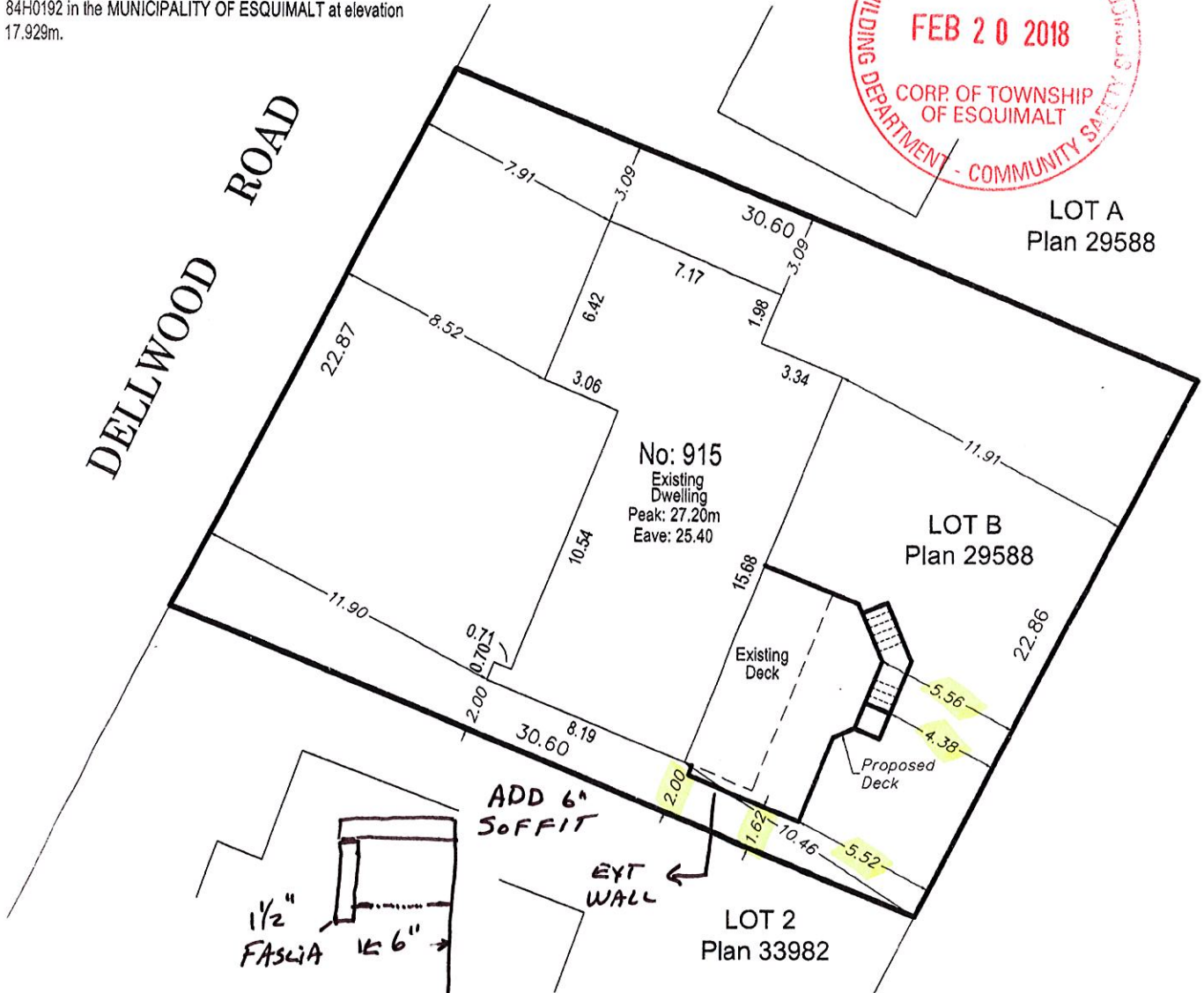
0 1:250 10

All distances are in METERS and decimals thereof.

Parcel Identifier: 001-378-449
In the MUNICIPALITY OF ESQUIMALT

Prepared exclusively for: RICKY CHARLIE DANNY MONGRAIN

101 - 2610 Douglas Street
Victoria, B. C. V8T 4M1
Tel: (604) 355-8879
Email: kenneth@explorersurvey.com
File: 11464



Kenneth Ng PICY1F

Digitally signed by Kenneth Ng
PICY1F
DN: cn=Kenneth Ng
PICY1F, o=BC Land Surveyor,
ou=Verify ID at
www.juncert.com/LKUP.cfm?
id=PICY1F
Date: 2017.06.21 10:32:58
-07'00'

Kenneth Ng, BCLS

Field Survey - 15 June, 2017.
Dated this 21st of June, 2017.

This document is not valid unless originally signed and sealed.
This document is copyrighted and may not be copied, transmitted or altered in whole or part without the consent of the signatory.

The signatory accepts no responsibility or liability for any damages that may be suffered by a third party as a result of any decisions made, or action taken based on this document.

I have inspected the residential premises shown at 915 Dellwood Road hereby certify that the said structure is situate with respect to nearby boundaries as shown on this sketch.

This document is prepared for the use of the stated party and their interests, and is for inspection purposes only. This document does not represent any form of boundary or lot redefinition, and should not be used in a manner which would assume so.

The property is subject to charges, legal notations, and interests shown on the title as of the survey date.

There are no encroachments by the said improvements shown.



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

APC Meeting: March 20, 2018

STAFF REPORT

DATE: March 13, 2018

TO: Chair and Members of the Advisory Planning Commission

FROM: Alex Tang, Planner
Bill Brown, Director of Development Services

SUBJECT: REZONING APPLICATION
638 Constance Avenue
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Esquimalt District, Plan 2854]
640 Constance Avenue
[PID 000-380-911 Amended Lot 88 (DD 208442I), of Suburban Lot 44,
Esquimalt District, Plan 2854]
and 637 Nelson Street
[PID 006-386-466 Lot D (DD367731-I), Suburban Lot 44, Esquimalt
District, Plan 2854]

RECOMMENDATION:

The Esquimalt Advisory Planning Commission recommends that the application for Rezoning, authorizing a 20.5 metre [6 storeys], 77 unit, multiple family residential building sited in accordance with the BCLS Site Plan provided by J.E. Anderson and Associates Surveyors-Engineers, stamped "Received January 18, 2018", and incorporating height and massing consistent with the architectural plans provided by Praxis Architects Inc., stamped "Received January 16, 2018", detailing the development proposed to be located at 638 Constance Avenue [PID 000-546-437 Lot B (DD 237133I), of Lots 79 and 89, Suburban Lot 44, Esquimalt District, Plan 2854], 640 Constance Avenue [PID 000-380-911 Amended Lot 88 (DD 208442I), of Suburban Lot 44, Esquimalt District, Plan 2854], and 637 Nelson Street [PID 006-386-466 Lot D (DD367731-I), Suburban Lot 44, Esquimalt District, Plan 2854] be forwarded to Council with a recommendation **to either approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

BACKGROUND:

Purpose of the Application:

The applicant is requesting a change in Zoning from the current RM-1 [Multiple Family Residential] to a Comprehensive Development District zone [CD]. This change is required to accommodate the proposed 6 storey, 77 unit, multiple family residential building including a 61 space parking garage.

This site is located within Development Permit Area No. 1 – Multi-Unit Residential. Should the rezoning application be approved, the applicant would need to obtain a Development Permit respecting the character of the development, including landscaping, and the siting, form, exterior design and finish of the proposed 6 storey, 77 unit, multiple family residential building, which would be considered by both the Design Review Committee and Council in the future.

Evaluation of this application should focus on the proposed siting, height, mass, density, lot coverage, usable open space, parking, fit with the neighbourhood, and consistency with the overall direction contained within the Official Community Plan.

Context

Applicant: Praxis Architects Inc. [Heather Spinney]

Owner: Constance Apartments Inc. No. BC1128254

Property Size: Metric: 2840 m² Imperial: 30569 ft²

Existing Land Use: Single Family Residential

Surrounding Land Uses:

North: DND Federal Land

South: Single Family Residential

West: Multiple Family Residential [4 storeys]

East: Multiple Family Residential [3 storeys]

Existing OCP Designation: Multi-Unit, High-Rise Residential

Existing Zoning: RM-1 [Multiple Family Residential]

Proposed Zoning: CD [Comprehensive Development District]

Zoning

Density, Lot Coverage, Height and Setbacks: The following chart compares the floor area ratios, lot coverage, setbacks, height and usable open space of this proposal with the requirements of the RM-5 [Multiple Family Residential Zone]:

	Proposed Comprehensive Development Zone [Apartment with 30 Residential Units]	RM-5 [Multiple Residential – High Density]
Floor Area Ratio	1.57	1.5
Lot Coverage	39%	30%/ 25% [above 2 nd]
Setbacks		
• Front	5.9 m	7.5 m
• Rear	6.4 m	7.5 m
• Interior Side [North]	5.0 m	7.5 m
• Interior Side [South]	7.0 m	7.5 m
Building Height	20.5 m [6 storeys]	20 m

Off Street Parking	61 spaces [0.80/unit]	101 spaces [1.3/unit]
Usable Open Space	870 m ² [30.6%]	213 m ² [7.5%]
Bicycle Parking	116 resident + 6 visitor	116 resident + 6 visitor

The Floor Area Ratio of this proposal is 1.57, which is comparable to the allowable amount of 1.5 in the RM-5 [Multiple Family Residential]. The Lot Coverage measures 39% of the site while allowing for substantial 870 m² [30.6%] of usable open space. The principal building is set back slightly less than the RM-5 requirements on all sides. The proposed height of the building is 20.5 metres, which is slightly higher than the allowed 20.0 metres in a RM-5 zone.

Parking Bylaw, 1992, No. 2011 requires 1.3 parking spaces per unit to be provided for multiple family developments. This proposal incorporates 61 residential parking spaces within the structure which equates to 0.80 parking spaces per unit. The subject property has a Walk Score of 72 which is considered 'Very Walkable'. This property is served by transit routes 25 and 46 along Admirals Road and by routes 15 and 26 along Esquimalt Road, which is 150 metres to the south. As this development is planned to be a purpose-built rental residential building, the demand for parking is decreased to 61 parking spaces according to Watt Consulting Group in their parking study.

Official Community Plan

This proposed development is consistent with the current Land Use Designation of "Multi-Unit, High-Rise Residential". This proposal is for a 6 storey, 77 unit apartment building with a Floor Area Ratio of 1.57 while the Land Use Designation of "Multi-Unit, High-Rise Residential" accepts buildings up to 12 storeys with a Floor Area Ratio of up to 3.0.

OCP Section 2 - Managed Growth – Land Use and Development states that the objectives and policies in this section are designed to promote sustainable land use and development in the community.

OCP 2.0.1(a) states the Township should encourage high quality development that enhances and benefits the community as a whole.

OCP 2.0.2(a) states Esquimalt's future new development, infill and redevelopment will be in accordance with the land use designations shown on OCP Schedule A, together with the guidelines set out in Development Permit Areas (OCP Section 9).

OCP Section 2.2 - Residential Land Use of the Official Community Plan recognizes that modest growth is likely to occur through the infilling of vacant or under-utilized parcels, redevelopment of existing residential properties to higher densities (such as townhouses, apartment buildings and mixed commercial-residential uses) and the replacement of existing buildings.

Section 2.2.1(a) states the Township should work toward a more complete community by maintaining a healthy mixture of housing types, accommodating people with a wide range of income levels.

Section 2.2.1(b) states the Township should encourage new residential development with high design standards for building and landscaping and which enhance existing and new neighbourhoods.

OCP Section 2.2.4.1 Multi-Unit Residential Policies [attached] are intended to provide more predictability for residents and give direction to design teams preparing development proposals.

This proposal for a 77 unit residential building is consistent with many policies contained in this section while it is unclear at this time whether it is consistent with the following policy as no units are explicitly proposed to be constructed to accessibility standards:

Section 2.2.4.1(f) states that wherever desirable and achievable consideration will be given to special needs and assisted housing including seniors, disabled persons and families.

OCP Section 2.2.4.4 Multi-Unit, High-Rise Residential states that in areas designated Multi-Unit, High-Rise Residential on Schedule A, building heights of up to 12 storeys are acceptable with a Floor Area Ratio of up to 3.0. Buildings with shallow setbacks must step down to no more than three storeys at street level in order to provide appropriate human scale along the sidewalk. The requirements and guidelines of Development Permit Area No. 1 apply.

This proposed building sets back at the fourth storey for only a small portion of the Principal Building above the main entrance and is hence inconsistent with this policy.

OCP Section 3.3.1(a) Affordable Housing Objectives states that the Township should encourage a range of housing by type, tenure, and price to ensure that people of all ages, household types, abilities and incomes have a diversity of housing choice in Esquimalt.

OCP Section 9.3 Development Permit Area No. 1 - Multi-Unit Residential [attached] contains Development Permit Guidelines for land designated Multi-Unit Residential. As the Development Permit is not being considered at this time it would be inappropriate to address many of these guidelines with the following exceptions that are relevant to the discussion of zoning and parking issues:

Section 9.3.5(b) states, in part, that new buildings should be designed and sited to minimize visual intrusion onto the privacy of surrounding homes and minimize the casting of shadows onto the private outdoor space of adjacent residential units. The majority of the areas with shadows casted by this development onto adjacent properties is parking areas of adjacent apartment buildings.

Section 9.3.5(c) states that high density multi-unit residential buildings should be designed so that the upper storeys are stepped back from the building footprint with lower building heights along the street front.

The massing of the proposed building sets back at the fourth storey for only a small portion of the Principal Building above the main entrance and is hence inconsistent with this policy.

Section 9.3.5(f) states that underground parking will be provided for any multi-unit residential building exceeding four storeys.

This proposal for a 6 storey residential building provides one level of underground parking.

Green Building Features

The applicant has completed the Esquimalt Green Building Checklist [attached].

Comments From Other Departments

The plans for this proposal were circulated to other departments and the following comments were received:

Building Inspection: Building to be constructed to requirements of BC Building Code 2012 and Municipal Building Code Bylaw, 2002, No. 2538. Applicant must address all issues contained within the Township Development Protocol should application be approved. Plans will be reviewed for compliance with BC Building Code upon submission of a Building Permit application.

Engineering Services: Engineering staff have completed a preliminary evaluation of Works and Services that would be required for the proposed 77 unit multiple family residential building. Staff confirms that the design appears achievable on the site and that appropriate works and services are available in the immediate area. If approved, the development must be serviced in accordance with bylaw requirements including, but not limited to, new sewer and drain connections, underground hydro, telephone and cable services and new road works may be required up to the centre line of Constance Avenue and Nelson Street. Should the application be approved, additional comments will be provided when detailed civil engineering drawings are submitted as part of a Building Permit application.

Parks Services: Parks staff has completed a preliminary review of the proposed on-site and off-site landscaping and commented that the landscape plan looks appropriate.

Fire Services: Fire Services staff has completed a preliminary review of the proposed plans and recommended an upgrade of the water main along the full length of Constance Avenue.

Public Notification

As this is a Rezoning application, should it proceed to a Public Hearing, notice would be mailed to tenants and owners of properties within 100m (328ft) of the subject property. Two signs indicating that the property is under consideration for a change Zoning have been installed on the Constance Avenue frontage while two signs have been installed on the Nelson Street frontage. These signs would be updated to include the date, time, and location of the Public Hearing.

ALTERNATIVES:

1. Forward the application for Rezoning to Council with a **recommendation of approval including reasons for the recommendation.**
2. Forward the application for Rezoning to Council with a **recommendation of approval including specific conditions and including reasons for the recommendation.**
3. Forward the application for Rezoning to Council with a **recommendation of denial including reasons for the recommendation.**



1340

1338

Saunders St

1337

Nelson St

640

638

637

633

630

626

Constance Ave

651

639

625

Subject Property Map:
638, 640 Constance Avenue, and 637 Nelson Street



1344

1342 1340

1338

SAUNDERS ST

NELSON ST

640

638

637

633

630

626

CONSTANCE AVE

651

639

625

- g) The Township is not supportive of new applications for infill housing, including rezoning and subdivision for panhandle lots in the 1100 and 1200 blocks of Old Esquimalt Road and the 600 block of Fernhill Road.

2.2.4 Multi-Unit Residential

Over the years, townhouses and apartment buildings have tended to be developed in clusters throughout the neighbourhoods of Esquimalt. They are generally located in the following areas:

- On both sides of Esquimalt Road from Grafton Street to Dunsmuir Road;
- The area around Craigflower Road and Selkirk Avenue;
- Admirals Road, Astle and Nelson Streets;
- West Bay south of Dunsmuir Road; and
- West Parklands.

Smaller clusters of multi-unit development are also found along Lampson Street between Devonshire and Old Esquimalt Roads, Lampson Street south of Lyall Street, and Ellery Street south of Esquimalt High School. This scattered pattern of development has contributed to residents' concerns related to the proliferation of multi-unit developments in neighbourhoods where single-unit and two-unit homes have been the predominant land use.

2.2.4.1 Multi-Unit Residential Policies

The following policies provide more predictability for residents in mixed residential use neighbourhoods and give direction to design teams involved in the preparation of development proposals.

- a) Multi-Unit Residential refers to three or more dwelling units on a parcel. Multi-unit Residential does not refer to a single-unit home with a secondary suite.
- b) The Township encourages the concentration of multi-unit residential development where such development is in keeping with the overall goals of this Plan.
- c) Wherever practical, multi-unit residential housing will be located near a Major Road as shown on "Schedule B". This supports transit service and also helps maintain the integrity of single-unit and two-unit housing neighbourhoods;
- d) Wherever feasible, major multi-unit residential projects will be located within reasonable distance of one of Esquimalt's commercial areas in order to encourage walking and cycling;
- e) A mix of housing types will be provided in multi-unit residential areas in order to provide visual interest and to meet the varying housing needs of Esquimalt's current and future residents;
- f) Wherever desirable and achievable, consideration will be given for special needs and assisted housing, including seniors, disabled persons and families.
- g) Within the areas designated on "Schedule A" as Townhouse Residential, Multi-Unit, Low-Rise Residential and Multi-Unit, High-Rise Residential, the following criteria

will be used to evaluate development proposals requiring an application for rezoning:

- o The massing and height of the project will respond sensitively to the prevailing character of the immediate neighbourhood. This will vary by location;
 - o The project will relate to the street. Its exterior finishes, scale, treatment of parking areas, and landscaping, will enhance the appearance of the neighbourhood and contribute positively to the streetscape;
 - o The proponent will demonstrate that the neighbourhood has been consulted in a fair and meaningful way, and that residents' concerns have been appropriately responded to in the proposal; and
 - o Where new multi-unit residential projects are proposed, they should not "land-lock", otherwise isolate, or negatively affect the development potential of adjacent parcels. Projects must either consolidate the isolated parcels or leave a sufficient area available to allow for the eventual redevelopment of the adjacent land.
- h) Development proposals with heights and /or densities greater than those set out in policies 2.2.4.2 to 2.2.4.4. may be considered, where appropriate, through variances to zoning and/or parking regulations and density bonusing of floor-space where new affordable, accessible or special needs housing units or amenities are provided for the benefit of the community.
- i) For the purposes of density bonuses, "amenities" may include, but not be limited to:
- o Privately-owned, publicly-accessible open space;
 - o Public art;
 - o Contributions towards the enhancement of public recreation facilities;
 - o Contributions towards street and boulevard enhancements, including street furniture and decorative lighting;
 - o Daycare facilities; and
 - o Preservation of heritage structures or features.
- j) In new multi-unit residential developments, secure bicycle storage for residents should be provided in the ratio of 1.5 storage spaces per dwelling unit. In addition to the residents' parking, each multi-unit building should have six (6) bicycle lock-up spaces for the use of visitors.



A bicycle storage requirement may be waived or varied in a Development Permit where, in the opinion of Council, there is no demonstrated need, such as in a congregate care facility.

Development Permit Area No. 1 – Multi-Unit Residential

9.3.1 Scope

All land designated Multi-Unit Residential on Schedule “C” are part of DPA No. 1.

9.3.2 Category

Section 919(1)(f) of the *Local Government Act* – form and character, multi-family residential.

9.3.3 Justification

This Plan emphasizes the importance of protecting residential neighbourhoods and encouraging a high quality of construction for new development. It is essential that new multi-unit residential development not have a negative impact on, or be out of character with, existing residential neighbourhoods. The primary objective of Development Permit Area No. 1 is to ensure that the development of multi-unit residential sites is compatible with surrounding uses.

9.3.4 Requirements of Owners of Land within the Development Permit Area

a) Owners of land within Development Permit Area No. 1 must not do any of the following without first obtaining a development Permit in accordance with the guidelines for this Development Permit Area:

- i) subdivide lands; or
- ii) construct or alter a building or structure;

without first obtaining a Development Permit in accordance with the guidelines of this Development Permit Area.

b) Exemptions:

The following do not require a development permit:

- i) construction of buildings or structures less than 10 square metres in area;
- ii) minor additions to existing dwellings where the floor area of the addition does not exceed 10 percent of the ground floor area of the dwelling;
- iii) emergency repairs to existing structures and public walkways where a potential safety hazard exists;
- iv) fences;
- v) the cutting of trees as permitted upon application under the municipal tree protection bylaw; and
- vi) placement of signs less than 1.5 sq. metres in area.

9.3.5 Guidelines for Owners of Land within the Development Permit Area

a) The size and siting of buildings that abut existing single- and two-unit and townhouse dwellings should reflect the size and scale of adjacent development and complement the surrounding uses. To achieve this, height and setback restrictions may be imposed as a condition of the development permit.

- b) New buildings should be designed and sited to minimize visual intrusion onto the privacy of surrounding homes and minimize the casting of shadows onto the private outdoor space of adjacent residential units.
- c) High-density multi-unit residential buildings or mixed commercial/residential buildings in commercial areas with a zero front setback should be designed so that the upper storeys are stepped back from the building footprint, with lower building heights along the street front.
- d) Landscaping of multi-unit residential sites should emphasize the creation of an attractive streetscape, as well as provide privacy between individual buildings and dwellings, screen parking areas and break up large expanses of paving.
- e) Surface parking areas in multi-unit residential developments less than five storeys in height, will be situated away from the street and screened by berms, landscaping or solid fencing or a combination of these three.
- f) Underground parking will be provided for any multi-unit residential buildings exceeding four storeys.
- g) The retention of public view corridors particularly views to the water should be encouraged wherever possible.
- h) To preserve view corridors and complement natural topography, stepped-down building designs are encouraged for sloping sites.
- i) Retention and protection of trees and the natural habitat is encouraged wherever possible.
- j) Townhouses will be designed such that the habitable space of one dwelling unit abuts the habitable space of another unit and the common wall overlap between adjoining dwellings shall be at least 50 percent.
- k) Site lighting in multi-unit residential developments should provide personal safety for residents and visitors and be of the type that reduces glare and does not cause the spill over of light onto adjacent residential sites.
- l) Garbage receptacle areas and utility kiosks should be screened by solid fencing or landscaping or a combination of the two.
- m) For waterfront sites, retention of natural features and existing trees should be a priority in site planning considerations.
- n) When any existing single-unit residence or duplex residence is being redeveloped to a multi-unit residential use by adding on of one or more dwelling units, such addition will be designed so that all of the units form a cohesive whole. In order to achieve cohesiveness:
 - i) both, the existing and proposed structures will be in the same architectural style;
 - ii) variations between the roofline of the existing building and any proposed addition(s) will be no greater than 1.5 metres;



- iii) roof styles and pitches must be complementary;
- iv) architectural features such as sloping roofs and dormers should be incorporated into the design to unite the various parts of the structure; and
- v) the existing and proposed structure will be constructed using the same or complimentary exterior finishes including roofing materials, window treatments, door styles and other finishing details.
- o) Within the area bounded by Tillicum, Craigflower, Lampson and Transfer Streets, redevelopment to multi-unit residential use will require that vehicular access to these sites be off Lampson Street rather than Tillicum, in recognition of the high levels of traffic currently using Tillicum Road.
- p) To create a more aesthetic and functional design that links each multi-unit residential project with the streetscape, the following guidelines are recommend:
 - i) Avoid long, narrow parcels with minimal road frontage (consolidate one or more parcels where necessary);
 - ii) Place parking areas away from the street; and
 - iii) Design porches and windows overlooking the street to increase personal interaction and safety.



Township of Esquimalt
1229 Esquimalt Road
Esquimalt, BC V9A 3P1

January 05, 2018

RE: 638 – 640 CONSTANCE AVENUE + 637 NELSON STREET

Dear Mayor and Council,

The proposed project at 638 – 640 Constance Avenue + 637 Nelson Street will be a new purpose-built rental multiple residential building, with 4 and 6 storeys of wood-frame construction. According to the current Official Community Plan, Schedule A - Land Use Designation, these properties are identified Multi-Unit, High-Rise Residential which means 5+ storeys are permitted and a maximum floor area ratio (FAR) of 3.0 is permitted.

The properties are currently zoned RM-1 which permit only townhouses, duplexes or single family dwellings. We understand from informal conversations with Esquimalt Planning Department that rezoning to a Comprehensive Development would be the appropriate approach for the redevelopment of these properties.

This proposal was presented at a neighbourhood meeting which was held December 11, 2017. Our impression from those who attended was that the proposal was generally well received. Attached with this letter please find a copy of the notification for the neighbourhood meeting as well as sign-in sheets from the meeting.

A parking study prepared by Watt Consulting Group provides supporting information for requested parking variance, and is included with this package.

Talbot Mackenzie and Associates Consulting Arborists have also been retained to provide a tree protection plan, which will be submitted separately.

Trusting this is sufficient for submission requirements,

Sincerely,

Heather Spinney, Architect AIBC
Praxis Architects Inc.





638 / 640 Constance Avenue + 637 Nelson Street
Parking Study



Prepared for: **GT Mann Contracting**
Prepared by: **Watt Consulting Group**
Our File: **2258**
Date: **December 6, 2017**

TABLE OF CONTENTS

1.0	INTRODUCTION	1
1.1	Subject Site.....	1
1.2	Site Characteristics.....	2
2.0	PROPOSED DEVELOPMENT	3
2.1	Proposed Parking Supply.....	3
3.0	PARKING REQUIREMENT	3
4.0	EXPECTED PARKING DEMAND	4
4.1	Resident Parking, Observations.....	4
4.2	Visitor Parking.....	6
4.3	Summary of Expected Parking Demand.....	6
5.0	ON-STREET PARKING CONDITIONS	7
6.0	TRANSPORTATION DEMAND MANAGEMENT	8
6.1	Bike Parking.....	8
7.0	SUMMARY	9
7.1	Recommendation.....	9

1.0 INTRODUCTION

Watt Consulting Group was retained by GT Mann Contracting to conduct a parking study for the proposed development at 638/640 Constance Avenue and 637 Nelson Street in the Township of Esquimalt. The purpose of this study is to assess the adequacy of the proposed parking supply by considering parking demand at representative sites and to identify transportation demand management (TDM) options.

1.1 SUBJECT SITE

The proposed redevelopment site is 638/640 Constance Avenue and 637 Nelson Street in the Township of Esquimalt. The site is zoned RD-3 | Two Family/Single Family Residential + CD-75 | Comprehensive Development District No.75 . See **Figure 1**.

FIGURE 1. SUBJECT SITE



1.2 SITE CHARACTERISTICS

The following provides information regarding services and transportation options in close proximity to the subject site.



SERVICES

The site is located approximately 285m from the intersection of Admirals Road and Esquimalt Road that has a grocery store, liquor store, and a few small-scale restaurants and retail shops. Esquimalt Village is less than 1km from the site and has a library, medical services, postal services, and various commercial uses that will accommodate the daily needs of site residents.



TRANSIT

The closest bus stop to the site is 200m away (westbound service) on Esquimalt Road and just under 300m from the site (eastbound service) on Esquimalt Road that serves Route 15 | Uvic/Esquimalt and Route 26 | Dockyard/Uvic. There are also bus stops on Admirals Road 300m from the site that serves Route 25 | Maplewood/Admirals Walk/Colwood. These routes provide service and/or connection to the majority of destinations in Greater Victoria including the University of Victoria, Camosun College, Downtown Victoria, and the Western Communities. Route 15 is a Regional Route with a service frequency of 15 to 60 minutes with limited stops; Routes 25 and 26 are local routes with service frequency of 20 to 120 minutes.

BC Transit's Transit Future Plan has identified Esquimalt Road as a "Frequent Transit Corridor"¹ that will provide frequent service (15 minutes or better between 7am and 10pm, 7 days per week) with improved transit travel times achieved by fewer stops, transit priority measures and enhanced bus stop infrastructure. With the Frequent Transit Network projected to carry a large share of the future transit system's total ridership, the subject site will benefit from frequent, reliable and convenient transit service.



WALKING

There are no sidewalks on Constance Avenue or Nelson Street that provide a connection to Esquimalt Road. There are adequate sidewalks on both sides of Esquimalt Road with marked crossings at major intersections and crosswalks at various locations along Esquimalt Road, most notably at Constance Avenue and Esquimalt Road. Admirals Road also has sidewalks on both sides of the road. The site has a Walkscore² of 72 that suggests most errands can be accomplished on foot.

¹ More information on the Victoria Transit Future Plan is available online at: <http://bctransit.com/victoria/transit-future/victoria-transit-future-plan>

² Walkscore. <https://www.walkscore.com/score/638-constance-ave-victoria-bc-canada>



CYCLING

There are bike lanes on both sides of Esquimalt Road, beginning at Park Place (approximately 400m from the site) which provides service to Downtown Victoria. There are also bike lanes on both sides of Admirals Road between Lyall Street and Maplebank Road, which was a part of the Admirals Road improvement project in 2015. This provides direct service to the Esquimalt and Nanaimo (E+N) Rail Trail (approximately 1km from the site), which provides an off-road cycling route to View Royal and the Western Communities.

2.0 PROPOSED DEVELOPMENT

The proposal is for 77 Multi-family Residential units. This will be a rental apartment building with units offered at market rates (i.e., no subsidy) consisting of a combination of studio, one- and two-bedroom units. See **Table 1**.

TABLE 1. PROPOSED UNIT COMPOSITION³

Number of Bedrooms	Quantity
Studio	12
One-Bedroom	40
One-Bedroom + Den	13
Two-Bedroom	6
Two-Bedroom + Den	6
Total	77

2.1 PROPOSED PARKING SUPPLY

The proposed parking supply is 61 spaces - a parking supply rate of 0.79 spaces per unit.

The proposal also includes the provision of 116 long-term bike parking spaces (1.5 bike parking spaces per unit) and a six-space bike rack at the building entrance.

3.0 PARKING REQUIREMENT

The Township of Esquimalt Parking Bylaw No. 2011⁴ identifies a minimum parking supply rate of 1.3 spaces per unit for Medium and High Density Apartment uses (assumes RM-4 zoning). Applied to the subject site, this results in a requirement for 100 parking spaces. The Bylaw requires that 25 of the required spaces are reserved for visitors, and one space is designed and designated as Disabled Persons' parking (74 resident, 25 visitor, 1 disabled).

³ Unit composition information per email correspondence from Praxis Architects, received September 18 2017

⁴ The Township's Zoning Bylaw is available online at:
www.esquimalt.ca/sites/default/files/docs/municipal-hall/bylaws/parking_bylaw_2011_july.pdf

4.0 EXPECTED PARKING DEMAND

Expected parking demand is estimated in the following sections based on observations and research.

4.1 RESIDENT PARKING, OBSERVATIONS

Observations of parked vehicles were completed for eight representative sites within Esquimalt to determine an appropriate parking demand rate for the subject site. Study sites are generally located in the western portion of the Township with similar access to public transit and cycling routes as the proposal site. All study sites are market rental apartment buildings.

Observations were conducted on Thursday October 5 and Wednesday October 11 between 9:00pm and 10:00pm (representing peak period for residential land uses). All representative sites have surface parking, which allowed for access to complete counts of parked vehicles.

Results suggest an average peak parking demand of 0.61 vehicles per unit and an 85th percentile of 0.72 vehicles per unit, with rates ranging from 0.45 to 0.72 vehicles per unit. See **Table 2**. The 85th parking demand rate applied to the subject site suggests a total parking demand of 55 vehicles.

Study sites that are in close proximity to the subject site were assessed in more detail to calculate an accurate representation of parking demand at the subject site. Average peak demand of those sites (625 Constance Avenue, 639 Constance Avenue, 1337 Saunders Street, 1340 Sussex Street) is 0.56 vehicles per unit; lower than the average among all sites. The majority of these sites are in close proximity to CFB Esquimalt and it is assumed that a portion of residents are CFB employees and do not require a vehicle.

TABLE 2. SUMMARY OF OBSERVATIONS AT REPRESENTATIVE SITES

Location	Number of Units	Thursday October 5, 9:00pm		Wednesday October 11, 9:00pm	
		Vehicles Observed	Demand Rate (vehicles per unit)	Vehicles Observed	Demand Rate (vehicles per unit)
625 Constance Ave	29	15	0.52	13	0.45
639 Constance Ave	19	8	0.42	10	0.53
1337 Saunders St	28	16	0.57	15	0.54
1340 Sussex St	39	21	0.54	24	0.62
1357 Esquimalt Rd	50	32	0.64	36	0.72
611 Admirals Rd	25	16	0.64	18	0.72
850 Admirals Rd	20	13	0.65	13	0.65
841 Kindersley Rd	11	8	0.73	7	0.64
Average			0.59		0.61
85th Percentile			0.65		0.72

Research suggests that parking demand varies based on the size of unit - the higher the number of bedrooms, the higher the parking demand. For the four sites closest to the subject site, the total parking demand has been redistributed based on number of bedrooms.

Overall vehicle ownership at the study sites closest to the subject site have been factored to account for unit configuration (i.e., number of bedrooms) as follows (see **Table 3**):

1. Overall adjusted peak vehicle ownership data for each site⁵;
2. The breakdown of unit type (i.e., number of bedrooms) at each site⁶; and
3. The assumed “ratio differences” between each unit type based on the King County Metro⁷ study which recommends one-bedroom units have a 20% higher parking demand than bachelor units, two-bedroom units have a 60% higher parking demand than one-bedroom units, and three-bedroom units have a 15% higher parking demand than two-bedroom units.

⁵ The peak parking demand rates were also factored up to account for any residents that may not have been home during observations. A conservative factor of 10% is applied to each site (this is based on known ratio differences between results from observations and vehicle ownership information at similar sites)

⁶ Actual breakdown by unit type was unknown at each site, and thus an assumed breakdown was used for each site of 10% bachelor, 60% one-bedroom, 30% two-bedroom (based on averages of multiple representative sites)

⁷ King County Metro. (2013). Right Size Parking Model Code. Table 2, page 21.
Available online at: <http://metro.kingcounty.gov/programs-projects/right-size-parking/pdf/140110-rsp-model-code.pdf>

Results suggest that average parking demand when factored for unit configuration is as follows:

- Bachelor Units (12) = 0.44 vehicles per unit, 5 vehicles
- One-Bedroom Units (53) = 0.53 vehicles per unit, 28 vehicles
- Two-Bedroom Units (12) = 0.85 vehicles per unit, 10 vehicles
- Total Vehicles = 43 vehicles

TABLE 3. PARKING DEMAND BY UNIT TYPE AT SELECT REPRESENTATIVE SITES

Location	Adjusted Demand Rate	Assumed Vehicle Ownership Distribution (vehicles per unit)		
		Bachelor	1-Bedroom	2-Bedroom
625 Constance Ave	0.57	0.41	0.49	0.78
639 Constance Ave	0.58	0.42	0.50	0.80
1337 Saunders St	0.63	0.45	0.54	0.86
1340 Sussex St	0.68	0.49	0.59	0.94
		0.44	0.53	0.85

4.2 VISITOR PARKING

Observations were conducted as part of a study by Metro Vancouver⁸ that concluded typical visitor parking demand is less than 0.1 vehicles per unit. This is similar to observations that were conducted for parking studies in the City of Langford and the City of Victoria, and suggests that visitor parking demand is not strongly influenced by location.

As such, it is estimated that visitor parking demand will be no more than 0.1 vehicles per unit.

4.3 SUMMARY OF EXPECTED PARKING DEMAND

Expected parking demand is approximately 51 vehicles, 10 less than the proposed parking supply. See **Table 5**.

TABLE 5. SUMMARY OF EXPECTED PARKING DEMAND

		Units	Expected Parking Demand	
			Rate	Total
Resident	Bachelor	12	0.44 / unit	5
	One Bedroom	53	0.53 / unit	28
	Two Bedroom	12	0.85 / unit	10
Visitor		77	0.1 / unit	8
Total Expected Parking Demand				51

⁸ Metro Vancouver Apartment Parking Study, Technical Report, 2012.
Available online at: www.metrovancouver.org/services/regional-planning/PlanningPublications/Apartment_Parking_Study_TechnicalReport.pdf

5.0 ON-STREET PARKING CONDITIONS

On-street parking conditions were observed surrounding the site on Constance Avenue, Astle Street, Nelson Street, and Saunders Street. Parking restrictions on these road segments are either unrestricted or there is no parking available. See **Table 6** and **Figure 2**.

Observations were completed during a weekday afternoon and evening to reflect the anticipated “peak” periods. Observations were conducted during the following time periods:

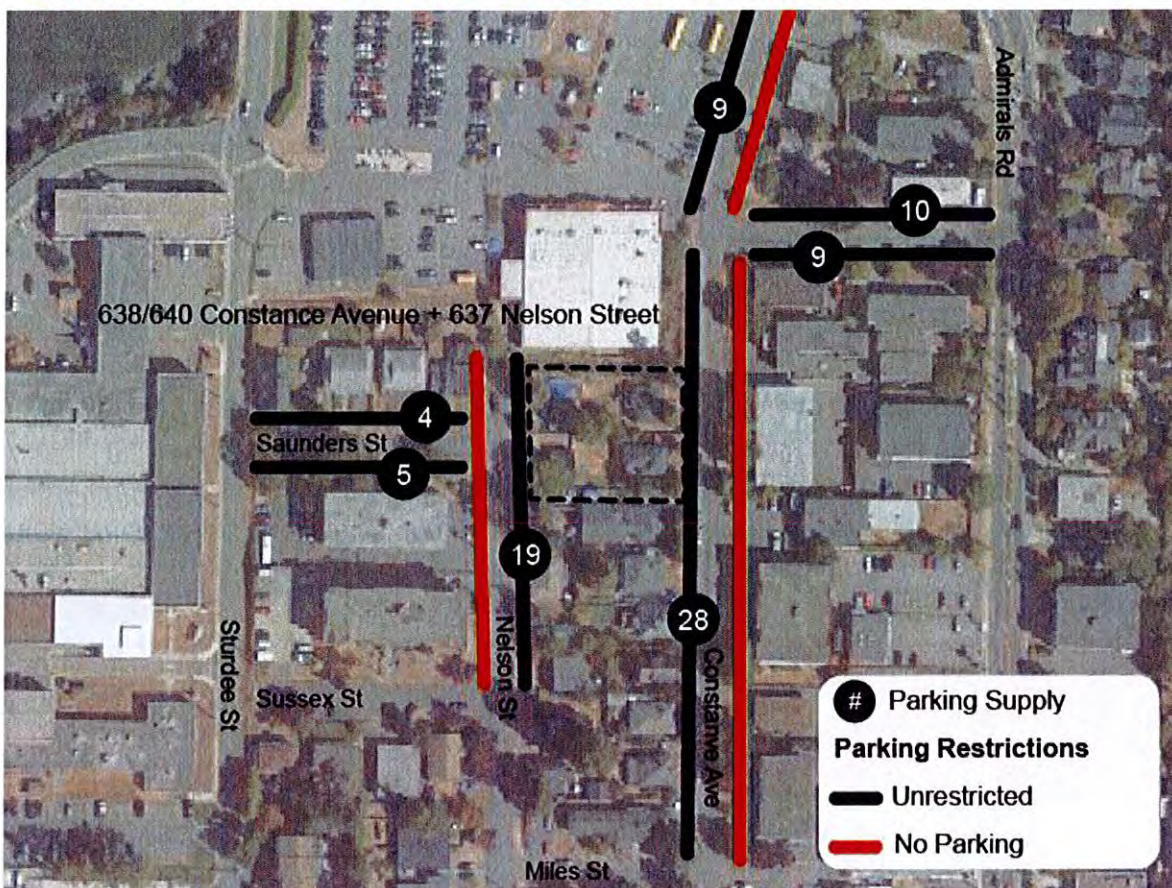
- Tuesday September 19, 2017 at 9:30pm
- Friday September 22, 2017 at 3:30pm

Peak occupancy was observed during the weekday evening observation (Tuesday at 9:30pm) when available parking was 48% occupied, with 44 parking spaces unoccupied. Parking directly adjacent the site on Constance Avenue (Miles Street to Astle Street) had a peak occupancy of 68% with 9 spaces still available. Parking adjacent the site on unoccupied. This demonstrates sufficient availability of on-street parking in case of spillover.

TABLE 6. SUMMARY OF ON-STREET PARKING CONDITIONS

Street	Side	Restrictions	Parking Supply (spaces)	Vehicles Observed	
				Tues. 09/19/17 @ 9:30pm	Fri. 09/22/17 @ 2:45pm
Constance Ave	Miles St – W	--	28	19	12
	Astle St E	No Parking	-	-	-
	Astle St – W	--	9	2	2
	Cul-de-sac E	No Parking	-	-	-
Astle St	Constance Ave – N	--	10	6	2
	Admirals Rd S	--	9	2	2
Nelson St	Sussex St – W	No Parking			
	cul-de-sac E	--	19	9	5
Saunders St	Nelson St – N	--	4	1	0
	cul-de-sac S	--	5	1	0
			84	40 48%	23 27%

FIGURE 2. SUMMARY OF ON-STREET PARKING CONDITIONS



6.0 TRANSPORTATION DEMAND MANAGEMENT

Transportation demand management (TDM) is the application of strategies and policies to influence individual travel choice, most commonly to reduce single-occupant vehicle travel. TDM measures can be pursued to encourage sustainable travel, enhance travel options and decrease parking demand. The following are identified for the applicant's consideration.

6.1 BIKE PARKING

Bike parking is not currently required in the Township's Parking Bylaw. However, the Township of Esquimalt Official Community Plan includes a policy that states:

In new multi-unit residential developments, secure bicycle storage for residents should be provided in the ratio of 1.5 storage spaces per dwelling unit. In addition to the residents' parking, each multi-unit building should have six (6) bicycle lock-up spaces for the use of visitors.

The applicant is providing bike parking as per the policy in the OCP, which is higher than typical bike parking requirements in other communities.

7.0 SUMMARY

The proposed development is for 77 units and 61 off-street parking spaces – a parking supply rate of 0.79 space per unit. The Township’s Parking Bylaw identifies a required minimum parking supply of 100 parking spaces; 39 more than is proposed.

Expected parking demand was calculated for the site based on observations. Results suggest an expected parking demand of 43 resident vehicles and eight visitor vehicles – a total site parking demand of 51 vehicles. Site parking demand is expected to be accommodated within the proposed off-street parking supply and without impacting the surrounding neighbourhood.

Long- and short-term bicycle parking will be provided, consistent with the policy in the Township’s OCP (1.5 long-term bike parking spaces per unit and a six-space rack at the building entrance).

7.1 RECOMMENDATION

1. It is recommended that the Township grant the requested variance to allow for provision of 61 parking spaces (0.79 spaces per unit)



GREEN BUILDING CHECKLIST

The purpose of this Checklist is to make property owners and developers aware of specific green features that can be included in new developments to reduce their carbon footprints to help create a more sustainable community.

Creating walkable neighbourhoods, fostering green building technologies, making better use of our limited land base and ensuring that new development is located close to services, shops and transit are some of the means of achieving sustainability.

The Checklist which follows focuses on the use of **Green Technologies** in new buildings and major renovations. The Checklist is not a report card, it is a tool to help identify how your project can become 'greener' and to demonstrate to Council how your project will help the Township of Esquimalt meet its sustainability goals. It is not expected that each development will include all of the ideas set out in this list but Council is looking for a strong commitment to green development.

There are numerous green design standards, for example, Built Green BC; LEED ND; Living Building Challenge; Green Shores; Sustainable Sites Initiative. Esquimalt is not directing you to follow any particular standard, however, you are strongly encouraged to incorporate as many green features as possible into the design of your project .

As you review this checklist, if you have any questions please contact **Development Services at 250.414.7108** for clarification.

**New development is essential to Esquimalt.
We look forward to working with you
to ensure that development is
as green and sustainable as possible.**

Other documents containing references to building and site design and sustainability, which you are advised to review, include:

- Esquimalt's Official Community Plan
- Development Protocol Policy
- Esquimalt's Pedestrian Charter
- Tree Protection Bylaw No. 2664
- A Sustainable Development Strategic Plan for the Township of Esquimalt



“One-third of Canada’s energy use goes to running our homes, offices and other buildings. The federal government’s Office of Energy Efficiency (Natural Resources Canada) reports that a corresponding one-third of our current greenhouse gas (GHG) emissions come from the built environment.”
 [Green Building and Development as a Public Good, Michael Buzzelli, CPRN Research Report June 2009]

Please answer the following questions and describe the green and innovative features of your proposed development. Depending on the size and scope of your project, some of the following points may not be applicable.

Green Building Standards

Both energy use and emissions can be reduced by changing or modifying the way we build and equip our buildings.

1	Are you building to a recognized green building standard? If yes, to what program and level?	Yes	No ✓
2	If not, have you consulted a Green Building or LEED consultant to discuss the inclusion of green features?	Yes ✓	No
3	Will you be using high-performance building envelope materials, rainscreen siding, durable interior finish materials or safe to re-use materials in this project? If so, please describe them. TO MEET NECB 2011	Yes ✓	No
4	What percentage of the existing building[s], if any, will be incorporated into the new building?	N/A %	
5	Are you using any locally manufactured wood or stone products to reduce energy used in the transportation of construction materials? Please list any that are being used in this project. TBD DURING FURTHER DETAILED DESIGN		
6	Have you considered advanced framing techniques to help reduce construction costs and increase energy savings?	Yes ✓	No
7	Will any wood used in this project be eco-certified or produced from sustainably managed forests? If so, by which organization? <u>TBD DURING FURTHER DETAILED DESIGN</u> For which parts of the building (e.g. framing, roof, sheathing etc.)? <u>SHEATHING</u>		
8	Can alternatives to Chlorofluorocarbon’s and Hydro-chlorofluorocarbons which are often used in air conditioning, packaging, insulation, or solvents] be used in this project? If so, please describe these. _____ THE GOAL WILL BE TO MINIMIZE USE OF CFC AND HCFC - TBD DURING FURTHER DETAILED DESIGN	Yes ✓	No
9	List any products you are proposing that are produced using lower energy levels in manufacturing. <u>TBD DURING FURTHER DETAILED DESIGN</u>		
10	Are you using materials which have a recycled content [e.g. roofing materials, interior doors, ceramic tiles or carpets]?	Yes ✓	No
11	Will any interior products [e.g. cabinets, insulation or floor sheathing] contain formaldehyde?	Yes	No ✗

Water Management

The intent of the following features is to promote water conservation, re-use water on site, and reduce storm water run-off.

Indoor Water Fixtures

12	Does your project exceed the BC Building Code requirements for public lavatory faucets and have automatic shut offs? <u>N/A</u>	Yes	No	
13	For commercial buildings, do flushes for urinals exceed BC Building Code requirements? <u>N/A</u>	Yes	No	
14	Does your project use dual flush toilets and do these exceed the BC Building Code requirements? <u>TBD DURING FURTHER DETAILED DESIGN</u>	Yes	No	
15	Does your project exceed the BC Building Code requirements for maximum flow rates for private showers?	Yes	No	
16	Does your project exceed the BC Building Code requirements for flow rates for kitchen and bathroom faucets?	Yes	No	

Storm Water

17	If your property has water frontage, are you planning to protect trees and vegetation within 60 metres of the high water mark? [Note: For properties located on the Gorge Waterway, please consult Sections 7.1.2.1 and 9.6 of the Esquimalt Official Community Plan.]	Yes	No	N/A
18	Will this project eliminate or reduce inflow and infiltration between storm water and sewer pipes from this property?	Yes	No	N/A
19	Will storm water run-off be collected and managed on site (rain gardens, wetlands, or ponds) or used for irrigation or re-circulating outdoor water features? If so, please describe. <u>THERE WILL BE VERY LITTLE STORM WATER RUN-OFF</u>	Yes	No	N/A
20	Have you considered storing rain water on site (rain barrels or cisterns) for future irrigation uses? <u>IT HAS BEEN CONSIDERED, BUT IS NOT APPROPRIATE FOR THIS SITE</u>	Yes 	No	N/A
21	Will surface pollution into storm drains will be mitigated (oil interceptors, bio-swales)? If so, please describe. <u>OIL INTERCEPTORS</u>	Yes 	No	N/A
22	Will this project have an engineered green roof system or has the structure been designed for a future green roof installation?	Yes	No 	N/A
23	What percentage of the site will be maintained as naturally permeable surfaces?	_____ %		

Waste water

24	For larger projects, has Integrated Resource Management (IRM) been considered (e.g. heat recovery from waste water or onsite waste water treatment)? If so, please describe these. _____	Yes	No	N/A
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Natural Features/Landscaping

The way we manage the landscape can reduce water use, protect our urban forest, restore natural vegetation and help to protect the watershed and receiving bodies of water.

25	Are any healthy trees being removed? If so, how many and what species? <u>REFER TO REPORT PREPARED BY TALBOT MACKENZIE & ASSOCIATES</u> Could your site design be altered to save these trees? <u>NO</u> Have you consulted with our Parks Department regarding their removal? <u>YES</u>	Yes 	No	N/A
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26	Will this project add new trees to the site and increase our urban forest? If so, how many and what species? <u>REFER TO LANDSCAPE PLAN</u>	Yes 	No	N/A
27	Are trees [existing or new] being used to provide shade in summer or to buffer winds?	Yes 	No	N/A
28	Will any existing native vegetation on this site be protected? If so, please describe where and how. _____	Yes	No	N/A
29	Will new landscaped areas incorporate any plant species native to southern Vancouver Island?	Yes 	No	N/A
30	Will xeriscaping (i.e. the use of drought tolerant plants) be utilized in dry areas?	Yes 	No	N/A
31	Will high efficiency irrigation systems be installed (e.g. drip irrigation; 'smart' controls)?	Yes 	No	N/A
32	Have you planned to control invasive species such as Scotch broom, English ivy, Himalayan and evergreen blackberry growing on the property?	Yes	No	N/A
33	Will topsoil will be protected and reused on the site?	Yes 	No	N/A

Energy Efficiency

Improvements in building technology will reduce energy consumption and in turn lower greenhouse gas [GHG] emissions. These improvements will also reduce future operating costs for building occupants.

34	Will the building design be certified by an independent energy auditor/analyst? If so, what will the rating be? <u>TBD DURING FURTHER DETAILED DESIGN</u>	Yes 	No	N/A
35	Have you considered passive solar design principles for space heating and cooling or planned for natural day lighting?	Yes 	No	N/A
36	Does the design and siting of buildings maximize exposure to natural light? What percentage of interior spaces will be illuminated by sunlight? <u>_TBC_%</u>	Yes 	No	N/A
37	Will heating and cooling systems be of enhanced energy efficiency (ie. geothermal, air source heat pump, solar hot water, solar air exchange, etc.). If so, please describe. <u>TBD DURING FURTHER DETAILED DESIGN</u> If you are considering a heat pump, what measures will you take to mitigate any noise associated with the pump? _____	Yes	No	N/A
38	Has the building been designed to be solar ready?	Yes 	No	N/A
39	Have you considered using roof mounted photovoltaic panels to convert solar energy to electricity?	Yes	No	N/A
40	Do windows exceed the BC Building Code heat transfer coefficient standards? TO MEET NECB 2011	Yes	No	N/A
41	Are energy efficient appliances being installed in this project? If so, please describe. ENERGY STAR			
42	Will high efficiency light fixtures be used in this project? If so, please describe. LED	Yes 	No	N/A
43	Will building occupants have control over thermal, ventilation and light levels?	Yes 	No	N/A
44	Will outdoor areas have automatic lighting [i.e. motion sensors or time set]?	Yes 	No	N/A
45	Will underground parking areas have automatic lighting?	Yes 	No	N/A

Air Quality

The following items are intended to ensure optimal air quality for building occupants by reducing the use of products which give off gases and odours and allowing occupants control over ventilation.

46	Will ventilation systems be protected from contamination during construction and certified clean post construction?	Yes	No	N/A
47	Are you using any natural, non-toxic, water soluble or low-VOC [volatile organic compound] paints, finishes or other products? If so, please describe. <u>TBD DURING FURTHER DETAILED DESIGN</u>	Yes	No	N/A
48	Will the building have windows that occupants can open?	Yes	No	N/A
49	Will hard floor surface materials cover more than 75% of the liveable floor area?	Yes	No	N/A
50	Will fresh air intakes be located away from air pollution sources?	Yes	No	N/A

Solid Waste

Reuse and recycling of material reduces the impact on our landfills, lowers transportation costs, extends the life-cycle of products, and reduces the amount of natural resources used to manufacture new products.

51	Will materials be recycled during demolition of existing buildings and structures? If so, please describe. <u>EXPLORING OPTIONS REGARDING MOVING EXISTING HOUSES</u>	Yes	No	N/A
52	Will materials be recycled during the construction phase? If so, please describe. <u>WASTE WOOD</u>	Yes	No	N/A
53	Does your project provide enhanced waste diversion facilities i.e. on-site recycling for cardboard, bottles, cans and or recyclables or on-site composting?	Yes	No	N/A
54	For new commercial development, are you providing waste and recycling receptacles for customers?	Yes	No	N/A

Green Mobility

The intent is to encourage the use of sustainable transportation modes and walking to reduce our reliance on personal vehicles that burn fossil fuels which contributes to poor air quality.

55	Is pedestrian lighting provided in the pathways through parking and landscaped areas and at the entrances to your building[s]?	Yes	No	N/A
56	For commercial developments, are pedestrians provided with a safe path[s] through the parking areas and across vehicles accesses?	Yes	No	N/A
57	Is access provided for those with assisted mobility devices?	Yes	No	N/A
58	Are accessible bike racks provided for visitors?	Yes	No	N/A
59	Are secure covered bicycle parking and dedicated lockers provided for residents or employees?	Yes	No	N/A
60	Does your development provide residents or employees with any of the following features to reduce personal automobile use [check all that apply]: <input type="checkbox"/> transit passes <input checked="" type="checkbox"/> car share memberships <input type="checkbox"/> shared bicycles for short term use <input type="checkbox"/> weather protected bus shelters <input checked="" type="checkbox"/> plug-ins for electric vehicles			

Is there something unique or innovative about your project that has not been addressed by this Checklist? If so, please add extra pages to describe it.

CONSTANCE 638-640 + NELSON 637

ISSUED FOR REZONING - 2018.01.05



VIEW FROM S/E ON CONSTANCE



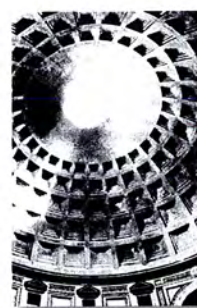
CONTEXT PLAN

PROPOSED PROJECT INFORMATION

EXISTING ZONING	637+638+640 RM-1 (MULTIPLE FAMILY RESIDENTIAL)	
REZONE TO	NEW COMPREHENSIVE ZONE	
SITE AREA	0.28 Ha / 0.70 Ac / 2,838 m ² / 30,548 ft ²	
NO. UNITS	77 (4 / 6 STOREYS) PURPOSE-BUILT RENTAL	
PARKING PROVIDED	61	
BIKE PARKING	116 + RACK FOR 6 @ ENTRANCE	
UNIT AREA (+/-)	38 m ² (409 ft ²) - 80 m ² (861 ft ²)	
TOTAL UNIT AREA	4,298 m ² (46,263 ft ²)	
BUILDING AREA	1,086 m ² (11,690 ft ²)	
FLOOR AREA RATIO	1.5 : 1	
COVERAGE	38%	
SETBACKS (PER RM-4)	FRONT	7.5m (24.6)
	REAR	7.5m (24.6)
	INTERIOR SIDE	6.0m (19.7)
	EXTERIOR SIDE	3.6m (11.8)



VARIANCE REQ'D AS NOTED
VARIANCE REQ'D AS NOTED
N/A



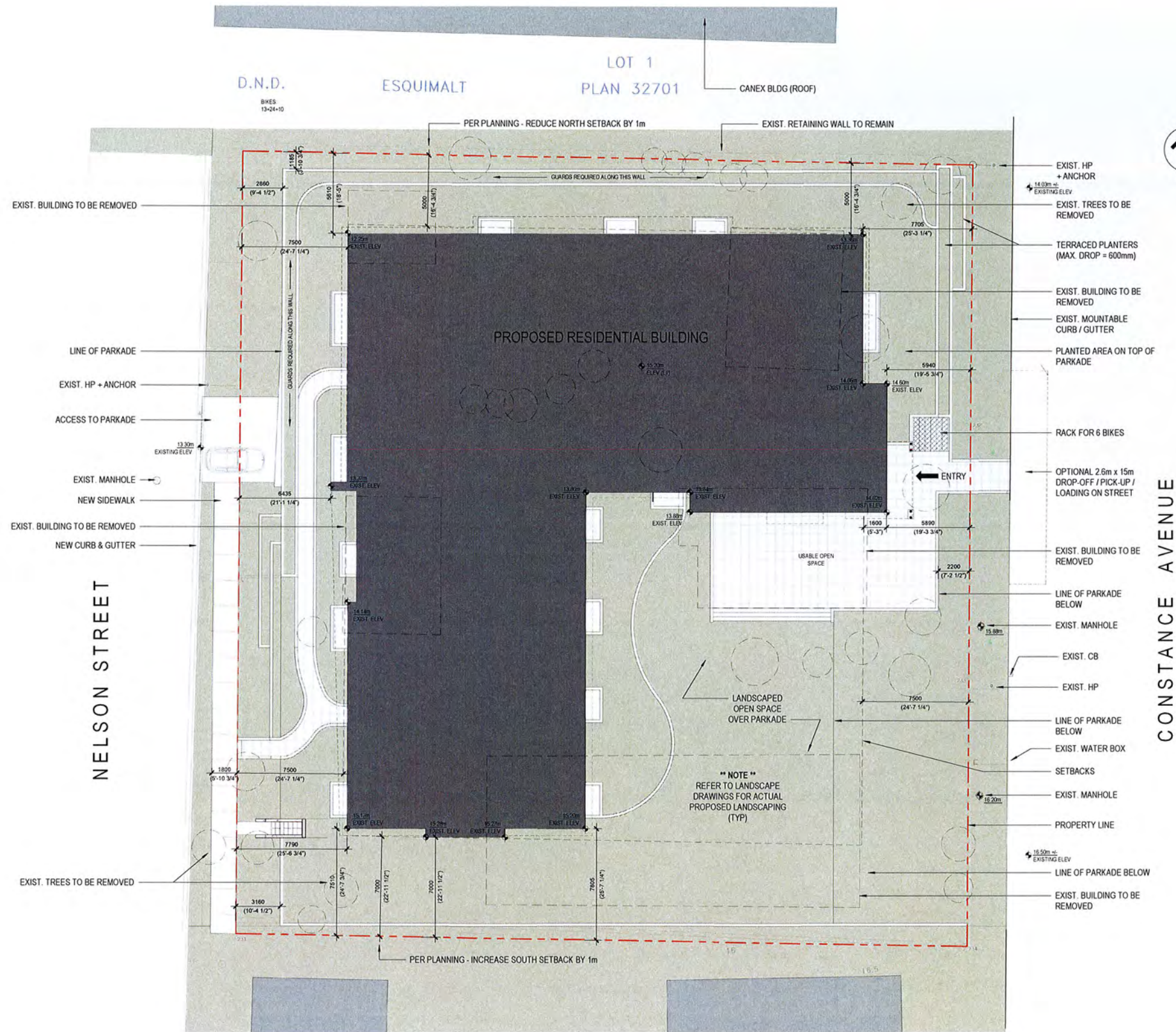
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CONSTANCE 638-640 + NELSON 637
638/640 CONSTANCE AVENUE + 637 NELSON STREET
PROJECT NO. 17-011

COVER SHEET

2018.01.05 - REZONING APPLICATION

A00



① SITE PLAN
1 : 150

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PROJECT NO. 17-011

SITE PLAN

2018.01.05 - REZONING APPLICATION



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A01



- 12.40m EXISTING ELEV.
- 12.65m EXISTING ELEV.
- 12.91m EXISTING ELEV.
- 13.17m EXISTING MH
- 13.09m EXISTING ELEV.
- 13.14m EXISTING ELEV.
- 13.30m EXISTING ELEV.
- 13.52m EXISTING MH
- 13.64m EXISTING MH
- 13.91m EXISTING ELEV.
- 13.91m EXISTING ELEV.

61 PARKING
(23 SMALL = < 50%)



① PARKADE
1:100



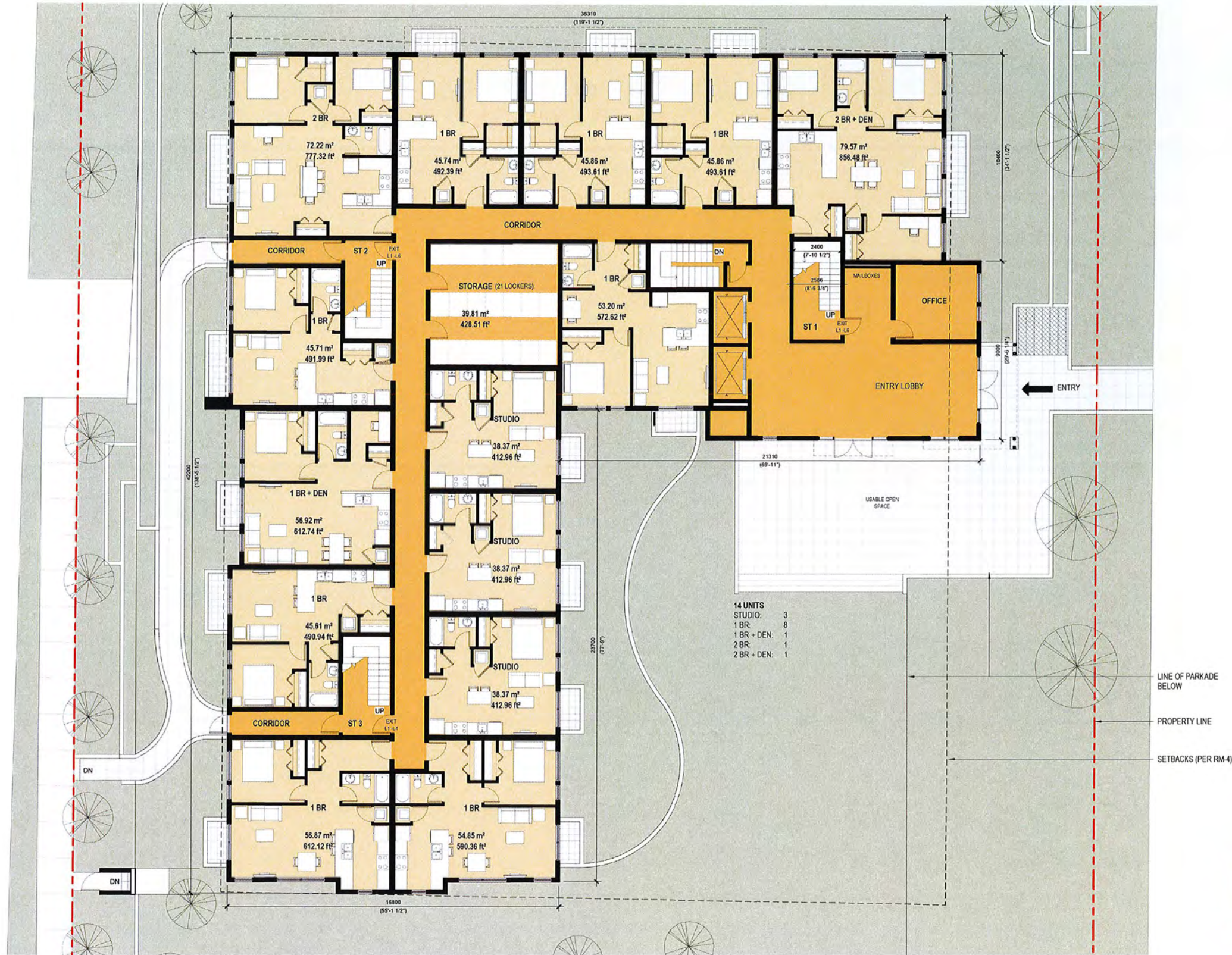
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PARKADE

2018.01.05 - REZONING APPLICATION

A02



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LEVEL 1
1: 100

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LEVEL 1

2018.01.05 - REZONING APPLICATION

A03



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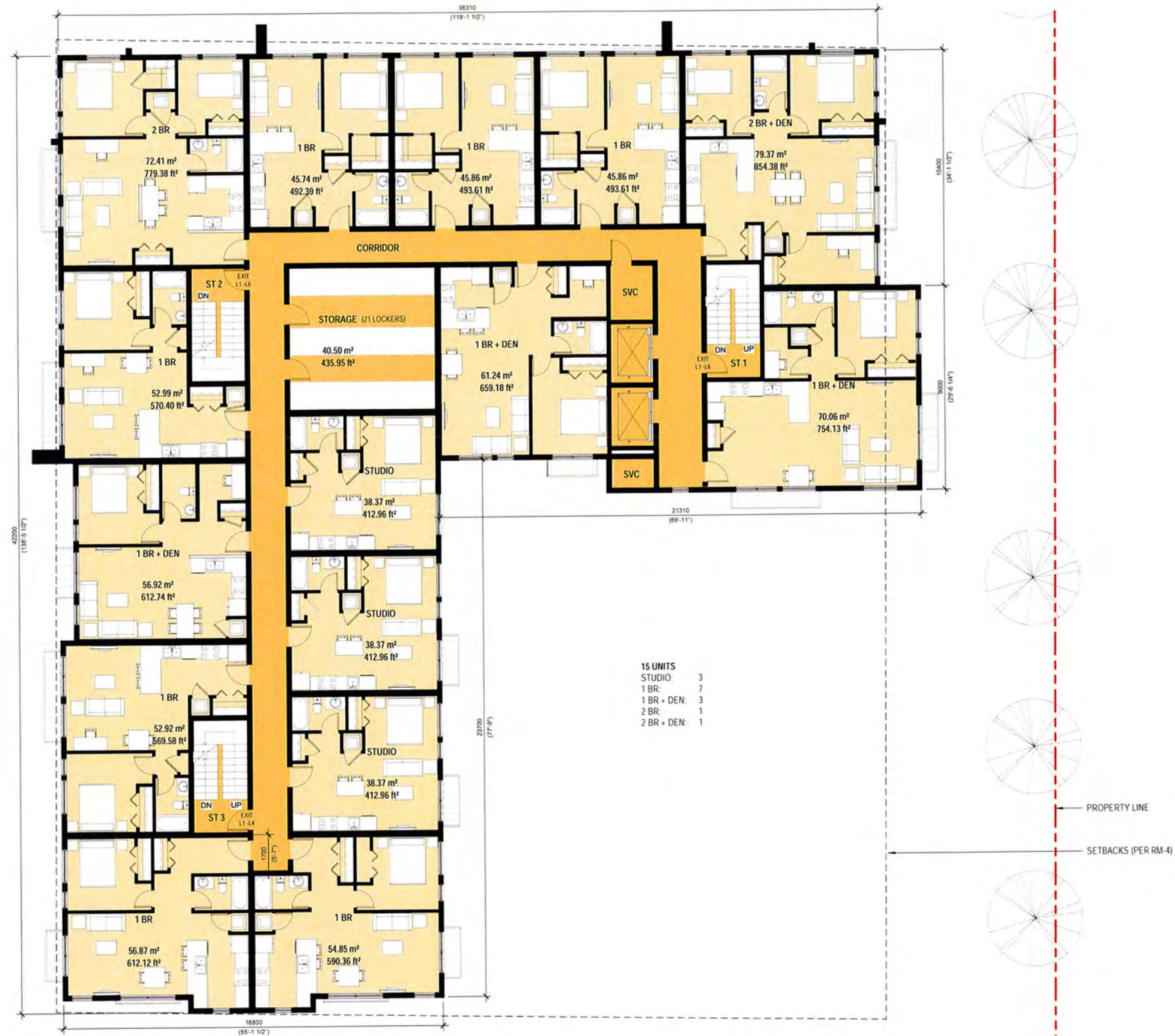
① LEVEL 2
1:100

CONSTANCE 638-640 + NELSON 637
638/640 CONSTANCE AVENUE + 637 NELSON STREET
PROJECT NO. 17-011

LEVEL 2

2018.01.05 - REZONING APPLICATION

A04



① LEVEL 3
1:100

CONSTANCE 638-640 + NELSON 637
638/640 CONSTANCE AVENUE + 637 NELSON STREET
PROJECT NO. 17-011

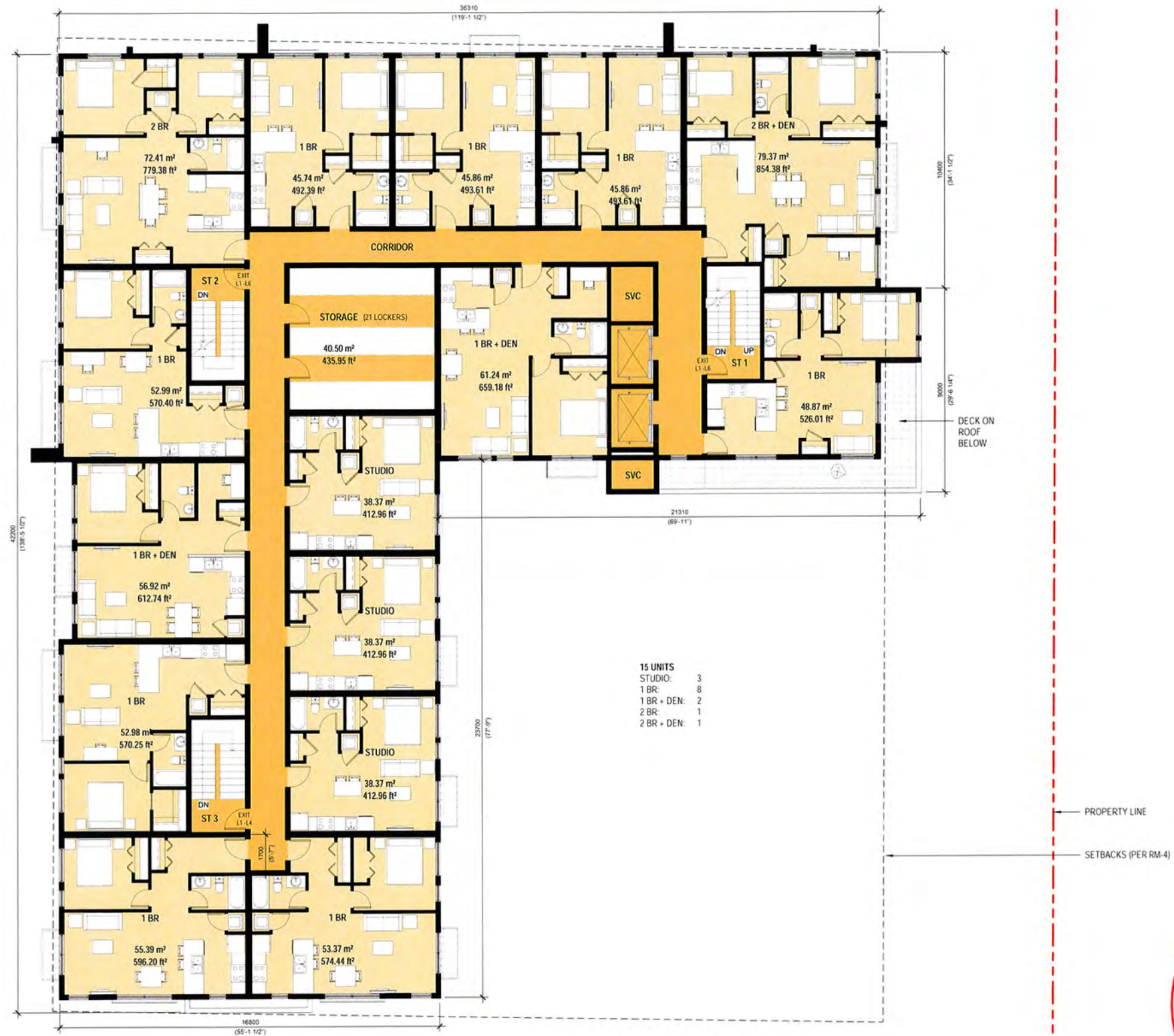
LEVEL 3

2018.01.05 - REZONING APPLICATION

A05



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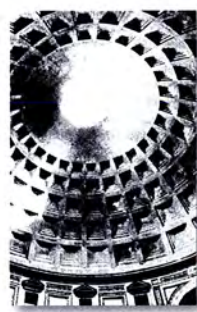


① LEVEL 4
1:100

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638/640 CONSTANCE AVENUE + 637 NELSON STREET
PROJECT NO. 17-011

LEVEL 4
2018.01.05 - REZONING APPLICATION

A06



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① LEVEL 5 + 6
1:100

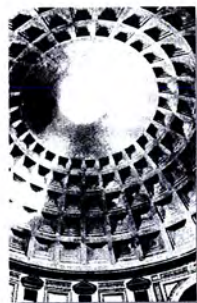
CONSTANCE 638-640 + NELSON 637
638/640 CONSTANCE AVENUE + 637 NELSON STREET
PROJECT NO. 17-011

LEVEL 5 + 6

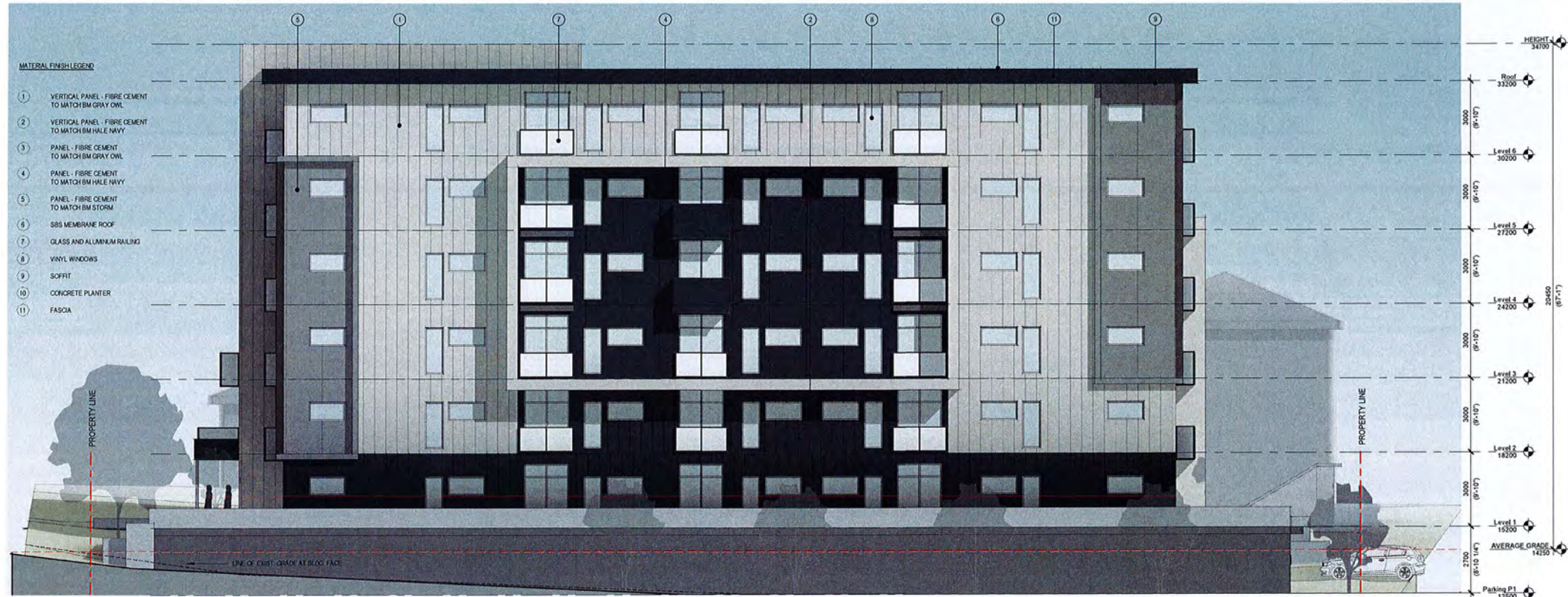
2018.01.05 - REZONING APPLICATION



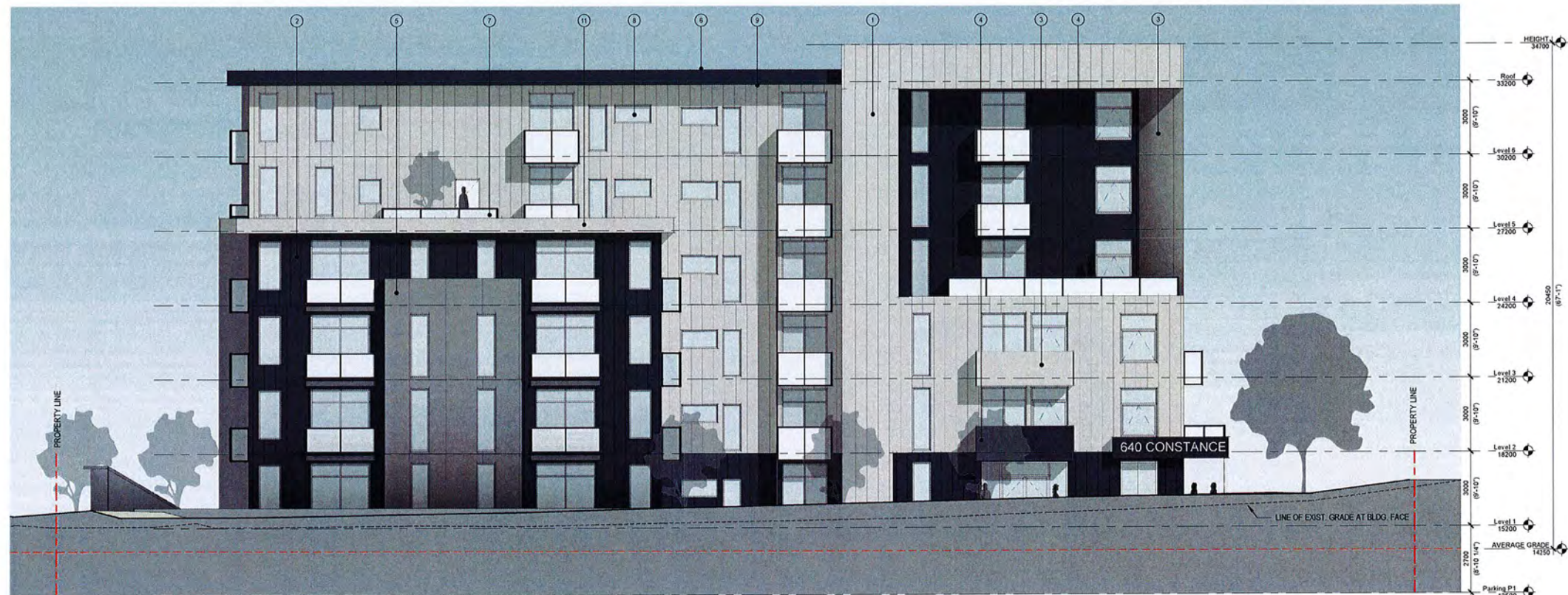
A07



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① NORTH
1: 100



② SOUTH
1: 100



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PROJECT NO. 17-011

ELEVATIONS - N/S

2018.01.05 - REZONING APPLICATION

A08



1 EAST
1:100



2 WEST
1:100



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PROJECT NO. 17-011

ELEVATIONS - E/W

2018.01.05 - REZONING APPLICATION

A09



1 SOUTHEAST PERSPECTIVE



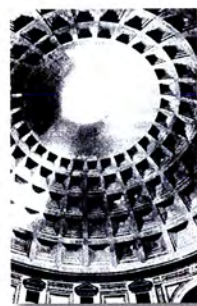
2 NORTHEAST PERSPECTIVE



3 NORTHWEST PERSPECTIVE



4 SOUTHWEST PERSPECTIVE



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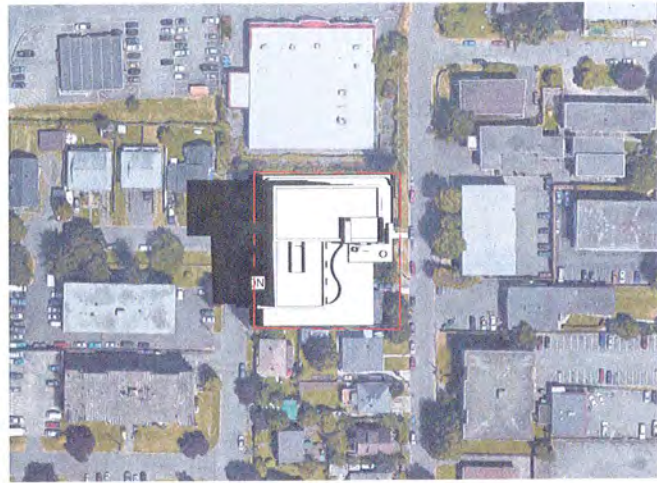
CONSTANCE 638-640 + NELSON 637
638/640 CONSTANCE AVENUE + 637 NELSON STREET
PROJECT NO. 17-011



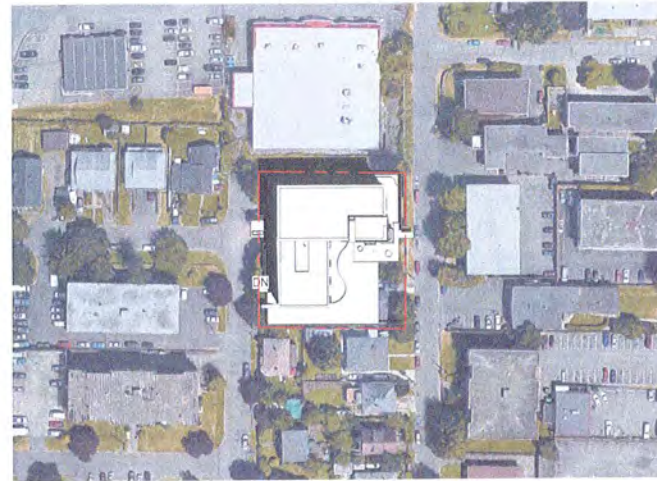
STREET VIEWS

2018.01.05 - REZONING APPLICATION

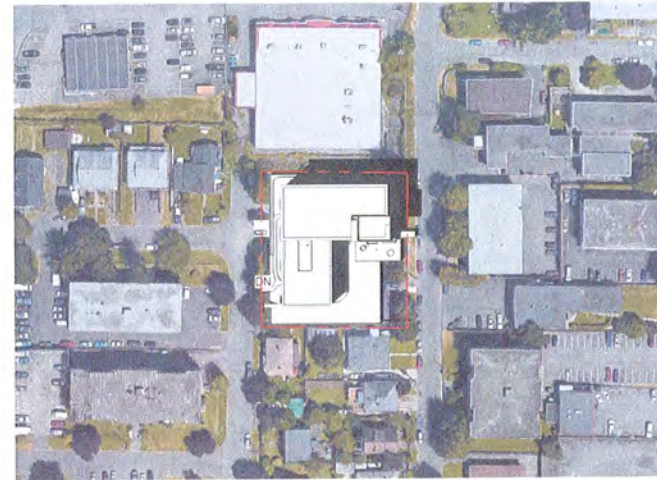
A10



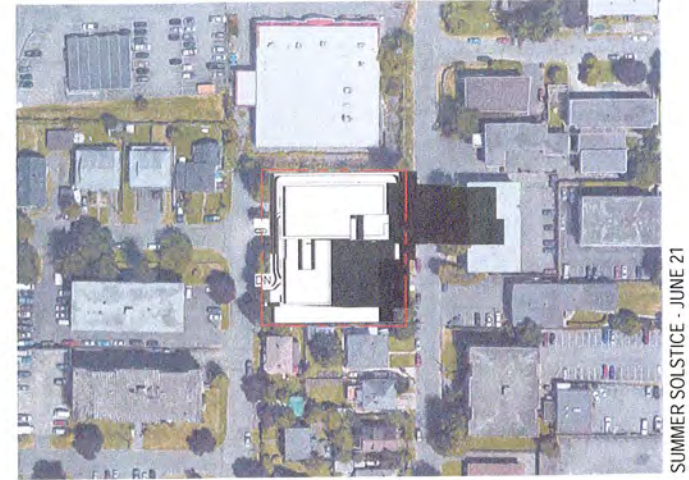
1 SHADOW STUDY - SUMMER 9AM
1: 1200



2 SHADOW STUDY - SUMMER 12PM
1: 1200

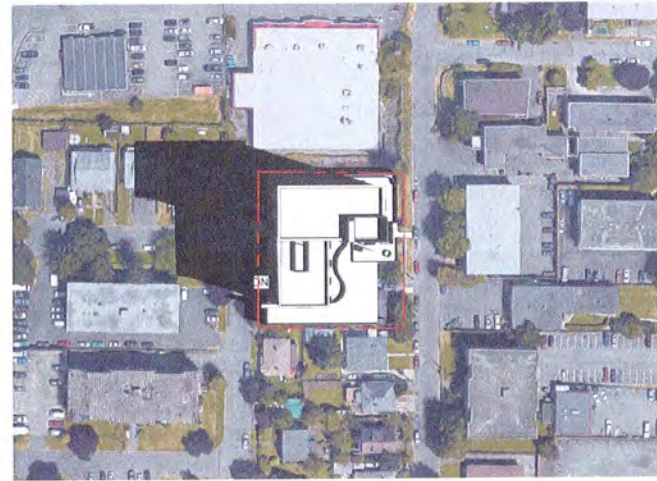


3 SHADOW STUDY - SUMMER 3PM
1: 1200

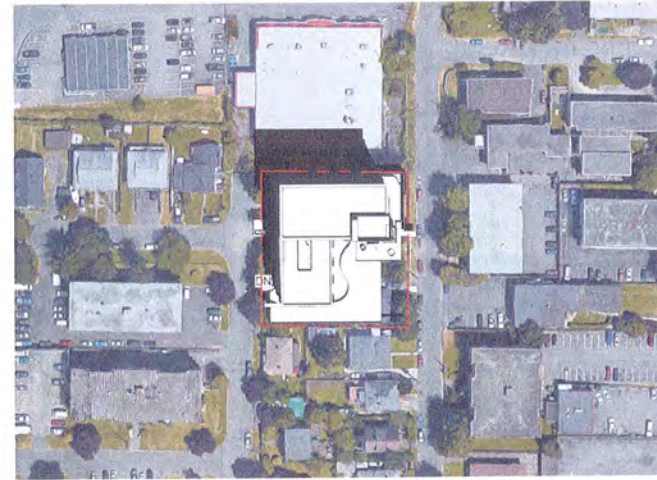


4 SHADOW STUDY - SUMMER 6PM
1: 1200

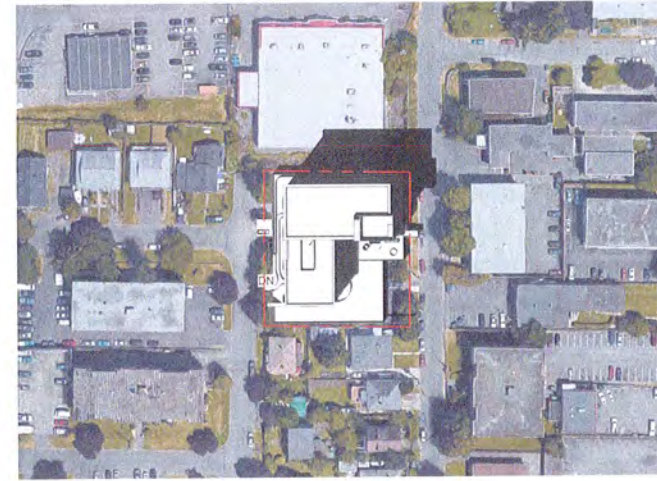
SUMMER SOLSTICE - JUNE 21



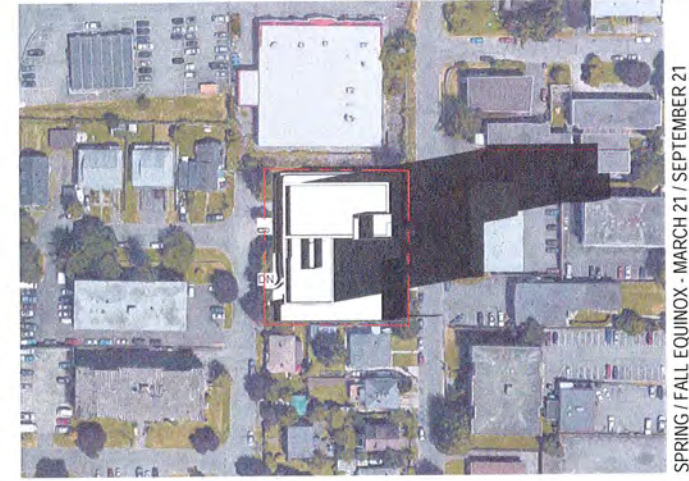
5 SHADOW STUDY - SPRING/FALL 9AM
1: 1200



6 SHADOW STUDY - SPRING/FALL 12PM
1: 1200

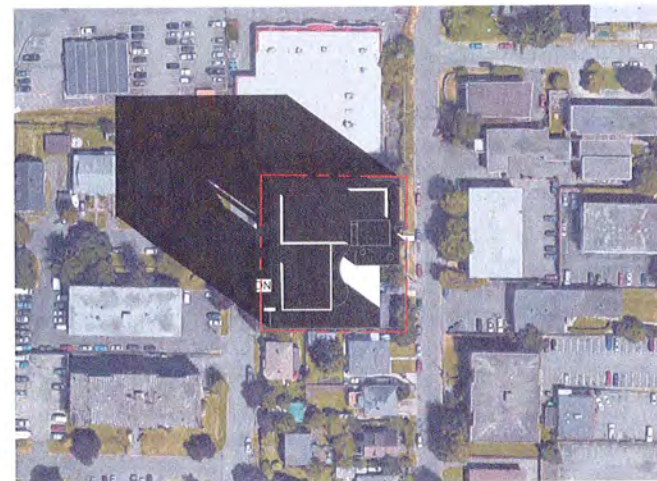


7 SHADOW STUDY - SPRING/FALL 3PM
1: 1200

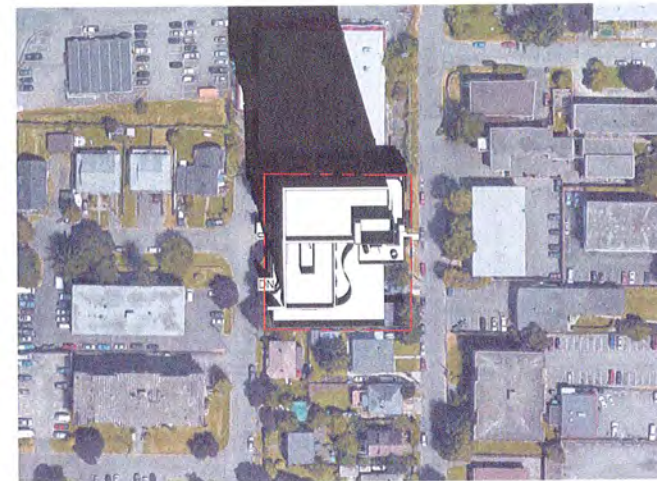


8 SHADOW STUDY - SPRING/FALL 6PM
1: 1200

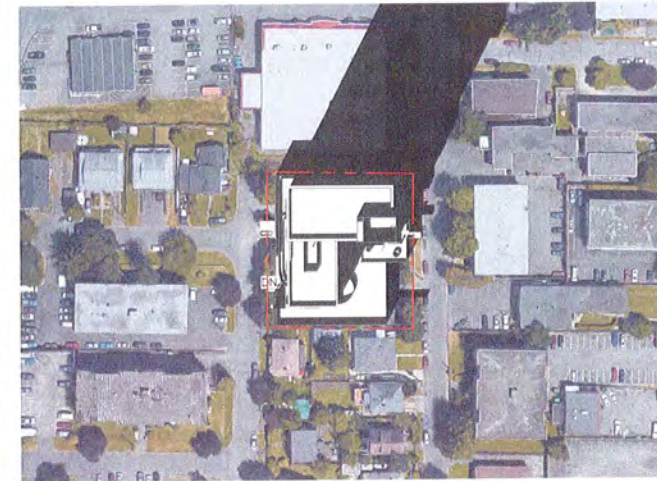
SPRING / FALL EQUINOX - MARCH 21 / SEPTEMBER 21



9 SHADOW STUDY - WINTER 9AM
1: 1200



10 SHADOW STUDY - WINTER 12PM
1: 1200



11 SHADOW STUDY - WINTER 3PM
1: 1200



12 SHADOW STUDY - WINTER 6PM
1: 1200

WINTER SOLSTICE - DECEMBER 21



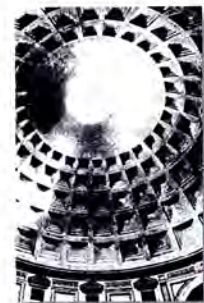
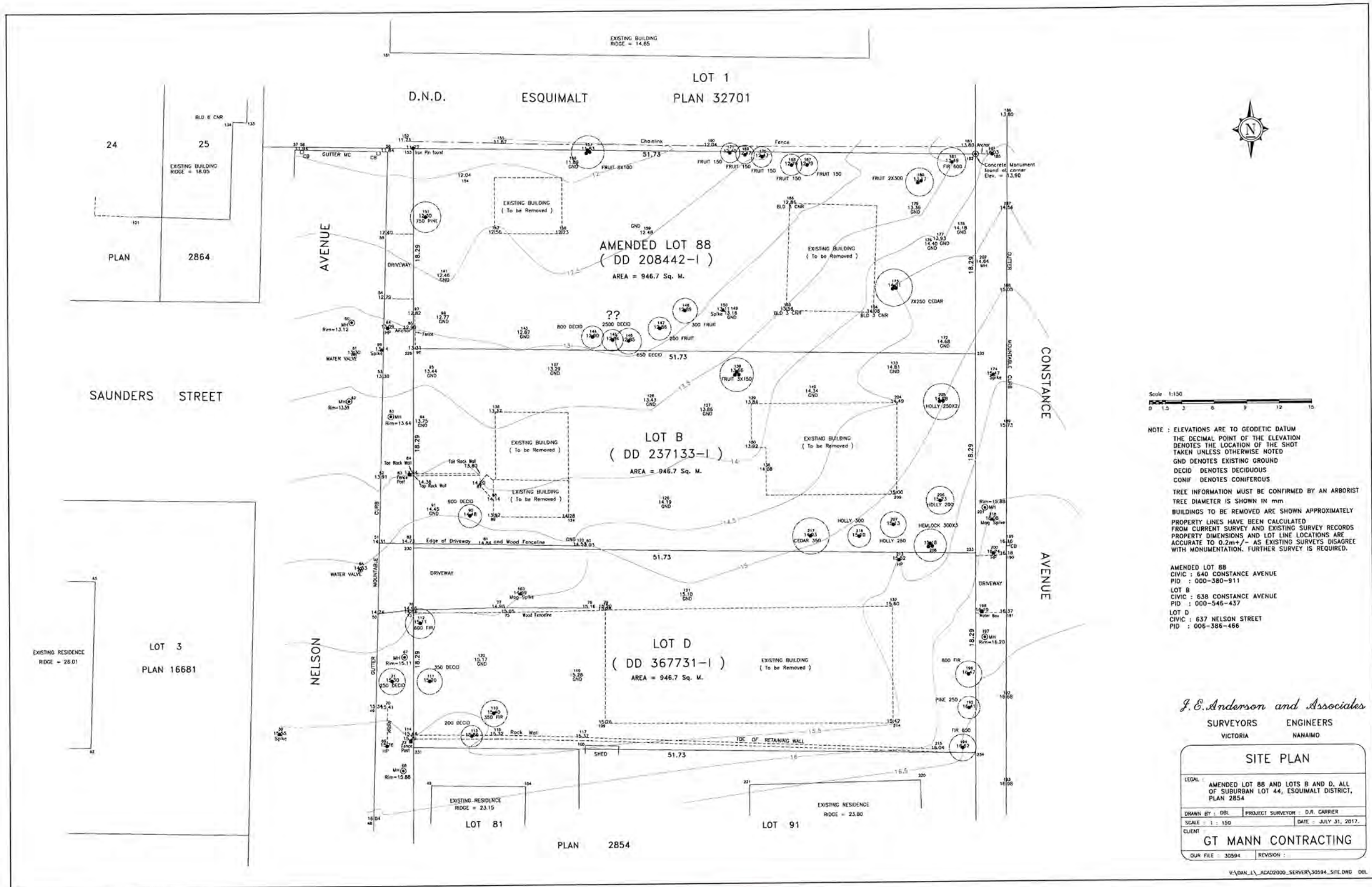
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SHADOW STUDIES
2018.01.05 - REZONING APPLICATION

A11



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638/640 CONSTANCE AVENUE + 637 NELSON STREET
PROJECT NO. 17-011

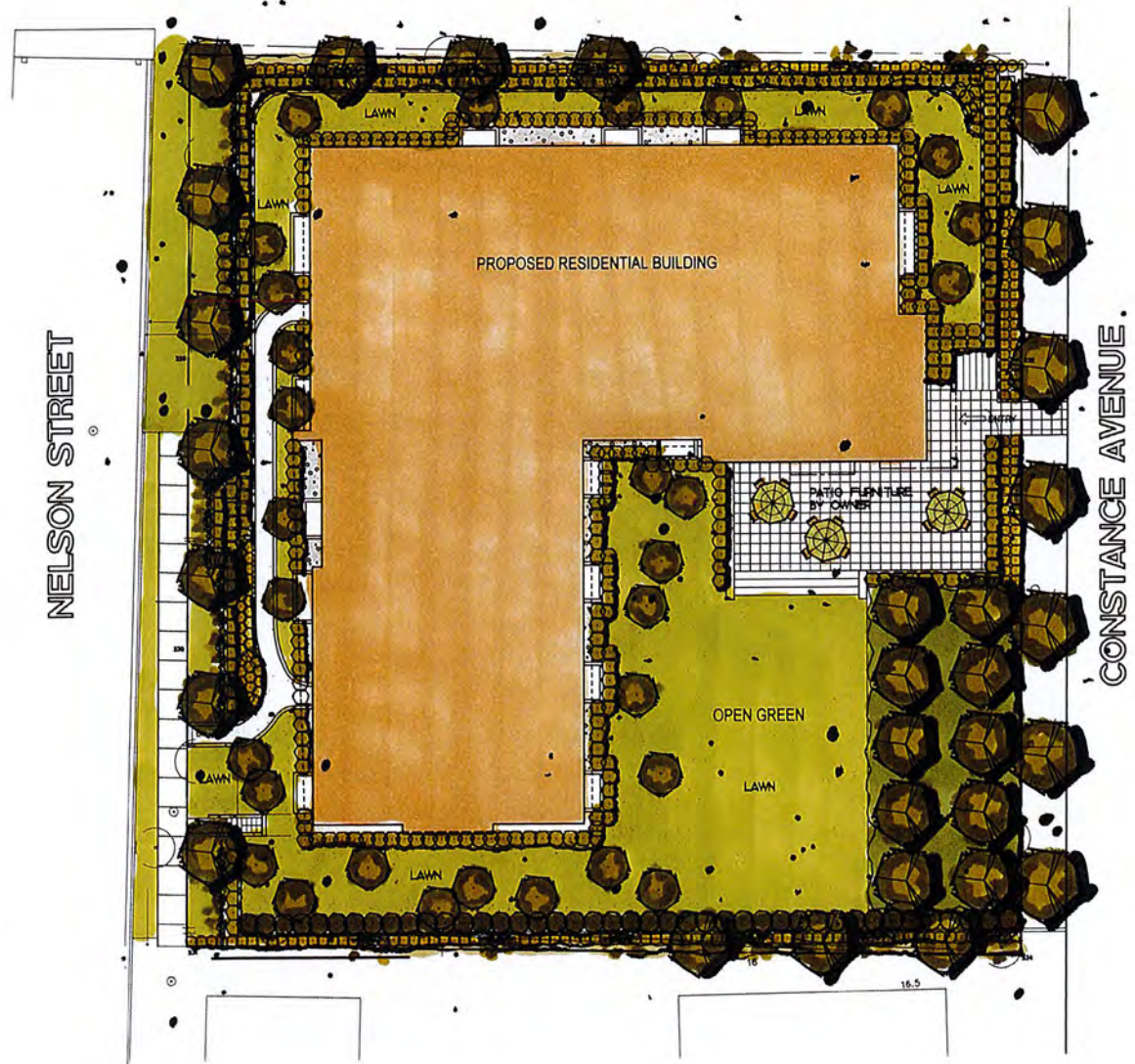


SURVEY

2018.01.05 - REZONING APPLICATION

SURV

LOT 1
PLAN 32701
D.N.D. ESQUIMALT



LEGEND

-  ORNAMENTAL DECIDUOUS TREE TO BE A SELECTION OF:
FLOWERING DOBWOOD, FLOWERING CHERRY,
FLOWERING QUAPLES, HAWTHORNE
SIZE 9 CM CAL., APPROXIMATE NO. - 07
-  COLUMNAR DECIDUOUS TREE TO BE:
COLUMNAR RED MAPLE, ARMSTRONG BOLD,
DANWICK BOLD BEECH, SIZE 7.0 CM CAL.,
APPROXIMATE NO. - 28
-  MULTISTEM TO BE A SELECTION OF: STAR MAGNOLIA (DEC),
STARBORN ELIAC (DEC), HOLLY (BL), CAPELIA (BL),
JAPANESE AZALEA (BL), LILAC (DEC),
SIZE 27 CM POT, APPROXIMATE NO. - 82
-  LARGE SHRUB TO BE A SELECTION OF: BLOSSY
AZALEA (BL), PIERIS (BL), RHODOODENDRON (BL),
MEXICAN ORANGE (BL), DECIDUOUS AZALEA (DEC),
COTONCASTER (BL), PORTUGUESE LAUREL (BL),
FIRETHORN (BL), HYDRANGEA (DEC),
SIZE 27 CM POT, APPROXIMATE NO. - 42
-  MEDIUM SHRUB TO BE A SELECTION OF:
RHODOODENDRON (BL), JAPANESE AZALEA (BL), PINK
ESCALLONIA (BL), BARBERRY (BL), BUXUS (BL), FERNS (BL),
SIZE 21 CM POT, APPROXIMATE NO. - 89
-  SMALL SHRUB TO BE A SELECTION OF: DWARF
RHODOODENDRON (BL), EDWARD BOUGHER AZALEA (BL),
LAVENDER (BL), GOLDEN SPIREA (DEC), DWARF
JAPANESE AZALEA (BL), NEWSPYRE DWARF ESCALLONIA (BL),
SIZE 15 CM POT, APPROXIMATE NO. - 266
-  VINES TO BE A SELECTION OF: PYRACANTHA (BL),
ENHARTMAN IVY (DEC), CLEMATIS (DEC),
SIZE 21 CM POT, APPROXIMATE NO. - 84
-  GROUNDCOVER TO BE A SELECTION OF: PERIWINKLE
PERIWINKLE (BL), KINKIDINKO (BL),
WINTERGREEN (BL), BARBERRY (BL),
SIZE 10 CM POT, PLANT 48 CM O.C.

NOTES

- LANDSCAPE AREAS ARE TO BE IRRIGATED WITH A FULLY AUTOMATIC UNDERGROUND IRRIGATION SYSTEM.
- THIS DRAWING IS CONCEPTUAL ONLY AND NOT INTENDED FOR CONSTRUCTION PURPOSES.
- THIS DRAWING IS FOR SOFT LANDSCAPE ONLY.



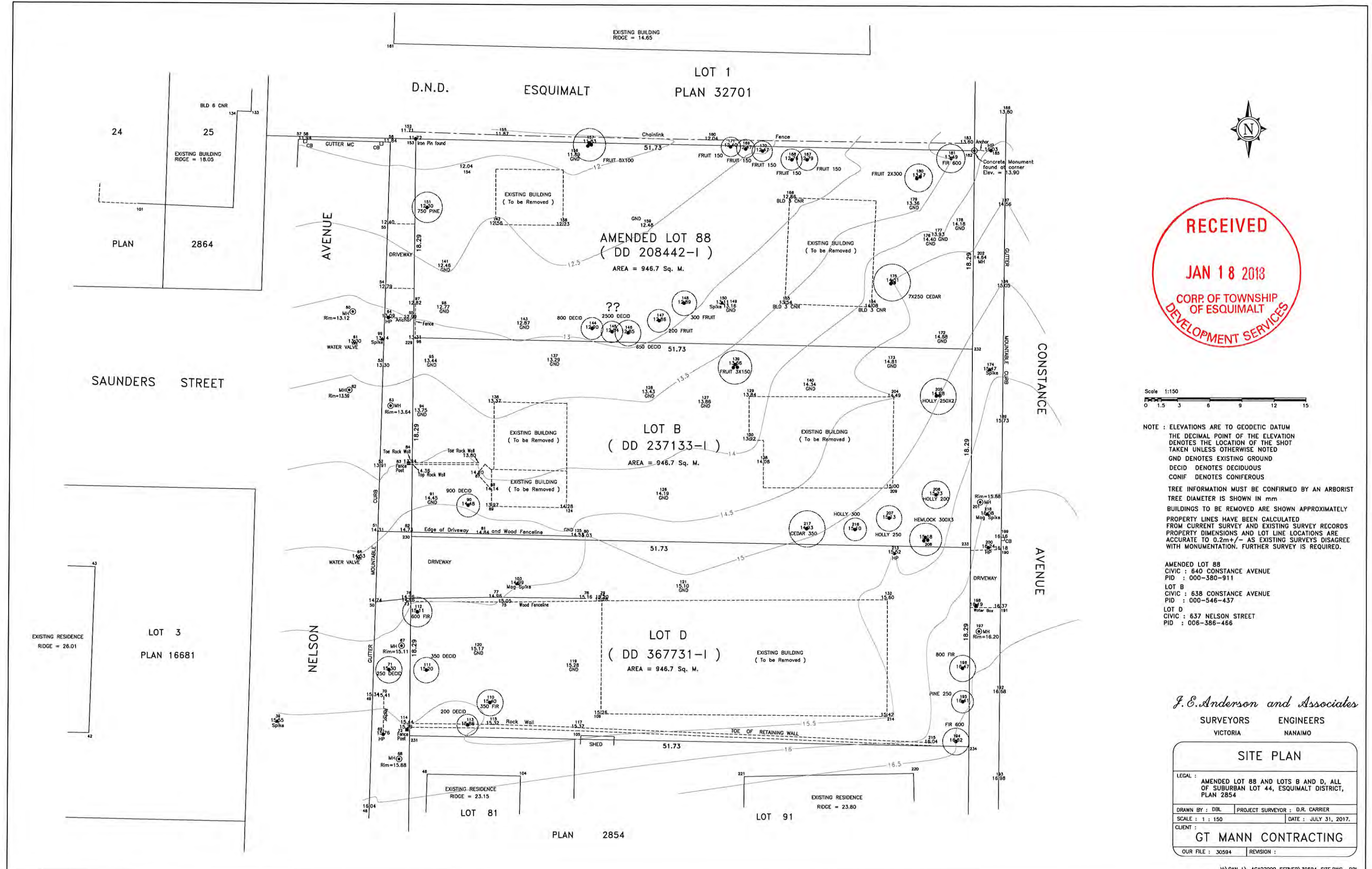
DATE	BY	REVISION
NOVEMBER 07, 2017	J.P.	S.P.
638 CA-P1.DWG		

Lombard North Group
Lombard North Group (BC) Inc.
838 CONSTANCE STREET
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638+640 CONSTANCE AVENUE
VICTORIA, B.C.



LANDSCAPE PLAN | P1



NOTE : ELEVATIONS ARE TO GEODETIC DATUM
 THE DECIMAL POINT OF THE ELEVATION DENOTES THE LOCATION OF THE SHOT TAKEN UNLESS OTHERWISE NOTED
 GND DENOTES EXISTING GROUND
 DECID DENOTES DECIDUOUS
 CONIF DENOTES CONIFEROUS
 TREE INFORMATION MUST BE CONFIRMED BY AN ARBORIST
 TREE DIAMETER IS SHOWN IN mm
 BUILDINGS TO BE REMOVED ARE SHOWN APPROXIMATELY
 PROPERTY LINES HAVE BEEN CALCULATED FROM CURRENT SURVEY AND EXISTING SURVEY RECORDS
 PROPERTY DIMENSIONS AND LOT LINE LOCATIONS ARE ACCURATE TO 0.2m+/- AS EXISTING SURVEYS DISAGREE WITH MONUMENTATION. FURTHER SURVEY IS REQUIRED.

AMENDED LOT 88
 CIVIC : 640 CONSTANCE AVENUE
 PID : 000-380-911
 LOT B
 CIVIC : 638 CONSTANCE AVENUE
 PID : 000-546-437
 LOT D
 CIVIC : 637 NELSON STREET
 PID : 006-386-466

J.E. Anderson and Associates
 SURVEYORS ENGINEERS
 VICTORIA NANAIMO

SITE PLAN	
LEGAL : AMENDED LOT 88 AND LOTS B AND D, ALL OF SUBURBAN LOT 44, ESQUIMALT DISTRICT, PLAN 2854	
DRAWN BY : DBL	PROJECT SURVEYOR : D.R. CARRIER
SCALE : 1 : 150	DATE : JULY 31, 2017.
CLIENT : GT MANN CONTRACTING	
OUR FILE : 30594	REVISION :



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

APC Meeting: March 20, 2018

STAFF REPORT

DATE: March 16, 2018
TO: Chair and Members of the Advisory Planning Commission
FROM: Bill Brown, Director of Development Services
SUBJECT: Review of the Draft Official Community Plan

PURPOSE:

To review the Draft Official Community Plan and provide Council with any recommended changes along with reasons for the proposed changes.

BACKGROUND:

At their March 12, 2018 Committee of the Whole meeting, Council reviewed the draft Official Community Plan and directed staff to refer it to the Advisory Planning Commission for review and recommendations.

The process began on September 28, 2015, when Council approved the Project Charter for the Review of the Official Community Plan. From the time the project began until now staff have guided the review process through numerous public consultation phases involving a variety of means and methods (open houses, surveys (online and paper), public workshops, Council workshops, Facebook®, studies (e.g. Esquimalt Age-friendly Assessment), stakeholder consultations (e.g. School Districts number 61 and 93), statutory referrals, booth at Buccaneer Days, and interdepartmental consultation). The following timeline provides a chronological overview of the process to date:

September 28, 2015	Council authorizes the commencement of the review of the Official Community Plan.
October 13, 2015	Council reviews the draft Official Community Plan survey.
January 11, 2016	Staff presents the Official Community Plan survey results to Council.
February 15, 2016	Council approves the Phase 2 consultation plan.
April 1, 2016	Council reviews the Township's Vision statement.

May 4, 2016	Looking Forward Forum held at the Recreation Centre.
December 12, 2016	Staff gives Council an Official Community Plan update.
February 20, 2018	Staff gives Council an Official Community Plan update.
May 5, 2017	Affordable Housing Workshop held at the Archie Browning Sports Centre.
June 7, 2017	Official Community Plan Open House held at the Archie Browning Sports Centre.
June 19, 2017	Staff present the Affordable Housing Workshop report to Council.
July 10, 2017	Staff present the Open House Report to Council.
October 3, 2017	Council holds a Special Committee of the Whole meeting to discuss housing policy.
September 2017 to February 2018	Staff draft policies and development permit guidelines.

For the past several months, the information that has been collected throughout the process has been collated and analyzed to develop the Draft Official Community Plan policies and Design Guidelines (Draft Plan) attached as Schedule "A".

The intent of the review process was not to rewrite the entire Official Community Plan but rather to update it to reflect emerging community values, changes to the law, and changes or new interpretations of the law. To this end, a fair amount of the 2007 Official Community Plan has been carried forward into the Draft Official Community Plan. On the other hand, the 2007 Official Community Plan also contained material that was either difficult to understand and interpret or should not have been in the Official Community Plan. These portions have been either removed from the Draft Plan or revised. There has also been substantial content added to the Draft Plan based on information received during the review process as well as changes to legislation. An overview of the major changes to the Draft Plan is outlined below:

- 1) The look and feel of the Plan has been updated to reflect contemporary legislative drafting practices and graphic design.
- 2) The introduction has been revised and updated, and now contains the Township's Vision, Mission, and Values statements.
- 3) The community profile has been updated using the results of the 2016 Federal Census. This section will be enhanced in the version of the Draft Plan that is presented to Council for first and second readings.
- 4) This Draft Plan follows the requirements of the Local Government Act in a more precise manner. This is manifested in a number of ways including the addition of another land use designation map. As per the requirements of

the Local Government Act, there are now two land use designation maps, one for the “Present” and one for the “Proposed” land use designations. Only a few changes to the land use designation map are proposed in the new Draft Plan. They are shown on the four maps in Schedule “B”. It should be noted that the relative distribution of building heights remains similar to those in the existing plan.

- 5) The Temporary Use Permit section has been made more robust and brought into line with the existing legislation. Under the 2007 Plan, Temporary Use Permits could only be issued for up to two years. Under the new Plan, they can be issued for up to three years in accordance with amendments made to the Local Government Act since the 2007 Plan was adopted.
- 6) The Development Approval Information Area section has also been significantly enhanced in order to better comply with the enabling provisions of the Local Government Act and recent case law.
- 7) The Housing and Residential Land Use Section has been significantly revised with the addition of a quantitative analysis of trends in housing based on data from recent Federal Censuses. The analysis concludes that the municipality has sufficient land to accommodate its housing needs for the next five years.
- 8) Policy has been added that would allow Council to amend the Zoning Bylaw to allow suites in duplexes.
- 9) Policy has been added that would allow Council to amend the Zoning Bylaw to allow “Detached Accessory Dwelling Units” (aka laneway houses, granny flats, garden suites, coach houses) in low density residential areas.
- 10) Policy has been added that would allow Council to amend the Zoning Bylaw in order to allow short-term accommodation in residential areas (e.g. Airbnb ®).
- 11) A section has been added on “Age-friendly Housing”.
- 12) Rather than differentiate multi-family housing based primarily on height as is the case in the 2007 Plan, the Draft Plan differentiates it based on density by referring to two classes of multi-family housing as either medium density with a floor area ratio of up to 2.0 and a height of up to 6-storeys, or a high density with a floor area ratio of up to 3.0 and a height of up to 12-storeys. Note that in the 2007 Plan, “Multi-Unit Low Rise Residential” was considered to be buildings up to 4-storeys with a floor area ratio of up to 1.5. Since the 2007 Plan was adopted, the BC Building Code has been amended in order to allow wood frame buildings up to 6-storeys. In order to ensure concurrence between the Draft Plan and the BC Building Code, the height of medium density buildings has been set at 6-storeys. In addition, the floor area ratio has been set at 2.0 in concert with the Plan’s overall goal of allowing for a moderate increase in density.
- 13) Policy has been added that encourages the use of the Revitalization Tax Exemption as an incentive for new or renovated purpose-built multi-unit rental buildings.

- 14) Policy has been added related to encouraging housing agreements that ensure that all strata units have the opportunity to be used as long-term rental.
- 15) The Parks, Recreation, and Trails section has been updated based on discussions with the Parks and Recreation Department's senior management team.
- 16) The Transportation and Infrastructure sections have been updated based on discussions with the Engineering and Public Works senior management team.
- 17) A number of seminal policies are carried forward from the 2007 Plan into the Draft Plan. These include the policy to pursue the acquisition of dedicated parkland along Esquimalt's waterfronts at the time of subdivision.
- 18) The target for the reduction of the community's greenhouse gas by 38% has been moved from 2020 (less than two years from now) to 2030.
- 19) The greenhouse gas emissions target for 2050 has been increased from a reduction of 83% to a target of "zero net community greenhouse gas emissions". This target would be consistent with targets set by the City of Victoria and the District of Saanich.
- 20) An Urban Food Production section has been added to the Draft Plan.
- 21) The Economic Development section has been updated to reflect the Economic Development Strategy and recent Economic Development initiatives. Specific policies have been added related to the "clean technology" sector and encouraging an advanced educational institute to locate in Esquimalt.
- 22) The Regional Context Statement has not been included in the Draft Plan as staff are waiting for guidance from the Capital Regional District. A Regional Context Statement will be included in the Plan that is presented to Council for first and second readings.
- 23) The Development Permit Areas section of the Draft Plan has been significantly updated by both removing material that was outside of the realm of development permit guidelines and adding new material in response to amendments to the Local Government Act.
- 24) The following five new "environmental" Development Permit Areas have been added to the Plan:
 - a. Protection of the Natural Environment;
 - b. Protection of development from hazardous conditions;
 - c. Establishment of objectives to promote energy conservation;
 - d. Establishment of objectives to promote water conservation; and
 - e. Establishment of objectives to promote the reduction of greenhouse gas emissions.

All areas except for the "protection of development from hazardous conditions" encompass the entire municipality. This means that many properties will be subject to four or more development permit areas. There are provisions in the Plan to ensure that only one development permit application will be required even if a property is subject to more than one development permit area.

- 25) Also new to this section is the West Bay development permit area. It is based on the West Bay Neighbourhood Design Guidelines that were completed several years ago. It was always envisioned that the guidelines in the West Bay Neighbourhood Design Guidelines would be incorporated into the Official Community Plan when it was updated. Although the West Bay Neighbourhood Design Guidelines appear as an appendix in the Draft Plan, they will be slotted into their appropriate position in the Plan that is submitted to Council for first and second readings.

Overall, the Draft Plan represents a significant updating of the 2007 Plan as well as a clean up and partial reorganization in order to make it more user-friendly. Changes have also been made in order to comply with existing legislation and recent case law. Most importantly, the Draft Plan incorporates the results of an extensive community consultation process. It is important that an Official Community Plan reflects the dreams and aspirations of its residents, employers, and business owners. In addition, the Plan incorporates the latest information available including: data from the 2016 Federal Census, the results of numerous studies and reports (e.g. Age Friendly Assessment, Healthy Housing Workshop, Affordability Housing Workshop, Economic Development round tables and resulting strategy, greenhouse gas emissions data, and published literature.) It is imperative that Council is comfortable with the content of the Draft Plan since it is the statutory document that will guide Council's land use decision making processes for the next five to ten years.



Township of Esquimalt

Draft Official Community Plan

March 2018



Township of
ESQUIMALT



CONTENTS

1. Introduction.....	5	5. Commercial & Commercial Mixed-Use Land Use.....	23
1.1 Overview	5	5.1 General	23
1.2 Purpose of an Official Community Plan	5	5.2 Revitalization.....	25
1.3 Plan Area	6	5.3 Esquimalt Town Centre.....	25
2. Township of Esquimalt.....	7	5.4 Head Street and Esquimalt Road	26
2.1 Vision, Mission and Values.....	7	5.5 Neighbourhood Commercial Mixed-Use	27
2.2 Community Profile	8	6. Industrial land use.....	29
2.3 Regional Profile.....	9	6.1 General	29
2.4 Planning Context.....	9	6.2 Esquimalt Industrial Park.....	29
3. Land Use Planning.....	11	6.3 Public Services and Procurement Canada Land.....	30
3.1 Land Use Designations.....	11	6.4 McLoughlin Point Special Use	30
3.2 General Development	11	7. institutional & public facilities land use.....	32
3.3 Temporary Use Permits	12	7.1 General.....	32
3.4 Development Approval Information Area	3	7.2 Federal Land.....	32
3.5 Sand and Gravel	15	7.3 Public and Private Service Facilities.....	33
3.6 Restriction on Land that is Subject to Hazardous Conditions.....	15	7.4 Schools	34
4. Housing & Residential Land Use	16	8. Parks, Recreation & Trails.....	35
4.1 General: Anticipated Housing Needs in the Next Five Years	16	8.1 Parks and Recreational Land Use	35
4.2 Low Density Residential Redevelopment	19	8.2 Parks	35
4.3 Medium and High Density Residential	19	8.3 Recreation.....	36
4.4 Affordable Housing	21	8.4 Trails and Waterfront Walkways.....	37
4.5 Age-friendly Housing.....	22		

- 9. Transportation..... 39
 - 9.1 Walking.....39
 - 9.2 Cycling40
 - 9.3 Public Transit40
 - 9.4 E & N Rail Corridor41
 - 9.5 Roads 41
 - 9.6 Public Parking42
- 10. Infrastructure..... 43
 - 10.1 Sanitary Sewer Collection Systems.....43
 - 10.2 Storm Water Collection System.....44
 - 10.3 Street Lights.....44
 - 10.4 Regional Sanitary Collection and Distribution Systems (Potable Water)45
 - 10.5 Utilities Distribution Systems45
- 11. Environment, Energy & Climate Change 46
 - 11.1 Natural Areas.....46
 - 11.2 Solid Waste Management46
 - 11.3 Reduction of Greenhouse Gas Emissions.....47
 - 11.4 Climate Change Adaptation50
- 12. Urban Agriculture 51
 - 12.1 Agricultural Land Use51
 - 12.2 Urban Food Production51
- 13. Arts, Culture & Heritage 53
 - 13.1 Arts and Culture53
 - 13.2 Heritage54
- 14.1 General57
- 14. Economic Development..... 57
 - 14.2 Commercial & Industrial58
 - 14.3 West Bay58
 - 14.4 Tourism59
- 15. Regional Context..... 60
 - 15.1 Regional Growth Strategy60
 - 15.2 Regional Context Statement (to come)60
- 16. Development Permit Areas..... 61
 - 16.1 General Regulations.....61
 - 16.2 General Development Permit Exemptions61
- 17 Development Permit Area No. 1 -Natural Environment 64
 - 17.1 Area:64
 - 17.2 Designation:64
 - 17.3 Justification64
 - 17.4 Exemptions66
 - 17.5 Guidelines67
- 18. Development Permit Area No. 2 - Protection of Development from hazardous conditions..... 71
- 19. Development Permit Area No. 3 - Enhanced Design Control Residential 72
 - 19.1 Area72
 - 19.2 Designation72
 - 19.3 Justification72
 - 19.4 Exemptions72
 - 19.5 Duplex Guidelines73
 - 19.6 Guidelines74

20 Development Permit Area No. 4 - Commercial.....	76	24. Development Permit Area No. 8 - Water Conservation	87
20.1 Area	76	24.1 Area:	87
20.2 Designation	76	24.2 Designation	87
20.3 Justification	76	24.3 Justification	87
20.4 Exemptions	76	24.4 Exemptions	88
20.5 Guidelines	77	24.5 Guidelines	88
21. Development Permit Area No. 5 - Industrial.....	78	25. Development Permit Area No. 9 - English Inn	90
21.1 Area	78	25.1 Area	90
21.2 Designations	78	25.2 Designations	90
21.3 Justification	78	25.3 Justification	90
21.4 Exemptions	78	25.4 Exemptions	91
21.5 Guidelines	79	25.5 Guidelines	91
22. Development Permit Area No. 6 - Multi- Family Residential	80	26. Development Permit Area No. 10 - Esquimalt Town Square	95
22.1 Area	80	26.1 Area	95
22.2 Designation	80	26.2 Designations	95
22.3 Justification	80	26.3 Justification	95
22.5 Exemptions	80	26.4 Exemptions	96
 		26.5 Design Guidelines.....	96
23. Development Permit Area No. 7 - Energy Conservation and Greenhouse Gas Reduction.....	82	27. Development Permit Area No. 11 - West Bay	101
23.1 Area:	82	27.1 Area	101
23.2 Designation:.....	82	27.2 Designations	101
23.3 Justification:.....	82	27.3 Justification	101
23.4 Exemptions.....	83		
23.5 Guidelines.....	83		

28. Development Permit Area No. 12 - McLoughlin Point..... 102

28.1 Area102

28.2 Designation102

28.3 Justification102

28.4 Exemption.....103

25.5 Guidelines.....103

Official Community Plan: Maps... 103

Appendices 123

Draft DPA #11 (West Bay)124

1 INTRODUCTION



1.1 Overview

This Official Community Plan [OCP or the Plan] provides a framework to guide future growth and change in Esquimalt in a sustainable manner. The Plan's statements, goals, objectives, policies, targets, actions, and map designations promote the long-term economic, environmental, social and cultural sustainability of the community.

The Plan is the legal vehicle that Council uses in establishing growth and development control tools such as development permit areas and providing the broad policy framework within which future zoning bylaws and amendments must align. The Plan also provides anyone with interest in the community, including residents, land and business owners, investors and developers, with a vision of the future of the community and a framework for their own decision making.

An Official Community Plan is not a static document, it should respond to changes in the community, including its values and social, economic and environmental conditions and changing regulations and authorities. Plan changes are required to be undertaken through bylaw amendments after consultation, as required under the Local Government Act which includes a Public Hearing process, to ensure that the public's input on significant land use decisions and policy changes are considered for approval.

1.2 Purpose of an Official Community Plan

The Local Government Act sets out mandatory and optional topics to be included in an OCP and provides a range of "tools" that allow municipalities to implement their plans. Where a topic is not within the jurisdiction of the local government, an OCP may only state the broad objectives related to that matter. Once adopted, the OCP does not commit the Township to carry out the projects in the Plan but it does require that works undertaken and bylaws enacted by Council are consistent with it.

In addition to local interests, the OCP takes into account the policies and regulations of the Capital Region, provincial and federal ministries and regulating bodies that are involved in the management of land and natural resources. Land that is owned by the Federal Government is immune from municipal land use regulations. Furthermore, the Provincial Government is not bound by municipal land use regulations in relation to the use or development of land, or in the planning, construction, alteration, servicing, maintenance or use of improvements – although that immunity does not extend to tenants or occupiers of Provincial Crown land.

1.3 Plan Area

The area covered by this OCP includes all water and land areas within the boundaries of the Township of Esquimalt, as shown on the “Present Land Use Designation Map.”



2 TOWNSHIP OF ESQUIMALT



Section 2 provides highlights of the Township of Esquimalt as it is in present day and where it is envisioned to go in the future. In this section you will find the community's vision, mission and values statements, as well as community and regional profiles.

2.1 Vision, Mission and Values

The Township's vision, mission and values statement provide the context for this Official Community Plan and guides the decision making of Council and staff in the provision and delivery of works and services.

Our Vision

Esquimalt is a caring and welcoming community that has strong relationships with our neighbours, the Esquimalt and Songhees Nations, regional municipalities, and the naval base.

Our vibrant economy is supported by an engaged community.

As a coastal community, we are stewards of our environment and value our past, present and future.

Our Mission

Focusing on community priorities, the Township of Esquimalt works to make our community and environment a better place for today and the future.

Our Values

- **Accountability** – we acknowledge and take responsibility for our actions, decisions and policies.
- **Integrity** – we practice high standards of ethical behaviour and open communication that inspire trust.
- **Respect** – we foster an environment of fairness where people are valued and treated with dignity.
- **Service** – we strive to meet community needs and achieve high-quality results through teamwork, innovation and creativity.

2.2 Community Profile

This profile draws on data from the 2016 Canada Census.

Population and Age Profile

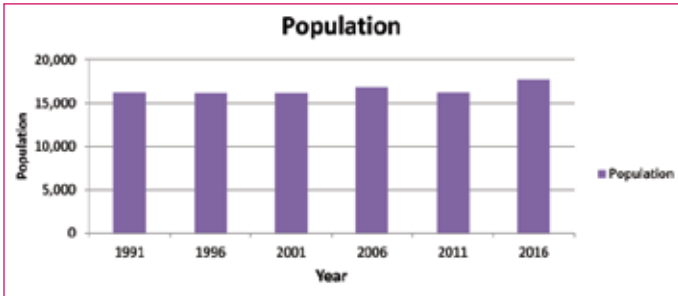


Figure 1. In 2016, Esquimalt had a population of 17,655, an increase of 8.9 percent from its 2011 population of 16,209.

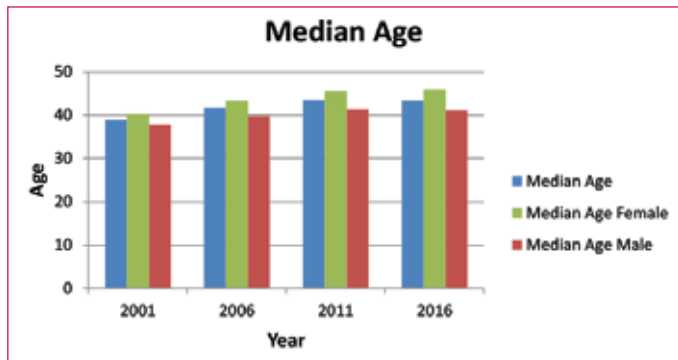


Figure 2. Esquimalt is experiencing an aging population. The median age in 2016 was 43.4, up 4.5 years from 15 years earlier in 2001 (Federal censuses for 2001 until 2016)

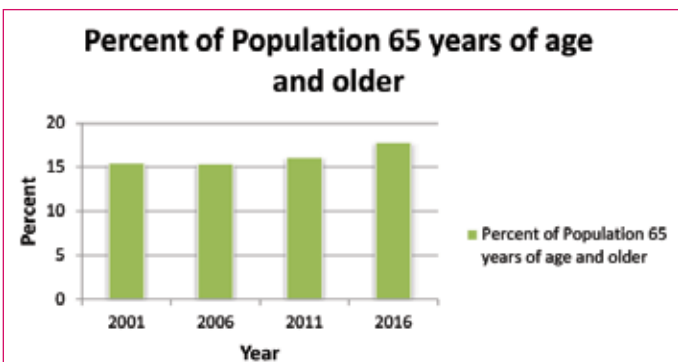


Figure 3. Esquimalt's aging population is also demonstrated by the increasing percentage of residents over 65 years of age.



2.3 Regional Profile

Esquimalt, with a population of approximately 17,655, is one of four “core municipalities” within the Capital Region. The municipality has one of the highest ratios of jobs to residents. CFB Esquimalt, located primarily in Esquimalt, is one of the region’s largest employers.

Through the Capital Regional District, residents of Esquimalt have access to services such as regional parks, recycling, an affordable housing portfolio, water supply and waste disposal systems.

2.4 Planning Context

The area that forms the Township of Esquimalt was settled by two First Nations — the Esquimalt and the Songhees. Members of these Nations reside on lands adjacent to Esquimalt’s north-western boundary.

The name Esquimalt reflects its maritime setting. In the language of the native Coast Salish peoples, Es-woy-malth means “place of shoaling waters”.

Looking into the foreseeable future, Esquimalt is likely to become a focus of renewed growth and redevelopment. The reasons for this include:

- Proximity and easy access to the main employment areas in the Capital Region;
- Established and attractive community amenities — schools, parks, trails, services, shopping;
- More than ten kilometres of saltwater shoreline and expansive water and mountain views from higher elevations;
- Affordable housing in comparison with many other areas of Greater Victoria, both rental and home ownership;
- Significant areas for potential redevelopment, particularly areas that may be surplus to the Department of National Defence. (Almost 30 percent of Esquimalt’s land base is under the jurisdiction of the federal government); and
- Mature residential character areas and many features of significant heritage interest.



3

LAND USE PLANNING



3.1 Land Use Designations

The existing land uses and development patterns are shown on the “Present Land Use Designation Map”. The “Present Land Use Designation Map provides a framework for the detailed regulations contained in the Zoning Bylaw. Future land use, development and redevelopment will be in accordance with the land use designations shown on the “Proposed Land Use Designation Map”, together with the guidelines set out in Development Permit Areas. Owners of land may choose to develop or redevelop their land in accordance with either the “Present Land Use Designation Map” or the “Future Land Use Designation Map” as long as the zoning of the land allows for the proposed development or redevelopment.

3.2 General Development

3.2.1 Goal: To knit together urban pattern, use, and form into a cohesive land use fabric that enhances community and protects the environment.

Policy

To consider and implement (as appropriate) the full breadth of statutory planning tools under the Local Government Act and Community Charter, as well as any other authority or agreements, to further enhance development within Esquimalt, including to achieve high quality design for the community.

Policy:

Encourage development proponents to demonstrate how the design of a new development addresses crime prevention principles.

Policy:

Encourage universal design in all new developments.

Policy:

Encourage owners to locate new buildings in order to retain protected trees, as well as healthy and mature trees, existing on the property.

Policy:

Encourage new developments that are sited and designed to maintain or enhance public view corridors. Public view corridors are shown on the “Public Views Corridors Map”.

3.3 Temporary Use Permits

3.3.1 Objective: To allow for the temporary use of a parcel of land for a use that is not permitted on the parcel under current zoning.

Policy

The entire Township is designated as an area where temporary uses may be allowed.

Policy

Temporary Use Permits may be approved and issued for residential, commercial, industrial, recreational, public utility, agricultural, and institutional uses, including any combination of the foregoing uses, that do not conform to the permitted uses for a parcel as prescribed in the Zoning Bylaw.

Policy

Temporary Use Permits may be issued for a period in accordance with the Local Government Act, presently of up to three-years and may be renewed for up to an additional three-year period.

Policy

In considering an application for a Temporary Use Permit, Council may consider the following:

- i. Suitability of the site for the proposed use;
- ii. Compatibility with adjacent land uses;
- iii. Potential environmental impacts, and
- iv. Potential economic impacts.

Policy

Temporary Use Permits may be issued with conditions, including with respect to the following:

- i. Requirement for security deposits in order to ensure removal of the use and related buildings and structures on termination or expiry of the permit, rectification of unsafe conditions and/or damage to the environment as a result of the temporary use and/or compliance with any conditions associated with the approval and issuance of a Temporary Use Permit;
- ii. Restrictions on duration and timing of use;
- iii. Restrictions on the intensity of use;
- iv. Provision of adequate parking;
- v. Provision of landscaping and buffering;
- vi. Provisions related to the structures and buildings associated with the temporary use;

- vii. Provisions for traffic management;
- viii. Provisions for control of nuisances including but not limited to: noise, dust, fumes, odour, glare, vibration, and light; and
- ix. To allow for the better integration of the temporary use with the surrounding neighbourhood and the community of Esquimalt as a whole.

3.4 Development Approval Information Area

3.4.1 Background

Pursuant to Section 485 of the Local Government Act, Statutes of BC, an Official Community Plan may do one or more of the following:

- Specify circumstance in which development approval information may be required;
- Designate areas for which development approval information may be required;
- Designate areas for which, in specified circumstances, development approval information may be required.

The purpose of Development Approval Information is to provide information on the anticipated impact of a proposed activity or development on the community, including, without limiting this, information regarding impact on such matter as the following:

- Transportation patterns including traffic flow;
- Local infrastructure;
- Public facilities including schools and parks;
- Community services; and
- The natural environment of the area affected.

3.4.2 Designated Area

The entire area within the Township of Esquimalt municipal boundary is designated as a Development Approval Information Area.

3.4.3 Justification – Special Conditions and Objectives

As outlined throughout this Plan, there are many special conditions and features of the Township to justify the designation of the entire Township for which development approval information may be required. These include but are not limited to the following:

- The Township is surrounded by sensitive shoreline with high ecological values and biological diversity, abuts a National Migratory Bird Sanctuary, is located in a rare Garry Oak Ecosystem, is a home of rare plant and animal species, and has other important environmental values and considerations.
- On September 1st, 2012, Historic Esquimalt celebrated 100 years as an incorporated Township.

First Nations people of the Coast Salish linguistic group had used the area now known as Esquimalt for approximately 4000 years before the advent of European settlement. Cultural and Heritage resources are a significant component of the Township's history and urban fabric, with many examples of high aesthetic values and other significance.

- Esquimalt possesses one of the finest natural harbours on the west coast of the Americas, and is the headquarters of the Royal Navy's Pacific Squadron, complete with graving dock and ship-repair and fitting.
- The Township is largely developed and/or urbanized with few (if any) greenfield development sites, and undergoing significant redevelopment and renewal. Infrastructure is, in some cases, old and assessing and allocating capacity for new development and redevelopment can be challenging without appropriate assessment information including with respect to highways, fire protection systems, water supply systems, sewage systems, solid waste disposal and recycling facilities, energy and communications utilities, and local parking facilities. Municipal infrastructure represents a fundamental component of the efficient and effective functioning of the Township.
- The Township has numerous local facilities, including schools, parks, health care services, local transportation services including public transit, that may be impacted by medium and high density residential development.
- The Township is located in one of the most earthquake prone areas in Canada, and must contend with tsunami, land slip, erosion and other natural hazardous conditions.

In addition to the above, Township objectives include the following:

- To protect the ecological integrity of Esquimalt's shorelines.
- To protect the habitat of Migratory Birds from disturbances related to development.
- To protect the ecological integrity of Esquimalt's remaining Garry Oak Ecosystem.
- To conserve energy and water.
- To protect existing municipal infrastructure and properly plan for future infrastructure needs.
- To understand, mitigate and otherwise plan for potential impacts of traffic generated by proposed developments and redevelopments on the Township's road system, as understanding potential traffic impacts of development that involves significant vehicle traffic is crucial to making sound land use and development decisions.
- To ensure that existing local facilities can adequately service new residents and properly plan for future service needs.
- To protect the integrity of the Township's economy, which is critical to the well being of its residents.
- To protect the integrity of the Township's tax base, as that base is fundamental to ensuring that the Township can provide the necessary services and infrastructure to its residents.
- To plan for earthquake preparedness and resiliency, including to preserve life and reduce property damage.
- To avoid approving land uses and developments that may negatively impact near-by developments or land uses or may be negatively impacted by existing land uses or developments, considering that nuisances such as noise, dust, glare, and odour can negatively impact the quality of life for the Township's residents and business owners.

- To protect, where feasible, the Township’s significant cultural and heritage resources.
- To maintain a high quality built environment, including aesthetic values, as this contributes to the vibrancy of the community.
- To facilitate the implementation of Development Permit purposes and guidelines.

In accordance with the Local Government Act, the Township identifies additional policies and procedures in a separate bylaw for the implementation of Development Approval Information requirements.

3.5 Sand and Gravel

There are no lands within the Township suitable for sand and gravel extraction.

3.6 Restriction on Land that is Subject to Hazardous Conditions

All lands abutting any body of water connected to the Pacific Ocean are in a potential Tsunami Hazard Area. Prior to rezoning, subdividing, or developing these lands a landowner may be required to submit a Tsunami Report with their application.



4

HOUSING & RESIDENTIAL LAND USE



4.1 General: Anticipated Housing Needs in the Next Five Years

Table 1 and Figure 4 show the housing statistics for Esquimalt based on the 2011 and 2016 Federal Censuses. Note that there are significant differences in the trends for each housing type. Single detached houses decreased by 1.7 percent while apartments in buildings that have five or more stories increased by 23.1 percent. This data in the following table clearly shows a trend toward multi family dwellings and away from single detached dwellings.

TABLE 1. PERCENT CHANGE IN HOUSING TYPES BETWEEN 2011 AND 2016.

DWELLING TYPE	NUMBER OF DWELLING UNITS - 2011 CENSUS	NUMBER OF DWELLING UNITS - 2016 CENSUS	PERCENT CHANGE 2011 TO 2016
SINGLE DETACHED HOUSE	2030	1995	-1.7
APARTMENT IN BUILDING THAT HAS FIVE OR MORE STORIES	455	560	23.1
SEMI-DETACHED HOUSE	665	690	3.8
ROW HOUSE	385	405	5.2
APARTMENT OR FLAT IN DUPLEX	795	910	14.5
APARTMENT IN A BUILDING THAT HAS FEWER THAN FIVE STOREYS	3695	3910	5.8
OTHER SINGLE - ATTACHED HOUSE	15	15	0.0
TOTAL	8040	8485	5.5

Table 2 contains simple arithmetic population projections estimates based on various growth rate scenarios. The net increase in population under each scenario is then used to estimate the number of new housing units will be required in the next five years based on an average household size of 2.0 as per the 2016 Federal Census.

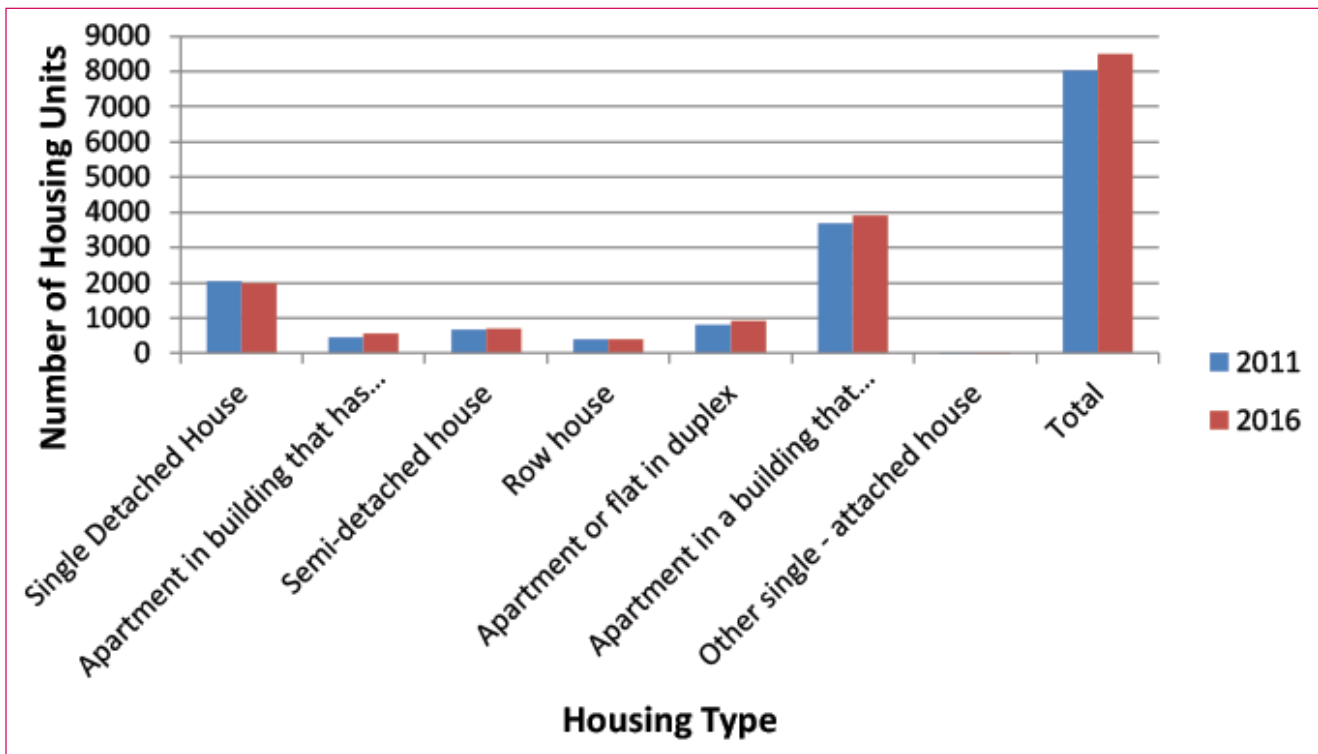


Figure 4. Percent change in housing types between 2011 and 2016

TABLE 2 POPULATION GROWTH SCENARIOS FOR THE NEXT FIVE YEARS AND ESTIMATED NUMBER OF NEW HOUSING UNITS REQUIRED.

2016 POPULATION	RATE OF GROWTH OVER FIVE YEARS (%)	ESTIMATED 2021 POPULATION	INCREASE IN POPULATION	ESTIMATED NUMBER OF NEW HOUSING UNITS REQUIRED AT AN AVERAGE OF 2.0 PEOPLE PER UNIT
17,665	2.5	18107	442	221
17,665	5	18548	883	442
17,665	7.5	18990	1325	662
17,665	10	19432	1767	883

Table 3 summarizes the number of units in all developments where Council has approved the zoning but the developments have not received an occupancy permit. The data in the table indicates that 664 housing units of various forms and tenures have been approved but have not yet receive an occupancy permit. The current approvals would be sufficient to accommodate a population increase of approximately 7.5 percent over the next five years based on an average household size of 2.0. Given the amount of land designated for multi-unit development as indicated in the Future Land Use Designation Map that is not yet yielding it maximum potential number of dwelling units, the Township has more than adequate land to accommodate its housing needs for the next five years at growth rates beyond 10 percent.

TABLE 3 ESTIMATED NUMBER OF HOUSING UNITS THAT HAVE RECEIVED ZONING APPROVAL BUT NOT AN OCCUPANCY PERMIT.

DWELLING TYPE	LOCATION	NUMBER OF PROPOSED UNITS - RENTAL	NUMBER OF PROPOSED UNITS - OWNER	DENSITY (UNITS PER NET HA.)	STATUS
SINGLE DETACHED HOUSE (INFILL)	VARIOUS (INFILL)		7	21.6	
APARTMENT IN BUILDING THAT HAS FIVE OR MORE STORIES	ESQUIMALT TOWN SQUARE	34	68	130	DEVELOPMENT PERMIT APPROVED
	LEGION	152		415	ZONING APPROVED
	ENGLISH INN		173	165	DEVELOPMENT PERMIT APPROVED
	856 ESQUIMALT ROAD		168	230	ZONING APPROVED
SEMI-DETACHED HOUSE	VARIOUS		4	29.5	ONGOING
ROW HOUSE	618 LAMPSON		12	55	UNDER CONSTRUCTION
	ENGLISH INN		6	40	DEVELOPMENT PERMIT APPROVED
APARTMENT OR FLAT IN DUPLEX	VARIOUS				
APARTMENT IN A BUILDING THAT HAS FEWER THAN FIVE STOREYS	615 FERNHILL		10	130	ZONING APPROVED
	TRIANGLE LANDS			160	ZONING APPROVED
	826 ESQUIMALT ROAD		30	330	UNDER CONSTRUCTION
OTHER SINGLE - ATTACHED HOUSE	WEST BAY				
TOTAL		186	478		

4.1.1 Objective: Support expansion of housing types within Esquimalt while addressing concerns such as tree protection, parking, traffic, noise, effects on neighbouring properties, and neighbourhood character.

Policy:

Support the inclusion of secondary suites within present and proposed low density residential land use designated areas.

Policy:

Support the inclusion of detached accessory dwelling units on appropriate low density residential land use designated areas where only one principal dwelling unit exists.

Policy:

Consider new townhouse residential proposals with a Floor Area Ratio of up to 0.70, and up to 3 storeys in height, in areas designated Townhouse Residential on the “Proposed Land Use Designation Map”, provided the design responds effectively to both its site and surrounding land uses.

Policy:

Recognize that Floathomes provide an alternative housing choice, and support the continued existence of Esquimalt’s only Floathome community located in West Bay, as designated on the “Present Land Use Designation Map” and “Proposed Land Use Designation Map”.

Policy:

Support the inclusion of live/ work units on residential parcels located outside of areas designated commercial on the “Proposed Land Use Designation Map” and on Major Roads as designated on the “Road Network Map”.

Policy:

Recognize short-term accommodation use as appropriate within designated residential land use areas only where appropriate zoning has been sought and secured.

Policy:

Support the development of a variety of housing types and designs to meet the anticipated housing needs of residents. This may include non-market and market housing options that are designed to accommodate young and multi-generational families, the local workforce, as well as middle and high income households.

4.2 Low Density Residential Redevelopment

4.2.1 Objective: Strive for redevelopment and infill development that improves and enhances the appearance and livability of neighbourhoods and the community as a whole.

Policy:

Proposed redevelopment or infill within present low density residential land use designated areas should be built to high quality design and landscaping standards and respond sensitively to existing neighbourhood amenities.

Policy:

Consider the inclusion of secondary suites in infill developments where it is demonstrated that neighbourhood impacts can be mitigated.

Policy:

Discourage new applications for infill housing, including rezoning and subdivision, for panhandle lots in the 1100 and 1200 blocks of Old Esquimalt Road and the 600 block of Fernhill Road.

4.3 Medium and High Density Residential

4.3.1 Objective: Support compact, efficient medium density and high density residential development that integrates with existing and proposed adjacent uses.

Policy:

Encourage new medium density and high density residential development with high quality design standards for building and landscaping and which enhance existing neighbourhoods.

Policy:

Prioritize medium density and high density residential development in proposed land use designated areas that:

- i. reduce single occupancy vehicle use;
- ii. support transit service; or
- iii. is located within close proximity to employment centres.

Policy:

Consider new medium density residential development proposals with a Floor Area Ratio of up to 2.0, and up to 6 storeys in height, in areas designated on the "Proposed Land Use Designation Map".

Policy:

Consider new high density residential development proposals with a Floor Area Ratio of up to 3.0, and up to 12 storeys in height, in areas designated on the "Proposed Land Use Designation Map".

Policy:

Notwithstanding other policies set out in this OCP, maximum heights in medium density and high density residential land use designated areas in West Bay are limited to those parcel heights identified in the West Bay development permit area.

Policy:

Consider, where appropriate, development proposals with densities greater than those set out in the OCP through density bonus of floor-space provided that the additional density results in the provision of community amenities deemed appropriate by Council for the benefit of the community.

Policy:

Recognize, for the purposes of density bonuses, "amenities" may include but not limited to:

- i. Privately-owned, publicly-accessible open space;
- ii. Public art;
- iii. Contributions towards the enhancement of public recreation facilities;
- iv. Contributions towards street and boulevard enhancements, including street furniture and decorative lighting;
- v. Building to a higher step of the BC Energy Step Code than required under the Building Bylaw;
- vi. Group daycare and respite facilities for children and adults;
- vii. Preservation of heritage structures, features or assets;
- viii. Affordable housing units;
- ix. Special needs housing units; and

- x. Other as may be appropriate to the development proposal or surrounding community as deemed appropriate by Council

Policy:

A mix of dwelling unit sizes should be provided in medium density and high-rise residential land use designated areas in order to meet the varying housing needs of Esquimalt residents.

Policy:

Buildings with shallow setbacks must step down to no more than three storeys at street level in order to provide an appropriate human scale along the sidewalk.

Policy:

Encourage the installation of electric vehicle charging infrastructure in medium and high density residential developments.

4.4 Affordable Housing

4.4.1. Objective: Encourage a range of housing by type, tenure and price so that people of all ages, household types, abilities and incomes have a diversity of housing choice in Esquimalt.

Policy:

Encourage the provision of affordable housing by the private market and the non-profit housing sector. Partnerships between private, public or non-market housing providers may be supported. These might include innovative approaches such as limited equity, rent-to-own, co-op, mixed market and non-market projects.

Policy:

Encourage the placement of new rental, affordable, special needs, and seniors housing in accordance with designated residential land use areas as they are integral components of inclusive neighbourhoods.

Policy:

Avoid the spatial concentration of affordable and special needs housing in neighbourhoods.

Policy:

Promote Housing Agreements to ensure that all strata units have the opportunity to be used as long-term residential rentals within strata buildings.

Policy:

Consider offering a Revitalization Tax Exemption to all new or renovated purpose-built multi-unit rental buildings within the Township.

Policy:

Consider bonus density, parking relaxations or other development variances where a development proposal includes affordable, special needs or seniors housing. This may apply to both market and non-market housing, and mixed-use proposals. A "housing agreement" may be entered into between the Township and the owner.

Policy:

Work in co-operation with the provincial and federal governments, the real estate community, social service agencies, faith-based organizations, service clubs and other community resources to provide affordable housing and support services for seniors, the frail elderly and other special needs groups.

Policy:

Encourage the provision of “missing middle” medium density housing types such as two-unit dwellings (duplexes), townhouses and small lot infill as one avenue to address housing affordability.

4.5 Age-friendly Housing

4.5.1 Objective: Expand and protect seniors housing in Esquimalt to enable citizens to “age in place”.

Policy:

Support and facilitate development of multi-generational housing, including in medium and high density residential developments.

Policy:

Encourage child friendly developments that provide appropriate amenities such as outdoor play areas for young children that are well-separated from traffic circulation and parking areas.

Policy:

Encourage adaptable design for all dwellings created through rezoning.

Policy:

Encourage more accessible housing for people with mobility limitations on the ground floor of medium and high density residential buildings.

Policy:

Support aging-at-home through means such as low or no fee permits for modifications.

Policy:

Encourage development of more assisted living residences.

Policy:

Promote the development of a complex care facility within Esquimalt.

Policy:

Promote modifying homes to enable shared housing for seniors.

Policy:

Encourage the development of seniors housing that is within close proximity and accessible to services and amenities.

Policy:

Recognize that the provision of bicycle storage may be waived or varied if, in the opinion of Council, there is no demonstrated need, such as in a congregate care facility.

5

COMMERCIAL & COMMERCIAL MIXED-USE LAND USE



5.1 General

The neighbourhood commercial mixed use and commercial/commercial mixed-use areas are designated on the “Present and Proposed Land Use Designation Maps”.

5.1.1 Objective: Esquimalt is a complete community where commercial enterprises serve the needs of area residents, local businesses, and visitors.

Policy:

Encourage a mix of uses in the commercial/commercial mixed-use areas.

Policy

Exclusive commercial development is appropriate in commercial/commercial mixed-use areas.

Policy:

All commercial/commercial mixed-use areas are designated Development Permit Areas, in order to ensure that future development and infill contributes positively to the visual and aesthetic character of its site, setting and surrounding properties.

Policy:

Lands outside the Commercial/Commercial Mixed-use designation on the “Proposed Land Use Designation Map” should not be considered for commercial zoning unless the following criteria are met:

- i. The project is desirable to the community and cannot be appropriately located within established commercial areas;
- ii. The density and scale of the project is sensitive to the prevailing character of surrounding lands;
- iii. The project, through its exterior finishes enhances the aesthetics of the neighbourhood;
- iv. The project’s parking requirements can be satisfied on-site; and
- v. The project’s parking requirements should not unduly affect neighbouring land uses.

Policy:

In commercial mixed-use areas, buildings with a floor area ratio of up to 3.0 for the residential portion of the building may be acceptable.

Policy:

Consider, where appropriate, development proposals with densities greater than those set out in the OCP through density bonus of floor-space provided that the additional density results in the provision of community amenities deemed appropriate by Council for the benefit of the community.

Policy:

Recognize, for the purposes of density bonuses, “amenities” may include but not limited to:

- Privately-owned, publicly-accessible open space;
- Public art;
- Contributions towards the enhancement of public recreation facilities;
- Contributions towards street and boulevard enhancements, including street furniture and decorative lighting;
- Building to a higher step of the BC Energy Step Code than required under the Building Bylaw;
- Group daycare and respite facilities for children and adults;
- Preservation of heritage structures, features or assets;
- Affordable housing units; and
- Special needs housing units.

Policy:

The majority of the frontage for commercial mixed-use buildings at the ground floor, should be commercial.

Policy:

On corner sites, the ground floor commercial use in mixed-use buildings should wrap around the corner.

Policy:

In commercial/commercial mixed-use developments, parking should be located behind or under the building.

Policy:

The installation of electric vehicle charging infrastructure in commercial/commercial mixed-use developments is encouraged.

5.2 Revitalization

5.2.1. Objective: To encourage growth through revitalization and redevelopment of commercial areas.

Policy:

Encourage public and private sector initiatives to improve streetscapes and accessibility for pedestrians and cyclists to all commercial areas.

Policy:

Design and install entrance signage at key street locations, including entrances to Esquimalt.

Policy:

Encourage the provision of amenities such as mini-parks/plazas, street furniture, public art and decorative lighting on private lands in all commercial areas.

Policy:

To encourage the use of bicycles, provision should be made in all commercial/commercial mixed-use developments for bicycle parking for employees and visitors.

Policy:

Encourage the provision of end of trip facilities for cyclists such as secure bicycle parking/storage, lockers, change rooms and showers, should be provided to encourage cycling as a viable form of transportation.

5.3 Esquimalt Town Centre

5.3.1 Objective: To foster the creation of an Esquimalt Town Centre that successfully integrates commercial, institutional and residential activity.

Policy:

Esquimalt Town Centre is located along Esquimalt Road, between Constance Avenue and Joffre Street.

Policy:

Esquimalt Town Centre will continue to be the principal commercial area within Esquimalt.

Policy:

Encourage a range of retail stores, services, professional offices, community services, recreation, institutional, educational, and arts / cultural uses that encourage activity throughout the day and evening.

Policy:

Encourage high quality development that supports and reinforces Esquimalt Town Centre as the main commercial, civic and recreational service centre. Designs are encouraged which incorporate a pedestrian orientation and include such features as sidewalk cafes, attractive landscaping, public art and other amenities.

Policy:

Encourage high density mixed-use commercial/residential complexes in Esquimalt Town Centre.

Policy:

Esquimalt Town Centre should continue to provide opportunities for Esquimalt’s young people, including places for activities and programs.

Policy:

Improvements to the Esquimalt Road frontage within Esquimalt Town Centre should encourage pedestrian use and facilitate connections to nearby public facilities and open spaces.

Policy:

Create a clearly defined, well-landscaped, and well-lit pedestrian network throughout the Esquimalt Town Centre.



5.4 Head Street and Esquimalt Road

5.4.1 Objective: To create a vibrant commercial mixed-use node centred around the intersection of Esquimalt Road and Head Street.

Policy:

Esquimalt’s secondary commercial area is located along Esquimalt Road at Head Street.

Policy:

Encourage redevelopment in the Head Street/Esquimalt Road commercial/commercial mixed-use area.

Policy:

The commercial/commercial mixed-use policies for Esquimalt Town Centre also apply to the Head Street/Esquimalt Road area.

5.5 Neighbourhood Commercial Mixed-Use

5.5.1 Objective: To encourage the development of a series of neighbourhood commercial mixed-use nodes throughout Esquimalt that will primarily serve the daily needs of the surrounding neighbourhood but may also include destination uses.

Policy:

The Neighbourhood Commercial Mixed-Use areas are shown on the “Present and Proposed Land Use Designation Maps”.

Policy:

In Neighbourhood Commercial Mixed-Use areas, buildings with a floor area ratio of up to 1.5 for the residential portion of the building may be acceptable.

Policy:

Consider, where appropriate, development proposals with densities greater than those set out in the OCP through density bonus of floor-space provided that the additional density results in the provision of community amenities deemed appropriate by Council for the benefit of the community.

Policy:

Recognize, for the purposes of density bonuses, “amenities” may include but not limited to:

- Privately-owned, publicly-accessible open space;
- Public art;
- Contributions towards the enhancement of public recreation facilities;
- Contributions towards street and boulevard enhancements, including street furniture and decorative lighting;
- Building to a higher step of the BC Energy Step Code than required under the Building Bylaw;
- Group daycare and respite facilities for children and adults;
- Preservation of heritage structures, features or assets;
- Affordable housing units; and
- Special needs housing units.

Neighbourhood Commercial Mixed-Use: West Bay

Objective: West Bay neighbourhood is that it is a thriving, attractive and walkable harbour front neighbourhood. Its unique identity and sense of place is shaped by the natural and human history of its marine environment. A diverse mix of local, pedestrian-oriented shops, businesses and housing, together with active and attractive streets, harbour front recreation, and sunny open spaces with strong connections to the harbour, make this a truly livable neighbourhood and a popular destination for locals and visitors. The built, historical and natural environments are sensitively integrated to ensure a healthy shoreline and upland ecology.

Policy:

Encourage the West Bay commercial area be oriented to pedestrians and boat traffic.

Policy:

Notwithstanding other policies set out in this OCP, maximum heights in Neighbourhood Commercial/Neighbourhood Commercial Mixed-Use land use designated areas in West Bay are limited to those parcel heights identified in the West Bay development permit area.

Neighbourhood Commercial Mixed-Use: Tillicum Road/Craigflower Road

5.5.3 Objective: To encourage the development of new services that meet the needs of the residents of the Panhandle, Gorge Vale Golf Course and the area east of Tillicum Road.

Policy:

As the nearby residential areas intensify and population grows, redevelopment of this commercial area is encouraged as a mix of small shops and services, potentially with residential above.

Mixed-Use: English Inn

The English Inn consists of a 1.8-hectare parcel of land as shown on the “Land Use Designation Maps”. The English Inn Mixed-Use land use designation is a variation of the Commercial/Commercial Mixed-Use designation intended to achieve heritage protection of the significant features and trees on the property and the main building.

5.5.4. Objective: To create a vibrant mixed use development that integrates within the existing neighbourhood.

Policy:

The English Inn Mixed-Use designation permits and encourages a mixture of commercial (tourist accommodation), residential and institutional uses (congregate care senior citizens apartments and similar), but it also allows the possibility of only residential uses.

Policy:

Subdivision, covenants and variances to parking and zoning regulations may be considered to further facilitate protection and maintenance of the main Maclure House building concomitant with redevelopment of the remainder of the property.

Policy:

Given the land contours and existing trees and vegetation, the uses in the main heritage building itself may require some parking spaces to be provided on adjacent lands if/when the original parcel is subdivided.

6

INDUSTRIAL LAND USE



6.1 General

The industrial lands are identified on the present and proposed land use maps. The lands are very valuable as employment and wealth generating lands and should be protected as such.

Lands designated Industrial on the “Present and Proposed Land Use Designation Maps” will be used for industrial and accessory uses.

All industrial areas are designated a Development Permit Area.

6.2 Esquimalt Industrial Park

6.2.1 Objective: To encourage the maintenance and viability of an industrial sector within Esquimalt to provide for local employment and a diversified tax base.

Policy:

Consider innovative proposals that intensify the light industrial uses.

Policy:

Industrial uses associated with clean technology are strongly encouraged to locate in the industrial park in order to take advantage of the proximity to the regions high tech sector.

Policy:

Industrial uses associated with ship repair and fitting are strongly encouraged to locate in the industrial park in order to take advantage of its proximity to the federal graving dock.

Policy:

Support public and private initiatives that enhance the aesthetics and identity of the Esquimalt Industrial Park.

Policy:

Encourage the use of bicycles in new industrial developments with the provision of secure bicycle parking and shower facilities for employees and visitors.

Policy:

Reductions in off-street parking requirements may be considered in new industrial developments

where the following are provided:

- i. an appropriate number of secure bicycle storage spaces;
- ii. shower and change rooms;
- iii. visitor bicycle parking spaces; and
- iv. the building being located within short walking distance of a regional bus route.

6.3 Public Services and Procurement Canada Land

6.3.1 Objective: To support the economic viability of the graving dock through the provision of housing for its workforce and the provision of land in the industrial park for allied industries.

Policy:

Encourage the continued use of the Public Services and Procurement Canada's drydock for industrial purposes.

Broad Objective: In the event that Public Services and Procurement Canada lands are disposed of or privatized, the lands will remain industrial and be subject to local government bylaws and development permit guidelines. Until such time, the Township respects the Federal prerogative to use its lands for Federal purposes and respectfully requests the Government of Canada and its departments and agencies to respect the Official Community Plan if the property is not used for Federal purposes.

6.4 McLoughlin Point Special Use

The Capital Regional District included McLoughlin Point in the regional Liquid Waste Management Plan for purposes of a sewage treatment plant, a public facility as shown on Schedule F . This Plan recognizes the provincial prerogative under the Environmental Management Act, and seeks to facilitate a variety of high technology, industrial, institutional, business and professional mixed-uses of impeccable design and cohesive operation, preferably with public access primarily for educational purposes, to showcase the development and proposed sewage treatment plant to the community and world, particularly given the prominence of McLoughlin Point as the entrance to Victoria Harbour.

6.4.1 Objective: To encourage the development of the site for a variety of uses in a manner that reflects its prominent location and the ecology of the area.

Policy:

At the time of redevelopment for other uses that do not include the sewage treatment plant, any plan should be prepared in consideration of the surrounding DND lands, and to alternative uses for this site that complement and support a mixed-use (including high-tech, clean industrial) redevelopment plan of impeccable design with public access.



7

INSTITUTIONAL & PUBLIC FACILITIES LAND USE



7.1 General

Institutional lands are identified on the present and proposed land use designation maps.

7.1.1 Objective: To encourage the retention of Esquimalt’s civic, faith-based and educational institutions as important components of the social fabric of the community.

Policy:

Institutional uses are generally considered to be appropriate within any land use designation and need not be specifically designated on the “Proposed Land Use Designation Map”. However, any proposal for new institutional zoning will be reviewed in detail and assessed on its own merits.

7.2 Federal Land

Federal Land: General

7.2.1 Objective: To ensure that any divested federal lands are subject to appropriate land use planning prior to development.

Policy:

Future land use on any lands divested by the federal government will be consistent with the general goals and objectives of this or any future Official Community Plan

Policy:

A comprehensive area redevelopment plan will be developed for any lands divested by the federal government. The plan is to be sympathetic to adjacent neighbourhoods and respectful of the heritage assets and values of the site.

Federal Land: CFB Esquimalt – Work Point

7.2.2 Objective: To ensure that any divested federal lands in the Work Point area are appropriately integrated into adjacent neighbourhoods and compatible with nearby land uses.

Policy:

If divested by the Federal government, it is envisaged that parts of the CFB Esquimalt - Work Point area could be redeveloped to a mix of townhouse and medium density residential developments surrounded by greenspace.

Policy:

Continuous public waterfront access linking Macaulay with the West Bay Walkway and downtown Victoria will be a key planning principle in any future concept plan.

Federal Land: CFB Esquimalt – North Naden

7.2.3 Objective: To ensure that any divested federal lands in the North Naden area are appropriately integrated into adjacent neighbourhoods and compatible with nearby land uses.

Policy:

If the CFB Esquimalt – North Naden area is developed, natural greenspaces and a local neighbourhood park should be negotiated as part of any development proposal.

7.3 Public and Private Service Facilities

There is a range of public and private service-oriented institutional facilities, such as health services, schools and child care facilities, seniors and youth services and emergency services, library services and places of worship and civic uses that contribute to the amenities and social fabric of Esquimalt. Established public facilities are shown on the “Public Parks and Facilities Map”.

7.3.1 Objective: To encourage public and private service facilities that serve the needs of residents to locate in Esquimalt.

Policy:

Encourage health services and wellness-related businesses to locate in Esquimalt.

Policy:

Encourage a senior-friendly community environment, including medical and support services, pedestrian system, public transit service and facilities, and recreational opportunities that are designed with accessibility in-mind.

Policy:

Recognize the value of maintaining the Esquimalt Municipal Hall as the primary emergency operations centre and Esquimalt Public Works Yard as a secondary site.

Policy:

Recognize the important role the library plays in the community to provide education and learning opportunities.

Policy:

Support the development of library services within the community as needed.

Policy:

Recognize the valuable role places of worship play in the community and continues to support

the development of facilities in accordance with local community needs.

Policy:

Encourage multiple uses in places of worship, including but not limited to, daycares, emergency centres, non-profit organizational and assembly uses.

Policy:

A regional wastewater treatment facility at McLoughlin Point is supported. Sludge treatment in proximity to residential uses is strongly discouraged, as an affront to an established residential community, nearby businesses and schools, due to the potential negative health, safety, risk, nuisance and other considerations

7.4 Schools

Established schools are shown on the “Public Parks and Facilities Map”.

7.4.1 Objective: To encourage the integration of schools into the community.

Policy:

During the next five years it is not anticipated that additional school sites will be needed by School District No. 61 within Esquimalt. Esquimalt would support an expansion of School District No. 61 school facilities if a need were identified in the future.

Policy:

Support opportunities for L’Ecole Brodeur to expand their school facility in Esquimalt.

Policy:

Recognize L’Ecole Brodeur as a centre for the francophone community in Esquimalt and the region.

Policy:

Support multiple uses in schools, including but not limited to, daycares, emergency centres, non-profit organizational and assembly uses.

Policy:

Consider partnering with local schools to provide improved and new community facilities such as sports fields and theatre space.

Policy:

Encourage walking and cycling by improving the safety of walking and cycling routes to schools.

8 PARKS, RECREATION & TRAILS



The delivery of Parks and Recreation services in Esquimalt will aim to provide benefits, either direct or indirect, to all citizens in the community. There will be a strong focus on promoting health and wellness, arts, culture and preserving and enhancing the Township’s parks, open spaces and public facilities. Present and proposed public parks and facilities are shown on the “Public Parks and Facilities Map”.

8.1 Parks and Recreational Land Use

Parks and recreational facilities are shown on Schedule F.

8.2 Parks

8.2.1 Objective: Aim to balance the multiple purposes and uses of parks, such as cultural events, recreation, sports, ecosystem services, commemoration and aesthetic enjoyment, so that they contribute positively to the quality of life of residents.

Policy:

Continue to strategically manage and enhance our urban forest, creating a thriving and sustainable mix of tree species and ages, resulting in a healthy ecosystem that is valued and cared for by the Township and its residents.

Policy:

Continue to develop a network of parks and open spaces meets Township wide and local area user needs.

Policy:

Develop a well-connected, clearly identifiable, and multi-functional network of greenways.

Policy:

Include universal accessibility principles in the construction of new park facilities.

Policy:

Investigate the feasibility of artificial turf playing fields in Esquimalt.

Policy:

Actively plan for the integration of passive recreational opportunities throughout the urban forest in the public realm.

Policy:

Develop plans for infrastructure upgrades to enhance access and public use at Saxe Point Park.

Policy:

Develop plans for new facilities and park infrastructure at Esquimalt Gorge Park.

Policy:

Balance recreational use of the waterfront with the protection and enhancement of areas of natural foreshore habitat for fish, birds and other wildlife.

Policy:

Protect the sensitive habitat found in the Gorge Park Estuary.

8.3 Recreation

Facilities

8.3.1 Objective: That recreation and wellness services, programs, spaces and places meet diverse community needs

Policy:

Support existing and future demographics by incorporating flexibility into the design of recreation, wellness, and cultural facilities.

Policy:

Design new spaces to: eliminate physical access barriers, encourage programmed and unstructured activity, support flexibility of use and that space can be altered at a later date to accommodate changing demands from users.

Active Living

8.3.2 Objective: Encourage people to make healthy, active lifestyle choices throughout their lives.

Policy:

Develop facilities so that children and youth have an opportunity to build healthy habits through physical activity – both through unstructured play and organized activities and programs.

Policy:

Reduce design barriers to living a physically active life.

Policy:

Utilize design solutions to support active lifestyles for aging populations and programs.

Policy:

Design the built environment to support and promote opportunities for people of all ages to be

active for life and achieve one's highest potential.

Community Connections

8.3.3 Objective: Provide opportunities to connect with neighbors, feel a sense of belonging, and engage in lifelong learning

Policy:

Promote opportunities for people to connect with the physical environment through the design of parks and public spaces.

Policy:

Continue to maintain and enhance our multi-use trail network to provide connections and access to waterfront areas, parks and open space, regional trails and residential neighbourhoods.

Policy:

Create a network of trail and sidewalk infrastructure connecting Saxe Point Park to Esquimalt Gorge Park.

Partnerships

8.3.4 Objective: Collaborate with others so that there is a coordinated efficient response to quality of life, recreation, and community wellness needs.

Policy:

Support the co-planning and co-delivery of sport hosting, festivals, community and neighborhood events within the Township.

Policy:

In consultation with stakeholders and residents, guide the future planning of recreation and wellness facilities to meet community needs.

8.4 Trails and Waterfront Walkways

8.4.1 Objective: Increase public waterfront access throughout the Township while protecting our vital ecosystem.

Policy:

If a subdivision of land adjacent to a waterfront requires the dedication of parkland, the Township will require the dedication of land adjacent to the waterfront.

Policy:

Enhance physical and visual public access to our marine waterfront throughout the Township while minimizing disturbance to waterfront vegetation and environmental features.

Policy:

Consider construction of a networked waterfront trail along the Gorge Waterway from Admirals

Road to Tillicum Road.

Policy:

Endeavour to obtain right-of-way agreements with landowners to allow for public access to the waterfront

Policy:

Waterfront lands will be considered in future park land acquisition.

Policy:

Maintain or create, as the case may be, waterfront access in the design of waterfront development where possible.

Policy:

Enhance public access points to the Gorge Waterway.

Policy:

Investigate marine access facilities for recreational users including kayaks, rowboats, kite surfers, paddle boarders, canoes and other watersports.

Policy:

As opportunities arise, the Township will seek to develop a continuous walkway around West Bay and connecting to Head Street.



9 TRANSPORTATION



Esquimalt's transportation system provides the means that allows residents and visitors to move between their homes, places of work, commercial and recreation activities. It connects the community with itself and other communities.

9.1 Walking

Walking is an alternative mode of transportation that shares the road corridor cross section with motor vehicle and cycling traffic.

9.1.1 Objective: The pedestrian network should be developed into the road corridor infrastructure that provides a safe, enjoyable and continuous network to promote its use.

Pedestrians

Policy:

Encourage the inclusion of pathways and laneways that promote safe pedestrian travel, in new private developments and in the public realm.

Policy:

Improve existing sidewalks, street furniture, crosswalks and other street amenities to make walking a safer and more enjoyable choice for people of all ages and abilities.

Policy:

Develop pedestrian movement plans that will encourage pedestrians to use transit from their homes and major facilities in a manner that encourages this movement.

Policy:

Planting of trees along the public boulevards will reinforce the role and value of sidewalks.

9.2 Cycling

Cycling is an alternative mode of transportation that shares the road cross section with motor vehicles and pedestrians.

Public Cycling Infrastructure

Objective: To encourage increased use of cycling for recreational and commuting purposes.

Policy:

Consider providing a variety of end of trip facilities by transit stops, parks and institutional facilities.

Policy:

Where feasible and supported by reliable data - enhance infrastructure to encourage cycling as a healthy form of transportation.

Policy:

Consider best management practices to provide guidance on decisions regarding the expansion and improvement of Esquimalt's cycling infrastructure.

New Development

9.2.1 Objective: To encourage the inclusion of bicycle facilities in new developments.

Policy:

Encourage developers to provide a variety of end of trip facilities for active transportation.

Policy:

Encourage bike lockers in multi-unit residential and commercial/commercial mixed-use developments.

9.3 Public Transit

9.3.1 Objective: To encourage transit orientated development that both takes advantage of the transit system and increases use of the transit system.

Policy:

Strive to increase the number of shelters and street furniture to encourage the use of public transit.

Policy:

Encourage the incorporation of transit shelters into new developments along transit routes.

Policy:

Encourage the provision of handyDART accessible transit service for people with disabilities.

Policy:

Support densification along frequent and regional transit routes.



9.4 E & N Rail Corridor

The Esquimalt and Nanaimo (E & N) Railway is a railway between Victoria and Courtney. In its past life has seen the corridor utilized to a great degree. With additional transportation corridors and mechanism this usage has declined. The Island Corridor Foundation (ICF) has been assigned control of the right-of-way.

9.4.1 Objective: To encourage multi-use of the E & N rail right-of-way.

Policy:

Support the use of the rail line right-of-way for multi-modal transportation use and technology.

Policy:

Continue to support further enhancements to the multi-use of the rail line right-of-way as part of the regional system to accommodate walking and cycling.

Policy:

Where appropriate, consider residential densification in areas in close proximity to the E & N rail right-of-way.

9.5 Roads

Roads function is to provide a surface that allows for the safe and smooth movement of motor vehicles and bicycles from origin to destination. The major road network is shown on Schedule C. No new major phasing of extensions to the existing road system are planned.

9.5.1 Goal: The road network will be designed and maintained in order to provide a satisfactory level of service to all users.

Major Road System

9.5.1 Objective: To improve mobility and access for local and regional travel and reduce intrusion on local traffic roads.

Policy:

Admirals Road, Craigflower Road, Esquimalt Road, Lampson Street, and Tillicum Road, will continue to serve as the municipality's principal through-traffic routes. Through-traffic will be encouraged to use these routes rather than local streets.

Policy:

Work with DND and Shipyards/Graving Dock on developing a Transportation Demand Management Strategy that promotes alternatives to single occupancy vehicles .

Roadway Upgrades

9.5.1 Objective: To maintain road infrastructure to an acceptable level of service.

Policy:

Encourage multi-modal street design and accessibility for pedestrians, cyclists, transit users, goods and services providers, and motorists where supported by research based analysis.

Policy:

Recognize roads as a “community resource” benefiting all users. They increase opportunities for personal communication and interaction among residents and businesses of the community and are not simply conduits for motor vehicle travel.

Policy:

Pedestrians’ and cyclists’ needs and neighbourhood amenities will be taken into consideration in designing road improvements and road safety.

Policy:

When rebuilding or improving roads, consideration will be given to accommodating pedestrians, cyclists, and motorists in the interest of road safety and community needs.

Policy:

Consider development of a transportation management plan if a neighbourhood or corridor develops significant traffic issues. This plan will be neighbourhood or stakeholder driven and include their input in its formation.

9.6 Public Parking

9.6.1 Objective: To encourage onsite parking with all land use.

Policy:

New developments should meet the needs of the land use designation and bylaws to achieve onsite parking or have variances supported by a parking study.

Policy:

Where appropriate, consider using public parking areas for parking of bicycles and electric vehicles.



10 INFRASTRUCTURE



Within the Township a number of different types of infrastructure that exist and are maintained. There are three primary groups of providers of infrastructure. They are:

- The Township, responsible for:
 - Local sanitary sewer collection system
 - Storm water collection system
 - Streetlights
 - Traffic signals and signage
 - Bus shelters
- Other local governments, responsible for:
 - City of Victoria – local potable water distribution network
 - CRD – trunk potable water distribution network
 - CRD – sanitary sewer trunk collection system
 - CRD – Waste water treatment plant
- Other providers are either Crown corporations (electricity) or private service providers (gas, fibre optic cable, cable)

The various types of infrastructure are provided throughout the Township and are located within the road right of way, utility lanes or rights of way on private property, and on Federal property.

10.1 Sanitary Sewer Collection Systems

The sanitary sewer collection system collects and transports sanitary flows. It is an older system that has had upgrades having been performed on it periodically. The Township is actively making improvements to the system to address issues of cross connection of services and inflow/infiltration (II) that reduces the system's capacity and ability to manage flows. The capacity of the system is sufficient for today's needs but may require modification as densification of the land types occur in the future. The approximate location of the sanitary sewer system is shown on Schedule D. A new trunk main to

transport sludge from the sewage treatment plant to the Hartland Landfill is planned to start construction in 2018. The main will be built and owned by the Capital Regional District (CRD).

10.1.1 Objective: To provide a sanitary sewer collection system that can respond to the demands of growth and manage the system to minimize disruptions due to failure in the system.

Policy:

Monitor capacity of the collection system vis a vis the capacity to address the requirements of new development.

Policy:

Continue as a partner in the CRD Core Area Liquid Waste Management Plan to address issues of source control, watershed management, harbours health and the wastewater treatment plant.

10.2 Storm Water Collection System

The storm water collection system provides the means for the collection of storm water through the Township and transport the storm water to receiving water bodies. The Township is actively making improvements to the system to address issues of water quality and cross connections. The capacity of the system is sufficient for today's needs but may require modification as changing weather conditions evolve and become the norm.

10.2.1 Objective: To provide a storm water collection system that can respond to the requirements of the environment and manage the system to minimize disruptions due to failures in the system.

Policy:

Endeavour to provide a storm water collection system that has the capacity to service new development and anticipated climate change.

Policy:

Consider the review and development of best management practices related to storm water management to reduce the potential for pollution into the regions receiving water bodies.

Policy:

Strive for the reduction of soil, hydrocarbons, and other pollutants through the use of mechanical and natural based storm water treatment systems.

Policy:

Where appropriate, support rain water collection systems on private property for rain water harvesting as a strategy to deal with increased rainfall due to climate change.

Policy:

Encourage developers and home owners to use storm water management techniques to maximize the management and control of rainfall on their property in order to slow and treat storm water before it enters local waterways.

10.3 Street Lights

The Township owns the street light system with it being located on either stand-alone poles or BC Hydro poles. This system provides light to road right of way to assist in the movement of people, motor

vehicles, bicycles and goods thought the Township and the region.

10.3.1 Objective: To promote the use of energy efficient street lights that enhance public safety and street scape aesthetics.

Policy:

Incorporate best management practices for street lights.

Policy:

Street light design for new developments should consider pedestrian safety.

Policy:

Where required, street lights should incorporate cut-offs to prevent light spillage onto private property, waterways, and into the night sky.

Policy:

Where practical, consider the use of appropriately designed street light stanchions.

10.4 Regional Sanitary Collection and Distribution Systems (Potable Water)

The ownership and management of several collection and distribution systems are operated within the Township by the CRD and the City of Victoria. These systems deal with potable water and sanitary wastewater conveyance.

10.4.1 Objective: To work with owners of these systems to encourage the upgrading.

Policy:

Coordinate with partners in the planning and execution of projects on the collection and distribution systems in order to minimize the impact on the road right of ways and existing infrastructure.

10.5 Utilities Distribution Systems

The ownership and management of several distribution systems are operated within the Township by variety of private corporations or a crown corporation. These systems deal with telecommunications, fibre optics, gas and power.

10.5.1 Objective: To work with owners of the systems to encourage upgrading.

Policy:

Support coordination with utility owners for the planning and execution of projects on the various systems in order to minimize the impact on the road right of ways and existing infrastructure.

Policy:

Encourage that various utilities owners consider utilities be located underground for new developments and upgrade projects.

11 ENVIRONMENT, ENERGY & CLIMATE CHANGE



11.1 Natural Areas

11.1.1 Objective: To protect and enhance natural areas.

Policy:

Encourage the protection of natural areas when properties are developed or redeveloped.

11.2 Solid Waste Management

The collection and management of solid waste has evolved from a single stream activity to four major streams. The four streams are: solid waste, kitchen scraps, recycling and yard and garden waste. For solid waste and kitchen scraps the Township is responsible for the collection of these streams. The CRD manages the Hartland Landfill where they are processed and disposed of. Recycling is a region wide initiative that is managed by the CRD. Yard and garden waste is a joint venture that the Township and Town of View Royal which together operate the collection and disposal of this stream.

Landfill Waste

11.2.1 Objective: See the reduction in tonnage of the garbage waste stream.

Policy:

Work with CRD for the extension of the capacity of Hartland Landfill by the identification and implementation of alternative processing technologies.

Waste Reduction

11.2.1 Objective: See the increase in the tonnage associated with the recycling, kitchen scraps and yard and garden streams.

Policy:

Promote an increase in residents' usage of the recycling, kitchen scraps, and yard and garden streams through education.

Policy:

Work with the CRD for the identification and implementation of alternative processing technology for all three streams.

Policy:

Work with the CRD to increase the number of material types that can be recycled and reused.

Policy:

Encourage new and redeveloped properties to incorporate facilities to divert solid waste collection into the following streams: kitchen scraps, recycling, and landfill.

Policy:

Encourage the use of on-site composting through building and landscape design.

Policy:

Consider the use of integrated resource management technologies to reduce the volumes of waste transported to the Hartland Landfill and utilizing the by-products of such technology within the built environment.

11.3 Reduction of Greenhouse Gas Emissions

11.3.1 Objective: To continue to take action on stabilizing GHG concentrations in the atmosphere by reducing emissions.

Targets

- Take action to reduce community greenhouse gas emissions by at least 38% by 2030.
- Strive for the ultimate objective of carbon neutrality by realizing zero net community GHG emissions by 2050.

Actions

- Continue as a signatory to the Climate Action Charter and continue to make progress towards corporate carbon neutrality.
- Develop plans to reduce both corporate and community-wide energy consumption and greenhouse gas emissions.
- Encourage contractors who deliver municipal services to work towards reducing the energy use and greenhouse gas emissions associated with those operations.
- Where feasible, adopt fuel-efficient and low carbon vehicles and equipment in the municipal fleet.
- Implement a corporate green buildings policy that sets an energy efficiency standard for new corporate buildings and for improving the energy efficiency of existing buildings.
- Protect and enhance the carbon sequestration value of natural systems by enhancing the urban forest and protecting local soil.
- Report annually on corporate energy and emissions, as well as actions taken to reduce both corporate and community-wide energy consumption and greenhouse gas emissions.
- Identify an appropriate site for the installation of a public Level 3 fast charging station.

- Educate developers and home owners on the future need for shading and cooling of buildings in the summer.

Building Energy Efficiency

11.3.1 Objective: Reduce building emissions by increasing energy efficiency in new buildings

Policy:

Encourage the adoption of passive, efficient, and renewable energy systems in new buildings and during building retrofits.

Policy:

Investigate options for encouraging developers to achieve high energy performance in new developments through such tools as density bonusing, expedited permit approval process, rebate of development fees, revitalization tax exemption, and other incentives.

Policy:

Pursue higher energy-efficiency performance in new developments, through the achievement of higher steps in the BC Energy Step Code, as a condition of rezoning or amenity associated with rezoning.

Policy:

Encourage heritage preservation and the re-use of existing buildings where possible.

Low Carbon Transportation

11.3.1 Objective: Increase the number of trips made by walking, cycling, or transit.

Policy:

Prioritize walking, cycling and public transit as preferred modes of transportation in infrastructure improvements.

Policy:

Develop a multi-modal transportation plan to identify future policy and infrastructure changes to increase mode share in active transportation.

Policy:

Where feasible, plan for cycling-only routes, or cycling routes on less-travelled local roads with traffic-calming devices, which are proven to be the safest and most popular types of routes for cyclists.

Policy:

Where feasible, improve the continuity of the bike network by linking existing and future bike-ways and trails.

Policy:

Increase the amount and diversity of infrastructure available to cyclists, such as public bike racks and public bike repair stations.

Policy:

Invest in public bike parking in public parks and facilities, such as parks and beach access points, to encourage cycling.

Policy:

Increase the minimum requirements, and set design guidelines, for bicycle parking facilities in all new developments for residents, workers, and visitors.

Passenger Vehicle Alternatives

11.3.1 Objective: Reduce impact of motor vehicles that derive energy from fossil fuels by increasing capacity for alternative fuelling and car sharing.

Policy:

Investigate the potential for increasing the capacity for alternative fuelling in the Township, including but not limited to electric and bio-diesel.

Policy:

Encourage the installation of electric vehicle charging infrastructure in all new multi-unit developments.

Policy:

Pursue the installation of electric vehicle charging capacity in new developments during the rezoning process.

Policy:

Encourage the inclusion of car share in new multi-unit residential developments.

Policy:

Support the reservation of on-street parking for car share vehicles.

Advocacy and Partnerships

11.3.2 Objective: Strive to reduce energy and emissions through partnerships and advocacy.

Policy:

Seek partners in piloting innovative approaches and technologies to reduce energy consumption and greenhouse gas emissions.

Policy:

Collaborate with major employers on developing transportation demand management strategies for workers.

Policy:

Promote energy saving programs and initiatives administered by third parties.

11.4 Climate Change Adaptation

11.4.1 Objective: To prepare the community to be adaptable and resilient to the impacts of climate change.

Policy:

Consider including climate change projections, risks, and measures for dealing with more frequent and severe weather events in Township strategies, plans and policies.

Policy:

Consider information on climate change risk and vulnerabilities when evaluating new development proposals.



12 URBAN AGRICULTURE



12.1 Agricultural Land Use

Agricultural Land Reserve (ALR) are shown on the “Present and Proposed Land Use Designation Maps.”

12.1.1 Objective: To support the rational use of Agricultural Land Reserve

Policy:

Continue to support the existing golf course as a grand-fathered permitted use in agricultural land reserve lands.

Policy:

Capitalize on opportunities to develop land removed from the agricultural land reserve to provide medium density and high density residential developments, trails to improve pedestrian connectivity, and other community amenities.

12.2 Urban Food Production

12.2.1 Objective: Support the establishment of allotment gardens, community gardens, edible landscapes, food bearing trees and other types of food production activities.

Policy:

Allow for community gardens as a accessory use in all zones.

Policy:

Support expansion of community gardens on public, private and institutional lands.

Policy:

Encourage developers to consider the provision of garden space in commercial developments and in particular food services.

Policy:

Encourage developments in food production gardens in all multi-unit residential developments including rooftop gardens.

12.2.2 Objective: Promote local food production as a means to mitigate climate change and support food resiliency in an emergency.

Policy:

Reduce restrictions to encourage placement of food producing plants on private property.

Policy:

Support small scale greenhouses, aquaponics, and aquaculture as an accessory use on private property.

Policy:

Continue to support public launch at Fleming Beach to the marine environment for food harvesting.

Policy:

Support the development of commissary kitchens that provide space for small scale commercial food production.

Policy:

Support urban food production and related sales as a home based business.

Policy:

Support farmers markets in appropriate park, institutional, commercial, and industrial land use designated areas.



13 ARTS, CULTURE & HERITAGE



13.1 Arts and Culture

13.1.1 Objective: Encourage and develop a mosaic of appealing, lively and distinctive areas, vibrant public spaces, festivals, events and activities.

Policy:

Promote an animated public realm where people can gather, connect and celebrate through art friendly public spaces and facilities.

Policy:

Build on the strengths of Public Art so that it is a key element in shaping, animating and enriching the public realm, civic pride and community identity.

Policy:

Recognize the importance of community festivals in the community's cultural development.

Policy:

Encourage new developments to incorporate public art into their design.

Policy:

Create opportunities for people to experience art in everyday life and for artists and communities to participate in the design, look and feel of the Township through public art initiatives.

Policy:

Promote opportunities to interpret Esquimalt's foreshore and natural environment through the arts.

13.2 Heritage

Heritage Values

13.2.1 Objective: That community heritage values are considered in all land use decisions.

Policy:

Use the Heritage Values provided in Appendix __ as a guiding document when considering the impact of land use decisions on the community’s heritage.

Policy:

Properties identified with community heritage value or character will be considered for addition to Esquimalt’s Community Heritage Register.

Policy:

Consider Statements of Significance of registered heritage properties when rendering land use decisions.

Policy:

Consideration will be given to the ‘Significant Tree’ list, [Schedule ‘D’ of Tree Protection Bylaw, 2015, No. 2837 as amended from time to time], when making land use decisions.

Policy:

Consider potential heritage resources when making land use decisions.

Policy:

Identify places within the community for placement of signage that celebrates local heritage and history.



Celebrate Esquimalt's Heritage

13.2.2 Objective: Respect the Township's proud history by celebrating and thereby enhancing community member's connections to the past.

Policy:

Support educational efforts and community events that recognize and celebrate the community's heritage including First Nations, navy, army, ship building and repair industry, and connection to nature and sea shore.

Policy:

Develop signage that celebrates Esquimalt history and heritage at key locations.

Policy:

Work with local First Nations to incorporate more Lekwungen place names within the Township of Esquimalt.

Policy:

Continue to support well maintained private properties, having heritage value or character through the application of various land use tools .

Policy:

Continue to use historic photos to wrap utility boxes, reflecting the interesting history and beauty of this area.

Policy:

Find bold, innovative means to celebrate the community's heritage assets.

Heritage Preservation and Conservation

13.2. Objective: Create a highly desired urban environment and strong Sense of Place.

Policy:

Maintain or enhance public view corridors, focal points, pedestrian links, view points, and landmarks at a community wide and neighbourhood level. Public view corridors are shown on the "Schedule G - Public Views Corridors Map".

Policy:

Wherever possible, preserve character buildings in their original location. Where this is not possible, for recognized heritage buildings; encourage relocation and restoration onsite, or if not possible, on an appropriate alternative site.

Policy:

Support new additions to heritage property that conserves and enhances that property, as consistent with the National Standards and Guidelines for the Conservation of Historic Places in Canada.

Policy:

Work with property owners to develop Heritage Designation Bylaws, Conservation Covenants and Heritage Revitalization Agreements during redevelopment.

Policy:

Consider using the provisions of the Community Charter and Local Government Act to promote the conservation, rehabilitation and maintenance of heritage resources.

Neighbourhood Design

13.2.3.2 Objective: Opportunities exist for new developments to build on existing neighbourhood and street features, to foster the sense of consistency over time and celebrate areas with unique character.

Policy:

Recognize the importance of built form in respecting neighbourhood/street character. This could include repetition of one or more elements or patterns that contribute to neighbourhood identity. Elements could include building heights, setbacks, rooflines, street orientation, front yard fencing; but does not compel new buildings to look like they were built in a previous era.

Policy:

Reinforce landscaping as a character element. Retain and augment mature landscaping and trees where possible.

Policy:

Establish new, and revise existing neighbourhood/street design guidelines as new character defining elements are recognized.



14 ECONOMIC DEVELOPMENT



14.1 General

14.1.1 Objective: Encourage a diversity of knowledge based businesses to locate in Esquimalt.

Policy:

Support economic development that:

- i. Fosters investment and growth in local businesses, services and facilities that cater to the community;
- ii. Encourages a wide range of marine-oriented businesses (e.g., ship fitting, repair and maintenance, marine-related science and technology);
- iii. Encourages businesses that provide goods and services to the military sector;
- iv. Attracts a wider range of business types, including advanced-technology, green economy and knowledge-based businesses;
- v. Expands tourism-related businesses that promote Esquimalt's unique regional offerings, including sports facilities, First Nations, military history, and saltwater recreational opportunities;
- vi. Expands the sport, arts and culture sector, promoting Esquimalt's sports facilities, arts and cultural heritage;
- vii. Continues to build relationships with the film industry and use of Esquimalt locations to attract more film productions; and
- viii. Works towards creating a complete community for families and seniors, including banking, medical and support services, daycare, an accessible trail system, public transit and recreational opportunities.

Policy:

Pursue opportunities to bring advance educational institutions into Esquimalt.

Policy:

Support local food production, including local markets in Esquimalt and home based businesses

14.2 Commercial & Industrial

Objective: Encourage the upgrading of commercial and industrial areas.

Policy:

Encourage revitalization of underdeveloped commercial and industrial areas that have been in decline.

Policy:

Encourage the creation of new office space, cafes, restaurants and pubs in commercial areas.

Policy:

Foster the revitalization of Esquimalt Road to give it more of a vibrant and pedestrian friendly downtown core with welcoming public spaces for residents and visitors.

Policy:

Encourage revitalization of commercial areas along Admirals Road, Esquimalt Road, corner of Tillicum and Craigflower Rd, and Head Street.

Policy:

Encourage the redevelopment and expansion of commercial areas along Craigflower Road at Dominion, Tillicum, and Old Island Highway.

Policy:

Recognize the value of the E&N Rail Trail as a competitive advantage to Esquimalt's Industrial Park and Dockyard. The railway can move people and goods to the area. The rail trail provides alternative travel options for employees.

Policy:

Encourage enhancements to the public realm in the industrial area, such as the development of public spaces or publicly available private spaces, for the local workforce, residents and users of the E&N Rail Trail.

14.3 West Bay

14.3.1 Objective: Support land use initiatives that contribute to the economic development of West Bay.

Policy:

Support expansion and commercial waterfront development around West Bay, provided it is planned to be compatible with the adjacent residential areas.

Policy:

Continue to work with the Greater Victoria Harbour Authority to promote economic opportunities for West Bay.

Policy:

Support the development of new tourism opportunities at West Bay.

Policy:

Support the continued usage of the West Bay (Westsong) Walkway as a scenic waterfront trail that links downtown Victoria via Victoria West to West Bay.

14.4 Tourism

14.4.1 Objective: Consider land use decisions that help promote tourism.

Policy:

Working with other organizations, the Township will continue to promote Esquimalt's visitor attractions, including community parks, the Gorge Waterway, West Bay and Esquimalt Harbour, the CFB Esquimalt Naval Base and Military Museum including Fort Macaulay and Work Point, sports facilities, and community events.

Policy:

Continue to support sports facilities as an important component of Esquimalt's economic diversity.

Policy:

Encourage development of compatible commercial opportunities along E&N rail trail.

Policy:

Consider short-term vacation rentals within legal dwelling units, where the dwelling unit is an owner or tenant's principal residence, for up to 60 days a year.



15 REGIONAL CONTEXT



15.1 Regional Growth Strategy

15.2 Regional Context Statement (to come)

16 DEVELOPMENT PERMIT AREA



Development permit areas are designated in order to promote development through the use of guidelines that meets the purposes outlined in Section 16.1.1.

16.1 General Regulations

16.1.1 Designated Development Permit Areas

Development permits are required for development proposals in the areas designated in “Schedule H” or as otherwise designated in narrative form in specific development permit areas.. This plan designates Development Permit Areas for:

- 16.1.1.1 Protection of the Natural Environment;
- 16.1.1.2 Protection of development from hazardous conditions;
- 16.1.1.3 Establishment of objectives for the form and character of intensive residential development;
- 16.1.1.4 Establishment of objectives for the form and character of commercial, industrial or multi-family residential development;
- 16.1.1.5 Establishment of objectives to promote energy conservation;
- 16.1.1.6 Establishment of objectives to promote water conservation;
- 16.1.1.7 Establishment of objectives to promote the reduction of greenhouse gas emissions.

- 16.1.1 Where land is subject to more than one Development Permit Area, development will be subject to the requirements of all applicable development permit guidelines. It may be possible to deal with all guidelines under one permit. The feasibility of this will be determined on a case-by-case basis.
- 16.1.2 Although compliance with any specific guideline may not necessarily be required, general and substantial compliance with the intent of all guidelines is expected by the Township.

16.2 General Development Permit Exemptions

Except for development within 20 m of the Gorge Waterway High Water Mark, a development permit is not required in the case of:

- 16.2.1 An internal alteration to an existing building that does not increase its footprint;
- 16.2.2 A minor alteration to the exterior of a building that does not change the architectural character of the development. For the purpose of this Section, “minor” is defined as a change which does not do any of the following:
 - Increase site coverage more than 5% of the approved coverage;
 - Increase any bylaw non-conformities; or
 - Comprise more than 10 m² of floor area as defined in the Zoning Bylaw.
- 16.2.3 A minor alteration to the exterior of a building for building envelope maintenance purposes (e.g. replacement of siding and/or windows), that does not change the form and character.
- 16.2.4 Temporary buildings or structures that are erected either for offices for construction or marketing purposes for a period that does not exceed the duration of such construction, or two years, whichever is less.
- 16.2.5 A fence less than 2.0 m in height;
- 16.2.6 The following types of subdivision:
 - Subdivisions resulting in three or fewer fee simple or bare land strata parcels;
 - Subdivisions for lot line adjustments;
 - Strata subdivision of buildings; and
 - Subdivisions as a result of leases.

For greater certainty, buildings and land alterations associated with subdivision continue to require a development permit.

- 16.2.7 Park or trail improvements within a dedicated park excluding new buildings with a floor area of greater than 100 m²;
- 16.2.8 Road or utility works within a dedicated highway;
- 16.2.9 For minor changes to design and finish of buildings, or landscaping.
- 16.2.10 Removal of trees with a valid tree cutting permit.
- 16.2.11 Emergency repairs to property where a safety hazard exists subject however to a post repair development permit if temporary measures exceed three months..
- 16.2.12 Replacement of sign faces with no change in the location, size or type of signage.
- 16.2.13 Signage approved by the Director of Engineering and Public Works.
- 16.2.14 The erection of temporary tent structures, provided that:
 - A building permit has been issued with respect to the tent structure;
 - The tent structure is accessory to a commercial, business park, or industrial use occurring on the same property;

- The structure does not remain in use for a period of more than 14 days.

- 16.2.15 Utility buildings (e.g. pump stations, electrical and telephone kiosks) with a floor area of less than 10 m².
- 16.2.16 Accessory buildings with a floor area of less than 10 m².
- 16.2.17 A suite in a residential building provided that a building permit has been issued.
- 16.2.18 Alterations to landscaping where there is an approved landscape plan provided that there is no net loss of landscaping area and function as per approved landscape plan.
- 16.2.19 Single detached dwellings except infill housing.

17 DPA NO 1: NATURAL ENVIRONMENT



17.1 Area

Land within the administrative municipal boundaries of the Corporation of the Township of Esquimalt.

17.2 Designation

Development Permit Area No. 1 is designated for the purpose of establishing objectives for the:

- Section 488 (1) (a) – protection of the natural environment, its ecosystems and biological diversity;

17.3 Justification

- The gradual erosion of the natural environment incrementally degrades the value and function of local and global ecosystem services. The average person owns a parcel of land for a relatively short time period of time (10-15 years); care can be taken to ensure larger trees and natural areas are available for the next generation.
- Landscapes and ecosystems are composed of patches of diverse habitats that may include 'species at risk', 'environmentally sensitive areas', and micro-ecosystems containing 'ecological memory'. Even small patches of native soil and vegetation support indigenous species; therefore, all areas can contribute to regional biodiversity and support ecosystem services.
- Protecting Esquimalt's natural environmental features [including but not limited to: local Garry Oak and Douglas-fir ecosystems, rock outcrops, hilly terrain, and rugged clean shorelines] contributes to natural ecosystem functioning and protection of biodiversity; distinguishing this area from other areas of the world.
- Esquimalt has over 20 kilometres of shoreline that serves industrial, commercial and residential purposes, recreation enjoyment, and is vital habitat for numerous plants and animals (e.g. otters, whales, seal, oyster catchers, gulls, various species of waterfowl, and eagles).. Shoreline ecology and fish habitat can be protected and enhanced by regulating development near shorelines, and by mitigating the impacts of stormwater entering local waterways. Keeping the urban environment absorbent helps lessen marine ecosystem damage from: introduced pollutants, sudden changes in water salinity and temperature, and shoreline erosion from surges in volume at storm-water pipe outfalls.

- The Gorge waterway is a sensitive, tidal-influenced watercourse that connects the fish-bearing fresh water of Craigflower and Colquitz Creeks with the salt waters of Victoria Harbour. Over the past few decades, significant public expenditures and efforts have gone into removing sources of pollution and contamination that once plagued this waterway. However, the removal of native shoreline vegetation and the construction of extensive sea walls have substantially diminished the quality of the shoreline as supportive habitat for fish and wildlife. Both private and public land-owners are responsible for this loss of habitat, often through well meaning and beneficial projects such as shoreline walkways.
- Protecting and maintaining current wave energy patterns and natural patterns of erosion along Esquimalt's shorelines will contribute to the protection of natural features and dynamic processes.
- Invasive alien plant species pose a significant threat to regional biodiversity. Managing and reducing the introduction and spread of these species protects local ecosystem structure and function and biodiversity.
- Esquimalt has several natural area parks of varying sizes distributed across the municipality. These pieces of fragmented natural habitat can be better connected through the use of native plant landscaping along roadways and in private yards; thereby supporting regional biodiversity.
- Biodiversity can be enhanced in an urban region through the thoughtful selection of building methods and landscape design. Siting buildings to provide space for trees of varying species and sizes provides vertical habitat for birds, pollinators and other creatures. In addition, the plants of a traditional Garry Oak meadow ecosystem are well adapted for the seasonally dry conditions found on local building rooves, and therefore can be effectively used in a 'living roof' system (a green roof with enhanced ecosystem services). A living roof will moderate stormwater discharge while providing habitat for indigenous plants, invertebrates, and ground feeding and nesting birds.
- Food security has been identified as an important resiliency objective in this OCP with many Esquimalt residents enjoy growing their own food or sourcing their food from local farmers; protecting pollinator biodiversity contributes to successful local food cultivation.
- Native birds are an important component of urban biodiversity and provide important ecosystem services that contribute to human health. Esquimalt is located within the 'Pacific Flyway' (A chain of habitats used by at least one billion birds biannual as their migratory route along the west coast of North and South America, from Alaska to Patagonia.) and much of Esquimalt's shoreline is part of the 'Victoria Harbour Migratory Bird Sanctuary'. Reasonable actions can be taken to enhance bird habitat in an urban setting; including providing vertical habitat through protection and enhancement of the urban forest, and protecting local shorelines and waterways.
- Habitat corridors for pollinators and other wildlife will be enhanced; linking natural areas within and through the urban matrix to support biodiversity and local food gardens

17.4 Exemptions

17.4.1 For all properties:

1. Interior renovations or alterations of existing buildings where residential density is not being increased.
2. Municipal public works, undertaken or authorized by the Township of Esquimalt, provided that activity does not disturb natural areas.
3. Ecological restoration projects undertaken or approved by the Township of Esquimalt.
4. Routine planting and maintenance of gardens and landscaping where areas of native vegetation are conserved and restored.
5. Removal of invasive alien plants [noxious weeds] [as identified by the BC Forest, Lands and Natural Resource Operations – Invasive Plant Program], where extensive disturbance of the soil is avoided.
6. Installation of unpaved paths or walking trails that are less than 1 metre in width and covered in naturally permeable materials [wood chips, bark mulch, sand or loose gravel] where the soil remains undisturbed.
7. Farm uses within the Agricultural Land Reserve.

17.4.2 For all lands located more than 7.5 metres from the high water mark of the Gorge Waterway:

1. Repair, maintenance or reconstruction of existing legal or legally non-conforming buildings, patios, driveways, parking areas and utilities, provided there is no alteration to natural soil or native vegetation.
2. Construction of fencing where no native trees are removed and disturbance to native vegetation is negligible.
3. The addition of small temporary landscape amenities including; benches, tables, garden ornaments, playground equipment, raised garden beds (not including retaining walls).

17.4.3 For lands located more than 20 metres from the high water mark of the Gorge Waterway, and more than 15 metres of the high watermark of the Strait of Juan de Fuca:

1. Minor additions [less than 10 square metres in area] to an existing legal or legally non-conforming building or structure.
2. Construction of buildings and structures less than 10 square metres in area.
3. Installation of seasonal recreation equipment such as children's play equipment, patio furniture, temporary above natural ground level pools/hot tubs.
4. Temporary tent/carport structures.

17.5 Guidelines

The expertise of qualified environmental professionals (retained by applicants), is strongly encouraged and may be required in certain circumstances.

17.5.1 Lands to remain free of development, with conditions

1. Lands within 7.5 metres of the high water mark of the Gorge Waterway shall be retained in as natural a state as possible. Where the land has been previously altered, the area shall be restored with native trees and plants.
2. New buildings/ structures shall not be located within 20 metres of the high water mark of the Gorge Waters.
3. New buildings/ structures shall not be located within 10 metres the high watermark of the Strait of Juan de Fuca.
4. Replacement of, expansion of, densification and intensification of the use of existing buildings within 20 metres of the high-water mark of the Gorge Waterway is discouraged; detached accessory dwelling units are strongly discouraged in this location.
5. Replacement of, expansion of, densification and intensification of the use of existing buildings within 15 metres of the high-water mark of the Strait of Juan de Fuca is discouraged and detached accessory dwelling units are strongly discouraged in this location.
6. Variances to 'Building Height' and 'Siting Requirements' will be considered where natural areas, trees are being protected.
7. Consider the use of conservation covenants for areas having high ecosystem conservation values. Property owners are encouraged to work with local land trusts to protect natural features and valuable habitat areas through land covenants.

17.5.2 Natural features and areas to be preserved, protected, restored, enhanced where feasible

1. Retain existing healthy native trees, vegetation, rock outcrops and soil wherever possible.
2. Preserve and enhance native tree and shrub clusters that overhang the waters edge as these provide shade, protection and feeding habitat for fish and wildlife.
3. Preservation of natural topography is favoured over blasting or building of retaining walls.
4. Narrower manoeuvring aisles, fewer and smaller parking spaces can be considered where natural areas are being conserved.
5. Design new development and landscaping to frame rather than block public views.
6. Avoid disturbing, compacting and removing areas of natural soil as this can lead to invasion by unwanted plant species, poor water absorption and poor establishment of new plantings. Use of local natural soil in disturbed and restored areas will support re-establishment of ecosystem functions.

17.7.3 Landscaping features that will protect restore and enhance biodiversity, where feasible

1. New landscaping shall consist predominantly of native plant and tree species. Plants that are native to the Coastal Douglas-fir biogeoclimatic zone are preferred in landscape treatments; as they provide habitat for threatened indigenous flora and fauna. Drought tolerant plants native to western North-America, that are known to be non-invasive, are a good alternative choice for landscaped areas.
2. In residential locations plan for 'nature out front'; for new landscaping in front and exterior side yards use a variety of site-appropriate, native species; thereby contributing positively to pedestrian friendly urban streets, future greenways and habitat enhanced corridors.
3. Choose trees and plants for site conditions; consider shade, sunlight, heat, wind-exposure, salt spray tolerance, and year round moisture requirements in their placement.
4. Consider the habitat and food needs of birds, pollinators, and humans in tree and plant species selection and placement; native plantings and food gardens compliment each other.
5. Encourage native plant and food gardens to spill from private land into boulevards.
6. Avoid monoculture plantings, especially expanses of turf grass outside of playing field sites.
7. Snags, logs, driftwood and rock cairns may be used as interesting landscaping features that also provide habitat for native flora and fauna.
8. Include a thick layer of natural leaf mulch in landscaped areas to help re-establish natural soil structure [layers] in disturbed areas.
9. Avoid using fast-growing non-native plants to cover and retain soils as they may become invasive and a constraint to the establishment of other plants.
10. Locate civil servicing pipes/lines under driveways or other paved areas and to minimize tree root damage. (Note that the majority of trees have their roots in the top 0.6 metres of the soil).
11. Design retaining wall spacing and landscape planting areas of sufficient width and depth to support plantings (eg. Provide larger spaces for trees).
12. Support the daylighting of portions of the stormwater system for enhanced habitat.
13. Aim to meet the Canadian Landscape Standards in all landscaping installations.

17.7.4 Measures to protect restore and enhance the natural environment (Limit noise, light and air pollution); where it is reasonable

1. Strategically locate leafy trees/ hedges and water features to mask urban noises such as traffic, garbage collection and delivery locations. Consider that leafy rough barked trees, vine covered walls and natural ground cover materials (mulch, soil) will help dampen urban noise.
2. Use International Dark-Sky Association approved lighting fixtures in outdoor locations. Outdoor lighting shall be no brighter than necessary, be fully shield (directed downward and designed to serve pedestrian needs), have minimal blue light emissions, and only be on when needed. Avoid vanity lighting, and lighting directed into the night sky and trees tops.
3. Light spillage onto waterways is strongly discouraged.
4. Place trees and vegetation near sources of air pollution including busy roadways, to assist in reduction of air pollution through the collection of particulate matter on leaves and needles, and absorption of toxic gases, including but not limited to: ozone, nitrogen dioxide, sulfur dioxide,

carbon monoxide, carbon dioxide, cadmium, chromium, nickel and lead.

5. Install a 'living roof' (a green roof of native species) on commercial, institutional and public buildings to lessen the local heat island effect, cool and insulate buildings, mitigate stormwater surges, and conserve and enhance regional biodiversity.

17.7.5 Measures to control drainage and shoreline erosion; where it is reasonable

1. Preserve, restore and enhance treed areas. Trees are the most effective form of absorbent landscaping due to their extensive root zones and their ability to both absorb water from the soil and intercept precipitation on leaves, needles and branches. Consider that native conifers are well adapted to local wet winters.
2. Reduce the impact of surges in stormwater on shorelines by designing on-site stormwater retention systems to contain the first 3 centimetres [1.25 inches] of precipitation on site, per precipitation event; and incorporating rainwater collection systems into roof design and landscaping.
3. Consider using shared private/ public rain gardens. Direct a portion of stormwater to adjacent public open spaces, when deemed appropriate by the municipal engineer.
4. Maximize the ratio of planted and pervious surfaces to unplanted surfaces, and design paved areas to direct water towards vegetated areas, to help reduce surface run off. Where paved surfaces are needed, intersperse with drought resistant vegetation and trees, to help absorb storm water, provide shade and reduce the local heat island effect.
5. Use porous surfaces to enhance stormwater infiltration, permeable paving is preferable for all open air parking areas. Ensure installation methods contribute to sustained permeability and retention of stormwater on the site.
6. Choose absorbent landscaping materials; leaf mulches, wood chips and good quality top soil, over gravel, pavers and concrete. Provide mulch of organic, locally derived materials; leaf mulch from local tree leaves is most desirable.
7. Incorporation of rain gardens, bio-swales, rain barrels, and even small depressions (puddles) into landscaping will help reduce surges of storm water entering local waterways.
8. Planting densities should ensure that vegetated areas will have near 100% plant coverage after two full growing seasons.

17.7.6 Measures to protect, restore and enhance local shorelines, (reducing shoreline hardening and dock development); when it is feasible

1. Waterfront property owners are encourage to become familiar with and adopt a 'soft shore' restoration approach to the care of their foreshore property [i.e. Green Shores for Homes]
2. Remove structures known to contain creosote from local waters.
3. Avoid the expansion of dock area, bulkheads, groins or other shoreline hardening structures. Removal or reductions in the surface area of existing private docks in encouraged.
4. Where shoring methods are required to prevent erosion or the sloughing of the shoreline, choose bio-engineering methods over the use of sea-walls or retaining walls. Where sea-walls or retaining walls are the only means of effectively preventing erosion, design in consultation with qualified environmental professionals, as well as engineering professionals.

17.7.7 Measures to protect, restore and enhance native bird biodiversity; where it is reasonable

1. Protect and enhance habitat features like mature trees, shrub clusters, native fruit bearing shrubs, fresh water ponds and ephemeral damp areas (puddles).
2. Encourage increased front yard habitat along quieter streets to reduce bird vehicle conflicts and enhance the pedestrian experience through native plantings.
3. Sustain a mix of habitat types; including forest, shrub-land, meadow, riparian wetland and coastal shoreline ecosystems in landscaping.
4. Incorporate a vertical vegetation structure [vertical habitat] including layers of ground cover, shrub, understorey and canopy in landscape design.
5. Choose a range of native plant species and sizes; a mix of coniferous and deciduous trees will enhance bird species diversity.
6. Incorporate architectural features that limit collisions between birds and windows: including patterned, frosted or tinted glass, exterior louvers, blinds, sun shades and canopies.
7. Cap and screen all ventilation pipes and grates, avoid openings greater than 2.0 x 2.0 cm.

18 DPA NO 2: PROTECTION OF DEVELOPMENT FROM HAZARDOUS CONDITIONS



- 18.1 Area**
- 18.2 Designation**
- 18.3 Justification**
- 18.4 Requirements**
- 18.5 Exemptions**
- 18.6 Objectives**
- 18.7 Guidelines**

19 DPA NO 3: ENHANCED DESIGN CONTROL RESIDENTIAL



19.1 Area

All lands zoned for two-unit dwellings or zoned as Comprehensive Development Districts for residential developments only are designated as part of Development Permit Area No. 3 — Enhanced Design Control — Intensive Residential as shown on Schedule “H” of this Plan.

19.2 Designation

Development Permit Area No. 3 is designated for the purpose of establishing objectives for:

Section 488(1) (e) – establishment of objectives for the form and character of intensive residential development

19.3 Justification

The following policies and guidelines were developed to allow for the better utilization and redevelopment of parcels within residential neighbourhoods and ensure that development occurs in a manner that retains the desirable physical characteristics of a neighbourhood.

In-fill housing represents an important component of the housing spectrum. Because infill housing represents new development that is located within the context of an existing residential neighbourhood, it is important that there is harmonious compatibility between the form and character of the new development and the existing development

19.4 Exemptions

19.4.1 The following do not require a development permit:

- additions or renovations to, or the construction of, one single-unit dwelling (excluding infill units) situated on a fee simple parcel where the floor area of the addition does not exceed 10 percent of the ground floor area of the dwelling;

- additions or renovations to any two-unit dwelling where the floor area of the addition does not exceed 10 percent of the ground floor area of the dwelling;
- construction of buildings or structures less than 10 square meters in area; and

19.5 Duplex Guidelines

- 19.5.1 Design to be consistent with and preserve the proportions and patterns of existing residential buildings in the immediate vicinity, including the wall-to-window area ratios and the amount and type of open space provided.
- 19.5.2 Where it is not possible to achieve buildings of similar size and proportion to the surrounding residential buildings, the fronts of the buildings should be designed to create the appearance of smaller structures either by staggering the dwelling units or visually breaking up the façade with architectural detailing.
- 19.5.3 Innovative and creative site-specific two-unit dwellings are encouraged where yard space is maintained either on the ground or as rooftop gardens. Setbacks to the street may be reduced to maximize property use.
- 19.5.4 Front to back duplexes are generally discouraged unless they can be designed to eliminate negative impacts to the immediate neighbours such as shading of gardens, overlook of outdoor amenity areas and violation of privacy.
- 19.5.5 Side by side, up and down, or staggered unit configurations are preferred as these result in a greater number of units facing the street, less disruption of privacy, and a more equitable division of outdoor amenity areas between the two dwelling units.
- 19.5.6 The use of exterior building materials similar to those used in older residential neighbourhoods (i.e. combinations of wood, brick, stucco, and stone) is encouraged.
- 19.5.7 Rooflines of new development should relate in height, shape and pitch to existing residential buildings in the immediate area. For corner sites, the building design should avoid having large unbroken sloped roof areas facing the street.
- 19.5.8 To create interest in the façade of the buildings facing the street, the incorporation of architectural elements such as bay windows, covered porches, verandas and prominent front doors is encouraged.
- 19.5.9 Buildings should be designed to minimize visual intrusion onto the privacy of surrounding homes. Some overlook of adjoining yards and neighbouring decks may be unavoidable; however, additional privacy should be achieved by inseting balconies, decks and patios into the building or by screening them with latticework or landscaping. Windows should be spaced so that they do not align directly with those of other buildings.
- 19.5.10 The height and massing of new two-unit dwellings should be designed to minimize the casting of shadows onto the private outdoor space of adjacent residential dwellings
- 19.5.11 A landscaping plan showing ground cover areas, planting beds, shrubbery and trees (both existing and proposed) is required. Landscaping should add to the aesthetic appeal of the streetscape as well as provide privacy between dwelling units.
- 19.5.12 The provision of private open space should be part of an overall site development and landscape plan and should take into consideration general site circulation patterns (including parking), existing landscape features, sun access, privacy and usability.

- 19.5.13 Retention and protection of trees and the natural habitat is encouraged where possible.
- 19.5.14 Parking areas, garages and driveways should appear as a minor component of the site when viewed from the street. The building of curving access roads and driveways helps to avoid views from the street of large expanses of paving. The use of shared driveways is encouraged.
- 19.5.15 The use of permeable and decorative surfacing materials, such as brick, concrete pavers, textured concrete, coloured paving or grasscrete is encouraged in place of solid expanses of asphalt or concrete.
- 19.5.16 Where possible, hydro meters should not be placed on the front façade of a building and, if placed in a manner which is visible from the street, will be appropriately screened by the owner in a manner not inconsistent with BC Hydro requirements.
- 19.5.17 Where an existing single unit dwelling is being converted to a two-unit dwelling both the original structure and the addition shall be in the same architectural style and constructed of the same exterior finishes including roofing material, window treatments, exterior finishes, door styles and trims.
- 19.5.18 Roof styles and pitches of the original and new portions of the building must be complimentary.
- 19.5.19 For new or converted two unit dwellings, garages and parking areas are encouraged to be located in the rear yard. Shared driveways are preferred to access the rear yard.
- 19.5.20 Where two single lane driveways serve a two unit lot, landscaping features are encouraged between the driveways.

19.6 Guidelines for Single-Unit Infill Housing

19.6.1 Relationship to Existing Houses

1. Where an existing single-unit residence is to be retained and a second residence placed on the parcel, the existing dwelling is to be upgraded and made to blend with the new construction.
2. Where two or more new separate dwellings are situated within a comprehensive development zone, the buildings shall be designed as part of a comprehensive scheme with all buildings being finished in complementary materials and incorporating similar architectural details.
3. Where new infill single houses are proposed, the design of the new houses should be complementary in scale, size, exterior finishes, rooflines, and colours to the predominant styles of housing in the neighbourhood. It is important to ensure that the new construction fits with the overall scale and character of existing houses.

The intent of this guideline is not to encourage the replication or imitation of surrounding buildings but rather the design of structures that complement the streetscape.

19.6.2 Massing

1. New structures should be designed so that the overall massing is in keeping with other single-unit residences in the immediate area. New structures for lots other than corner or double frontage lots should be limited to one and one half storeys.
2. New structures, which are two storeys in height, should be designed so that the second storey is

partially concealed within the slope of the roof to minimize the height of the building. The use of dormers set into the roof is preferred to a flat roof or a peaked roof set over the second storey.

19.6.3 Privacy/Screening/Shadowing

1. Proposed infill dwellings should have only a minimal impact on adjacent homes and be separated from neighbouring residences by vegetation, screening, natural elevation differences, or a combination of these features.
2. Windows, decks and patios should be located so as to minimize intrusion onto the privacy of adjacent properties.
3. Infill dwellings should be sited to minimize the casting of shadows onto the private outdoor space of adjacent residential dwellings.

19.6.4 Landscaping

1. Proposals for single-unit infill housing must include a landscape plan showing hard landscaping (i.e., parking areas, fences, and patios) as well as lawns, trees, shrubs, planting areas and proposed plant species.

Retention and protection of trees and the natural habitat is encouraged wherever possible..5 Private Open/Yard Space

1. Any proposal for single-unit infill housing should provide for useable, private outdoor areas for each dwelling, at grade.

20 DPA NO 4: COMMERCIAL



20.1 Area

All lands designated Commercial on Schedule “H” are part of DPA No. 4.

20.2 Designation

Development Permit Area No. 4 is designated for the purpose of establishing objectives for:

- Section 488 (1)(d) – revitalization of an area in which commercial use is permitted.
- Section 488(1)(f) – form and character of commercial development.

20.3 Justification

Traditionally, Esquimalt’s commercial areas have not been developed on the basis of a particular theme or concept. The design and form of commercial development has been rather haphazard and, as a result, the Esquimalt Village and other local commercial areas do not have the cohesiveness nor the attractiveness they could have.

When asked in a recent questionnaire to identify what they disliked most about Esquimalt, an overwhelming number of respondents identified the lack of a downtown commercial area, with appropriate shops and services, and the appearance of Esquimalt Road in the village core.

20.4 Exemptions

The following do not require a development permit:

- 20.5.1 Construction of buildings or structures less than 10 square metres in area;
- 20.5.2 Minor additions to existing structures where the floor area of the addition does not exceed 10 percent of the ground floor area of the structure; and

20.5 Guidelines

1. Facades should be appropriate to a pedestrian shopping area with windows facing the street and doors opening onto the street rather than onto a courtyard or laneway. (See image)
2. Ornamental lighting that not only highlights the building but also increases the amount of light falling onto pedestrian areas should be used wherever possible. However, lighting should not create unnecessary glare or shine directly into neighbouring residential properties.
3. Buildings should be designed and sited to minimize the creation of shadows on public spaces.
4. Where possible, weather protection (i.e. awnings and canopies) should be provided above all pedestrian walkways including walkways to on-site parking areas.
5. Off-street parking areas should be located either at the rear of commercial buildings or underground. Surface parking should be screened with landscaping. Large parking areas should contain additional islands of landscaping.
6. The design of new commercial buildings, including areas use for parking, should incorporate Crime Prevention through Environmental Design (CPTED) principles.
7. Buildings may be located at the front property line in order to create a pedestrian-oriented environment, except where vehicle visibility is affected and on those streets that have been identified as requiring future road widening.
8. Landscape screening and fencing should be located around outdoor storage areas and garbage and recycling receptacles.
9. Retention and protection of trees and the natural habitat is encouraged wherever possible.
10. Where new development is to occur within Esquimalt's commercial core, that development should add to the pedestrian appeal and overall appearance of the street through features such as easily accessible entrances, street furniture and public art, landscaping and attractive exterior finishing materials.



21 DPA NO 5: INDUSTRIAL



21.1 Area

All lands designated Industrial on Schedule “H” are part of DPA No. 5.

21.2 Designations

Development Permit Area No. 5 is designated for the purpose of establishing objectives for the form and character of:

- Section 488.1(1)(f) – form and character of industrial development

21.3 Justification

The Devonshire/Viewfield Road industrial area is surrounded by residential neighbourhoods. In order to ensure that any future development in this area is undertaken in a manner that will be compatible with the surrounding residential uses, it is desirable that new development incorporates a sympathetic form and character.

21.3.1 To encourage environmentally friendly light industry in the Devonshire/Viewfield industrial park; and

21.3.2 To encourage revitalization and enhancement of the existing industrial areas.

21.4 Exemptions

The following do not require a development permit:

21.5.1 construction of buildings or structures less than 10 square meters in area;

21.5.2 minor additions to existing buildings where the floor area of the addition does not exceed 10 percent of the ground floor area of the building;

21.5 Guidelines

- 21.6.1 Buildings should be designed to minimize the intrusion into the privacy of existing surrounding homes.
- 21.6.2 Buildings should be located to avoid casting shadows onto adjacent residential properties.
- 21.6.3 Outdoor storage and parking areas will be screened by berms, fences, landscaping or solid noise-absorbing barriers or a combination of these methods. Landscaping should also be incorporated within the parking areas to “break up” large expanses of pavement.
- 21.6.4 Retention and protection of trees and the natural habitat is encouraged wherever possible.

22 DPA NO 6: MULTI-FAMILY RESIDENTIAL



22.1 Area

All land designated Multi-Unit Residential on Schedule “H” are part of DPA No. 6

22.2 Designation

Development Permit Area No. 6 is designated for the purpose of:

- Section 488(1)(e) – establishment of objectives for the form and character of intensive residential development.
- Section 488(1)(f) – establishment of objectives for the form and character of multi-family residential development.

22.3 Justification

This Plan emphasizes the importance of protecting residential neighbourhoods and encouraging a high quality of construction for new development. It is essential that new multi-unit residential development not have a negative impact on, or be out of character with, existing residential neighbourhoods. The primary objective of Development Permit Area No. 1 is to ensure that the development of multi-unit residential sites is compatible with surrounding uses.

22.4 Exemptions

The following do not require a development permit:

- 22.4.1 construction of buildings or structures less than 10 m²;
- 22.4.2 minor additions to existing dwellings where the floor area of the addition does not exceed 10 percent of the ground floor area of the dwelling; and
- 22.4.3 placement of signs less than 1.5 m² in.

22.6 Guidelines

The size and siting of buildings that abut existing single- and two-unit and townhouse dwellings should reflect the size and scale of adjacent development and complement the surrounding uses. To achieve this, height and setback restrictions may be imposed as a condition of the development permit.

- 22.6.1 New buildings should be designed and sited to minimize visual intrusion onto the privacy of surrounding homes and minimize the casting of shadows onto the private outdoor space of adjacent residential units.
- 22.6.2 High-density multi-unit residential buildings or mixed commercial/residential buildings in commercial areas should be designed so that the upper storeys are stepped back from the building footprint, with lower building heights along the street front.
- 22.6.3 Landscaping of should emphasize the creation of an attractive streetscape, as well as provide privacy between individual buildings and dwellings, screen parking areas and break up large expanses of paving.
- 22.6.4 Surface parking areas in developments less than five storeys in height, will be situated away from the street and screened by berms, landscaping or solid fencing or a combination of these three.
- 22.6.5 Underground parking will be provided for any multi-unit residential buildings exceeding four storeys.
- 22.6.6 The retention of public view corridors, particularly views to the water, should be encouraged wherever possible.
- 22.6.7 To preserve view corridors and complement natural topography, stepped-down building designs are encouraged for sloping sites.
- 22.6.8 Retention and protection of trees and the natural habitat is encouraged wherever possible.
- 22.6.9 Townhouses will be designed such that the habitable space of one dwelling unit abuts the habitable space of another unit and the common wall overlap between adjoining dwellings shall be at least 50 percent.
- 22.6.10 Site lighting should provide personal safety for residents and visitors and be of the type that reduces glare and does not cause the spill over of light onto adjacent residential sites.

23 DPA NO 7: ENERGY CONSERVA- TION & GREENHOUSE GAS REDUCTION



23.1 Area

Land within the administrative municipal boundaries of the Corporation of the Township of Esquimalt.

23.2 Designation

Development Permit Area No. 7 – is designated for:

- Section 488 (1)(h) – energy conservation
- Section 488 (1)(j) – GHG emissions reduction

23.3 Justification

- The province of British Columbia has legislated greenhouse gas targets and requires the cooperation of local governments to achieve them. As part of its pledge to the Community Climate Action Charter, Esquimalt set its own target to reduce greenhouse gas emissions by 38% of 2007 levels by 2020; with the eventual goal of progressing towards carbon neutrality.
- The objectives in this DPA will:
 - Encourage a shift in practice and behavior to accelerate a reduction in fossil fuel dependence;
 - Support reductions in energy consumption in buildings, and reduced maintenance costs through the use of durable building materials; Support the best use of existing infrastructure and minimizing the need for system capacity expansion and extension;
 - Encourage and support innovation in redevelopment, siting and design;
 - Deliver neighbourhoods that support residents physical and mental health with long-term livability, including walkability;
 - Consider the long-term comfort of building occupants in design decisions;
 - Create neighbourhoods and buildings that respect, serve, and support the needs of all economic classes;

- Build neighbourhoods that support transit, walking and other forms of active transportation;
- Support the construction of new buildings that contribute to those neighbourhoods where people are delighted to live, work, walk and play;
- Support development to have a positive impact on the biosphere, community resilience and residents' health.

*"The average Canadian tree will sequester 200 kg of carbon in an 80 year period, in an urban environment".
(Tree Canada Foundation)*

23.4 Exemptions

- 23.5.1 Minor alteration/ addition to the exterior of a building. For the purpose of this Section, "minor" is defined as a change which does not:
- 23.5.2 Increase the lot coverage by the lessor of 5% of the parcel or 50 square metres (based on the site coverage of all buildings and structures);
- 23.5.3 Increase any bylaw non-conformities;
- 23.5.4 Comprise an addition of more than 50 m² of gross floor area;
- 23.5.5 Require a Development Permit for 'Form and Character'
- 23.5.6 Landscaping
- 23.5.7 Installation of temporary tent/carport structures intended to be used for less than one year

23.5 Guidelines

The expertise of qualified environmental professionals (retained by applicants), is strongly encouraged and may be required in certain circumstances.

23.5.1 Siting of Buildings and Structures; where it is feasible:

1. Orient buildings to take advantage of site specific climate conditions, in terms of solar access and wind flow; design massing and solar orientation for optimum passive performance.
2. Build new developments compactly, considering the solar penetration and passive performance provided for neighbouring sites, and avoid shading adjacent usable outdoor open spaces.
3. In commercial/residential designated areas with taller developments, vary building heights to strategically reduce the shading of adjacent buildings.
4. Provide space for pleasant pedestrian pathways between buildings.
5. Strategically site buildings to sustain and increase the community's urban forest tree canopy cover.
6. Provide space for significant landscaping including varying heights of trees, shrubs and ground covers.
7. Provide intuitive pedestrian access to storefronts and businesses with site connectivity to nearby amenities and services to help promote walking and the use of other active transportation modes.

8. Provide usable outdoor amenities such as seating, food gardens, mini-libraries, and play spaces in semi-public areas, to enhance the experience of walking and recreating in the neighbourhood.
9. In residential neighbourhoods provide space for larger trees and a second row of street trees as this will enhance the pedestrian experience by lowering wind velocity at street level, reducing excessive heating at ground level, and absorb vehicle and other urban noises.

23.5.2 Form and Exterior Design of Buildings and Structures; where it is feasible:

1. Orient larger roof surfaces to the south for potential use of solar panels or photovoltaic roofing.
2. Use roof designs that reduce heat transfer into neighbouring buildings, helping reduce the local heat island effect and the need for cooling of buildings in warmer months.
3. Place more windows on the south side of buildings to increase solar gain, and fewer/ smaller windows on the north side to minimize heat loss.
4. Use roof over-hangs, fixed-fins or other solar shading devices on south and west facing windows to reduce peak summer heat gain while enabling sunlight penetration in winter months.
5. Install adjustable overhangs above windows that can help control the amount of sun exposure in warmer months thereby reducing need for cooling.
6. Provide building occupants with control of ventilation; i.e. windows that open.
7. Skylights are discouraged as they decrease insulating values and can interfere with solar panel installation.
8. Use green [living] roofing to decrease heat island effect, improved insulation and reduced cooling costs for buildings – Consider a ‘biosolar’ roof that combines a green [living] roof with solar power.
9. Install photovoltaic roofing materials where the location is appropriate.
10. Add roof top patios and gardens, particularly food producing gardens, as they can contribute to local resilience, livability, and reduction in greenhouse gas production by reducing food transportation costs.
11. Install greenhouses for growing food on rooftops where neighbourhood privacy and light intrusion concerns are mitigated.
12. Avoid heavily tinted windows or reflective glass which will diminish the natural daylighting of interior spaces, thereby requiring increased energy requirements for interior lighting.
13. In exposed marine locations select durable materials that will withstand weather and salt spray, to ensure low maintenance costs and infrequent replacement needs.

23.5.3 Landscaping; where it is feasible:

1. Develop a front yard landscape design that is natural and delightful so residents do not need to leave the neighbourhood to experience nature.
2. Choose open space and landscaping over dedicating space to the parking and manoeuvring of private motor vehicles.
3. Conserve native trees, shrubs and soils, thereby saving the cost of importing materials and preserving already sequestered carbon dioxide.
4. Use deciduous trees for landscaping along southern exposures, as they provide shade in the sum-

mer and allow more sunlight through in the winter.

5. Strategically place taller trees and vegetation on the south and west sides of buildings where there is more direct sun exposure.
6. Strategically place coniferous trees such that they can buffer winter winds.
7. As context and space allow plant trees that will attain a greater mature size, for greater carbon storage; removal of healthy trees is discouraged as the loss of the ecosystem services provided by larger trees will take many years to recover.
8. Plant trees with a larger canopy cover along roadways and sidewalks, thereby providing shading of paved areas, lowering the heating of paved surfaces and reducing the wind velocities in these pedestrian areas.
9. Plant shorter and sturdier vegetation closer to buildings and other structures, and taller vegetation further away to avoid potential damage from strong winds blowing vegetation against buildings.
10. For commercial areas: strategically increase green space between buildings, allowing room for landscaped pathways to improve the pedestrian experience, promote walking, and providing for improved light penetration onto sidewalks.
11. For parking areas and along boulevard/ sidewalk edges; plant trees to provide shade, store carbon and reduce the heat island effect.
12. Install layers of compost and mulch to protect and improve soil health, nourish plant and tree growth and help sequester carbon dioxide on site, in the soil – avoid disturbing areas of natural soil.

23.5.4: Machinery, Equipment and Systems External to Buildings and Other Structures; where it is feasible:

1. 1. For external lighting;
 - Choose efficient low-energy and long life technologies;
 - Design lighting to reinforce and compliment existing street lighting;
 - Use motion-sensitive or solar-powered lights whenever possible;
 - Layer lighting for varying outdoor needs
 - Provide lighting systems that are easily controlled by building occupants.
2. Use heat pumps, solar panels, living [green] roofing or an innovative system to improve a building's energy performance.
3. Use durable, vandalism and graffiti resistant materials where neighbourhood surveillance may be limited.
4. Design for on-site heat recovery and re-use of water.
5. In commercial and industrial areas: design bicycle parking facilities to be inviting for cyclists. Locate bike racks near the main building entrance, with adequate lighting and weather protection. Provide adequate space for the parking of larger bicycles and the manoeuvring of the bikes into and out of the rack. Consider providing racks that are both functional and artistic in nature.
6. In commercial areas: provide fast charge electric vehicle charging stations near locations that have quick customer turnover; and ensure the station is easily accessible, well lit, and visible from the

public street.

7. Provide car sharing facilities that are well lit, available for residents, and easily accessed from the public street.

23.5.5 Special Features; where it is feasible:

1. Select building materials that have been shown to have a high level of durability for the use intended.
2. Use wood for construction as a means to sequester carbon dioxide - North American grown and sustainably harvested wood is preferable for building construction.
3. Select local and regionally manufactured building products whenever possible to reduce transportation energy costs.
4. Reuse of existing buildings and building materials is encouraged.
5. Choose materials that have a high likelihood of reuse or recycling at end of life.

24 DPA NO 8: WATER CONSERVATION



24.1 Area:

Land within the administrative municipal boundaries of the Corporation of the Township of Esquimalt.

24.2 Designation

Development Permit Area No. 8 is designated for:

Section 488(1)(i) – water conservation

24.2.1 For Water Conservation

24.3 Justification

Guidelines that conserve water also reduce energy use from treating and distributing potable water and treating wastewater, and help communities prepare for expected water shortages from climate change.

Urban areas have high water demands. Landscaping uses a considerable quantity of potable water. Stormwater can be either a burden on municipal infrastructure and local shorelines; or a resource used within the community to lessen water demand for landscaping;

- Wise use of potable and stormwater can lead to reduced energy consumption and costs associated with the treating and distribution of potable water;
- Reduced potable water consumption can lead to reduced energy consumption associated with the treating of wastewater;
- By making the best use of existing infrastructure, the need for system capacity expansion and extension can be reduced;
- Use of stormwater for landscaping can assist in the conservation of local water reserves; and
- Rain gardens, retention ponds, and bioswales can provide value as an urban design element that provides a source of delight in a passive recreation environment, and enhanced wildlife habitat and biodiversity.

The guidelines in this section are intended to implement the Township’s sustainability objectives to develop a ‘green economy’ and reduce the overall risks and impacts of climate change through:

- Reduced per capita water consumption in new developments.
- Make the best use of existing water system infrastructure and reduced need for system capacity expansion; Create a positive impact on the natural environment and hydrological systems;
- Encourage and support innovation in the use of stormwater to reduce landscaping water requirements; and
- Reduce the impacts on the stormwater management system from the over use of potable water for landscaping.

24.4 Exemptions

- 24.5.1 Changes to landscaping that does not decrease the permeability of a property
- 24.5.2 A minor alteration/ addition to the exterior of a building. For the purpose of this Section, “minor” is defined as a change which does not:
- Increase the lot coverage by the lessor of 5% of the parcel or 50 square metres (based on the site coverage of all buildings and structures);
 - Increase any bylaw non-conformities;
 - Comprise an addition of more than 50 m² of gross floor area;
- 24.5.3 Installation of temporary tent/carport structures to be used for less than one year

24.5 Guidelines:

The expertise of qualified environmental professionals (retained by applicants), is strongly encouraged and may be required in certain situations.

24.7.1 Building and Landscape Design; where it is feasible:

1. Reduce the burden on built stormwater infrastructure by designing on-site retention systems to retain the first 3 centimetres [1.25”] of stormwater on site, per precipitation event.
2. Provide space for absorbent landscaping, including significantly sized trees on the site; by not allowing underground parking structures to extend beyond building walls.
3. Incorporate rainwater collection systems into roof design; consider using living roofs and walls as part of a rainwater collection system.
4. Incorporate rain gardens into landscaping and direct rainwater towards vegetated areas.
5. Intersperse paved surfaces with drought resistant vegetation that will provide shade on those surfaces, and design the paved surfaces to drain into the vegetation.
6. Design landscaping with more planted and pervious surfaces than solid surfaces.
7. Direct stormwater towards adjacent public spaces, with rain gardens/ bioswales located on public property where it would benefit both the new development and the municipality; and is deemed appropriate by municipal staff.

24.7.2 Landscaping - Select Plantings for Site and Local Conditions; where it is feasible:

1. Retain existing native trees, vegetation, and soil on site.
2. Plant species native to the Coastal Douglas-fir biogeoclimatic zone, as they are most suited to our climate and require little additional irrigation once established.
3. Consider shade, sunlight, heat, wind-exposure and salt spray, as well as water needs in the selection and placement of plant species.
4. Group plants with similar water needs into hydro-zones.

24.7.3 Landscaping – Retaining Stormwater on Site (absorbent landscaping); where it is feasible:

1. Preserve and restore treed areas. Trees are the most effective form of absorbent landscaping due to their extensive root zones and their ability to both absorb water from the soil and intercept precipitation on leaves, needles and branches. Consider that native conifers are well adapted to local wet winters.
2. Use pervious landscaping materials to enhance stormwater infiltration; permeable paving is preferable for surface parking areas.
3. Avoid disturbing, compacting and removing areas of natural soil, as these are naturally absorbent areas.
4. Locate civil servicing lines along driveways and other paved areas, to lessen the disturbance of natural soils and loss of their natural absorption qualities.
5. Include a thick layer of leaf mulch in planted areas as a means to improve soil health and absorbency, and reduce evaporation.
6. Use good quality top soil and compost for the finish grading of disturbed areas to contribute to the water holding capacity of newly landscaped areas.
7. Choose bark mulches or woodchips for walking paths for enhanced absorption.
8. Plant at densities that will ensure vegetated areas have 100% plant canopy coverage after two full growing seasons. Consider that understory native plants are adapted to local climates; absorb seasonal soil moisture, and reduce compaction due to foot traffic.

24.7.4 Landscaping - Water Features and Irrigation Systems; where it is feasible:

1. Use automated high efficiency irrigation systems where irrigation is required.
2. Incorporate stormwater retention features into irrigation system design.
3. Use recirculated water systems for water features such as pools and fountains.
4. Install plantings and irrigation systems to the Canadian Landscape Standard.

25 DPA NO 9: ENGLISH INN



25.1 Area

Lands legally described as PID: 023-009-331 Lot B Section 11 Esquimalt District Plan VIP60066 is designated as Development Permit Area No. 9 – English Inn.

25.2 Designations

Development Permit Area No. 9 is designated for the purpose of establishing objectives for:

- Section 488 (1)(d) – revitalization of an area in which a commercial use is permitted
- Section 488 (1)(e) – form and character of intensive residential development
- Section 488 (1)(h) – promoting energy conservation
- Section 488 (1)(i) – promoting water conservation
- Section 488 (1)(j) – GHG emissions reduction

25.3 Justification

These guidelines were developed to steward the design of development on the property known as the “English Inn” site at 429 Lampson Street in Esquimalt. The intent is to encourage new development to be sympathetic with, and a good neighbour to both the existing heritage Samuel Maclure designed manor house, known as Rosemead and the surrounding neighbourhood context, while providing opportunity for alternative massing solutions to accommodate market and building programmes. A key objective is a harmonious and sensitive development respectful of the Protected Property under Heritage Designation Bylaw 2807, including as described in the schedules thereto.

As a significant development project within the community this signature site has the potential to support Esquimalt’s target to reduce greenhouse gas emissions by 38% of 2007 levels by 2020; with the eventual goal of progressing towards carbon neutrality. Therefore, the guidelines support reductions in energy consumption in new buildings, and reduced long-term maintenance costs through the use of durable building materials; and

Landscaping uses a considerable quantity of potable water. Stormwater can be either a burden on municipal infrastructure and local shorelines; or a resource used on the property to lessen water de-

mand for landscaping. The intent is for the strategic retention of stormwater on this site.

The guidelines in this section are intended to support the Township's sustainability objectives to develop a 'green economy' and reduce the overall risks and impacts of climate change.

25.4 Exemptions

The following do not require a Development Permit:

25.4.1 Construction of buildings or structures less than 10 square metres in area;

25.5 Guidelines

These guidelines are not intended to slavishly replicate the mock Tudor vocabulary of the original house, but rather listen to its basic form, texture, proportions and composition of elements on site. The guidelines are descriptive, and intended to be feasible to execute. The guidelines incorporate features to encourage the promotion of energy and water conservation and the reduction of greenhouse gases.

25.5.1 Landscape and Significant Features, where it is feasible:

1. Respect the qualities of the existing topography, natural rock outcrops and related significant trees (especially in the southeast corner).
2. Respect significant trees through appropriate building siting and design.
3. Landscape designs should reflect the character defining elements of the Manor house site and should use plant species suited to local climate and incorporate drought-tolerant, native species and other xeriscaping techniques that minimize the need for landscape irrigation.
4. The hard landscaping of the Manor house site; including but not limited to the pavilion, fountain, stonework and retaining walls, represent the formal landscaped gardens characteristic of a home of this stature and era. Any use of the site should respect the existing landscape features.
5. Landscaping at the rear of the Manor house site has been developed to form a courtyard for use by the buildings occupants and guests, and forms an integral part of the building context. All building siting and design should respect the site lines from these outdoor spaces.
6. The landscaped areas of the Manor house site, including the formal gardens, fountains, pavilions, hardscaping and courtyards are an important part of the character of the site and any proposed design should be sympathetic to these elements and this character. Use of materials should reflect the high quality already established on the site.
7. The property has many unique and mature plants and trees and any proposal should endeavour to reuse and incorporate this material on the site to the extent possible.
8. Fences as a part of the landscape should be of high quality material and the use of chain link fences should be avoided.

25.5.2 Access and Parking, where it is feasible:

1. Retain and simplify the existing driveway from Lampson Street to access the heritage property and lands beyond. Widen the north driveway judiciously around significant trees.
2. Maintain the domestic scale and character of the driveway onto Lampson Street including unobtrusive low level lighting ; retain the existing stone gate posts.
3. Any surface parking, especially on the Manor house site, should be appropriately screened with landscaping and be designed not to detract from the character of the landscaping of the site. The use of permeable paving materials for parking areas is encouraged.
4. Incorporate appropriate storm water management measures to ensure storm water from the driveway infiltrates back into the ground to ensure no net runoff offsite.
5. Incorporate below grade parking, for the development site, to take advantage of the approximately one storey north/south cross fall across the site.
6. Avoid long open cut parking access ramps by accessing underground parking from the lower levels of the existing grade.
7. Appropriate bicycle and scooter storage should be provided in commercial and multiple-family buildings.
8. Commercial and multiple-family buildings should include provision for charging stations for electric vehicles where appropriate.

25.5.3 Energy and Water Conservation, where it is feasible

1. Use green building standards and technology to reduce the environmental/ ecological footprint of development.
2. Use natural storm water management techniques and measures to ensure that all storm water is managed on the site with no net increase off site. It is a fundamental municipal requirement that all storm water runoff be managed on site. This will substantially improve the existing condition.
3. Use of outdoor lighting on buildings or in the landscape should be designed to minimize light pollution and spill over onto neighbouring properties. All outdoor lighting should minimize wattage and be directed downward. Use of motion detectors and timers is encouraged.

25.5.4 Building Form and Character, where it is feasible:

1. Break down building volumes into domestic sized increments.
2. Incorporate pitch roof language with dormers sympathetic to the heritage Maclure manor, reducing apparent building height and volume.
3. Consider relaxation of building setbacks where it can be shown that it is advantageous to building design and distribution of building mass and volume in relation to adjacent properties.
4. Respect significant trees through appropriate building siting and design.

25.6.5 Distribution of Building Volume, where it is feasible:

1. Concentrate higher building volume towards the middle of the site and towards the easterly portions adjacent to the neighbouring DND property.
2. Keep building volumes lower towards the edges and composed as if made up of individual dwelling units, particularly towards the south. Massing towards the northern edges can typically accommodate another storey, since the English Inn site is a nominal level below the neighbours to the north.

25.6.6 Basic Building Volume and Roof Forms; where it is feasible:

1. Employ basic building elements not much more than twice the bulk of the manor house proper to create an overall composition whereby the whole reads as an assemblage of these parts.
2. Compose building elements to shape and define spaces between and within; not to exist as objects in space.
3. Employ a language of roof pitch typically to reflect that of the manor house; to be inhabited within, not simply sit on top of habitable space.
4. Figuratively, pull the roof forms down around the occupied spaces.
5. Utilize dormers – pitched or single slope – to provide light and views from habitable space within the roof.
6. Utilize stepped down gables, or single pitch runoffs to further break down scale and create more intimate relationships with the ground. These elements can be used in succession.
7. Roof overhangs and window placement should be coordinated to provide cooling and shade during summer and solar access for passive heating in the winter.
8. Roof surfaces should be designed to accommodate solar energy collection devices. Skylights are discouraged, as a benefit of natural daylight penetration is not sufficient from an energy perspective, to outweigh their heat loss due to low insulation value.

25.6.7 Building Orientation and Access to Sunlight, where it is feasible:

1. Buildings should be located, oriented and designed to facilitate the retention of passive solar heat (e.g. south facing windows), reduce heat loss and support natural ventilation.
2. Reduce energy consumption of electric lighting by maximizing opportunities for the distribution of natural daylight into a building's interior spaces (excluding the use of skylights).
3. Avoid the use of heavily tinted or reflective glazing that reduces solar heat gain but also reduces the penetration of light.
4. Placement and retention of deciduous trees is encouraged such that these trees provide summer-season shading, and winter-season solar access.
5. While respecting the importance of the existing character of the site's landscape character design of on-site landscaping should minimize shading impacts and the potential for solar thermal or photovoltaic systems on the site and surrounding properties.

25.6.8 Windows – Types and Proportions, where it is feasible:

1. Employ bay windows, bracketed in upper stories, or stepped out on lower stories to form decks off upper stories, to break down scale of end walls.
2. Employ basic window element having a vertical proportion – 1:1.4 – 1:2.2.
3. Vary size from floor to ceiling to very small openings for secondary spaces.
4. Increase amount of transparency by stringing multiple units or by employing basic units at regular intervals.
5. Create horizontal strip glazing condition by exploring recurrent smaller units.
6. Break down scale and texture where appropriate with divided light muntins or zinc cam in double glazed units.
7. Large single well-proportioned sheets can be employed in conjunction with divided lites to capture views.

25.6.9 Renewable and Alternative Energy, where it is feasible:

1. Support where feasible, on-site renewable energy systems and technologies such as solar hot water, solar photovoltaic, micro wind turbines and heat pumps.
2. Encourage on-site resource recovery through technologies where possible such as heat exchangers on ventilation and domestic water supply.

25.6.10 Materials Management, where it is feasible:

1. Recycling infrastructure and facilities especially for organics is encouraged.
2. Building materials which are durable for the use intended should be sourced locally or regionally to reduce transportation requirements whenever possible and economic.
3. Reuse existing building and landscape materials on site where practical and economic.
4. Encourage construction waste diversion planning as part of the development process. Including the identification of designated areas for the collection of recyclable materials.

26 DPA NO 10: ESQUIMALT TOWN SQUARE



26.1 Area

Lands legally described as Lot 1, Section 11, Suburban Lot 40, Esquimalt District, Plan EPP32782 (PID: 029-349-311) are designated as Development Permit Area No. 10 - Esquimalt Town Square.

26.2 Designations

Development Permit Area No. 10 is designated for the purpose of:

- Section 488(1)(a) – protection of the natural environment, its ecosystems and biological diversity;
- Section 488(1)(d) – revitalization of an area in which a commercial use is permitted;
- Section 488(1)(e) - establishment of objectives for the form and character of intensive residential development;
- Section 488(1)(f) – establishment of objectives for the form and character of commercial and multi-family residential development;
- Section 488(1)(h) – establishment of objectives to promote energy conservation;
- Section 919.1(1)(i) – establishment of objectives to promote water conservation; and
- Section 919.1(1)(j) – establishment of objectives to promote the reduction of greenhouse gas emissions.

26.3 Justification

These guidelines were developed to steward the design of a mixed use development known as Esquimalt Town Square. The development needs to promote the revitalization of the Township's core area while being sympathetic to the surrounding land uses and built form and offering exemplary site and building design that incorporates the principles of urban ecology. In order to conserve energy and resources, the development should incorporate features that contribute to reduced consumption of water and energy.

26.4 Exemptions

The following do not require a Development Permit:

- 27.4.1 Alterations to landscaping as long as the form and character of the landscaping remains the same; and
- 27.4.2 Installation of public art.

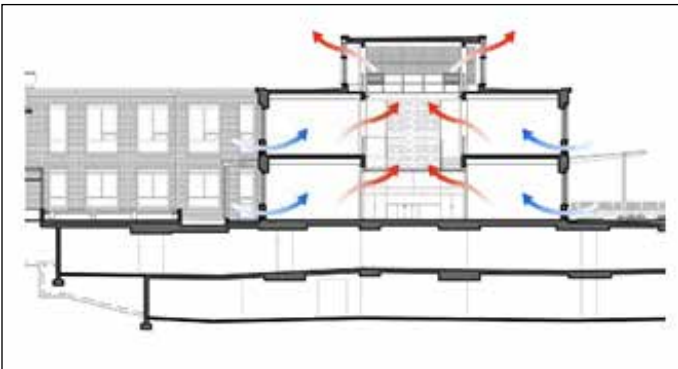
26.5 Development Permit Area Design Guidelines



26.6.1 Buildings should be designed to capture passive solar energy.



26.6.2 Sun shading should be considered to control heat gain in the summer.



26.6.3 Design for on-site heat recovery, and plan for future, long-term neighbourhood heat-resource opportunities.



26.6.4 Manage rainwater on the site including reduction of burden on storm-water sewer infrastructure and cleaning for down-stream use.



26.6.5 *Pollution management protocols (including sedimentation and oil- removal), as well as recycling programs should be followed during demolition, excavation and construction of the project.*



26.6.6 *Prioritize use of high quality, regionally sourced products & materials.*



26.6.7 *Where appropriate, make building uses visible from the outside.*



26.6.8 *Avoid mirrored or tinted glazing at the street level.*



26.6.9 *Locate clearly identified and weather-protected entrances to be accessible from public streets and squares.*



26.6.10 Integrate pedestrian-oriented signage with frontages and facades.



26.6.11 Way-finding and commercial signage, lighting and weather protection (canopies, etc) should be part of the architectural design.



26.6.12 Orient upper-storey windows and balconies to overlook adjoining streets and public space.



26.6.13 Use architectural emphasis to define street-corners.



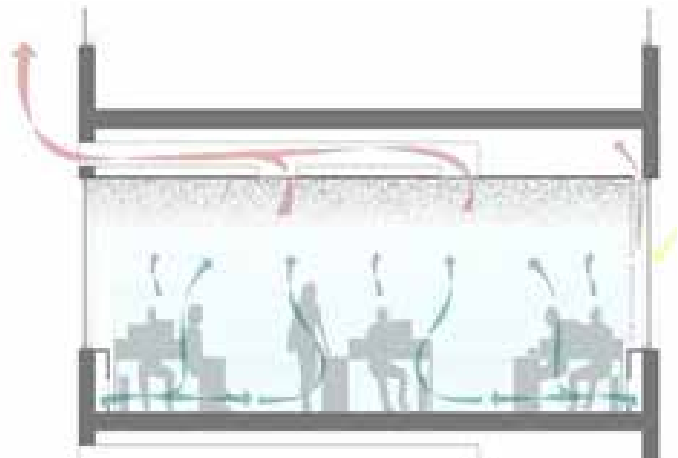
26.6.14 Locate on-street parking, where possible, in front of shops.



26.6.15 Maximize glazing at grade along commercial streets. Provide sight-lines from inside buildings to allow for casual surveillance of open public spaces, streets and sidewalks.



26.6.16 Avoid excessively long blank walls adjacent to public streets.



26.6.17 Follow green building practices in energy and water conservation, greenhouse gas production, indoor air quality and material durability in all aspects of building and site design.



26.6.18 Provide for building occupants to overlook public streets, parks, walkways and spaces, considering security and privacy of residents.



26.6.19 Provide for slightly raised entrances to ground floor residences along with private yards that are accessible from the fronting street or lane to encourage community interaction.



27.6.21 Use of indigenous plant species is encouraged.



27.6.22 Wherever possible, outdoor storage and parking areas should be screened from view.



27.6.21 All buildings should be bird friendly.



27.6.22 All exterior lighting should avoid excessive stray light pollution and should meet international dark skies standards.

27 DPA NO 11: WEST BAY



27.1 Area

All lands outlined and indicated as West Bay on Schedule “H” are part of DPA No. 11.

27.2 Designations

Development Permit Area No. 11 is designated for the purpose of:

- Section 488(1)(a) – protection of the natural environment, its ecosystems and biological diversity;
- Section 488(1)(b) – protection of development from hazardous conditions;
- Section 488(1)(d) – revitalization of an area in which a commercial use is permitted;
- Section 488(1)(e) - establishment of objectives for the form and character of intensive residential development;
- Section 488(1)(f) – establishment of objectives for the form and character of commercial and multi-family residential development;
- Section 488(1)(h) – establishment of objectives to promote energy conservation;
- Section 919.1(1)(i) – establishment of objectives to promote water conservation; and
- Section 919.1(1)(j) – establishment of objectives to promote the reduction of greenhouse gas emissions.

27.3 Justification

The West Bay Neighbourhood is a neighbourhood with unique characteristics that is under increasing development pressure. In order to retain the form and character of the neighbourhood while at the same time allowing redevelopment to occur a set of robust design guidelines is required. In addition, the neighbourhood’s coastal location means that there is potential harm to ecosystems which can be mitigated through the judicious use of guidelines. Redevelopment also offers opportunities to conserve energy and water and reduce greenhouse gas emissions. Finally, due to its adjacency to a marine environment, it is subject to hazards associated with Tsunamis and sea level rise.

Please see Appendix one for a complete draft of Section 27: WEST BAY

28 DPA NO 12: MCCLOUGHLIN POINT



28.1 Area

Development Permit Area No. 15 applies only to Lot 1, Section 11, Plan EPP36468. PID 030-006-813

28.2 Designation

Development Permit Area No. 15 is designated for the purpose of establishing objectives for:

- Section 488(1)(a) – natural environment, its ecosystems and biological diversity
- Section 488(1)(b) – protection of development from hazardous conditions
- Section 488(1)(d) – form and character, revitalization of area in which commercial use permitted
- Section 488(1)(f) – form and character of industrial development
- Section 488(1)(h) – energy conservation
- Section 488(1)(i) – water conservation
- Section 488(1)(j) – GHG emissions reduction

28.3 Justification

Site of a former oil storage facility, McLoughlin Point is an oceanfront site and a prominent entrance to Victoria Harbour. An objective is redevelopment of mixed uses, including revitalization when commercial uses are permitted, of impeccable design and cohesive operation, respecting adjacent DND lands as well as the broader communities. Public access from land and the water is encouraged, primarily for educational purposes, the latter recognizing historic uses and the heritage of the property, while recognizing the former is and may be limited given upland access through DND lands.

As an oceanfront site, there are nearby natural habitat considerations worthy of protection and preservation, as well as enhancement of the subject property itself given the history of heavy industrial use. The subject property is susceptible to high winds, and high tides, including risk of tsunami and therefore development on the site must occur in a manner to protect it from these natural hazards, while respecting the need to do so in a manner that does not increase risks for other properties or harm the natural environment. The report “Modelling of Potential Tsunami Inundation Limits and

Run-up” for the Capital Region was completed by the Capital Regional District’s Local Government Emergency Program Advisory Commission; it identified risks for McLoughlin Point that the CRD has indicated it has been considering to-date in its proposal for this site.

The CRD has also prepared and submitted a number of professional reports which identify special conditions at McLoughlin Point, including for a sewage treatment plant, and further justify designation to achieve objective both the CRD’s and Township’s objectives. These include but are not limited to the reports entitled:

“Core Area Wastewater Treatment Program McLoughlin Point Zoning Application, Township of Esquimalt McLoughlin Point Risk Assessment,” dated December 18, 2012 prepared by Stantec Consulting Ltd;

“Archaeological Review of the Proposed Capital Regional District McLoughlin Point Wastewater Treatment Facility,” dated January 2013 prepared by Tera Environmental Consultants;

“Capital Regional District Core Area Wastewater Treatment Program McLoughlin Point Wastewater Treatment Facility Traffic Considerations,” dated January 23, 2013 prepared by Bunt & Associates;

“Terrestrial Environmental Effects of the Proposed Capital Regional District McLoughlin Point Wastewater Treatment Facility,” dated January 2013 prepared by Tera Environmental Consultants; and,

“Community Impact and Mitigation Report,” dated January 2013 and prepared by CitySpaces Consulting Ltd.

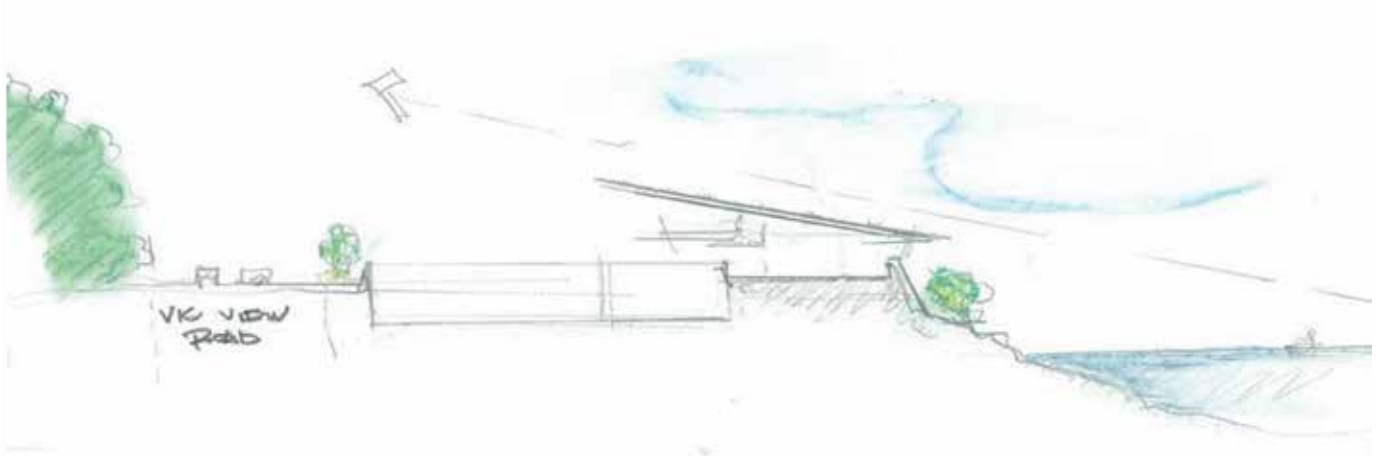
Prominent regional facilities should be models of energy and water efficiency and reduction of greenhouse gas emissions, and such is an objective of this development permit area. This is supported and/or required under the goals established pursuant to the 2007 Capital Region Community Energy Planning process.

28.4 Exemptions

There are no exemptions for McLoughlin Point except for 9.5.4(b)(iii).

28.5 Guidelines

1. Consider the establishment of an 8.0 m buffer from the High Water Mark;
2. Consider the establishment of a 4.0 m heavily landscaped buffer within the 8.0 m buffer to hide the building(s) on the site;
3. Consider stepping buildings back on the site with the lowest buildings (the tanks) located closest to the shore;
4. Consider the establishment of a seawall using as its design precedent, the convention centre in Seattle;
5. Consider the establishment of an historical interpretation program;
6. Consider the establishment of public access to the 8.0 m buffer area via a public dock;
7. Consider the incorporation of water features as public art within the design of the building;
8. Consider design and construction in a manner that mitigates environmental and human health impacts (in particular those related to odour and noise), and contributes to the visual quality and scenic beauty of the harbour entrance; and
9. Any proposed buildings or structures must incorporate the findings of the “Modelling of Potential Tsunami Inundation Limits and Run-up” for the Capital Region that has been completed by the Capital Regional District’s Local Government Emergency Program Advisory Commission.



10. The treatment system will support environmental, social and economic sustainability, and be considered part of CRD climate action initiatives.
11. Wastewater should be treated as a resource and, wherever possible and practical, provide opportunities for resource recovery and reuse.
12. The McLoughlin Point facility should meet, or exceed the CRD's and the Township of Esquimalt's policies on sustainability and building excellence.
13. Acknowledge and plan for major tsunami events, climate warming effects, and post-disaster resiliency.
14. Incorporate durable, long-lasting, and timeless materials and design strategies.
15. Respect view impacts from all sides, and from above.
16. Incorporate designs that, while respecting the site, ensure the highest standards of materials and workmanship, and are aesthetically pleasing.
17. Incorporate public art into the design.
18. Where feasible, design for on-site heat recovery, and plan for future, long-term, neighbourhood, heat-resource opportunities.
19. Incorporate a green roof system into the Operations and Controls building and other buildings, where appropriate.
20. While much of the site is impervious rocky shoreline, where possible, introduce methods to clean and reduce stormwater runoff, incorporate rain gardens, and consider practical ways to re-use water.
21. Restrict impact on the shoreline, except for those areas where wastewater lines enter or exit the treatment plant.
22. Building forms should respect the site.
23. Wall elements, relating to tsunami and associated catastrophic event protection, such as stepped walls that incorporate angled features, projections, wall terraces, and textures, should reflect the character of the rocky shoreline,.

24. The design must demonstrate how the buildings and structures will fit into the site, responding to the shoreline in the forefront, and the evergreen treeline and rocky knoll backdrop.
25. Building heights should vary, but not exceed 15 metres, from the finished grade.
26. Design aesthetics should be optimized with the use of appropriate, high quality materials.
27. Exterior building materials, including exterior details, must be selected to withstand intense weather and sea conditions, and must be of a high standard to ensure low maintenance.
28. Doors, overhead doors, and other closures (including hatches, grilles, and louvres) should be durable, thermally resistant, and suitably finished for the marine environment.
29. Windows should have high performance glazing, and be capable of providing natural ventilation, where appropriate.
30. Roof areas must consider views from above.
31. Clarifiers and aerated filters must be covered to meet noise and odour principles.

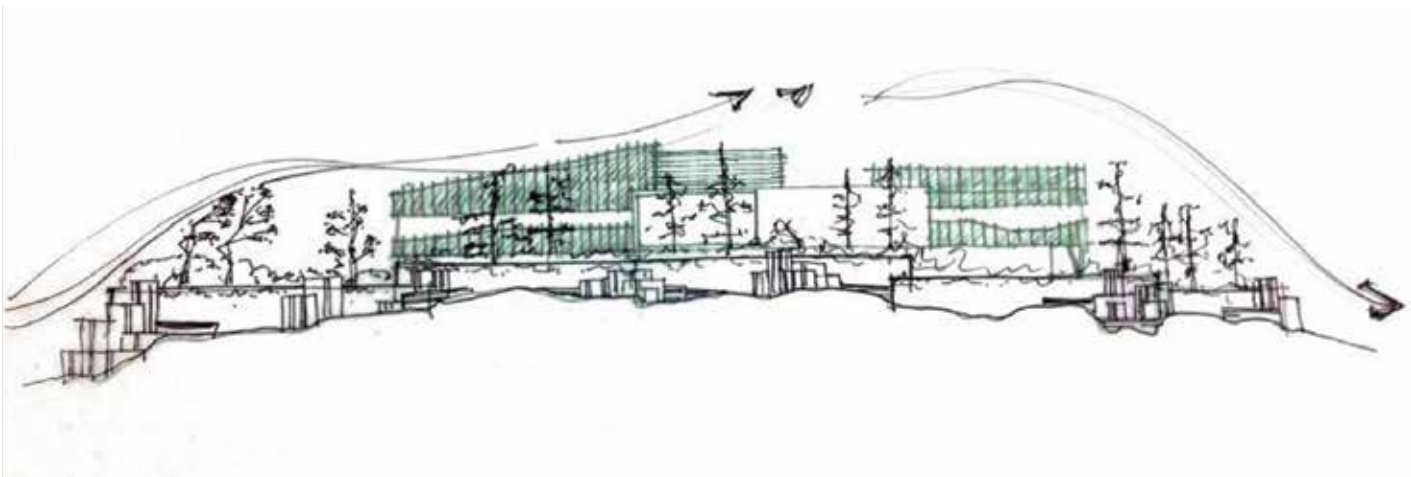


Lighting

1. Light fixtures should provide no more than the minimum lighting needed for their intended purposes, and not exceed levels recommended by the Illuminating Engineering Society for North America Recommended Practice Manual: Lighting for Exterior Environments.
2. Light fixture shields should be specified to reduce impacts on other properties, and when seen from the designated viewpoints.
3. All lighting should be directed downward, and not into the night sky.
4. Energy efficient fixtures should be specified, with consistent colour for all lighting.

Landscape Elements

1. The design concept is based on site conditions, views from the harbour, and a windswept rocky shoreline. With this in mind, landscape elements should include:
2. Use of plant species that are designated hardy to harsh, and for salt spray environments; situate plants such that the force of the wind shapes their future forms;
3. A retaining wall system designed to reflect the rugged and rough-textured surface of boulders and exposed-rock shorelines; and,
4. Outdoor storage and parking areas screened through the use of berms, fences, landscaping and/or solid noise-absorbing barriers.



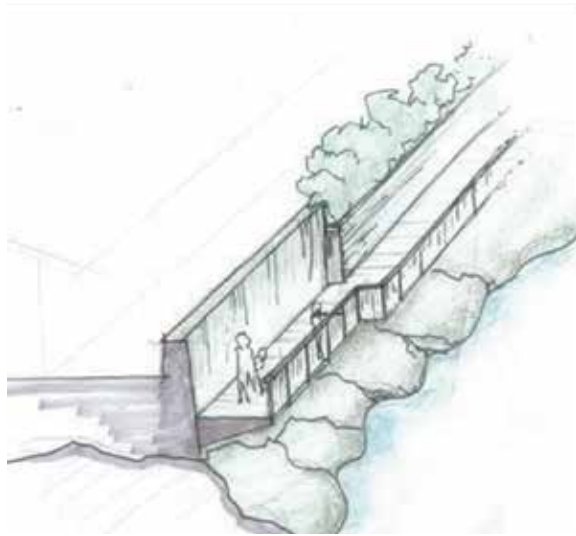
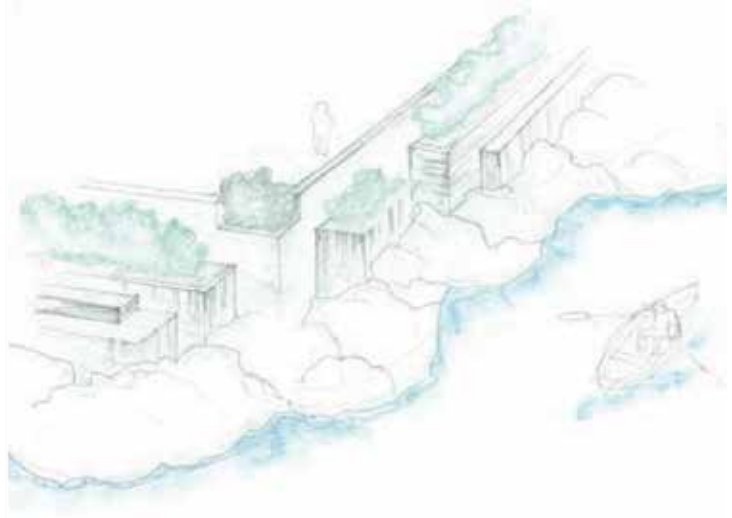
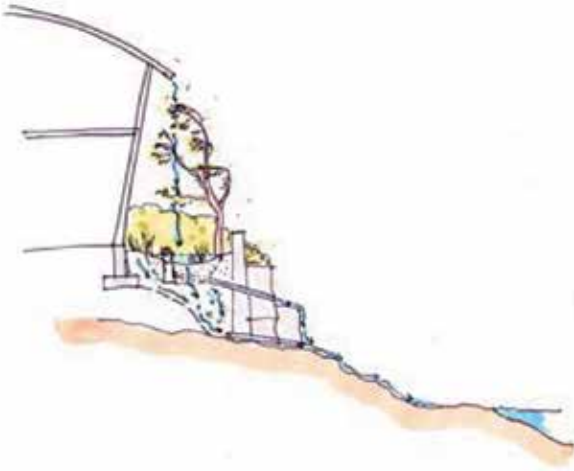
Guidelines for Seawalls and Walls

The retaining wall system should be designed to reflect the rugged and textured surface of the exposed-rock shorelines. To reduce view impacts for neighbouring communities and water/air traffic, the mass of the wall (combined height and width) will be broken up visually with features such as board form relief, wall projections, vertical elements, and wall protrusions. It should feature both rounded, smooth, and angular surfaces to reflect the natural shoreline.

Walls are divided into two types: 1) primary walls, which are prominent perimeter retaining walls, and feature walls within the plant; and 2) secondary walls, which serve as infill between the primary walls.



1. Walls must not protrude beyond the High Water Mark (HWM 1.804m geodetic).
2. The site must be protected by a continuous tsunami protection wall that has a top elevation of not less than 6.5 metres above the High Water Mark.
3. The appearance of wall heights greater than 4.0 metres must be minimized by placing step walls in the tsunami protection wall.
4. Planted stepped walls should be a minimum depth of 1.0 metre horizontally to allow for landscape elements to be introduced. Where this is not possible, shallower multiple steps may be used.
5. All surfaces of the primary perimeter retaining walls must be finished with random board-formed recesses or other suitable architectural treatment. Vertical recesses should be spaced randomly. A smooth finish should be considered for secondary walls.
6. The design should plan for development of a pedestrian pathway along the waterfront side of the site.

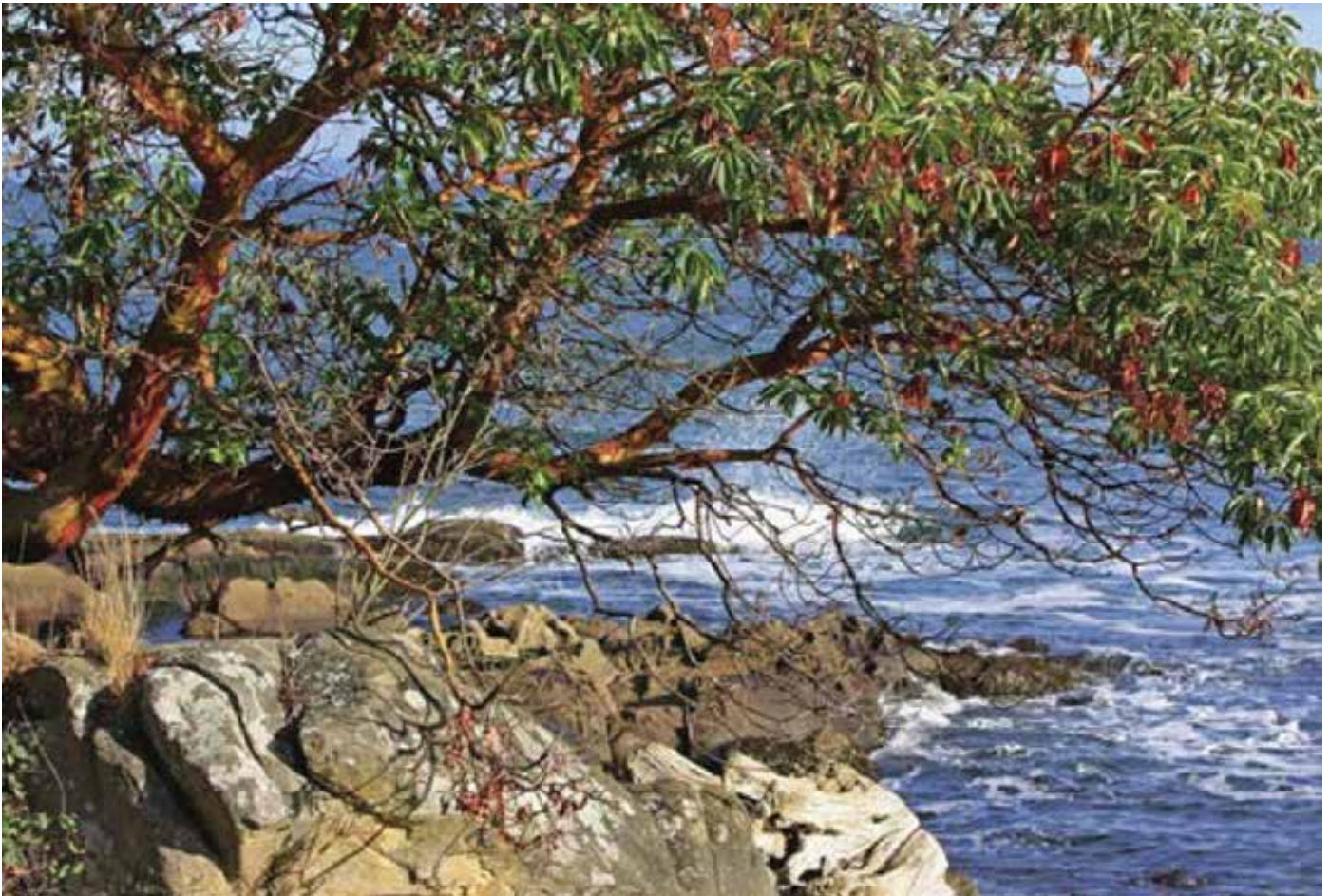


Guidelines for Planting - General

1. Distribution of plants will be limited due to salt spray and wind exposure, particularly on the south side of the site
2. Planting will exclude lawns.
3. Mature plant heights must be at least 60 cm tall for all planted areas to shade undesirable weed species.
4. Planting densities must ensure that vegetated areas will have 100% plant coverage after two full growing seasons.
5. Planted areas will be irrigated with a high efficiency irrigation system.
6. Plants should be drought tolerant and require minimal water after the two-year establishment period.
7. Green roofs must be installed fully established to minimize wind erosion and maintenance.
8. All planting will be to BCNLA/BCSLA Landscape Standards.

Guidelines for Planting Along Seawalls

1. Distribution of plants will be limited due to salt spray and wind exposure.
2. Trees must be situated more than 10 metres from the south facing wall, as this will be a high wind velocity area.
3. The following species are considered appropriate for use along the waterfront:
 - i. *Pinus contorta* var. *Contorta* (Shore Pine)
 - ii. *Arbutus menzesii* (Pacific Madrone)
 - iii. *Rosa nutkana* (Nootka Rose)
 - iv. *Symphoricarpus albus* (Snowberry)
 - v. *Arbutus unedo* (Strawberry Tree)
 - vi. *Myrica californica* (Sweet Gale)
 - vii. *Lonicera pileata* (Privet Honeysuckle)
 - viii. *Mahonia aquifolium* (Oregon Grape)

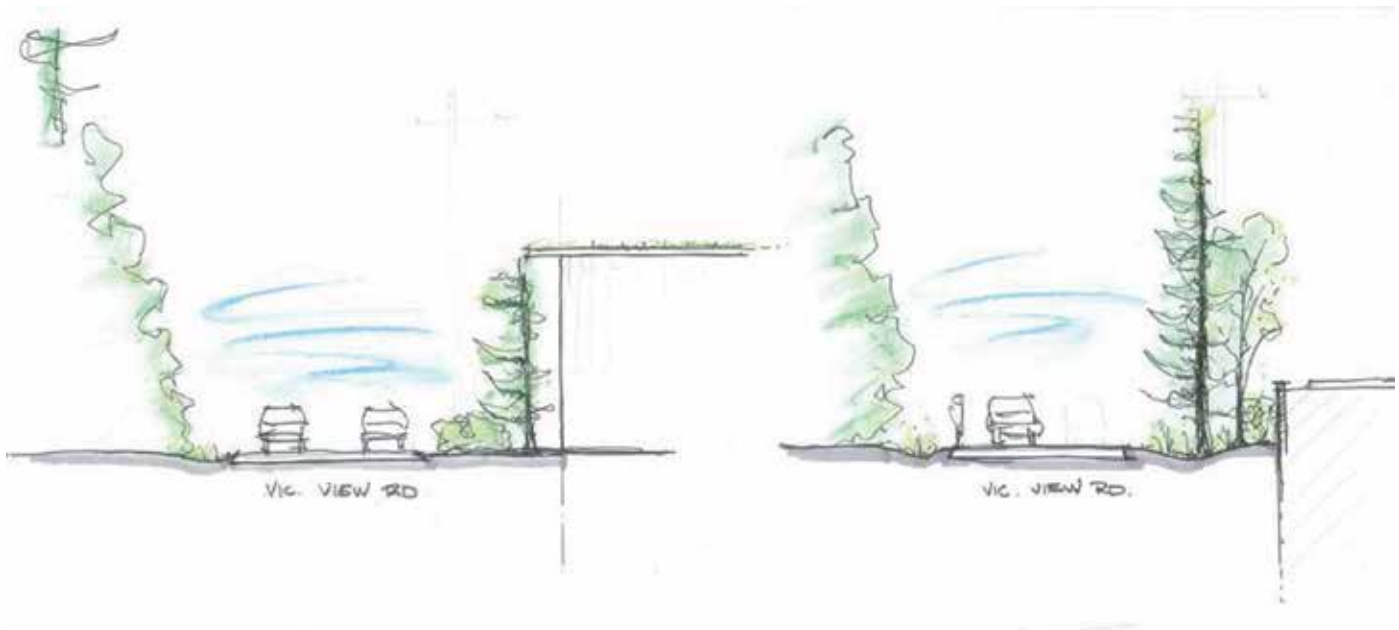


Guidelines for Planting Adjacent to Building Entrances

Planting around the building entrances can be more design driven, and specific hard and soft landscaping should complement the building architecture.

Guidelines for Screening on Victoria View Road

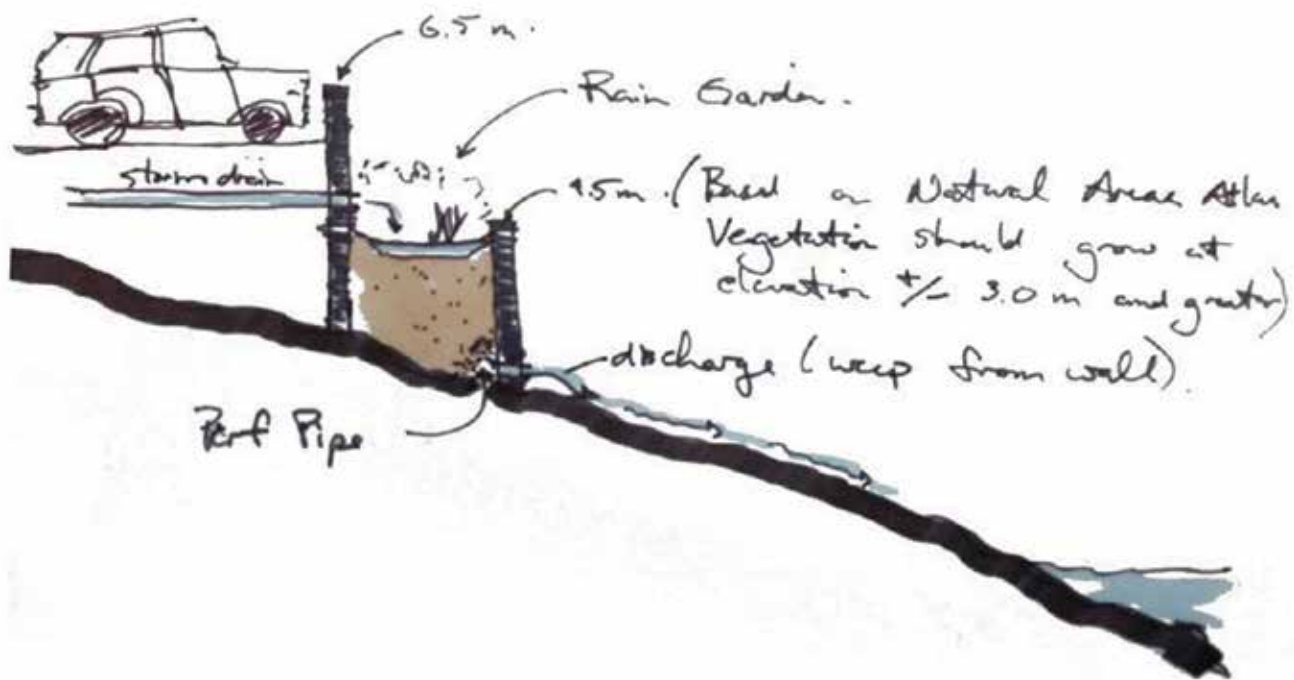
1. To break up the mass of concrete walls, introduce screening (mostly of coniferous tree plantings) along the road frontage and adjacent property lines. The CRD should work with the Department of National Defence to allow for landscaping along the road frontage adjacent to the site. Cluster trees to provide clear 8-metre wide gaps to allow for future maintenance access (from a crane).
2. A continuous shrub border will be required at the base of the wall to screen the lower retaining wall, and reduce the risk of vandalism. Shrubs in this area are to be native species only, with the exception of those adjacent to the two entrances, where lower evergreen screening is desirable.
3. In situations with larger retaining walls, vines can be considered, but must be supported by cable systems.
4. The following species are considered appropriate for use in screening applications:
 - i. *Pseudotsuga menziesii* (Douglas Fir);
 - ii. *Rosa nutkana* (Nootka Rose);
 - iii. *Symphoricarpus albus* (Snowberry); and
 - iv. *Parthenocissus tricuspidata* (Boston Ivy).



Stormwater Management

The following stormwater management measures should be considered for the site:

1. Stormwater from the internal roadways and parking areas will be treated to remove 80% of TSS from a 6-month rain event prior to discharge.
2. Treatment of roadway and parking run-off can come in the form of: • A combination of rain gardens and bioswales adjacent to the parking and roadways, complete with raised overflow basins, and under drains connected to the storm drain system;
3. Aqua-pave permeable paving, complete with an under drain system in discrete areas where direction of run-off to a bioswale is not feasible; or
4. A combination of these.
5. A conventional storm drain will be installed with an outfall to the ocean. All drainage from the site will eventually be discharged through this pipe.
6. The buildings will connect directly to the piped storm drain system. Building drainage will bypass the treatment system. However, a rain garden, stormceptor, or similar end-of-pipe treatment device could be installed if treatment of roof drainage is required.



Parking and Services

Parking for visitors, plant and system operation staff, and CRD maintenance vehicles should be suitably screened through the use of berms, fences, landscaping, and/or solid noise-absorbing barriers to minimize visual impact.

Signage

Limit signage to directional and identification as required for wayfinding.

Public Art and Education

1. Public art shall be provided. The CRD and Township of Esquimalt will work together to confirm the process and requirements.
2. Plans should include capacity for organized, educational site visits to learn about the functioning of the treatment system, the regional liquid waste management program, resource recovery, etc.

OFFICIAL COMMUNITY PLAN: MAPS



Schedule A

Present Land Use Designations

Schedule B

Proposed Land Use Designations

Schedule C

Roads Network

Schedule D

Sewer Network

Schedule E

Water Network

Schedule F

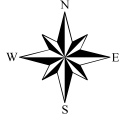
Public Parks and Facilities

Schedule G

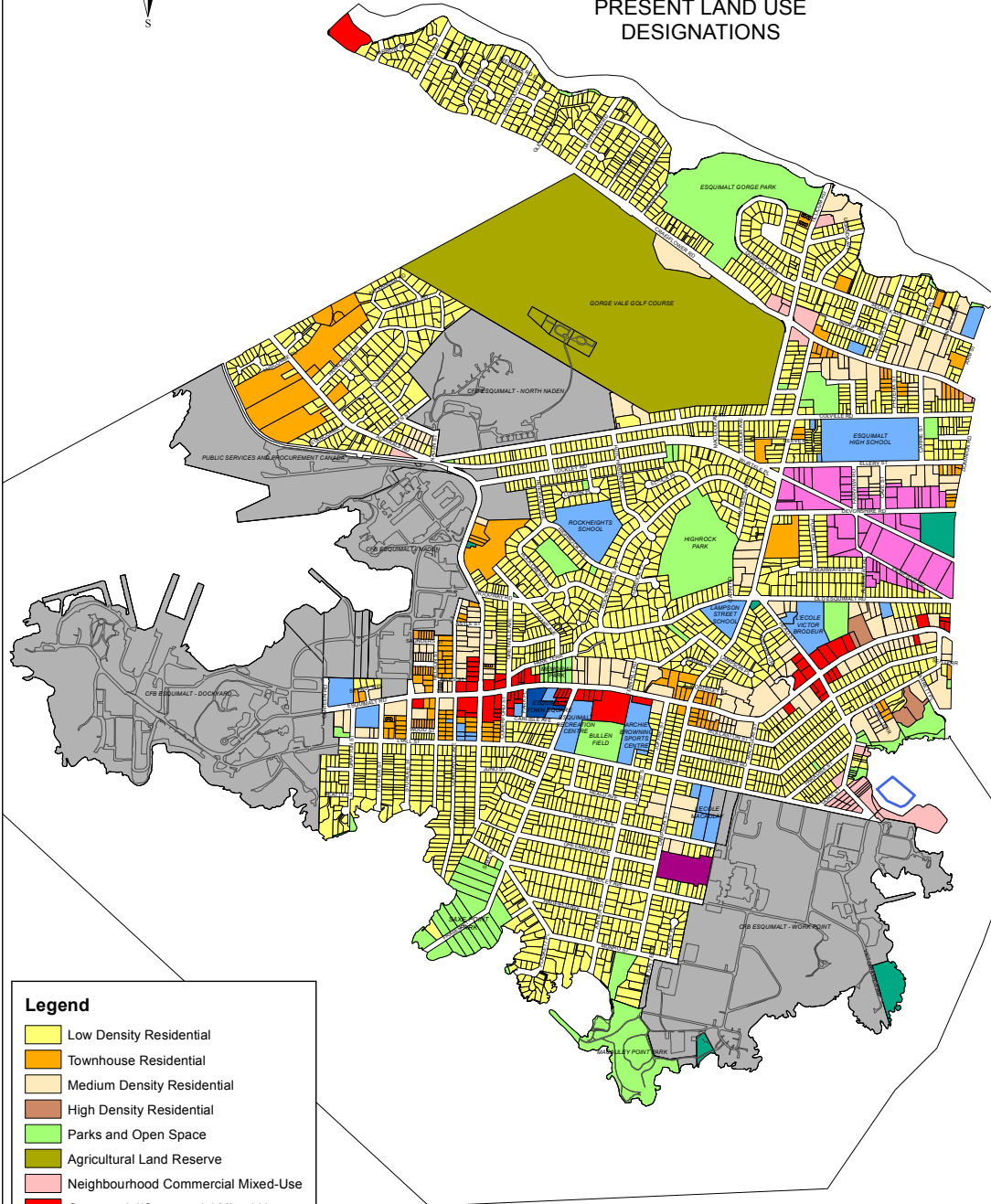
Public View Corridors

Schedule H

Development Permit Areas

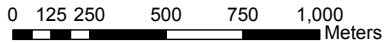


OFFICIAL COMMUNITY PLAN
SCHEDULE A
 PRESENT LAND USE
 DESIGNATIONS



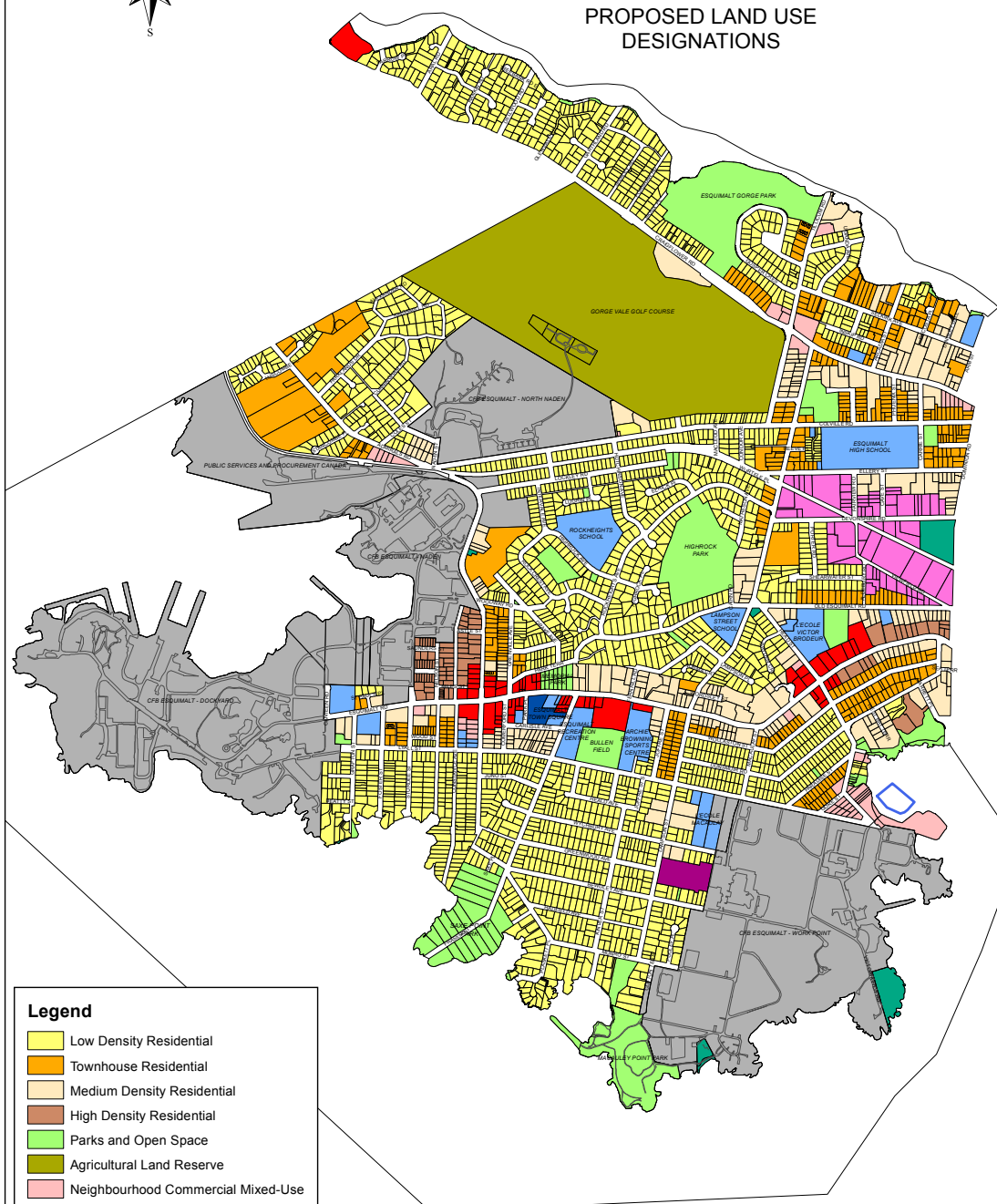
Legend

- Low Density Residential
- Townhouse Residential
- Medium Density Residential
- High Density Residential
- Parks and Open Space
- Agricultural Land Reserve
- Neighbourhood Commercial Mixed-Use
- Commercial/Commercial Mixed-Use
- Industrial
- Utilities
- Institutional
- Federal Land
- English Inn Mixed-Use
- Esquimalt Town Square
- Floathome Residential



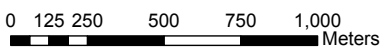


OFFICIAL COMMUNITY PLAN
SCHEDULE B
PROPOSED LAND USE
DESIGNATIONS



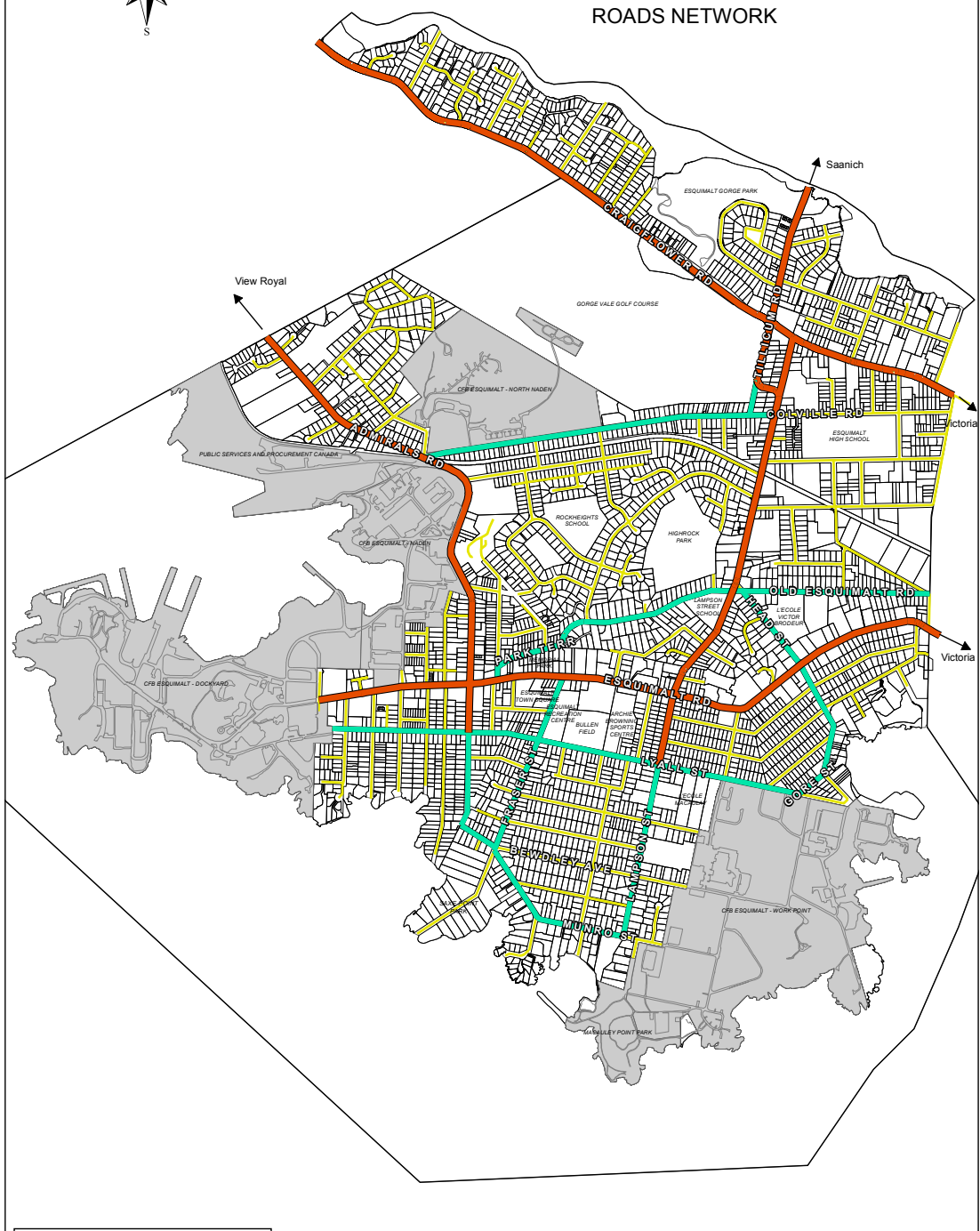
Legend

Low Density Residential
Townhouse Residential
Medium Density Residential
High Density Residential
Parks and Open Space
Agricultural Land Reserve
Neighbourhood Commercial Mixed-Use
Commercial/Commercial Mixed-Use
Industrial
Utilities
Institutional
Federal Land
English Inn Mixed-Use
Esquimalt Town Square
Floathome Residential



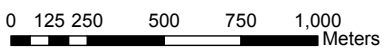


OFFICIAL COMMUNITY PLAN
SCHEDULE C
ROADS NETWORK

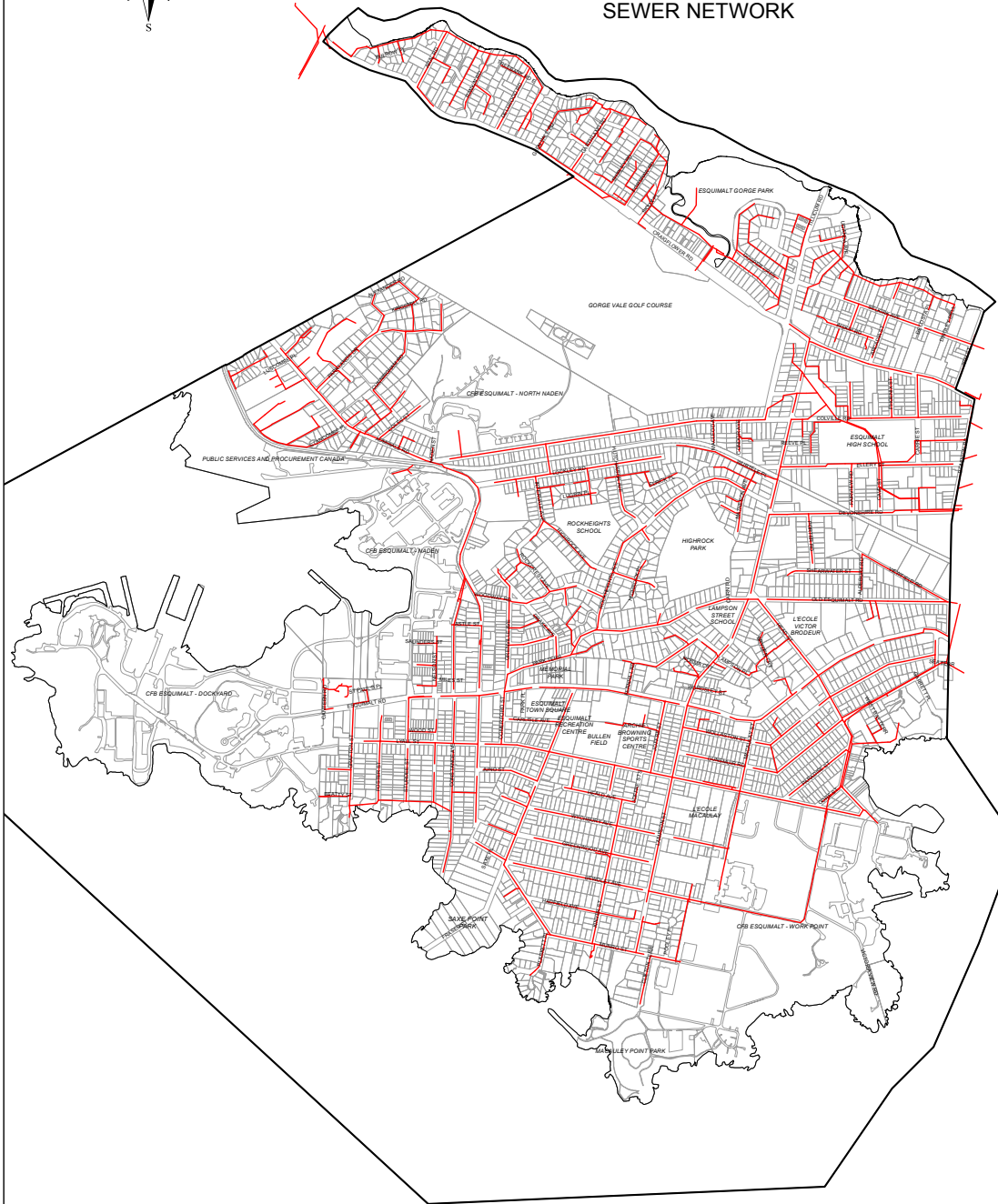


Legend

- Major Roads
- Residential Collector Roads
- Roads
- Township of Esquimalt Boundary

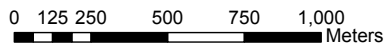


OFFICIAL COMMUNITY PLAN
SCHEDULE D
SEWER NETWORK



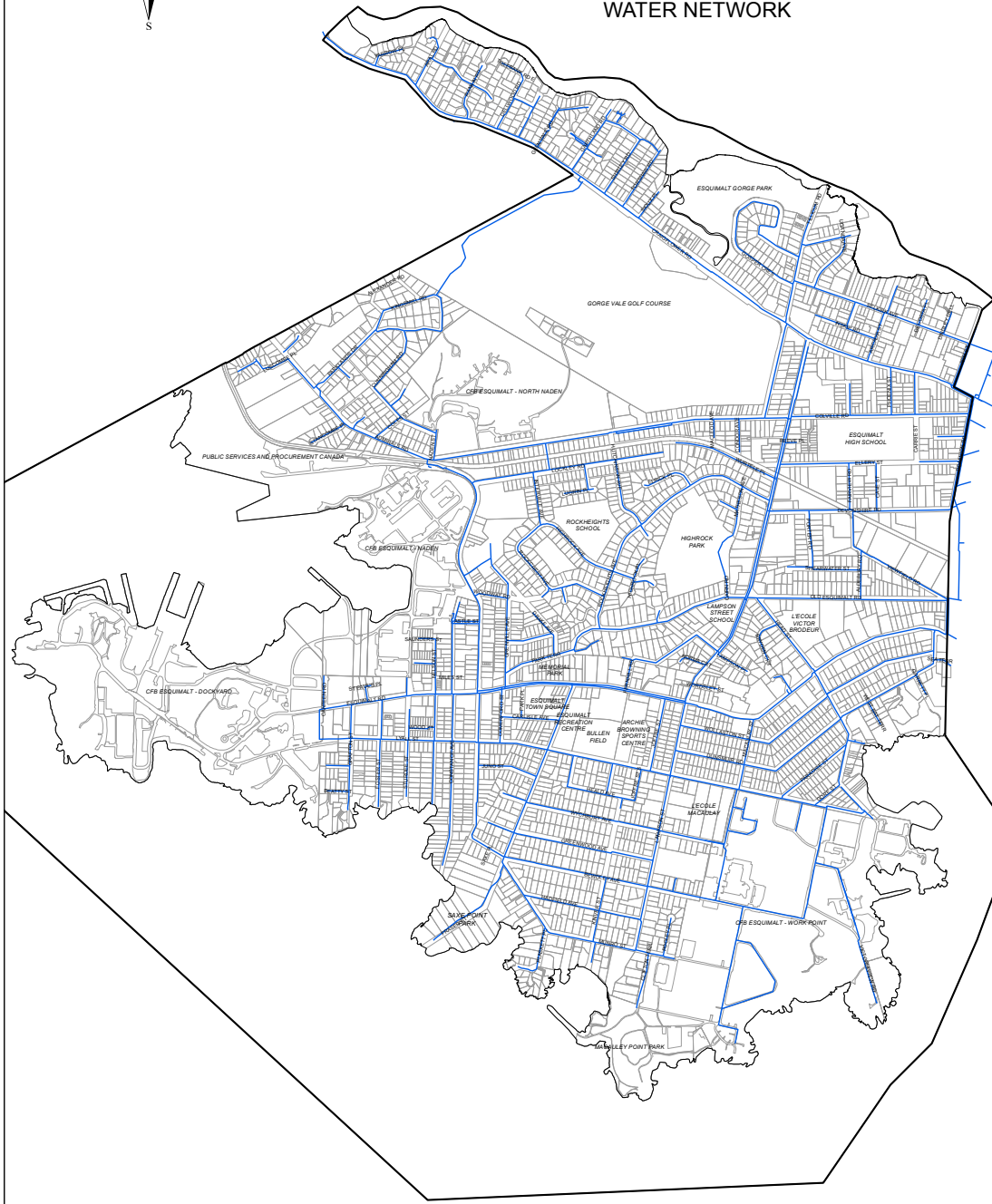
Legend

- Township of Esquimalt Boundary
- Sewer Mains



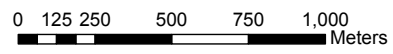


OFFICIAL COMMUNITY PLAN
SCHEDULE E
WATER NETWORK



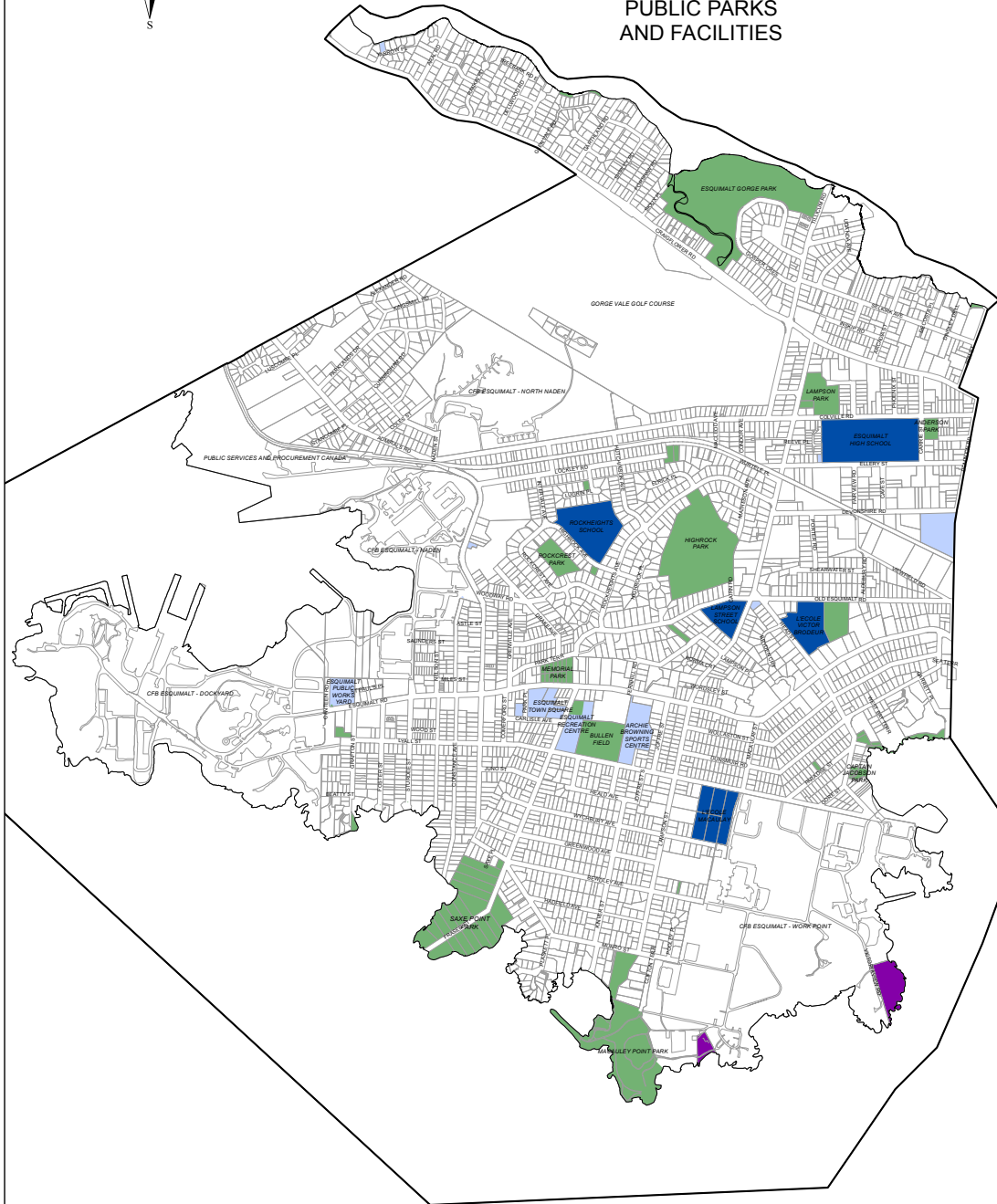
Legend

- Township of Esquimalt Boundary
- Water Mains



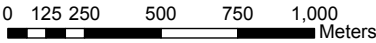
OFFICIAL COMMUNITY PLAN

SCHEDULE F
PUBLIC PARKS
AND FACILITIES



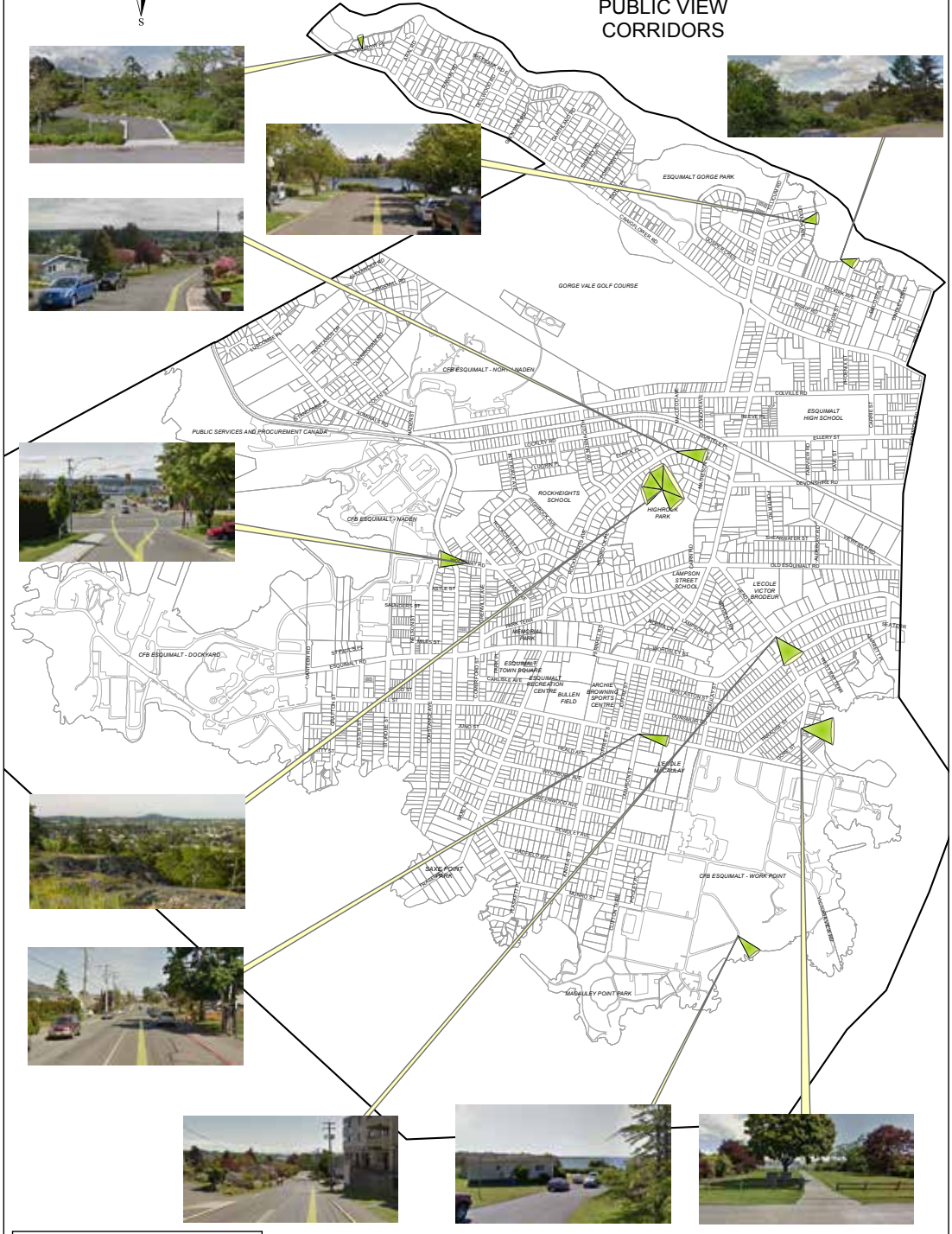
Legend

- Township of Esquimalt Boundary
- Parks
- Schools
- Public Facilities
- Waste Treatment or Disposal Sites



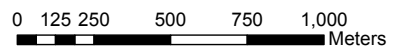


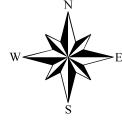
OFFICIAL COMMUNITY PLAN
SCHEDULE G
PUBLIC VIEW
CORRIDORS



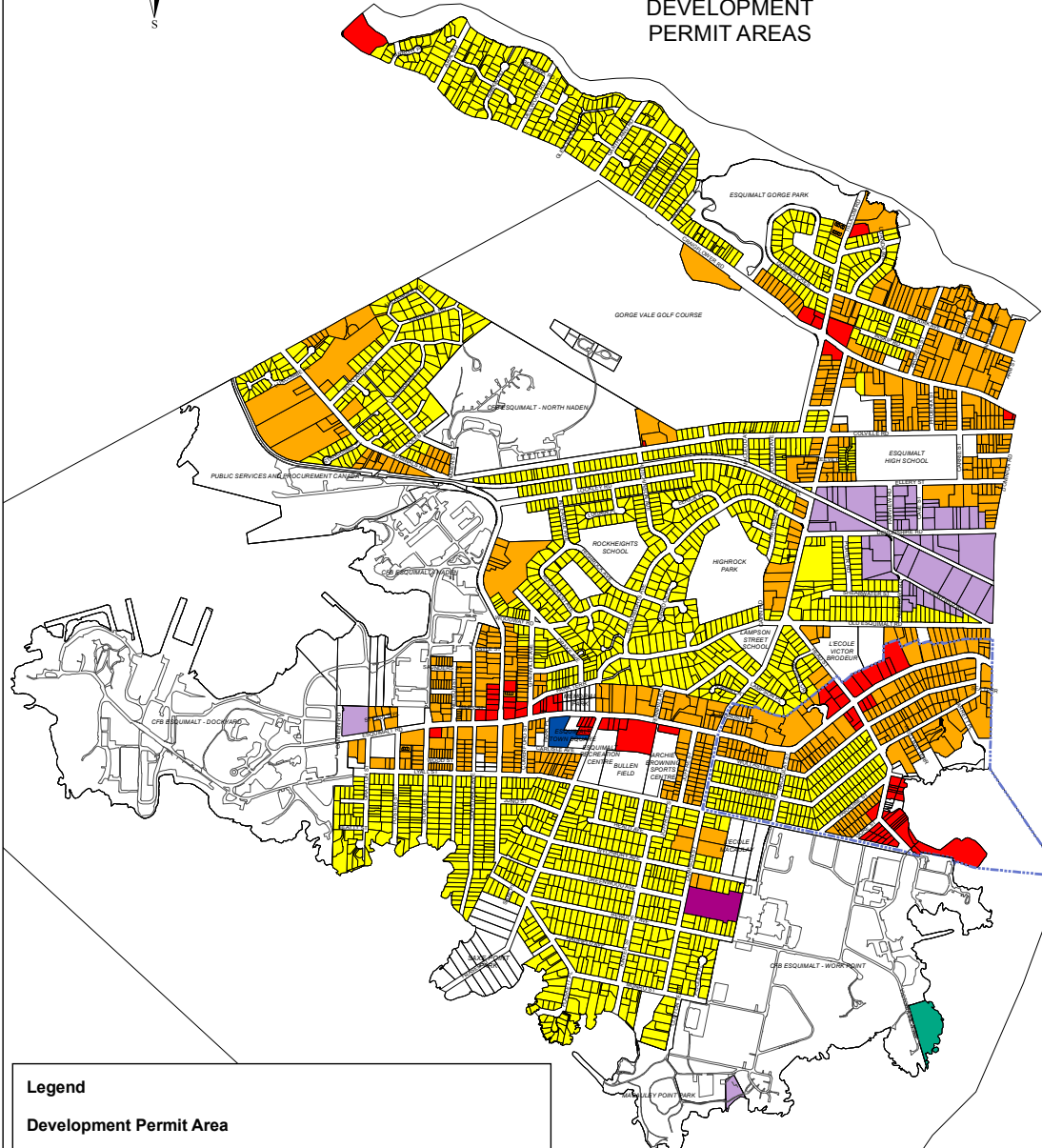
Legend

- Township of Esquimalt Boundary
- View Cones





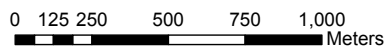
OFFICIAL COMMUNITY PLAN
SCHEDULE H
DEVELOPMENT
PERMIT AREAS



Legend

Development Permit Area

- 1 - Natural Environment - Entire Township
- 2 - Protection of Development from Hazardous Conditions
- 3 - Enhanced Design Control Residential
- 4 - Commercial
- 5 - Industrial
- 6 - Multi-Family Residential
- 7 - Energy Conservation and Greenhouse Gas Reduction - Entire Township
- 8 - Water Conservation - Entire Township
- 9 - English Inn
- 10 - Esquimalt Town Square
- 11 - West Bay
- 12 - McLoughlin Point



APPENDICES



Appendix One:

Draft of Development Permit Area No. 11 (Section 27 - West Bay) with design renderings (Pg 114 - 156)

Appendix One

27.0 Development Permit Area No. 11 - West Bay

27.1 Area

All lands outlined and indicated as West Bay on Schedule “H” are part of DPA No. 11.

27.2 Designations

Development Permit Area No. 11 is designated for the purpose of establishing objectives for:

Development Permit Area No. 11 is designated for the purpose of:

Section 488(1)(a) – protection of the natural environment, its ecosystems and biological diversity;

Section 488(1)(b) – protection of development from hazardous conditions;

Section 488(1)(d) – revitalization of an area in which a commercial use is permitted;

Section 488(1)(e) - establishment of objectives for the form and character of intensive residential development;

Section 488(1)(f) – establishment of objectives for the form and character of commercial and multi-family residential development;

Section 488(1)(h) – establishment of objectives to promote energy conservation;

Section 919.1(1)(i) – establishment of objectives to promote water conservation; and

Section 919.1(1)(j) – establishment of objectives to promote the reduction of greenhouse gas emissions.

27.3 Justification

The West Bay Neighbourhood is a neighbourhood with unique characteristics that is under increasing development pressure. In order to retain the form and character of the neighbourhood while at the same time allowing redevelopment to occur a set of robust design guidelines is required. In addition, the neighbourhood’s costal location means that there is potential harm to ecosystems which can be mitigated through the judicious use of guidelines. Redevelopment also offers opportunities to conserve energy and water and reduce greenhouse gas emissions. Finally, due to its adjacency to a marine environment, it is subject to hazards associated with Tsunamis and sea level rise.

27.4 Exemptions

All exemptions in underlying development permit areas apply.

27.4 Guidelines

27.5.1 GUIDELINES: COMMERCIAL AND MIXED-USE BUILDINGS

1. Locate publicly oriented active uses at grade and at or near the sidewalk edge.
2. Incorporate transparent shop-front windows, frequent entrances, weather protection and pedestrian oriented signage into ground floor facades;

3. A signage and lighting program for any commercial development should be designed as a totality, with signs, lighting, and weather protection architecturally integrated from the outset.



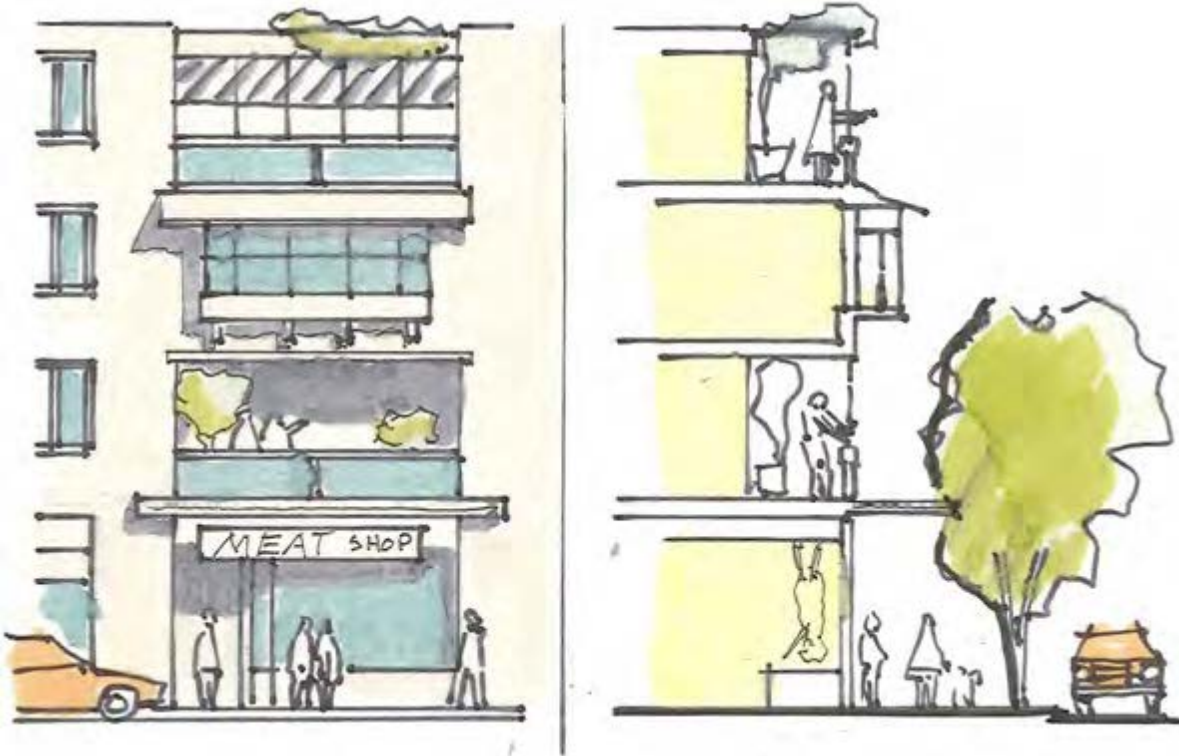
Active uses oriented to public streets and open spaces creates vibrant and attractive neighbourhoods (above)

4. Provide pedestrian access to storefronts and businesses from the adjacent public street, and orient upper-storey windows and balconies to overlook adjoining public open spaces.

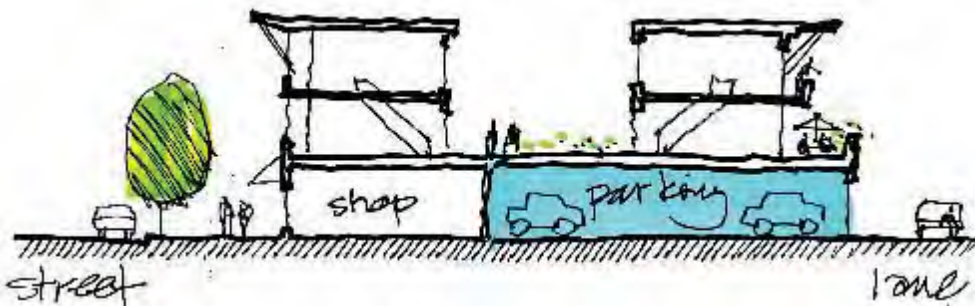
5. On corner sites, develop street-facing façades for both streets. Design front elevations with pronounced entrances oriented to the corner and/or primary streets.

6. Avoid locating off-street surface or structured parking adjacent to active public streets and open spaces. Locate off-street parking behind or underneath buildings. Laminate or wrap any above ground structured parking with active (residential or commercial) uses to buffer structured parking from public open spaces.

7. Achieve a minimum glazing area of 75% for frontages at grade along all commercial streets. Clear site lines from inside buildings to open public spaces should allow for casual surveillance of the street and sidewalk, and store interiors should be visible from the street.



Frequent entrances along the street and upper storey balconies overlooking public open spaces adds to street vitality.



A shop front buffers public space from above ground structured parking.



Orient active frontages towards streets.



Photo: Joaquin Karakas

On corner sites, develop street-facing façades for both streets.

8. Incorporate frequent entrances into commercial frontages facing public streets with a desired maximum spacing of 10 m.

9. Recessed entrances to buildings from the sidewalk or property line are encouraged in order to provide for door swings, to protect the entrance from rain or snow, and to emphasize building entrances.

1. Incorporate plantings, attractive lighting, signage, paving details, furnishings, street trees and other landscape details to create a comfortable, attractive, unique and well defined public realm.



Transparent shop fronts with lots of glazing and frequent entrances help create a welcoming and attractive streetscape (above).



Paving details, pedestrian oriented signage, furnishings and other landscape details add character.

11. Avoid expansive blank walls (over 5 m in length) and retaining walls adjacent to public streets. When blank walls and retaining walls are unavoidable, use an appropriate design treatment, such as the following:

- a) Install a vertical trellis in front of the wall with climbing vines or

other plant material

- b) Set the wall back slightly to provide room for evergreens and conifers to provide year-round screening
- c) Provide art (a mosaic, mural, relief, etc.) over a substantial portion of the wall surface
- d) Employ quality materials of different textures and colours to make the wall more interesting visually
- e) Provide special lighting, canopies, awnings, horizontal trellises or other human-scale features that break up the size of the blank wall surface and add visual interest
- f) Incorporate walls into a patio or sidewalk café space
- g) Terrace (step down) retaining walls



Incorporate plantings and other landscape details such as banners.



When unavoidable, blank walls should be screened with landscaping or through incorporate of a patio cafe or special materials to make it visually more interesting.



27.5.2 GUIDELINES: RESIDENTIAL BUILDINGS

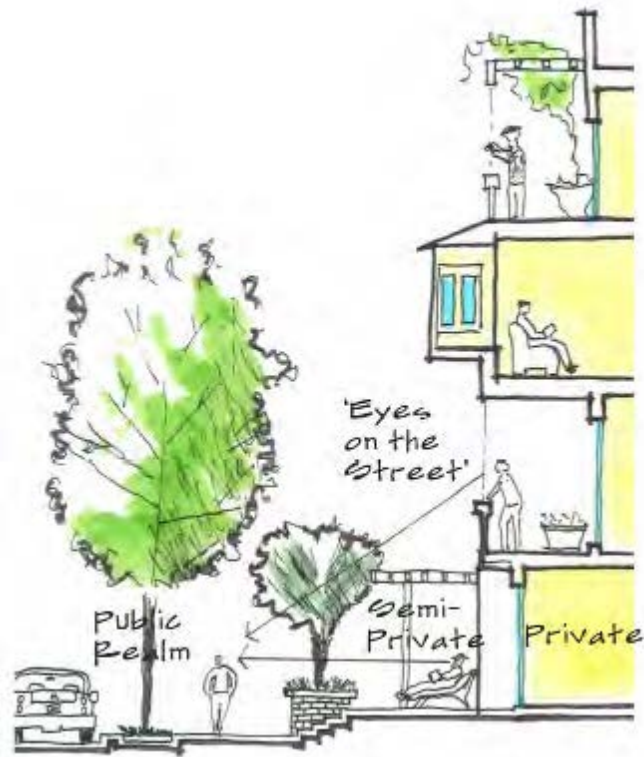
1. Site and orient multi-plex, townhouse and apartment buildings to overlook public streets, parks, walkways, and communal spaces, while ensuring the security and privacy of residents.
2. incorporate individual entrances to ground floor units in residential buildings that are accessible from the fronting street. This provides easy pedestrian connections to buildings, encourages street activity and walking, and enhances safety.
3. Residential entries should be clearly visible and identifiable from the fronting public street to make the project more approachable and

create a sense of association amongst neighbours.

4. Emphasize front doors by incorporating a front patio or stoop and orienting front entryways prominently towards public streets and open spaces.

5. Incorporation of a semi-elevated front entry way (1 m - 1.5 m) is encouraged to create a semi-private entry or transition zone to individual ground floor units. For these units, ensure an alternate access point that is accessible by wheelchair.





Incorporate a front patio or stoop to create street activity and association among neighbours (above, below and below left)



6. Locating off-street surface parking behind or underneath buildings. Off-street surface parking located between the front of the building and the public sidewalk or adjacent to other public openspaces is strongly discouraged and should be avoided. When parking is accessed from the fronting public street, recess parking garages and entrances from the front face of buildings.

7 A landscaped transition zone in between the entryway and public sidewalk should be considered on streets with high traffic volumes.

8 Apartment lobbies and main building entries should be clearly visible from the fronting street with direct sight lines into them.

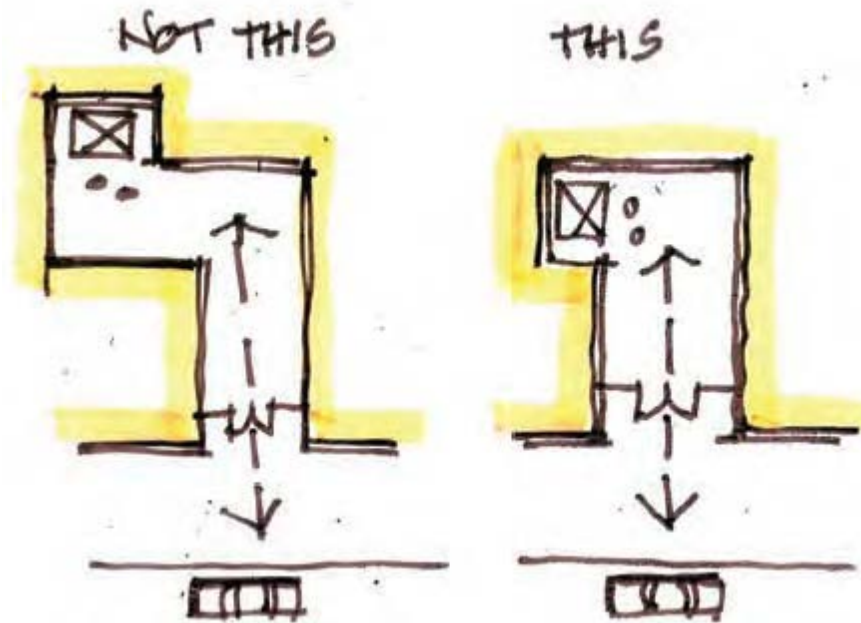
Where possible, apartment lobbies should have multiple access points to enhance building access and connectivity with adjacent open spaces.



Locate off-street parking behind or underneath buildings, but never between the front face of a building and the fronting public street.



Consider using landscape transition zone between entryways and public sidewalks.



Direct sight lines into elevator lobbies are safer

27.5.3 VISUAL & PHYSICAL CONNECTIONS TO THE HARBOUR

- 1 Physical and visual connections to landmark buildings, landscape features, the harbour, seascape, and other surrounding natural features are important components of West Bay's character and identity and therefore should be preserved and enhanced.

- 2 New development and landscaping should frame rather than block public views of parks and openspaces, natural features, prominent buildings, public art, and the harbour.

- 3 Locate and design buildings to preserve public street-end views (and where possible private views) to the harbour.

- 4 Where possible, create new public connections to harbourfront uses and activities at the waters edge, specifically Sailor's Cove, Hidden Harbour, and West Bay Marina's.

- 5 Mark/celebrate corners and street-end views through building and open space design.

- 6 Water access and views to the West Bay harbourfront and upland neighbourhood from the water are equally important elements of West Bay's identity. Therefore future development must consider visual and physical connections to the neighbourhood from the water in considering future development.

7 New development adjacent or near to the harbourfront should respond to relevant sections of the The Province of B.C. “Flood Hazard Area Land Use Management Guidelines”.

Photo: Franc D’Ambrosio Architecture and Urbanism



Create new public connections to harbour front uses and activities.

Photo: Bill Brown



Maintain street-end views to the harbour-front

27.5.4 NEIGHBOURLINESS

Buildings should respect adjacent properties by siting and designing new development to minimize disruption of the privacy and out-door activities of residents in adjacent buildings, and by ensuring buildings are sited to compliment the type, scale, and use of adjacent buildings.

1 New projects should provide a sensitive transition to near-by, less-intensive zones or areas with different uses. Projects on zone edges should be developed in a manner that creates a step in actual or perceived height, bulk, and scale between the anticipated development potential of adjacent zones.

2 Buildings and groups of buildings should step down to be

similar in height to adjacent buildings. This allows for an effective transition in scale and adequate sunlight penetration into open spaces and adjacent properties.

3. In a mixed use project adjacent to a less intensive zone, the more compatible use and building type should be sited near the zone edge.

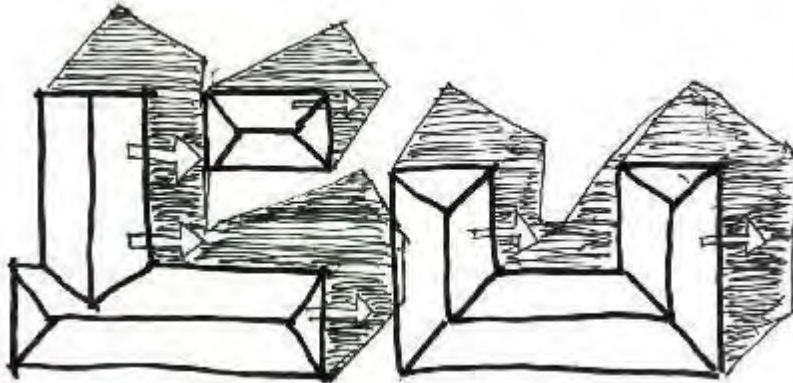
4. Face similar uses across the street and at compatible scales; avoid building scale differences of more than 2 storeys across streets.

5. Locate development to minimize view impacts on existing and planned future development.

6 Buildings should be positioned and scaled to minimize the impact of shadows on adjacent open spaces, buildings, and within the project.



Transition in scale to existing context

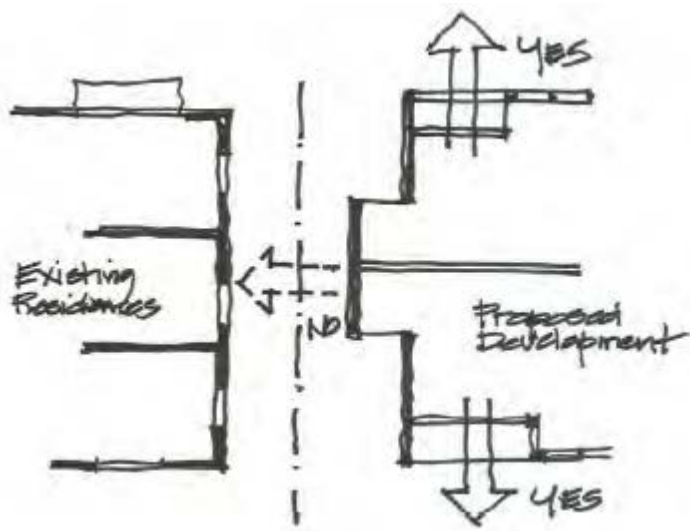


Sun shade analysis and diagrams help determine the siting of buildings to minimize overshadowing of adjacent open spaces and building.

7. Locate open space (plazas, parks, patios, cafes, etc.) south of permanently shading structures.
8. Locating off-street surface parking in front of buildings, at prominent corners or intersections, immediately adjacent to public sidewalks and open spaces, and other public oriented active openspaces is strongly discouraged and should be avoided.
9. Minimize impacts from sloping sites on neighbouring development. Examples of treatments to minimize impacts include using terraced retaining walls of natural materials, or stepping a building to respond to the slope.
10. Views from upper stories of new buildings should minimize

overlook into adjacent private yards, especially in less intensive areas. Following are some strategies which can be used to achieve this guideline:

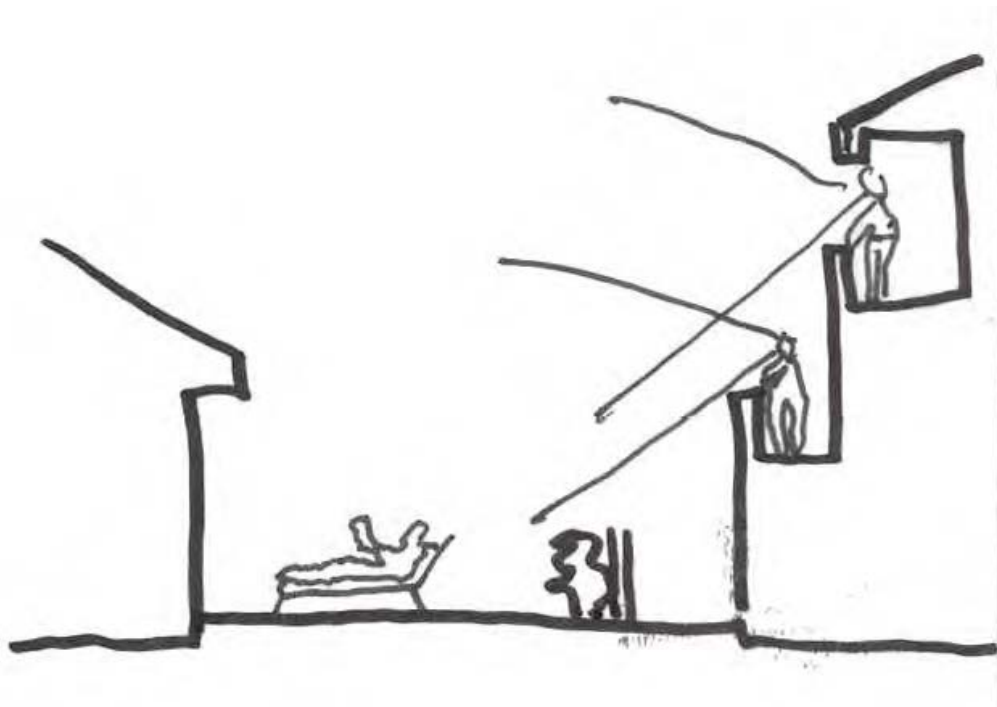
- i) Increase building separation so that the face of the building and hence the windows are setback farther from the property line.
- ii) Take advantage of site design that reduces impacts by using, for example, an adjacent ground floor area for an entry court.
- iii) Stagger windows to not align with adjacent, facing windows.
- iv) Primary windows into habitable spaces should not face interior side-yards



Protect privacy by placing primary (view) windows towards front and rear yards rather than interior side yards.



This building steps down to the slope to reduce the visual impact from its massing



Minimize overlook into adjacent private yards

**27.5.5 ARCHITECTURAL CONCEPT: ACHIEVING A HUMAN SCALE
OVERVIEW AND INTENT**

These general guidelines for architectural are not intended to be prescriptive, but rather to encourage flexibility and innovation in building design and character. The overall intent is to create buildings and other structural elements that are scaled to the pedestrian, encourage pedestrian activity and welcome users.

Human Scale

Achieving human scale refers to the use of architectural features, details, and site design elements that are of human proportion and clearly oriented for pedestrian activity. A building has good human scale if its details, elements, and materials allow people to feel comfortable using and approaching it.

Building Articulation

Many street frontage design elements, both horizontal and vertical, help to create an interesting and welcoming streetscape. These include building materials, special ground floor design treatments, façade modulation, corner treatments, building step-backs for upper storeys, and façade elements such as window treatments, building entries, and other architectural details. All of these help define the public realm as a welcoming place.



Human scale elements and building articulation arranged in modern composition

GENERAL GUIDELINES

1. The design of new buildings and renovated existing buildings should express a unified architectural concept that incorporates both variation and consistency in façade treatments (for example, by articulating façades into a series of intervals).
- .2. Design buildings to express their internal function and use.
3. Incorporate into building façades a range of architectural features and design details that are rich and varied to create visual interest when approached by pedestrians.

Examples of architectural features include:

- i. Building height, massing, articulation and modulation
- ii. Bay windows and balconies
- iii. Corner features accent, such as turrets or cupolas
- iv. Decorative rooflines and cornices
- v. Building entries
- vi. Canopies and overhangs

Examples of architectural details include:

- i. Treatment of masonry (ceramic tile, paving stones, brick patterns, etc.)
- ii. Treatment of siding (for example, the use of score lines, textures, and different materials or patterning to distinguish between different floors)
- iii. Articulation of columns and pilasters
- iv. Ornament or integrated artwork
- v. Integrated architectural lighting
- vi. Detailed grilles and railings
- vii. Substantial trim details and moldings
- viii. Trellises and arbors



Incorporation of a range of architectural details and features make this mixed-use building attractive when approached by pedestrians

4. Locate and design entrances to create building identity and to distinguish between individual commercial and residential ground floor units. Use a high level of architectural detail and, where appropriate, landscape treatment to emphasize primary entrances and to provide “punctuation” in the overall streetscape treatment.
5. Design balconies as integral parts of buildings and to maximize daylight access into dwellings through the use of glazed or narrow metal spindle guardrails.
6. Clearly distinguish the roofline from the walls of buildings (for example, through the use of a cornice, overhang, or decorative motif).
8. Windows can be used to reinforce the human scale of architecture by incorporating individual windows in upper storeys that:
 - i. Are vertically proportioned and approximately the size and proportion of a traditional window
 - ii. Include substantial trim or molding
 - iii. Are separated from adjacent windows by a vertical element
 - iv. Are made up of small panes of glass
 - v. Are separated with moldings or jambs but grouped together to form larger areas of glazing
9. The use of figured or frosted glass or tinted glazing is discouraged for windows facing the street except for compatible use of stained glass or where figured or frosted glass comprises a maximum 20% of the glazing. This creates a welcoming, visually interesting and transparent street frontage.
10. In general, new buildings should incorporate natural building

materials into façades to avoid a “thin veneer” look and feel, and combined with more modern treatments, such as glass, concrete, and steel.

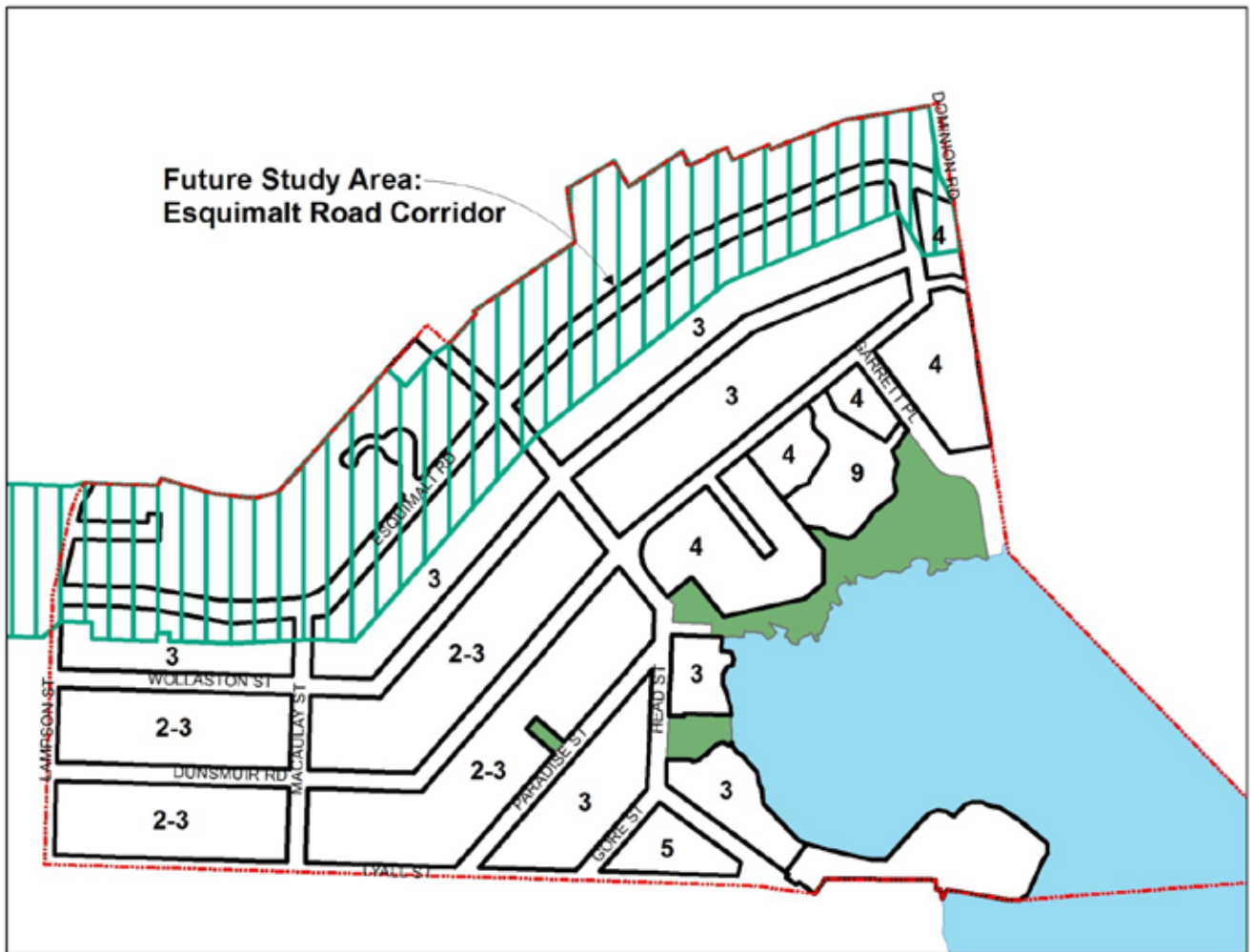
11. Vinyl siding, large expanses of stucco, swirl type stucco, and vinyl for window frames are generally discouraged.



Design entrances to distinguish between individual commercial and residential.

Height

Note that the heights shown are stories and are guidelines. They are not height regulations.

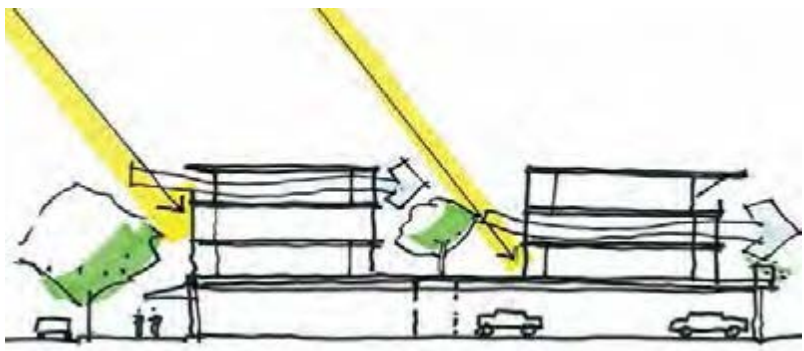
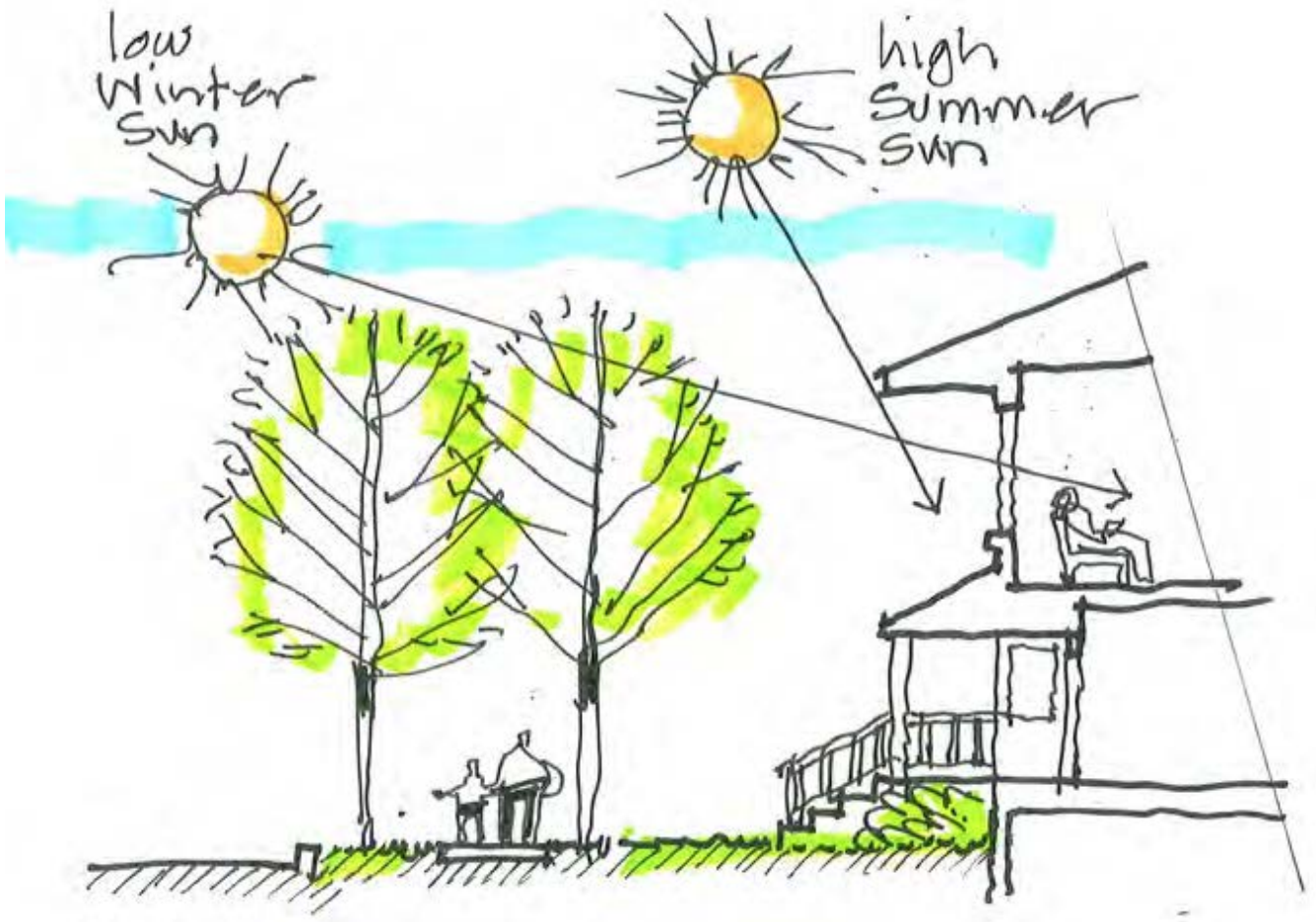


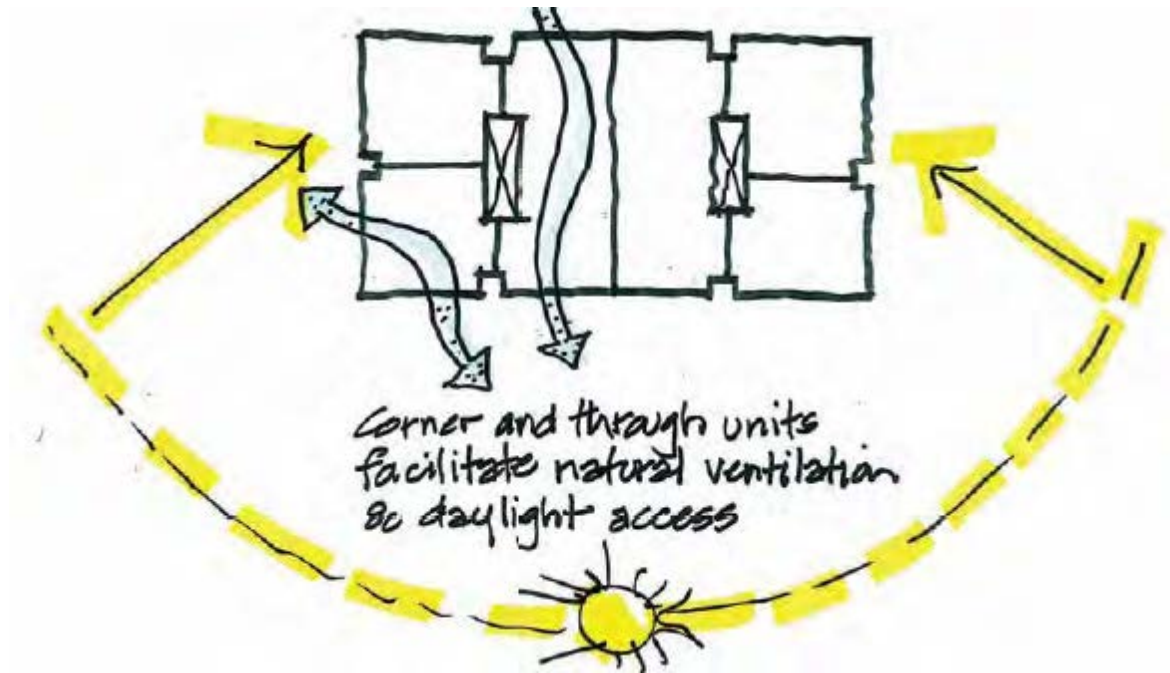
29.5.6 Green Healthy Buildings and Open Spaces

11 Building design and site planning should reduce the overall “ecological footprint” (energy use, waste, and pollution) of new development while also maximizing liveability. This can be achieved by maximizing passive lighting, heating and cooling, providing usable outdoor amenity spaces, and being responsive to the existing ecosystems and natural context.

2. Design residential buildings to receive daylight and natural ventilation from at least two sides of the building, or from one side and a roof. Where possible, dwellings should have a choice of aspect: front and back, or on two sides (for corner units).

3. Dwelling units with exterior access on only one side should always face a good view or the direction of the sun (ideally both) and are most suitable as wide frontages with shallow floor plans to allow adequate penetration of daylight.





Incorporate common areas as defining elements of projects.

27.5.6.3 New buildings should not block significant views or solar access to adjacent buildings and open spaces.

27.5.6.4 Incorporate courtyards, greenways, gardens and other common areas as defining elements of projects.

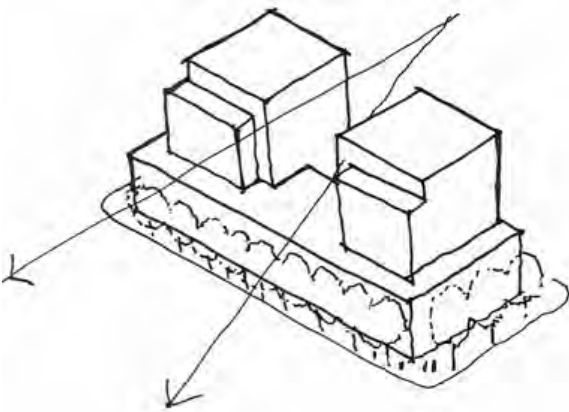
27.5.6.5 Where at-grade space is limited, rooftop patios, gardens and courtyards are encouraged.

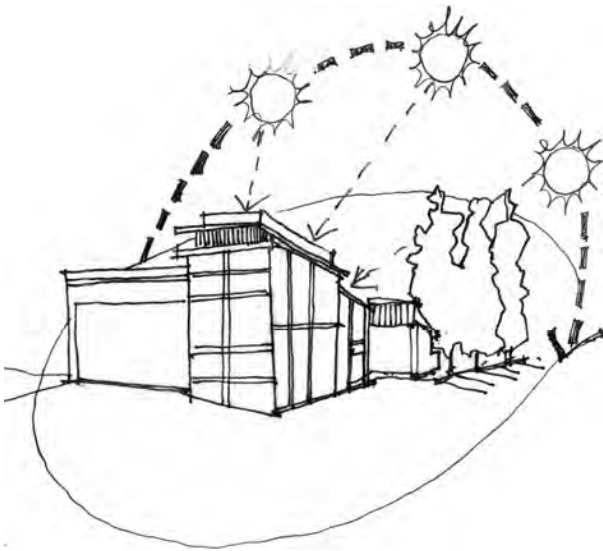
27.5.6.6 Retention and infiltration best management practices for rainwater should be used as appropriate.

27.5.6.7 Residential buildings should incorporate direct access to a usable private outdoor space such as a patio, balcony, or upper level terrace.



A landscaped pathway located adjacent to this residential building partially screens the lower floor units from the park (foreground) while also softening the building's appearance, helping to integrate it into the landscape.





Use best management practices for rainwater where appropriate



Urban village High Street looking north-west at corner of Head and Lyall Street



Urban village High Steet (Head Street South) looking south



Looking north to harbour and urban village high street



View towards urban village High Street from the harbour looking west



View from future building looking south-east to harbour across urban village High Street and Captain Jacobson Park



Head Street looking south towards Gore and Lyall



Urban village High Street: conceptual siting, form and massing



Urban village High Street: conceptual siting, form and massing



Urban village High Street: conceptual siting, form and massing

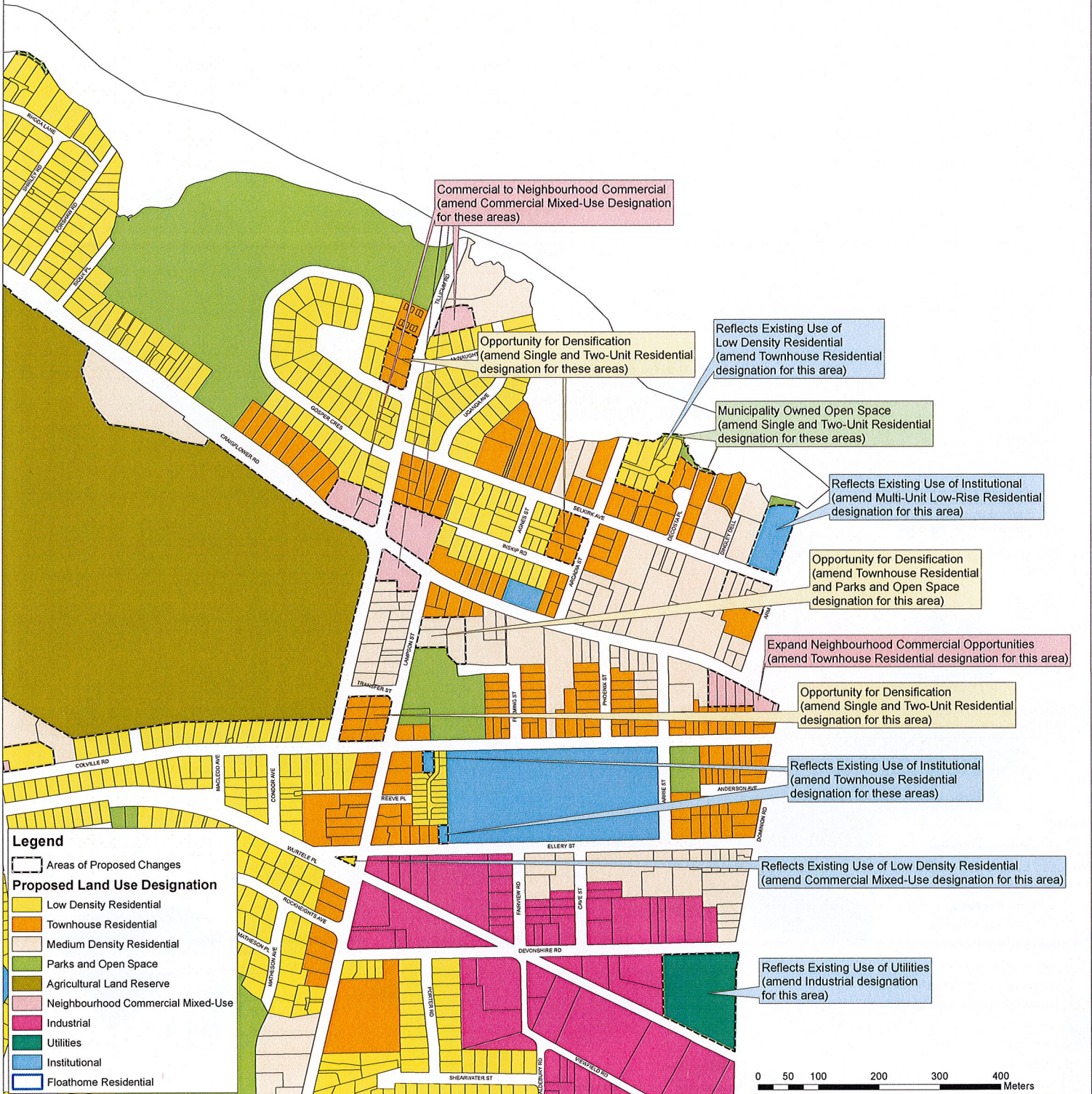
Township of
ESQUIMALT

The logo for the Township of Esquimalt features the words "Township of" in a smaller, blue, sans-serif font above the word "ESQUIMALT" in a large, dark blue, serif font. A stylized, dark blue wave graphic is positioned below the letters "SQUIMALT", starting under the 'S' and ending under the 'T'.

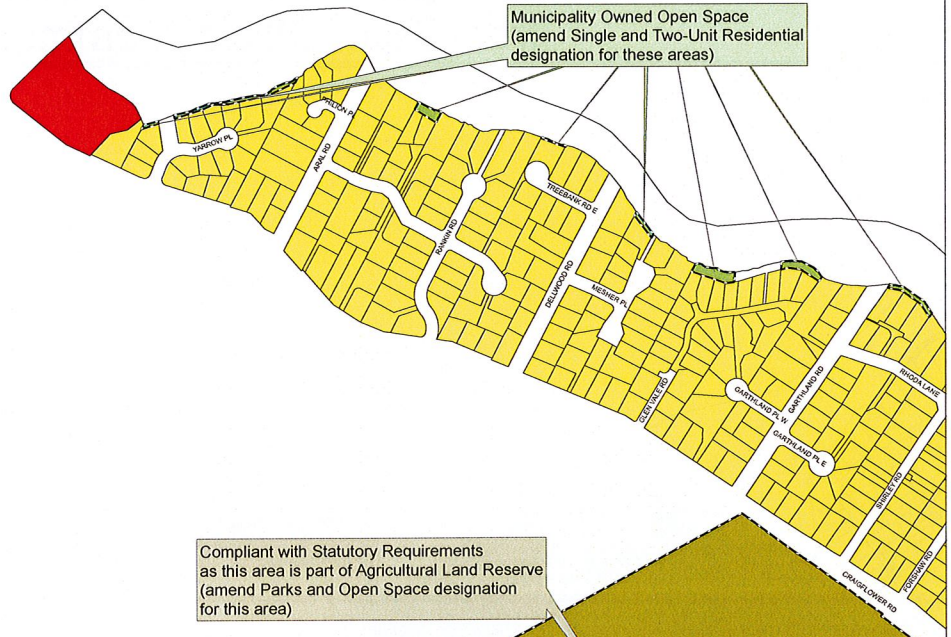
SCHEDULE B



Changes in Land Use Designations from 2007 Official Community Plan



Changes in Land Use Designations from 2007 Official Community Plan



Reflects Existing Use of Low Density Residential (amend Townhouse Residential designation for these areas)

Opportunity for Densification (amend Single and Two-Unit Residential designation for this area)

Expand Neighbourhood Commercial Opportunities (amend Townhouse Residential designation for this area)

Opportunity for Densification (amend Townhouse Residential designation for this area)

Reflects Existing Use of Utilities (amend Townhouse Residential designation for this area)

Municipality Owned Open Space (amend Single and Two-Unit Residential designation for these areas)

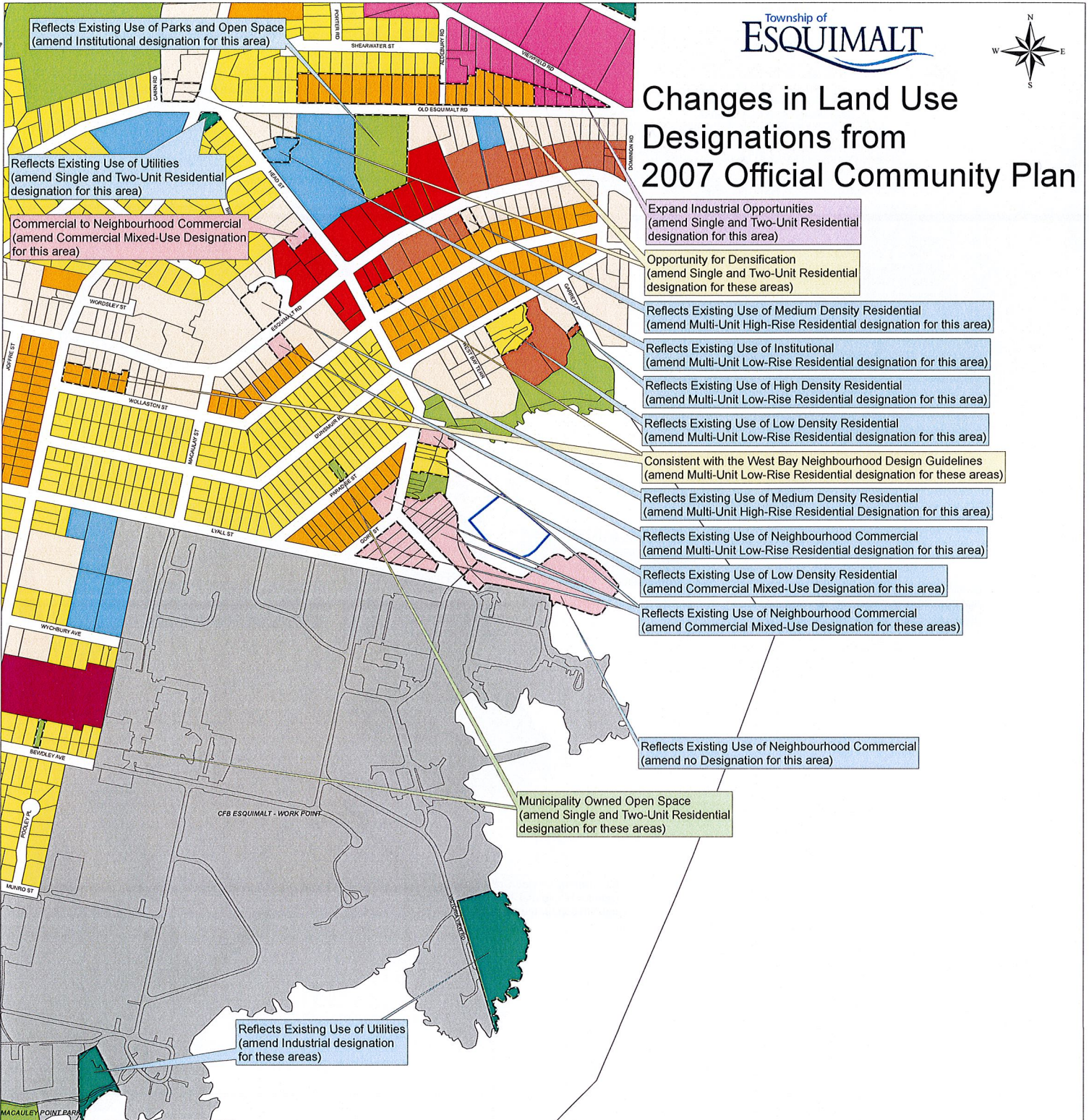
Legend

- Areas of Proposed Changes
- Proposed Land Use Designation**
- Low Density Residential
- Townhouse Residential
- Medium Density Residential
- Parks and Open Space
- Agricultural Land Reserve
- Neighbourhood Commercial Mixed-Use
- Commercial/Commercial Mixed-Use
- Utilities
- Institutional
- Federal Land
- Floathome Residential



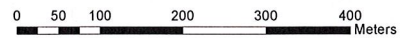


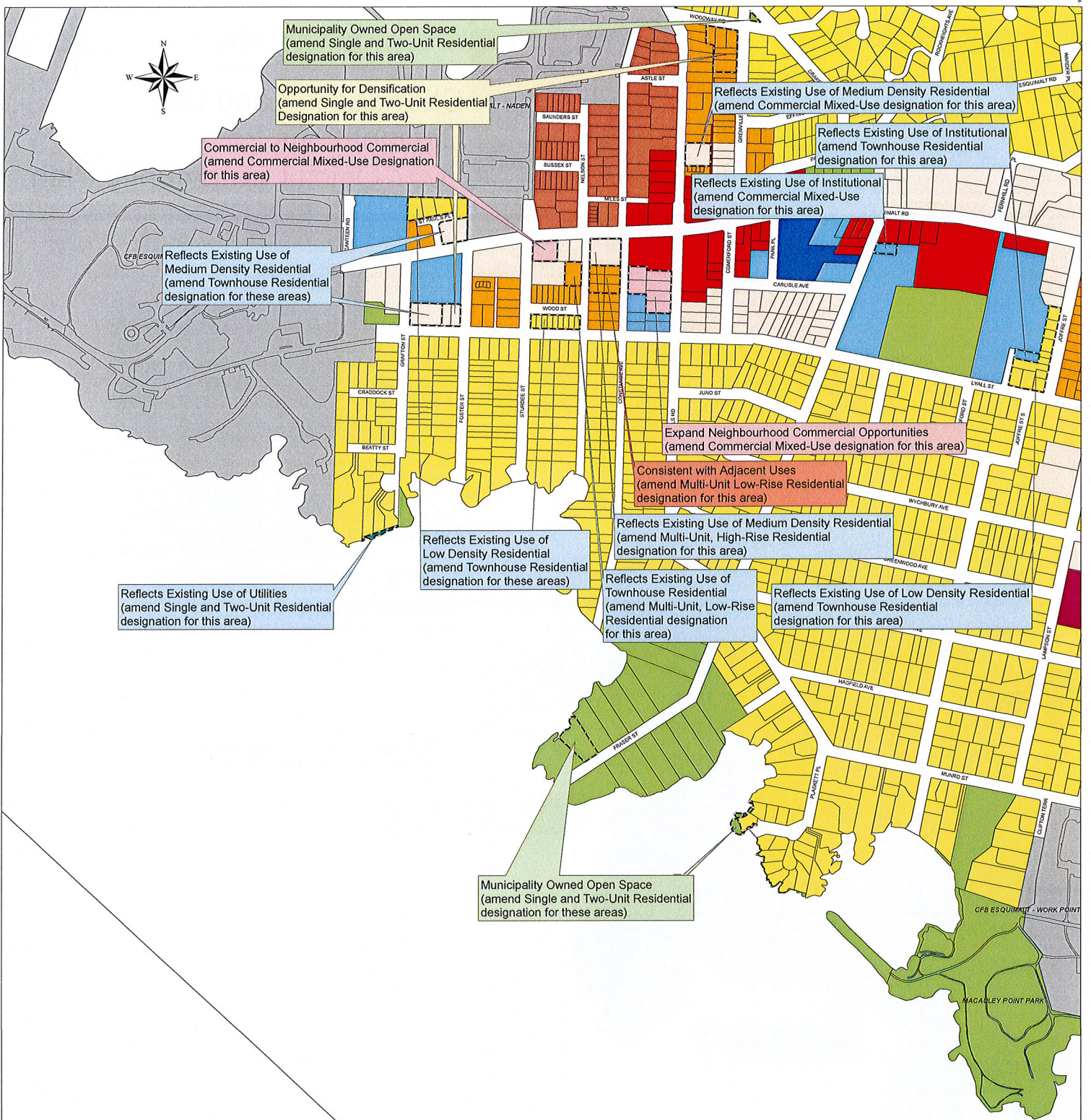
Changes in Land Use Designations from 2007 Official Community Plan



Legend

- Areas of Proposed Changes
- Proposed Land Use Designation**
 - Low Density Residential
 - Townhouse Residential
 - Medium Density Residential
 - High Density Residential
 - Parks and Open Space
 - Neighbourhood Commercial Mixed-Use
 - Commercial/Commercial Mixed-Use
 - Industrial
 - Utilities
 - Institutional
 - Federal Land
 - English Inn Mixed-Use
 - Floathome Residential





Legend

- Areas of Proposed Changes
- Proposed Land Use Designation**
 - Low Density Residential
 - Townhouse Residential
 - Medium Density Residential
 - High Density Residential
 - Parks and Open Space
 - Neighbourhood Commercial Mixed-Use
 - Commercial/Commercial Mixed-Use
 - Utilities
 - Institutional
 - Federal Land
 - English Inn Mixed-Use
 - Esquimalt Town Square
 - Floathome Residential

Changes in Land Use Designations from 2007 Official Community Plan

