



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

ADVISORY PLANNING COMMISSION

AGENDA

TUESDAY MAY 21, 2019

7:00 P.M.

COUNCIL CHAMBER, MUNICIPAL HALL

-
- I. CALL TO ORDER
 - II. LATE ITEMS
 - III. ADOPTION OF AGENDA
 - IV. ADOPTION OF MINUTES – April 16, 2019
 - V. STAFF REPORTS

1) **DEVELOPMENT PERMIT and DEVELOPMENT VARIANCE PERMIT**

1129 Munro Street

[PID 026-958-376, Lot 2, Section 11, Esquimalt District, Plan VIP82523]

Purpose of Application:

The applicant is proposing to construct a new detached garage. 1129 Munro Street is the original 80 year old building on the lot, which has previously been subdivided creating 2 further residential properties 325 and 327 Kinver Street. As such the only remaining space within the lot in question accessible by vehicles is to the front of the property. Therefore, the applicant has proposed to site the detached garage in front of the principal building. This is contrary to the zoning bylaw for RS-1, which requires any accessory buildings to be situated behind any primary buildings. As such, a Development Permit and Development Variance Permit are required.

The subject property falls under the following Development Permit Areas:

- Development Permit Area 1: Natural Environment,
- Development Permit Area 3: Enhanced Design Control Residential,
- Development Permit Area 7: Energy Conservation & Greenhouse Gas Reduction
- Development Permit Area 8: Water Conservation and as such this application requires a Development Permit.

Recommendation:

That the Esquimalt Advisory Planning Commission [APC] recommends to Council that the application for a Development Permit and a Development Variance Permit allowing construction of a new accessory building as illustrated in the architectural drawings and landscape plan prepared by Jeffrey A. Duffield, stamped "Received April 8 2019", sited as detailed on the survey plan prepared by Ryan P. Hourston BCLS, stamped "Received April 8 2019", and including the following variances for the property located at PID 026-958-376, Lot 2, Section 11, Esquimalt District, Plan VIP82523 [1129 Munro Street]] **to either approve, or deny the application; including the reasons for the recommendation.**

Zoning Bylaw, 1992, No. 2050, Section 34 (9)(b)(i) – Siting Requirements – Accessory Building – Front Setback: Exemption from the requirement that no

Accessory Building shall be located in front of the front face of the Principal Building

Parking Bylaw No. 1992, No. 2011, Part 4 (9)(4) – Provisions and Maintenance of Off-Street Parking and Loading Areas: Exemption to the requirement that Parking Spaces in Residential zones be located no closer to the Front Lot Line than the front face of the Principal Building [i.e. the one required parking space will be located in the new accessory building].

2) DEVELOPMENT VARIANCE PERMIT

1217 Juno Street

[PID: 007-993-501, Lot 5, Section 11, Esquimalt District , Plan 1104]

Purpose of the Application:

The applicant is seeking variances to enclose the existing carport to create a mudroom and office. A porch is also proposed to be located at the front face of the building which will encroach into the Front Setback. The enclosure of the carport will move the parking space beyond the front face of the building. In September 2018 the Esquimalt Board of Variance approved the relaxation of the Western Side Setback as a result a variance is required to the total Setback of all Side Yards.

Recommendation:

That the Esquimalt Advisory Planning Commission recommends to Council that the application for a Development Variance Permit, permitting the enclosure of the existing carport as per Architectural Plans prepared by Maverick Solution Limited stamped “Received May 2, 2019” and to include the following variances to the Zoning Bylaw 1992, No. 2050, for the property located at 1217 Juno Street [PID:007-993-501, Lot 5, Section 11, Esquimalt District, Plan 1104] be forwarded to Council with a **recommendation to either approve, approve with conditions, or deny the application; and provide reasons for the chosen recommendation.**

Zoning Bylaw, 1992, No.2050, Section 34 (9)(a)(i) – Setback Requirements – Principal Building – Front Setback. A 0.7 metre decrease to the requirement that no principal building shall be located within 7.5 metres of the Front Lot Line, [i.e. from 7.5 metres to 6.8 metres]

Zoning Bylaw, 1992, No. 2050, Section 34 (9)(a)(ii) – Setback Requirements – Principal Building – Side Setback. A 0.9 metres decrease to the total Setback of all Side yards not to be less than 4.5 metres. [i.e. from 4.5 metres to 3.6 metres]

Parking Bylaw, 1992, No. 2050, Section 9 (4) – Provision and Maintenance of Off – Street Parking and Loading Areas. An exemption from the requirement that parking spaces in residential zones shall be located no closer to the Front Lot Line than the front face of the Principal Building.

3) DEVELOPMENT VARIANCE PERMIT

502 Paradise Street

[PID: 009-173-242, Lot 37, Block F, Section 11, Esquimalt District, Plan 292]

Purpose of the Application:

The applicant is seeking to reconstruct the front stairs and construct a porch roof for the existing principal building on the property which is currently non-conforming. The current Front Setback is 3.3 metres and the Western Side

Setback is 0.7m. For the applicant to make these changes, the existing dwelling must come in to compliance therefore a Development Variance Permit is required.

Recommendation:

That the Esquimalt Advisory Planning Commission recommends to Council that the application for a Development Variance Permit, bringing the existing principal building into compliance to allow for the reconstruction of the front stairs and construction of a porch roof as per plans stamped “Received April 5, 2019” and BC Surveyor Site Plan prepared by Wey Mayenburg Land Surveying Inc. stamped “Received April 5, 2019” and to include the following variances to the Zoning Bylaw 1992, No. 2050, for the property located at 502 Paradise Street [PID:009-173-242, Lot 37, Block F, Section 11, Esquimalt District, Plan 292] be forwarded to Council with a **recommendation to either approve, approve with conditions, or deny the application; and provide reasons for the chosen recommendation.**

Zoning Bylaw, 1992, No.2050, Section 34 (9)(a)(i) – Setback Requirements – Principal Building – Front Setback. A 4.2 metre decrease to the requirement that no principal building shall be located within 7.5 metres of the Front Lot Line [i.e. from 7.5 metres to 6.8 metres]

Zoning Bylaw, 1992, No. 2050, Section 34 (9)(a)(ii) – Setback Requirements – Principal Building – Side Setback. A 0.8 metres decrease to the requirement that no principal building shall be located within 1.5 metres of an Interior Lot Line. [i.e. from 1.5 metres to 0.7 metres]

4) DEVELOPMENT PERMIT AND DEVELOPMENT VARIANCE PERMIT APPLICATION

10-300 PLASKETT PLACE

[PID 017-658-187 Strata Lot 10, Section 11, Esquimalt District, Strata Plan VIS2246, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1]

Purpose of the Application:

The applicant is proposing to construct a new deck that extends from an existing upper patio area over an existing lower patio area. As the proposed deck is a separate, self supporting structure, it is governed in the Zoning Bylaw as an Accessory Building.

This site is located within Development Permit Area No. 1 - Natural Environment, Development Permit Area No. 2 – Protection of Development From Hazardous Conditions, No. 3 – Enhanced Design Control Residential, No. 7 - Energy Conservation and Greenhouse Gas Reduction and No. 8 - Water Conservation of the Township’s Official Community Plan. Correspondingly, the Development Permit application will respect the guidelines of these areas where applicable.

Recommendation:

That the Esquimalt Advisory Planning Commission recommends to Council that the application for a Development Permit and a Development Variance Permit, authorizing construction of a patio deck as illustrated in the plans prepared by Java Designs, and sited in accordance with the BC Land Surveyor’s Site Plan prepared by Michael E. Claxton, both stamped “Received April 9, 2019”, and including the following variances for the property located at 10-300 Plaskett Place [PID 017-658-

187 Strata Lot 10, Section 11, Esquimalt District, Strata Plan VIS2246, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1] be forwarded to Council with a recommendation **to either approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

Zoning Bylaw, 1992, No. 2050, Section 34 (8)(a) – Lot Coverage: A 1.2% increase to the requirement that all Principal Buildings, Accessory Buildings and Structures combined, shall not cover more than 30% of the Area of a Parcel [i.e. from 30% to 31.2%].

Zoning Bylaw, 1992, No. 2050, Section 34 (9)(b)(i) – Siting Requirements – Accessory Building – Front Setback: Exemption from the requirement that no Accessory Building shall be located in front of the front face of the Principal Building.

Zoning Bylaw, 1992, No. 2050, Section 34 (9)(a)(ii) – Siting Requirements – Accessory Building – Side Setback: A 1.1 metre decrease to the requirement that no Accessory Building shall be located within 1.5 metres of an Interior Side Lot Line [i.e. from 1.5 metres to 0.4 metre].

Zoning Bylaw, 1992, No. 2050, Section 34 (9)(a)(iv) – Siting Requirements – Accessory Building – Building Separation: Exemption from the requirement that no Accessory Building shall be located within 2.5 metres of a Principal Building.

**5) DEVELOPMENT VARIANCE PERMIT APPLICATION
835 DUNSMUIR ROAD**

[PID 030-709-156 Parcel A (Being a consolidation of Lots 2 and 3, see CA7353737), Section 11, Esquimalt District, Plan 9757]

Purpose of the Application:

The applicant is requesting a variance to Zoning Bylaw, 1992, No. 2050, for a decrease in the setback to the Exterior Side Lot Line for the parking structure. The structural engineer has determined that additional structures were required in the parking level which resulted in the encroachment into the setback to the Exterior Side Lot Line. The setbacks at the First Storey and above will not change as a result of this requested variance.

Recommendation:

The Esquimalt Advisory Planning Commission recommends that the Development Variance Permit application for the following variance to Zoning Bylaw, 1992, No. 2050, as shown on the architectural plan of the parking level, provided by Praxis Architects Inc., stamped “Received May 15, 2019”, for the proposed 5 storey 32 unit apartment building at 835 Dunsmuir Road [PID 030-709-156 Parcel A (Being a consolidation of Lots 2 and 3, see CA7353737), Section 11, Esquimalt District, Plan 9757] be forwarded to Council with a recommendation **to either approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

Zoning Bylaw, 1992, No. 2050, Section 67.95. (9)(a)(v) – Siting Exceptions – Principal Building - A 0.7 metre decrease to the requirement that no principal building shall be located within 2.0 metres of the northern Exterior Side Lot Line to accommodate the parking structure situated below the First Storey [i.e. from 2.0 metres to 1.3 metre]

6) **REZONING APPLICATION**
937 COLVILLE ROAD
PID 003-679-144, Lot 4, Block 1, Section 10, Esquimalt District, Plan 6277

Purpose of the Application:

The applicant is requesting a change in zoning from the current Single Family Residential [RS-1] zone to a Comprehensive Development District zone to accommodate the proposed six (6), two bedroom, strata dwelling units, to be built in a single, stacked style townhouse building on the subject property.

The existing house would be demolished and the new building containing the six dwelling units would be constructed. The Official Community Plan 'Proposed Land Use Designation' for this area is Townhouse Residential. Should the rezoning be approved, the form and character of the buildings and landscaping would be controlled by a Development Permit that would be considered by Council at a future date.

Evaluation of this application should focus on issues relevant to zoning such as the appropriateness of the proposed height, density and massing, proposed unit sizes, siting, setbacks, lot coverage, useable open space, how the building relates to adjacent and surrounding sites and whether the proposed uses are appropriate and consistent with the overall direction contained within the Official Community Plan.

Recommendation:

The Esquimalt Advisory Planning Commission [APC] recommends to Council that the application for a rezoning, authorizing six (6) townhouse dwelling units as sited on the survey plan prepared by Island Land Surveying Ltd., stamped "Received May 14, 2019" and incorporating the height and massing consistent with the architectural plans provided by Christine Lintott Architects, stamped "Received April 23, 2019", detailing the development proposed to be located at 937 Colville Road [PID 003-679-144, Lot 4, Block 1, Section 10, Esquimalt District, Plan 6277], be forwarded to Council with a recommendation to either **approve, approve with conditions, or deny the application; and provide reasons for the recommendation.**

VI. ADJOURNMENT



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

**ADVISORY PLANNING COMMISSION
MINUTES OF APRIL 16, 2019
COUNCIL CHAMBERS, MUNICIPAL HALL**

PRESENT:	Graeme Dempster Fil Ferri Duncan Cavens	Helen Edley Chris Munkacsi Marie Fidoe
ABSENT:	Michael Angrove	
STAFF:	Bill Brown, Director of Development Services, Staff Liaison Alex Tang, Planner	
COUNCIL LIAISON:	Councillor Jacob Helliwell	

I. CALL TO ORDER

Graeme Dempster, Chair, called the Advisory Planning Commission meeting to order at 7:05 p.m.

II. LATE ITEMS

There were no late items

III. APPROVAL OF THE AGENDA

Moved by Fil Ferri, seconded by Marie Fidoe: That the agenda be approved as circulated.
Carried Unanimously

IV. ADOPTION OF MINUTES

Moved by Helen Edley seconded by Chris Munkacsi: That the minutes of March 19, 2019 be adopted as circulated. **Carried Unanimously**

V. STAFF REPORTS

1) REZONING APPLICATION

615 Bryden Court

David Lunt, T-Square Design & Consulting provided an overview of the Rezoning Application for 615 Bryden Court

The Committee had no concerns.

RECOMMENDATION:

Moved by Chris Munkacsi, seconded by Fil Ferri: That the rezoning application, authorizing two new single family dwellings sited in accordance with the BCLS Site Plan prepared by Wey Mayenburg Land Surveying Inc., stamped "Received January 31, 2019", and incorporating height and massing consistent with the architectural plans provided by T-Square Design & Consulting, stamped "Received January 31, 2019", detailing the proposed development at 915 Bryden Court [PID 005-340-004 Lot A, Section 11, Esquimalt District, Plan 9543] **be forwarded to Council with a recommendation by the Esquimalt Advisory Planning Commission to approve;** as the proposed development is reasonable, appropriate, is consistent with the infill policies in the Official Community Plan, and fits in the neighbourhood.
Carried Unanimously

2) **Official Community Plan Amendment and Rezoning Application**
681 Admirals Road
685 Admirals Road

Tanis Schulte, Thuja Architecture and Design and Frank Limshue, Westurban Developments Ltd. provided an overview of the Official Community Plan Amendment and Rezoning Application for 681 & 685 Admirals Road.

Commission comments and questions included (*response in italics*):

- How big are the three-bedroom units? *900 square feet*
- Is there an opportunity for community space or a small commercial unit? *There is no opportunity because of the site. Our business model focuses on residential.*
- What will the community get out of this proposal? *Community support is more likely if there is something in it for the community. No response*
- Concerns with the ingress and egress.
- Concerns that the visitor parking seems inadequate. *We can look at the visitor parking issue.*
- What about parking for caregivers? *The need for visitor parking is very important for the elderly.*
- What is the building behind? *It is a co-op.*

RECOMMENDATION:

Moved by Helen Edley, seconded by Chris Mukacsi: That the application for an Official Community Plan Amendment and Rezoning, authorizing a 20 metre [6 storeys], 50 unit, multiple family residential building sited in accordance with the Site Plan by Thuja Architectural and Design, and incorporating height and massing consistent with the architectural plans provided by Thuja Architecture and Design, both stamped “Received February 22, 2019”, detailing the development proposed to be located at 681 Admirals Road [PID 007-801-963 Lot 25 Block 4 Suburban Lot 50 Esquimalt District Plan 1153] + [PID 007-801-980 Lot 26 Block 4 Suburban Lot 50 Esquimalt District Plan 1153] and 685 Admirals Road [PID 007-802-013 Lot B (DD G17083), Suburban Lot 50, Esquimalt District, Plan 1153] **be forwarded to Council with a recommendation by the Esquimalt Advisory Planning Commission to approve;** as it responds to the need for rental accommodation, conforms to relevant policy in the Official Community Plan, fits the lot and features good design. **Motion Carried** (2 opposed Fil Ferri and Duncan Cavens)

VI. **ADJOURNMENT**

The meeting adjourned 8:05 p.m.

CERTIFIED CORRECT



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

APC Meeting: May 21 2019

STAFF REPORT

DATE: April 8 2019

TO: Chair and Members of the Advisory Planning Commission

FROM: Fred Billingham, Planning Technician
Bill Brown, Director of Development Services

**SUBJECT: DEVELOPMENT PERMIT and DEVELOPMENT VARIANCE PERMIT
1129 Munro Street
[PID 026-958-376, Lot 2, Section 11, Esquimalt District, Plan VIP82523]**

RECOMMENDATION:

That the Esquimalt Advisory Planning Commission [APC] recommends to Council that the application for a Development Permit and a Development Variance Permit allowing construction of a new accessory building as illustrated in the architectural drawings and landscape plan prepared by Jeffrey A. Duffield, stamped "Received April 8 2019", sited as detailed on the survey plan prepared by Ryan P. Hourston BCLS, stamped "Received April 8 2019", and including the following variances for the property located at PID 026-958-376, Lot 2, Section 11, Esquimalt District, Plan VIP82523 [1129 Munro Street] **to either approve, or deny the application; including the reasons for the recommendation.**

Zoning Bylaw, 1992, No. 2050, Section 34 (9)(b)(i) – Siting Requirements – Accessory Building – Front Setback: Exemption from the requirement that no Accessory Building shall be located in front of the front face of the Principal Building

Parking Bylaw No. 1992, No. 2011, Part 4 (9)(4) – Provisions and Maintenance of Off-Street Parking and Loading Areas: Exemption to the requirement that Parking Spaces in Residential zones be located no closer to the Front Lot Line than the front face of the Principal Building [i.e. the one required parking space will be located in the new accessory building].

BACKGROUND:**Purpose of the Application**

The applicant is proposing to construct a new detached garage. 1129 Munro Street is the original 80 year old building on the lot, which has previously been subdivided creating 2 further residential properties 325 and 327 Kinver Street. As such the only remaining space within the lot in question accessible by vehicles is to the front of the property. Therefore, the applicant has proposed to site the detached garage in front of the principal building. This is contrary to the zoning bylaw for RS-1, which requires any accessory buildings to be situated behind any primary buildings. As such, a Development Permit and Development Variance Permit are required.

The subject property falls under the following Development Permit Areas:

- Development Permit Area 1: Natural Environment,
- Development Permit Area 3: Enhanced Design Control Residential,
- Development Permit Area 7: Energy Conservation & Greenhouse Gas Reduction
- Development Permit Area 8: Water Conservation and as such this application requires a Development Permit.

Context

Applicant: Jeffrey A. Duffield

Owners: Jeffrey A. Duffield & Uta Gewald

Property Size: Metric: 567 m² Imperial: 6103 ft²

Existing Land Use: Single Family Dwelling

Surrounding Land Uses:

North: Single Family Residential

South: Single Family Residential

West: Single Family Residential

East: Two Family Residential

Existing Zoning: RS-1 [Single Family Residential]

Development Permit Guidelines

- The Official Community Plan Development Permit Area No.1: Natural Environment Guidelines (attached) are intended to provide protection of the natural environment, its ecosystems and biological diversity.
 - The applicant has met Guideline 18.5.1 by not locating the structure within 10m of the high watermark of the Strait of Juan de Fuca.
 - As per Guideline 18.5.2, existing native trees, vegetation, soil and topography are being retained wherever possible. The location of the proposed accessory building will not adversely affect any public views.

- Amendments to the existing landscape plan for the site have been provided, to conform with Guidelines 18.5.2, 18.5.3 and 18.5.4.
- The development location is within an area subject to Development Permit Area No.3: Enhanced Design Control Residential. These Guidelines (attached) are intended to establish objectives for the form and character of intensive residential development. The proposal is not for additional infill housing, but to construct an accessory building to an existing dwelling.
 - As per Guideline 20.6.2, the new structure is to be a single storey and is in keeping with the single unit residences in the immediate area. The roof line has been designed to match that of the existing principle building. It should also be noted that the materials that are to be used for the accessory building have been chosen so that they will match those of the existing principal building.
 - To conform to Guideline 20.6.4, a landscape plan for the site has been included as part of the submission.
- Development Permit Area No.7: Energy Conservation & Greenhouse Gas Reduction is intended to aid the reduction of greenhouse gas emissions within the municipality, by reducing energy consumption in buildings. Any proposal that will increase Lot Coverage by the less than 5% of the parcel or 50m², whichever is smaller, is exempt from the guidelines of this DPA. The proposed accessory building will increase the lot coverage by 24.56m² or 4% of the parcel of land, and as such is exempt.
- The proposed accessory building will increase the lot coverage by 24.56m², or 4% of the total parcel, and as such is exempt from the requirements of Development Permit Area No.8: Water Conservation, as the resultant increase in Lot Coverage will be less than 50m² or 5% of the parcel.

Public Notification

As this application includes a Development Variance Permit application, should it proceed to Council, a notice will be mailed to tenants and owners of properties within 50 metres (164 feet) of the subject property.

ALTERNATIVES:

1. Forward the application for a Development Permit and Development Variance Permit to Council with a **recommendation of approval.**
2. Forward the application for a Development Permit and Development Variance Permit to Council with a **recommendation of denial.**



Kinver Street

Munro Street



Subject Property Map
1129 Munro Street



Kinver Street

Munro Street

18 DPA NO. 1: NATURAL ENVIRONMENT



18.1 Area

Land within the municipal boundaries of the Corporation of the Township of Esquimalt.

18.2 Designation

Development Permit Area No. 1 is designated for the purpose of establishing objectives for:

- Section 488 (1) (a) – protection of the natural environment, its ecosystems and biological diversity.

18.3 Justification

- The gradual erosion of the natural environment incrementally degrades the value and function of local and regional ecosystem services. The average person owns a parcel of land for a relatively short time period of time (10-15 years); care can be taken to ensure larger trees and natural areas are available for the next generation.
- Landscapes and ecosystems are composed of patches of diverse habitats that may include 'species at risk', 'environmentally sensitive areas', and micro-ecosystems containing 'ecological memory'. Even small patches of native soil and vegetation support indigenous species; therefore, all areas can contribute to regional biodiversity and support ecosystem services.
- Protecting Esquimalt's natural environmental features [including but not limited to: local Garry Oak and Douglas-fir ecosystems, rock outcrops, hilly terrain, and rugged clean shorelines] contributes to natural ecosystem functioning and protection of biodiversity.
- Esquimalt has over 20 kilometres of shoreline that serves industrial, commercial and residential purposes, recreation enjoyment, and is vital habitat for numerous plants and animals (e.g. otters, whales, seal, oyster catchers, gulls, various species of waterfowl, and eagles). Shoreline ecology and fish habitat can be protected and enhanced by regulating development near shorelines, and by mitigating the impacts of stormwater entering local waterways. Keeping the urban environment absorbent helps lessen marine ecosystem damage from: introduced pollutants, sudden changes in water salinity and temperature, and shoreline erosion from surges in volume at storm-water pipe outfalls.

- The Gorge waterway is a sensitive, tidal-influenced watercourse that connects the fish-bearing fresh water of Craigflower and Colquitz Creeks with the salt waters of Victoria Harbour. Over the past few decades, significant public expenditures and efforts have gone into removing sources of pollution and contamination that once plagued this waterway. However, the removal of native shoreline vegetation and the construction of extensive seawalls have substantially diminished the quality of the shoreline as supportive habitat for fish and wildlife.
- Both private and public landowners are responsible for this loss of habitat, often through well meaning and beneficial projects such as shoreline walkways.
- Protecting and maintaining current wave energy patterns and natural patterns of erosion along Esquimalt's shorelines will contribute to the protection of natural features and dynamic processes.
- Invasive alien plant species pose a significant threat to regional biodiversity. Managing and reducing the introduction and spread of these species protects local ecosystem structure and function and biodiversity.
- Esquimalt has several natural area parks of varying sizes distributed across the municipality. These pieces of fragmented natural habitat can be better connected through the use of native plant landscaping along roadways and in private yards; thereby supporting regional biodiversity.
- Biodiversity can be enhanced in an urban region through the thoughtful selection of building methods and landscape design. Siting buildings to provide space for trees of varying species and sizes provides vertical habitat for birds, pollinators and other creatures. In addition, the plants of a traditional Garry Oak meadow ecosystem are well adapted for the seasonally dry conditions found on local building roofs, and therefore can be effectively used in a 'living roof' system (a green roof with enhanced ecosystem services). A living roof will moderate stormwater discharge while providing habitat for indigenous plants, invertebrates, and ground feeding and nesting birds.
- Native birds are an important component of urban biodiversity and provide important ecosystem services that contribute to human health. Esquimalt is located within the 'Pacific Flyway' (A chain of habitats used by at least one billion birds biannually as their migratory route along the west coast of North and South America, from Alaska to Patagonia.) and much of Esquimalt's shoreline is part of the 'Victoria Harbour Migratory Bird Sanctuary'. Reasonable actions can be taken to enhance bird habitat in an urban setting; including providing vertical habitat through protection and enhancement of the urban forest, and protecting local shorelines and waterways.
- Habitat corridors for pollinators and other wildlife will be enhanced; linking natural areas within and through the urban matrix to support biodiversity and local food gardens

18.4 Exemptions

18.4.1 Properties

For all properties:

1. Interior renovations or alterations of existing buildings where residential density is not being increased.
2. Ecological restoration projects undertaken or approved by the Township of Esquimalt.
3. Installation of unpaved paths or walking trails that are less than 1 m in width and covered in naturally permeable materials [wood chips, bark mulch, sand or loose gravel] where the soil remains undisturbed.

18.4.2 Gorge Waterway

For all lands located more than 7.5 m from the high watermark of the Gorge Waterway:

1. Repair, maintenance or reconstruction, on the same footprint, of existing legal or legally non-conforming buildings, patios, driveways, parking areas and utilities, provided there is no alteration to natural soil or native vegetation.
2. Construction of fencing where no native trees are removed and disturbance to native vegetation is negligible.
3. The addition of small temporary landscape amenities including benches, tables, garden ornaments, playground equipment, and raised garden beds (not including retaining walls).



18.4.3 High Watermark

For lands located more than 20 m from the high watermark of the Gorge Waterway, and more than 15 m from the high watermark of the Strait of Juan de Fuca:

1. Minor additions [less than 10 m² in area] to an existing legal or legally non-conforming building or structure.
2. Construction of buildings and structures less than 10 m² in area.
3. Installation of seasonal recreation equipment such as children's play equipment, patio furniture, temporary above natural ground level pools/hot tubs.
4. Temporary tent/carport structures.

18.5 Guidelines

The expertise of qualified environmental professionals (retained by applicants), is strongly encouraged and may be required in certain circumstances.

18.5.1 Lands Free of Development

Lands to remain free of development, with conditions:

1. Lands within 7.5 m of the high watermark of the Gorge Waterway shall be retained in as natural a state as possible. Where the land has been previously altered, the area shall be restored with native trees and plants.
2. New buildings/ structures shall not be located within 20 m of the high watermark of the Gorge Waterway.
3. New buildings/ structures shall not be located within 10 m the high watermark of the Strait of Juan de Fuca.
4. Replacement of, expansion of, densification and intensification of the use of existing buildings within 20 m of the high watermark of the Gorge Waterway is discouraged; detached accessory dwelling units are strongly discouraged in this location.
5. Replacement of, expansion of, densification and intensification of the use of existing buildings within 10 m of the high watermark of the Strait of Juan de Fuca is discouraged and detached accessory dwelling units are strongly discouraged in this location.
6. Variances to 'Building Height' and 'Siting Requirements' will be considered where natural areas and trees are being protected.
7. Consider the use of conservation covenants for areas having high ecosystem conservation values. Property owners are encouraged to work with local land trusts to protect natural features and valuable habitat areas through land covenants.

18.5.2 Natural Features

Natural features and areas to be preserved, protected, restored, and enhanced where feasible:

1. Retain existing healthy native trees, vegetation, rock outcrops and soil wherever possible.
2. Preserve and enhance native tree and shrub clusters that overhang the waters edge as these provide shade, protection and feeding habitat for fish and wildlife.
3. Preservation of natural topography is favoured over blasting or building of retaining walls.
4. Narrower manoeuvring aisles, fewer and smaller parking spaces can be considered where natural areas are being conserved.
5. Design new development and landscaping to frame rather than block public views.
6. Avoid disturbing, compacting and removing areas of natural soil as this can lead to invasion by unwanted plant species, poor water absorption and poor establishment of new plantings. Use of local natural soil in disturbed and restored areas will support re-establishment of ecosystem functions.

18.5.3 Biodiversity

Landscaping features that will protect, restore and enhance biodiversity. Where feasible:

1. New landscaping shall consist predominantly of native plant and tree species. Plants that are native to the Coastal Douglas-fir biogeoclimatic zone are preferred in landscape treatments as they provide habitat for threatened indigenous flora and fauna. Drought tolerant plants native to western North America, that are known to be non-invasive, are a good alternative choice for landscaped areas.
2. In residential locations plan for 'nature out front'; for new landscaping in front and exterior side yards use a variety of site-appropriate, native species; thereby contributing positively to pedestrian friendly urban streets, future greenways and habitat enhanced corridors.
3. Choose trees and plants for site conditions; consider shade, sunlight, heat, wind-exposure, sea spray tolerance, and year round moisture requirements in their placement.
4. Consider the habitat and food needs of birds, pollinators, and humans in tree and plant species selection and placement; native plantings and food gardens compliment each other.
5. Encourage native plant and food gardens to spill from private land into boulevards.
6. Avoid monoculture plantings, especially expanses of turf grass outside of playing field sites.
7. Snags, logs, driftwood and rock cairns may be used as interesting landscaping features that also provide habitat for native flora and fauna.
8. Avoid using fast-growing non-native plants to cover and retain soils as they may become invasive and a constraint to the establishment of other plants.
9. Locate civil servicing pipes/lines under driveways or other paved areas to minimize tree root damage. (Note that the majority of trees have their roots in the top 0.6 m of the soil).
10. Design retaining wall spacing and landscape planting areas of sufficient width and depth to support plantings (eg. provide larger spaces for trees).
11. Support the daylighting of portions of the stormwater system for enhanced habitat.
12. Aim to meet the Canadian Landscape Standards in all landscaping installations.

18.5.4 Natural Environment

Measures to protect, restore and enhance the natural environment (limit noise, light and air pollution). Where it is reasonable:

1. Strategically locate leafy trees/ hedges and water features to mask urban noises such as traffic, garbage collection and delivery locations. Consider that leafy rough barked trees, vine covered walls and natural ground cover materials (mulch, soil) will help dampen urban noise.
2. Use International Dark-Sky Association approved lighting fixtures in outdoor locations. Outdoor lighting shall be no brighter than necessary, be fully shielded (directed downward and designed to serve pedestrian needs), have minimal blue light emissions and only be on when needed. Avoid vanity lighting, and lighting directed into the night sky and trees tops.
3. Light spillage on to waterways is strongly discouraged.
4. Place trees and vegetation near sources of air pollution including busy roadways, to assist in reduction of air pollution through the collection of particulate matter on leaves and needles, and absorption of toxic gases, including but not limited to: ozone, nitrogen dioxide, sulfur dioxide, carbon monoxide, carbon dioxide, cadmium, chromium, nickel and lead.

18.5.5 Drainage and Erosion

Measures to control drainage and shoreline erosion. Where it is reasonable:

1. Preserve, restore and enhance treed areas. Trees are the most effective form of absorbent landscaping due to their extensive root zones and their ability to both absorb water from the soil and intercept precipitation on leaves, needles and branches. Consider that native conifers are well adapted to local wet winters.
2. Reduce the impact of surges in stormwater on shorelines by designing on-site stormwater retention systems to contain the first 3 centimetres [1.25 inches] of precipitation on site, per precipitation event; and incorporating rainwater collection systems into roof design and landscaping.
3. Consider using shared private/ public rain gardens. Direct a portion of stormwater to adjacent public open spaces, when deemed appropriate by the Director of Engineering and Public Works.
4. Maximize the ratio of planted and pervious surfaces to unplanted surfaces, and design paved areas to direct water towards vegetated areas, to help reduce surface run off. Where paved surfaces are needed, intersperse with drought resistant vegetation and trees, to help absorb stormwater, provide shade and reduce the local heat island effect.
5. Use porous surfaces to enhance stormwater infiltration, permeable paving is preferable for all open air parking areas. Ensure installation methods contribute to sustained permeability and retention of stormwater on the site.
6. Choose absorbent landscaping materials; leaf mulches, wood chips and good quality top soil, over gravel, pavers and concrete. Provide mulch of organic, locally derived materials; leaf mulch from local tree leaves is most desirable.
7. Incorporation of rain gardens, bio-swales, rain barrels, and even small depressions (puddles) into landscaping will help reduce surges of stormwater entering local waterways.
8. Planting densities should ensure that vegetated areas will have near 100% plant coverage after two full growing seasons.

18.5.6 Protect, Restore and Enhance Shorelines

Measures to protect, restore and enhance local shorelines (reducing shoreline hardening and dock development). When it is feasible:

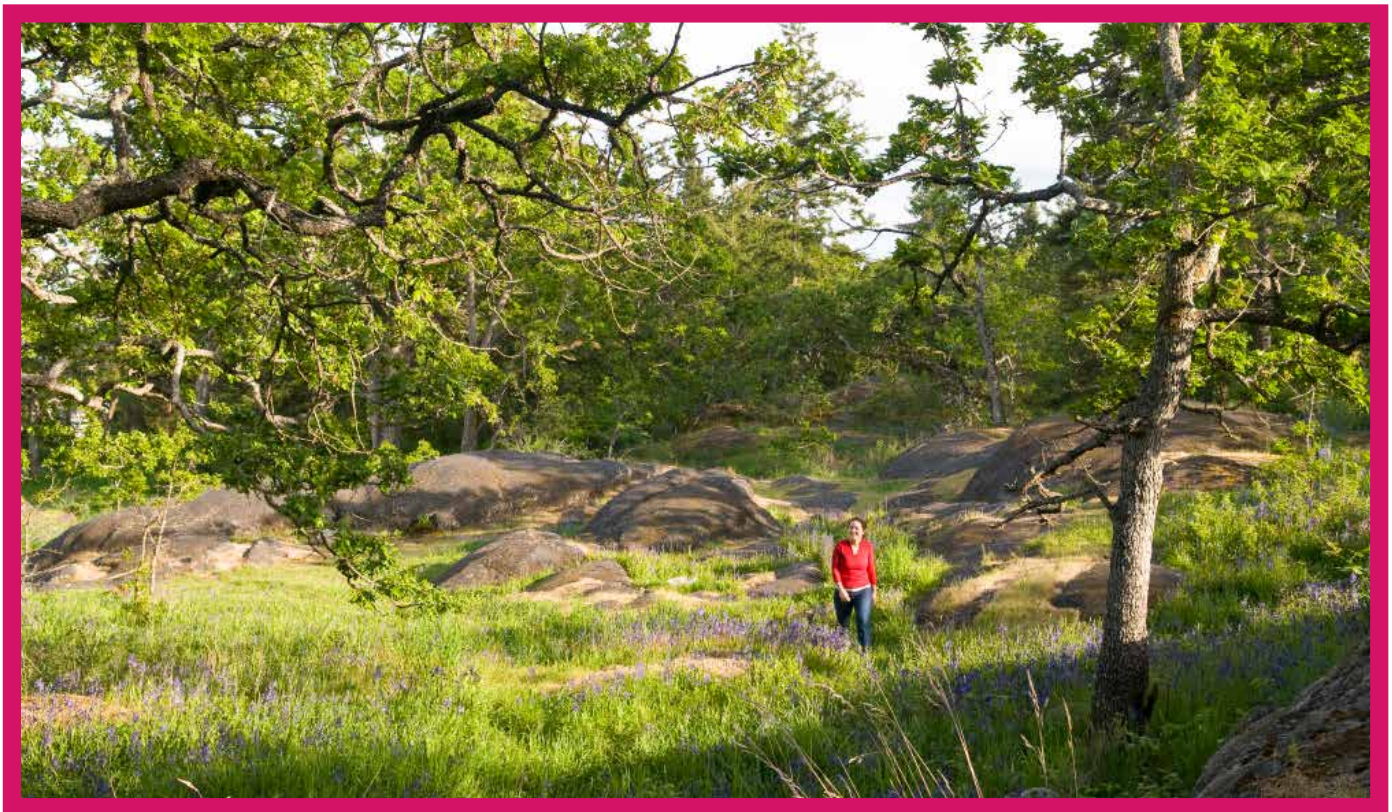
1. Waterfront property owners are encouraged to become familiar with and adopt a 'soft shore' restoration approach to the care of their foreshore property (i.e. Green Shores for Homes).
2. Avoid the expansion of dock area, bulkheads, groins or other shoreline hardening structures. Removal or reductions in the surface area of existing private docks is encouraged.
3. Where shoring methods are required to prevent erosion or the sloughing of the shoreline, choose bio-engineering methods over the use of sea-walls or retaining walls. Where sea-walls or retaining walls are the only means of effectively preventing erosion, design in consultation with qualified environmental professionals, as well as engineering professionals.

18.5.7 Native Bird Biodiversity

Measures to protect, restore and enhance native bird biodiversity. Where it is reasonable:

1. Protect and enhance habitat features like mature trees, shrub clusters, native fruit bearing shrubs, fresh water ponds and ephemeral damp areas (puddles).

2. Encourage increased front yard habitat along quieter streets to reduce bird vehicle conflicts and enhance the pedestrian experience through native plantings.
3. Sustain a mix of habitat types; including forest, shrub-land, meadow, riparian wetland and coastal shoreline ecosystems in landscaping.
4. Incorporate a vertical vegetation structure [vertical habitat] including layers of ground cover, shrub, understorey and canopy in landscape design.
5. Choose a range of native plant species and sizes; a mix of coniferous and deciduous trees will enhance bird species diversity.
6. Incorporate architectural features that limit collisions between birds and windows including patterned, frosted or tinted glass, exterior louvers, blinds, sun shades and canopies.
7. Cap and screen all ventilation pipes and grates, avoid openings greater than 2.0 x 2.0 cm.



20 DPA NO. 3: ENHANCED DESIGN CONTROL RESIDENTIAL



20.1 Area

All lands zoned for two-unit dwellings or zoned as Comprehensive Development Districts for residential developments only are designated as part of Development Permit Area No. 3 — Enhanced Design Control — Residential as shown on “Development Permit Areas Map” (Schedule “H”) of this Plan.

20.2 Designation

Development Permit Area No. 3 is designated for the purpose of establishing objectives for:

- Section 488(1) (e) – establishment of objectives for the form and character of intensive residential development.

20.3 Justification

The following guidelines were developed to allow for the better utilization and redevelopment of parcels within residential neighbourhoods and ensure that development occurs in a manner that retains the desirable physical characteristics of a neighbourhood.

Infill housing represents an important component of the housing spectrum. Because infill housing represents new development that is located within the context of an existing residential neighbourhood, it is important that there is harmonious compatibility between the form and character of the new development and the existing development

20.4 Exemptions

The following do not require a development permit:

1. Additions or renovations to one single-unit dwelling (excluding infill units) situated on a fee simple parcel where the floor area of the addition does not exceed 10 percent of the ground floor area of the dwelling; and
2. Additions or renovations to any two-unit dwelling where the floor area of the addition does not exceed 10 percent of the ground floor area of the dwelling.

20.5 Guidelines - Duplex Housing

1. The fronts of the buildings should be designed to create the appearance of smaller structures either by staggering the dwelling units or visually breaking up the façade with architectural detailing while still maintaining a complementary streetscape.
2. Innovative and creative site-specific two-unit dwellings are encouraged where usable open space is maintained either on the ground (yard) or as rooftop gardens. Setbacks to the street may be reduced to maximize property use.
3. Front to back duplexes are generally discouraged unless they can be designed to eliminate negative impacts to the immediate neighbours such as shading of gardens, overlook of outdoor amenity areas and violation of privacy.
4. Side by side, up and down, or staggered unit configurations are preferred as these result in a greater number of units facing the street, less disruption of privacy, and a more equitable division of outdoor amenity areas between the two dwelling units.
5. The use of exterior building materials similar to those used in older residential neighbourhoods (i.e. combinations of wood, brick, stucco, and stone) is encouraged.
6. Rooflines of new development should relate in height, shape and pitch to existing residential buildings in the immediate area. For corner sites, the building design should avoid having large unbroken sloped roof areas facing the street.
7. To create interest in the façade of the buildings facing the street, the incorporation of architectural elements such as bay windows, covered porches, verandas and prominent front doors is encouraged.
8. Buildings should be designed to minimize visual intrusion on to the privacy of surrounding homes. Some overlook of adjoining yards and neighbouring decks may be unavoidable; however, additional privacy should be achieved by inseting balconies, decks and patios into the building or by screening them with latticework or landscaping. Windows should be spaced so that they do not align directly with those of other buildings.
9. The height and massing of new two-unit dwellings should be designed to minimize the casting of shadows on to the private outdoor space of adjacent residential dwellings
10. A landscaping plan showing ground cover areas, planting beds, shrubbery and trees (both existing and proposed) is required. Landscaping should add to the aesthetic appeal of the streetscape as well as provide privacy between dwelling units.
11. The provision of private open space should be part of an overall site development and landscape plan and should take into consideration general site circulation patterns (including parking), existing landscape features, sun access, privacy and usability.
12. Retention and protection of trees and the natural habitat is encouraged where possible.
13. Parking areas, garages and driveways should appear as a minor component of the site when viewed from the street. The building of curving access roads and driveways helps to avoid views from the street of large expanses of paving. The use of shared driveways is encouraged.
14. The use of permeable and decorative surfacing materials, such as brick, concrete pavers, textured concrete, coloured paving or grasscrete is encouraged in place of solid expanses of asphalt or concrete.

15. Where possible, hydro meters should not be placed on the front façade of a building and, if placed in a manner which is visible from the street, will be appropriately screened by the owner in a manner consistent with BC Hydro requirements.
16. Where an existing single-unit dwelling is being converted to a two-unit dwelling both the original structure and the addition shall be in complementary architectural style and constructed of complementary exterior finishes including roofing material, window treatments, exterior finishes, door styles and trims.
17. Roof styles and pitches of the original and new portions of the building must be complimentary.
18. For new or converted two unit dwellings, garages and parking areas are encouraged to be located in the rear yard. Shared driveways are preferred to access the rear yard.
19. Where two single lane driveways serve a two unit lot, landscaping features are encouraged between the driveways.

20.6 Guidelines - Single-unit Infill Housing

20.6.1 Relationship to Existing Houses

1. Where an existing single-unit residence is to be retained and a second residence placed on the parcel, the existing dwelling is to be upgraded and made to be complementary with the new construction.
2. Where two or more new separate dwellings are situated on a parcel or within a comprehensive development zone, the buildings shall be designed as part of a comprehensive scheme with all buildings being finished in complementary materials and incorporating complimentary architectural details.
3. Where new infill single houses are proposed, the design of the new houses should be complementary in scale, size, exterior finishes, rooflines, and colours to the predominant styles of housing in the neighbourhood. It is important to ensure that the new construction fits with the overall scale and character of existing houses.

The intent of this guideline is not to encourage the replication or imitation of surrounding buildings but rather the design of structures that complement the streetscape.

20.6.2 Massing

1. New structures should be designed so that the overall massing is in keeping with other single-unit residences in the immediate area. New structures for lots other than corner or double frontage lots should be limited to one and one half storeys.
2. New structures, which are two storeys in height, should be designed so that the second storey is partially concealed within the slope of the roof to minimize the height of the building. The use of dormers set into the roof is preferred to a flat roof or a peaked roof set over the second storey.

20.6.3 Privacy/Screening/Shadowing

1. Proposed infill dwellings should have only a minimal impact on adjacent homes and be separated from neighbouring residences by vegetation, screening, natural elevation differences, or a combination of these features.
2. Windows, decks and patios should be located so as to minimize intrusion on to the privacy of adjacent properties.
3. Infill dwellings should be sited to minimize the casting of shadows on to the private outdoor space of adjacent residential dwellings.

20.6.4 Landscaping

1. Proposals for single-unit infill housing must include a landscape plan showing hard landscaping (i.e., parking areas, fences, and patios) as well as lawns, trees, shrubs, planting areas and proposed plant species.
2. Retention and protection of trees and the natural habitat is encouraged wherever possible.

20.6.5 Private Open/Yard Space

1. Any proposal for single-unit infill housing should provide for usable, private outdoor areas for each dwelling, at grade.

DIVISION 1 - RESIDENTIAL ZONES

34. SINGLE FAMILY RESIDENTIAL [RS-1]

The intent of this Zone is to accommodate Single Family Dwellings on individual Parcels of land.

(1) **Permitted Uses**

The following Uses and no others are permitted:

- (a) Single Family Residential
- (b) Home Occupation
- (c) Secondary Suite: subject to the requirements of Section 30.6
- (d) Boarding: subject to the requirements of Section 30.3
- (e) Urban Hens: subject to the requirements of Section 30.4 of this bylaw.

(2) **Parcel Size**

The minimum Parcel Size for Parcels created by subdivision shall be 530.0 square metres.

(3) **Minimum Lot Width**

The minimum width of Parcels created by subdivision shall be 16 metres measured at the Front Building Line.

(4) **Floor Area Ratio**

The Floor Area Ratio shall not exceed 0.35.

(5) **Floor Area**

The minimum Floor Area for the First Storey of a Principal Building shall be 88 square metres.

(6) **Building Height**

- (a) No Principal Building shall exceed a Height of 7.3 metres.
- (b) No Accessory Building shall exceed a Height of 3.6 metres.

(7) **Building Width**

The minimum width for any Single Family Dwelling shall be 7 metres.

(8) **Lot Coverage**

- (a) All Principal Buildings, Accessory Buildings and Structures combined, shall not cover more than 30% of the Area of a Parcel.
- (b) All Accessory Buildings and Structures combined shall not exceed 10% of the Area of Parcel.

(9) **Siting Requirements**

(a) **Principal Building**

- (i) Front Setback: No Principal Building shall be located within 7.5 metres of the Front Lot Line.
- (ii) Side Setback: No Principal Building shall be located within 1.5 metres of an Interior Side Lot Line, with the total Setback of all Side Yards not to be less than 4.5 metres. In the case where a Parcel is not served by a rear lane, one (1) Side Yard shall not be less than 3 metres. In the case of a Corner Lot, no Principal Building shall be located within 3.6 metres of an Exterior Side Lot Line
- (iii) Rear Setback: No Principal Building shall be located within 7.5 metres of a Rear Lot Line.

(b) **Accessory Building**

- (i) Front Setback: No Accessory Building shall be located in front of the front face of the Principal Building.
- (ii) Side Setback: No Accessory Building shall be located within 1.5 metres of an Interior Side Lot Line nor 3.6 metres of an Exterior Side Lot Line.
- (iii) Rear Setback: No Accessory Building shall be located within 1.5 metres of a Rear Lot Line.
- (iv) Building Separation: No Accessory Building shall be located within 2.5 metres of a Principal Building.

(10) **Fencing**

Subject to Section 22, no fence shall exceed a Height of 1.2 metres in front of the front face of the Principal Building and 2 metres behind the front face of the Principal Building.

(11) **Off Street Parking**

Off street parking shall be provided in accordance with the requirements of Parking Bylaw, 1992, No. 2011 (as amended).

150mm

3.660 (12')

A

89x89 Corner Stud
with 38x89 studs 305 o/c

22.3 m² (240 ft²)

OVERHEAD GARAGE DOOR
2134 H 2438 W
HEADER FRAMING PER
MANUFACTURERS INSTRUCTIONS
OR CODE REQUIREMENTS.



CT1
⊕ duplex
receptical
in ceiling.

dbl stud at
window jamb (typ)

6.710 (22')

B

ROOF TRUSSES BY VICTORIA TRUSS

D

water line run from house for R/I
of sprinkler system control & Cedar
tree line watering

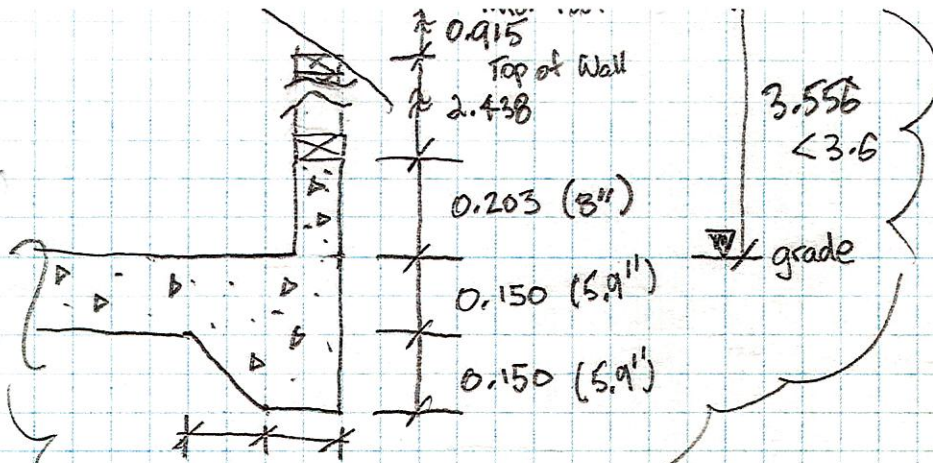
50 amp electrical panel.
- lighting & garage door open hose
- (2) future electric car charging.

Revised SEPT 26, 2018

C

1129 MUNRO ST
ACCESSORY BUILDING
PLAN

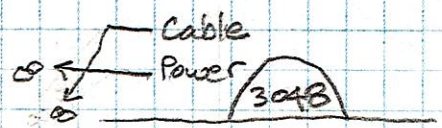
R1 - April 6, 2019
Roof line changed to
12-12 pitch.



A-1

150mm

R1



150mm

2134 (7')

150mm

5438 (8')

A

A-1

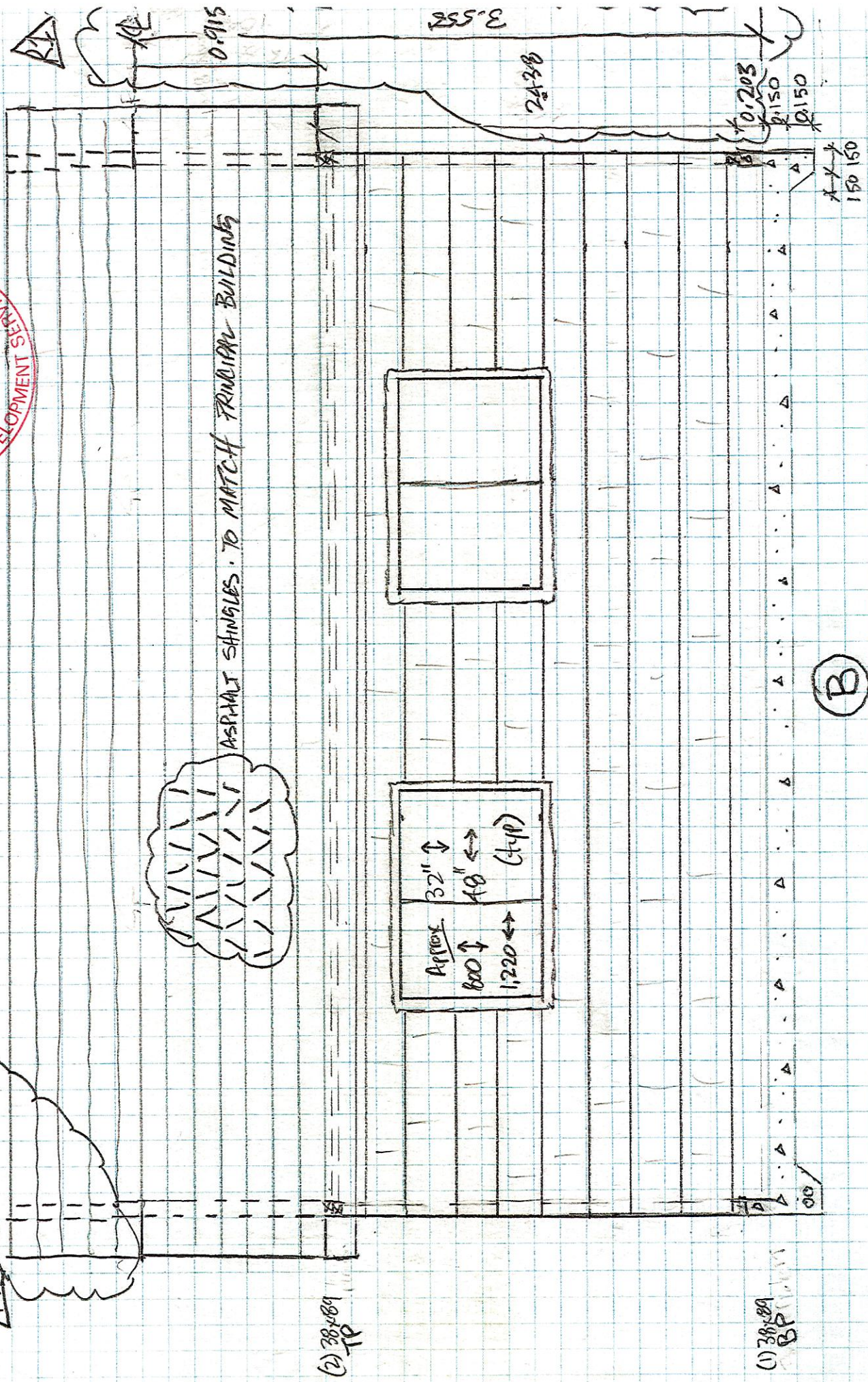
typical
EXISTING
GRADE

75mm
Existing
per
grade
50mm
added per
grade

RI - April 6 2019
Roof line changed to 12-12 pitch.

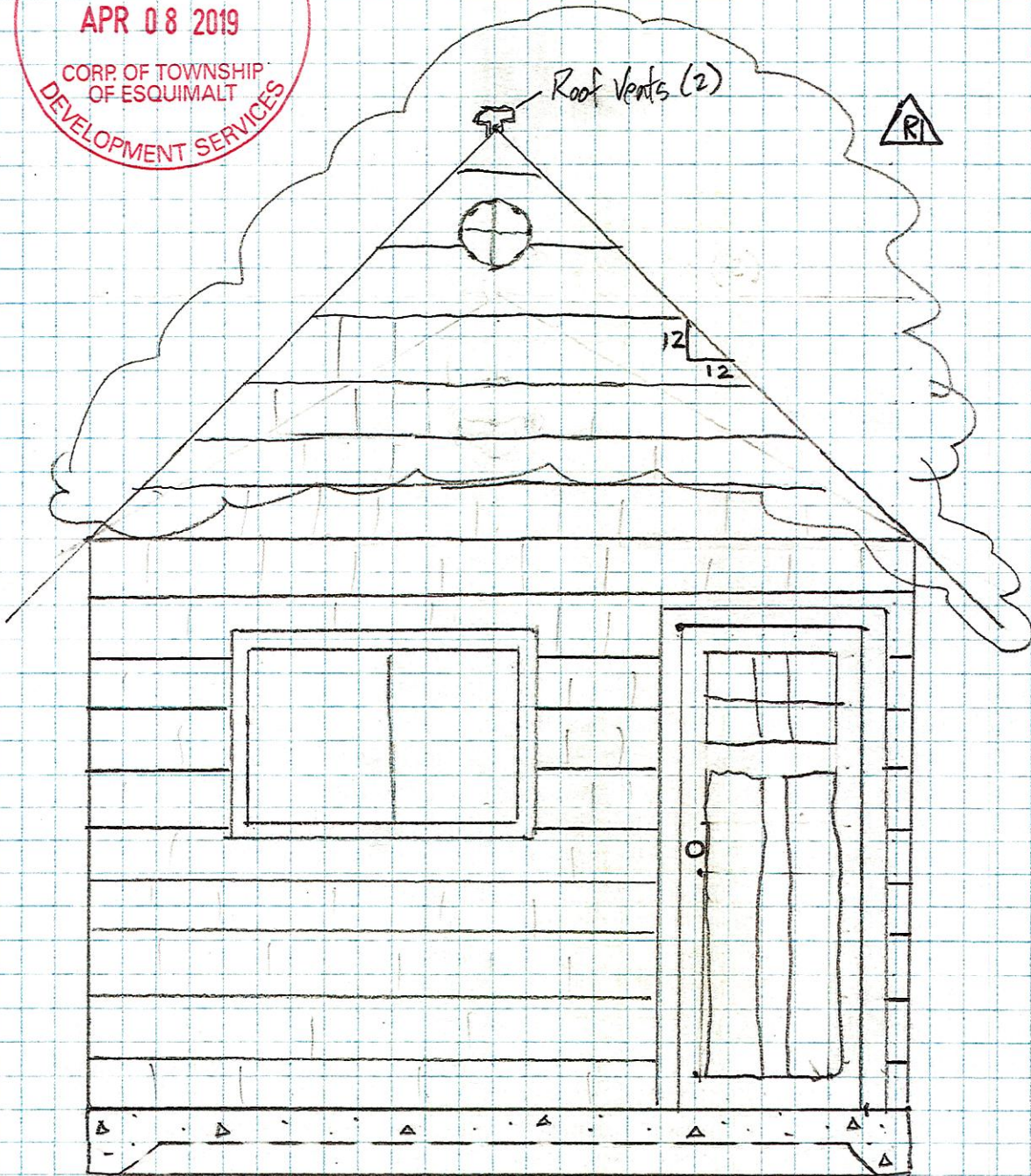
RECEIVED
APR 08 2019
CORP OF TOWNSHIP
OF ESQUIMAULT
DEVELOPMENT SERVICES

X-A
150 mm



RI - April 6, 2019
Roof line changed to 12-12 pitch.

KK
15MM



©

8'3 (32")

RA April 6, 2019
roof line changed to 12-12 pitch.

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OF ESQUIMALT
DEVELOPMENT SERVICES

18" CEDAR BOXED
SIDING SHINGLES
TO MATCH 80 YR OLD
PRIMARY BUILDING WITH
MITERED CORNERS.

single windows
to match
primary building

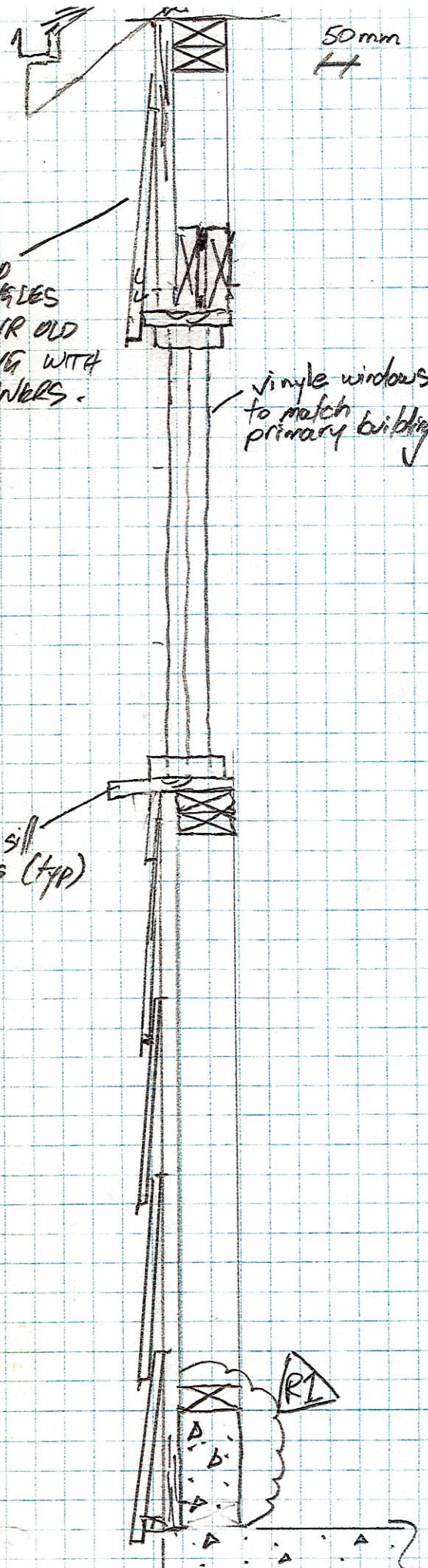
RA
2438
1219 (4) to window sill
may vary to suit window or horizontal siding line architecture

Cedar sill
4 sides (typ)

(D)

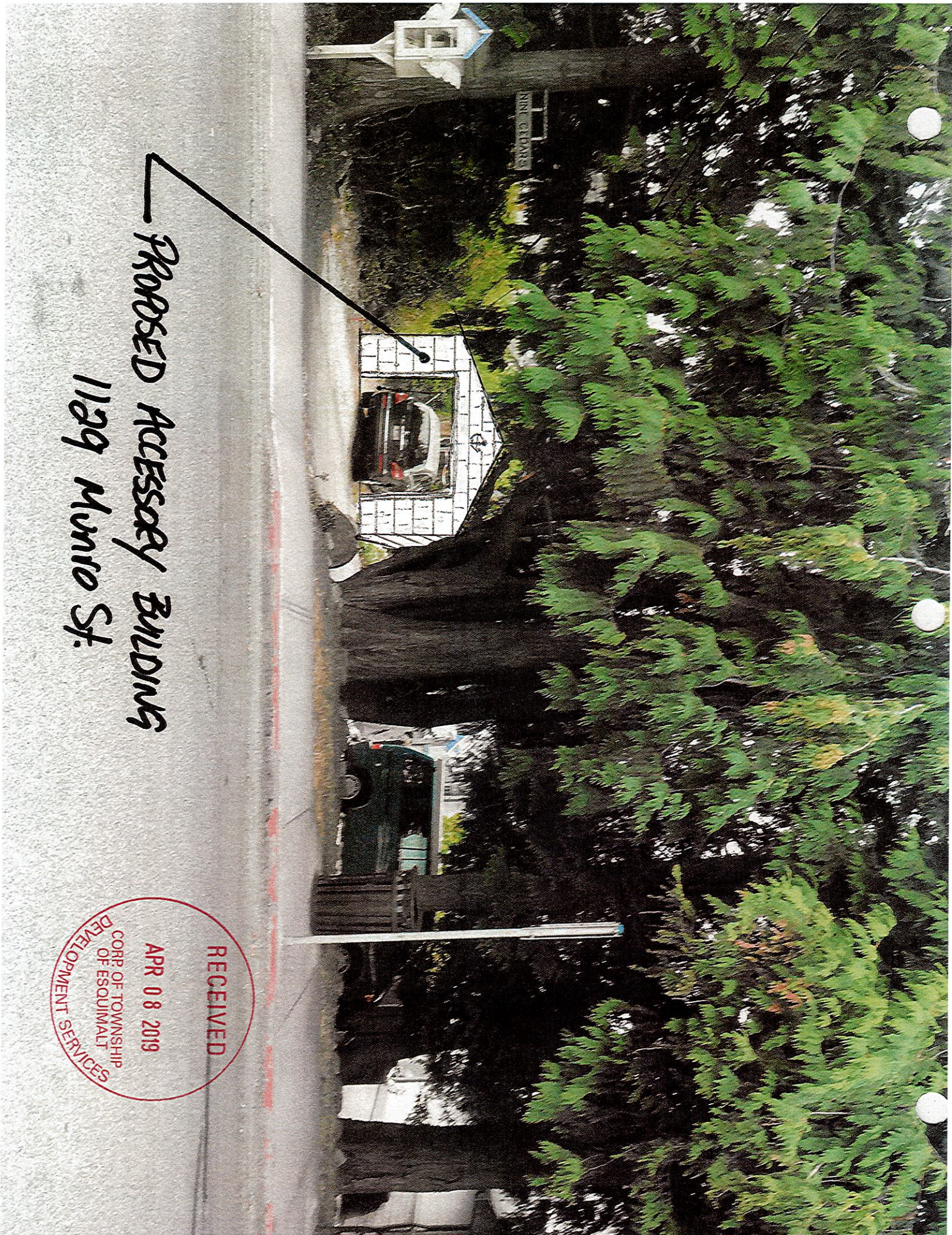
RA

50mm
H



PROPOSED ACCESSORY BUILDING
1129 Munro St.

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OF ESQUIMALT
DEVELOPMENT SERVICES



Sketch Plan of Lot 2, Section 11, Esquimalt District, Plan VIP82523

RECEIVED

APR 08 2019

CORP. OF TOWNSHIP OF ESQUIMALT
DEVELOPMENT SERVICES



BUILDING SUMMARY

● Lot Coverage

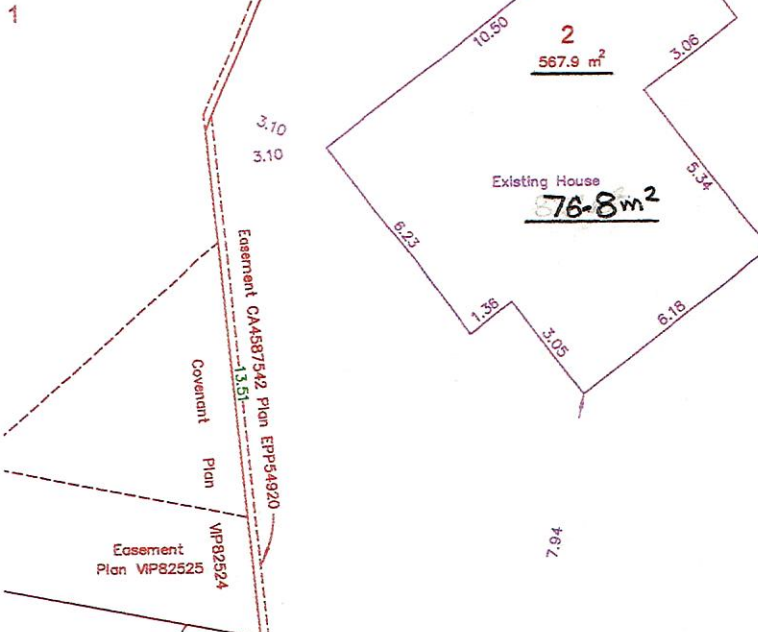
a) = $\frac{(76.8 + 24.6)}{567.9}$
 a) = 17.9% < 30% ✓
 b) = $\frac{24.6}{567.9}$
 b) = 4.3% < 10% ✓

● Floor Area Ratio
no change

● Parking Spaces Proposed.

1

1



VARIANCE:
 ← 34(9)(b)(i)
 Located in front of principal building

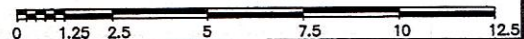
Certified correct this 30th day of June, 2016

Ryan P. Hourston, BCLS

Distances are in metres

This sketch does not constitute a redefinition of the legal boundaries hereon described and is not to be used in any manner which would assume same.

Scale 1:125 File: 29728





RO - Sept 27, 2018
 • original submission.
 R1 - Apr 6, 2019
 • landscape package added
 • roof line changed to 12-12 pitch.

Development Variance Request

1129 Munro street, Esquimalt BC, V9A 5P2

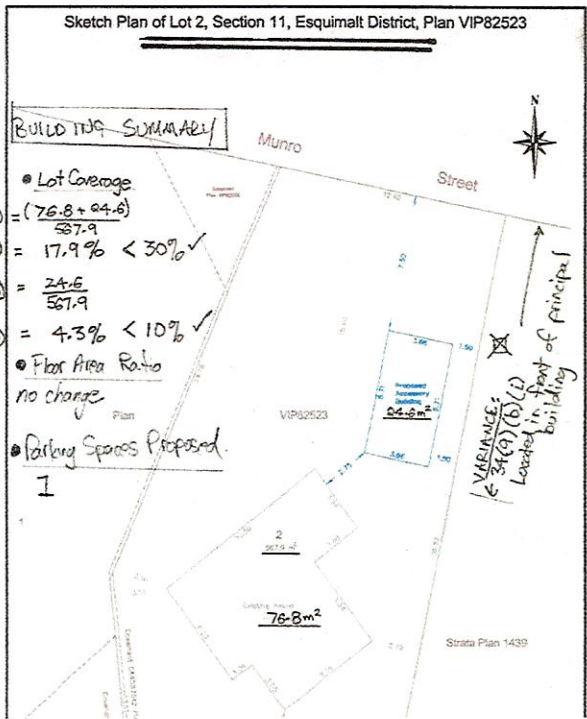
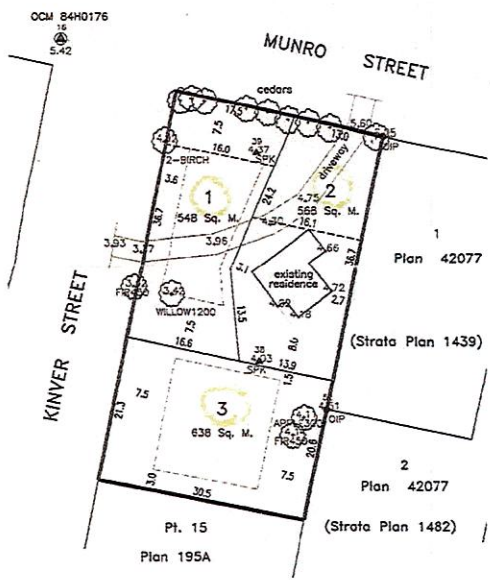
Thank you for taking your time to review this request. I will try to summarize the nature of this request as follows;

At 1129 Munro street there does not exist a garage parking stall as a result of a previous subdivision.

My wife and I are the owners of property 2 shown here, which was the original 80 year old principal building for properties 1, 2 and 3 combined.

When the subdivision was approved the garage for lot 2 was lost.

To maintain the principal building I need an accessory building (garage).



All provisions for the construction of an accessory building on this property are met with the exception of 34.9.b.i which does not allow accessory buildings from being located in front of the principal building, where the only space exists.

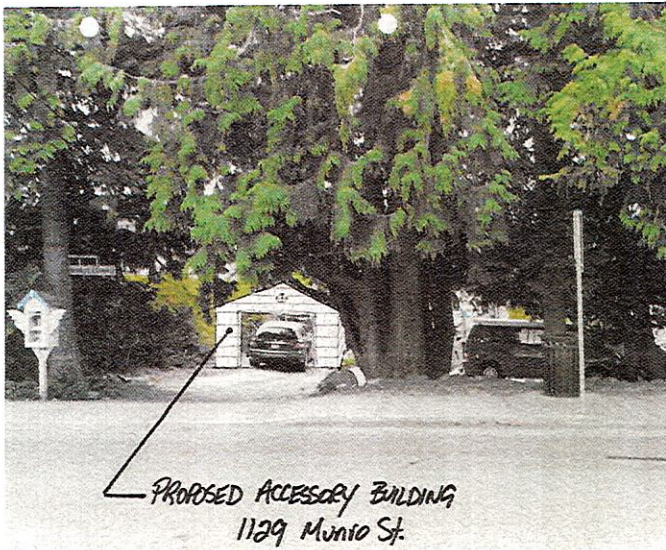
The finish on the side of the garage will be 18" boxed cedar siding shingles, sourced from a mill in Vancouver from samples off the principal building. The garage truss system will be sourced locally and engineered to best match the eaves of the principal building. The elevation of the SOG will be established so as not to change existing surface drainage, but allow for a complete skim coat of pea gravel to add to the existing. The existing principal building sanitary and storm drainage system was scoped and located (functioning well and located outside the footprint of the garage).

I have discussed the proposed garage with neighbors on both sides of our property, and across the street. Feedback was mostly positive with regard to matching the existing architecture, and the need for a garage. One observation made was with regard to the densification of the root system under the mature cedar trees lining Munro street, including both properties beside our property.

My response which seemed to be accepted, was that the garage is outside the drip line of the cedars where the surface root system is most prevalent, and the densification from vehicle traffic is well distributed by the existing pea gravel surface, and that an extra skim of pea gravel is planned for on final cleanup of the construction to further prevent densification.

The cedar trees at the front of our property are a valued asset to us in which we invest yearly in deep root fertilization, and professional trimming. The construction of a garage would also provide the opportunity to bring water to the front of our house for better maintenance of these prized trees during the extremes of summer drought.

Located near the waterfront on both Admirals and Constance there are several existing instances where properties have garages in front of their principal building. Many of these variances do not meet the current 7.5 meter setback requirement, and or, are not designed to match the principal building.



The siting of this garage proposal is approximately 1.2m below the sidewalk and largely covered by the cedar trees, will be built maintaining the character of an 80 year old house, and improves the streetscape.

I believe our request has no impact outside of our property line, and I look forward to a positive outcome.

Thank you again for your consideration of this variance.

Sincerely,

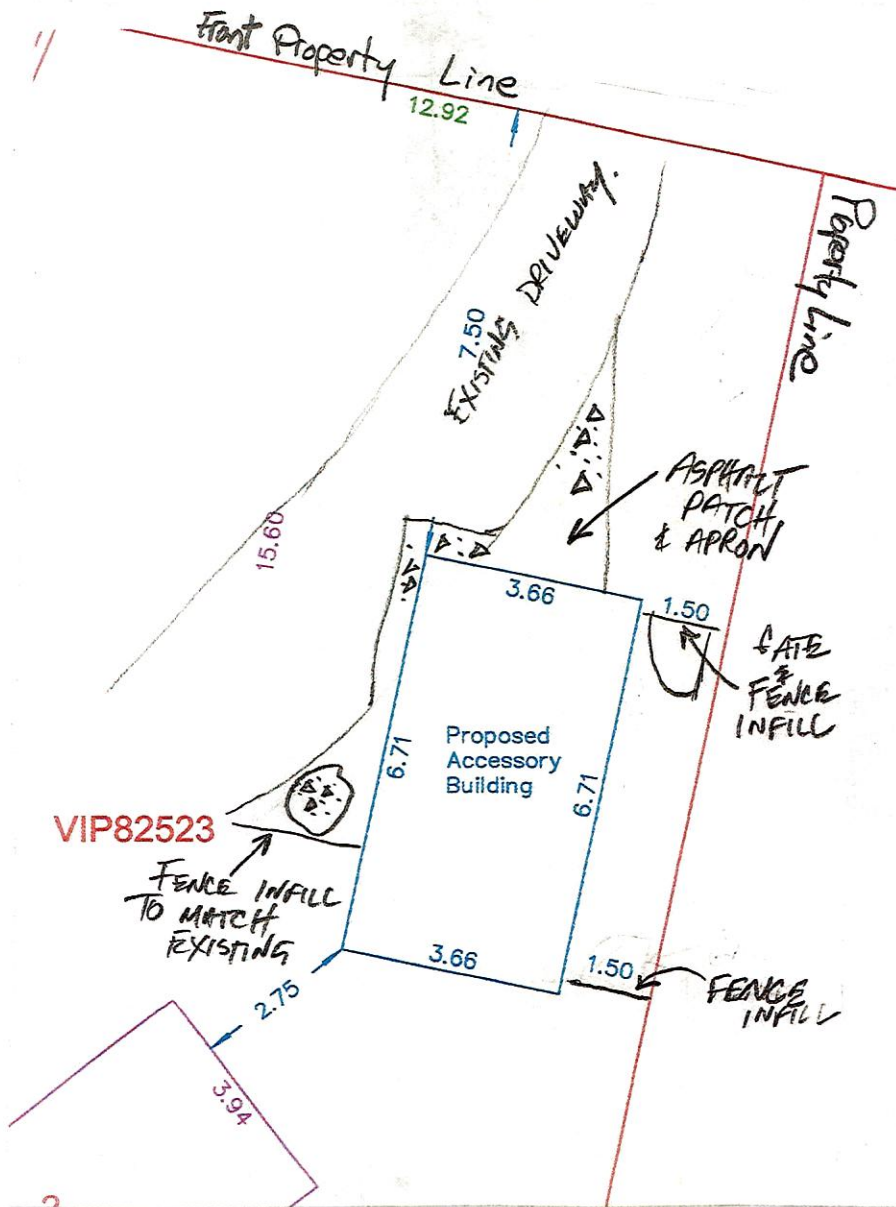
A handwritten signature in black ink, appearing to read "Jeff Duffield". The signature is stylized and somewhat cursive, with a long horizontal line extending to the right.

Jeff Duffield
1129 Munro Street

September 27, 2018

Dated

1129 Munro St. Plan VIP82523



RECEIVED

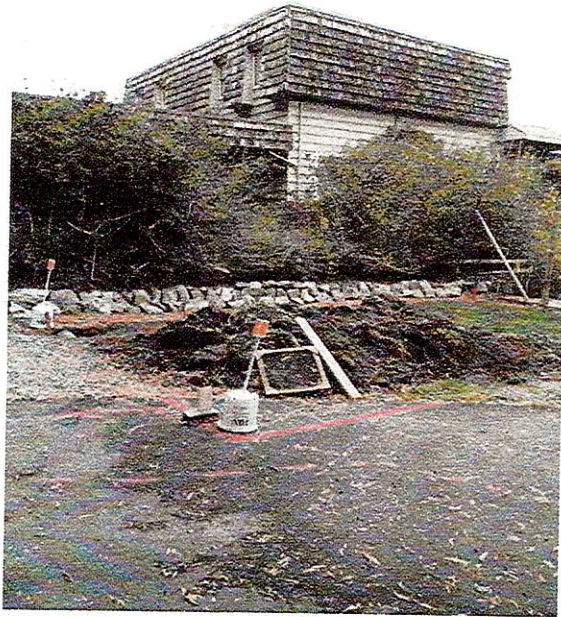
APR 08 2019

CORP. OF TOWNSHIP OF ESQUIMALT
DEVELOPMENT SERVICES

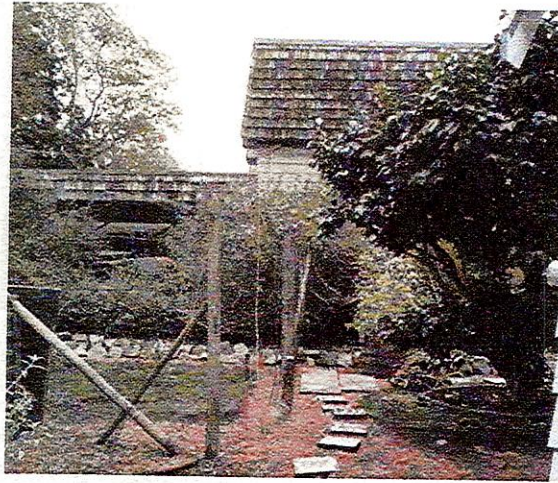
RO Oct 16, 18

1129 Munro St. - Plan VIP82523
PROPOSED ACCESSORY BUILDING
CHANGES TO EXISTING
LANDSCAPE PLAN L-2

EXISTING LANDSCAPE ELEVATIONS



1



2



3



4



R0 - Nov 4, 18

1129 Munro St - Plan VIP82523	
PROPOSED ACCESSORY BUILDING	
EXISTING ELEVATIONS	L-3



1129 Munro St - Plan VIP 82523

PROPOSED ACCESSORY BUILDING

EXISTING

LANDSCAPE PLAN L-1

RE APR 8, 19

RO OCT 16, 18

Sketch Plan of Lot 2, Section 11, Esquimalt District, Plan VIP82523



Munro

Street

Easement Plan VIP82525

Plan

VIP82523

2
567.9 m²

Existing House

Proposed Accessory Building

Strata Plan 1439

1

3

Easement CA4587542 Plan EPP54920
Covenant Plan
VIP82524

Certified correct this 30th day of June, 2016

Ryan Hourston

Ryan P. Hourston, BCLS

Distances are in metres

This sketch does not constitute a redefinition of the legal boundaries hereon described and is not to be used in any manner which would assume same.

Scale 1:125 File: 29728





CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

APC Meeting: May 21, 2019

STAFF REPORT

DATE: May 16, 2019

TO: Chair and Members of the Advisory Planning Commission

FROM: Janany Nagulan, Planner
Bill Brown, Director of Development Services

SUBJECT: DEVELOPMENT VARIANCE PERMIT
1217 Juno Street
[PID: 007-993-501, Lot 5, Section 11, Esquimalt District, Plan 1104]

RECOMMENDATION:

That the Esquimalt Advisory Planning Commission recommends to Council that the application for a Development Variance Permit, permitting the enclosure of the existing carport as per Architectural Plans prepared by Maverick Solution Limited stamped "Received May 2, 2019" and to include the following variances to the Zoning Bylaw 1992, No. 2050, for the property located at 1217 Juno Street [PID:007-993-501, Lot 5, Section 11, Esquimalt District, Plan 1104] be forwarded to Council with a **recommendation to either approve, approve with conditions, or deny the application; and provide reasons for the chosen recommendation.**

Zoning Bylaw, 1992, No.2050, Section 34 (9)(a)(i) – Setback Requirements – Principal Building – Front Setback. A 0.7 metre decrease to the requirement that no principal building shall be located within 7.5 metres of the Front Lot Line, [i.e. from 7.5 metres to 6.8 metres]

Zoning Bylaw, 1992, No. 2050, Section 34 (9)(a)(ii) – Setback Requirements – Principal Building – Side Setback. A 0.9 metres decrease to the total Setback of all Side yards not to be less than 4.5 metres. [i.e. from 4.5 metres to 3.6 metres]

Parking Bylaw, 1992, No. 2050, Section 9 (4) – Provision and Maintenance of Off – Street Parking and Loading Areas. An exemption from the requirement that parking spaces in residential zones shall be located no closer to the Front Lot Line than the front face of the Principal Building.

BACKGROUND:**Purpose of the Application**

The applicant is seeking variances to enclose the existing carport to create a mudroom and office. A porch is also proposed to be located at the front face of the building which will encroach into the Front Setback. The enclosure of the carport will move the parking space beyond the front face of the building. In September 2018 the Esquimalt Board of Variance approved the relaxation of the Western Side Setback as a result a variance is required to the total Setback of all Side Yards.

Context

Applicant/ Owner: Emma Day & Graham Busch

Property Size: Metric: 626 m² **Imperial:** 6738.21 ft²

Existing Land Use: Single Family Dwelling

Surrounding Land Uses:

North: Single Family Dwelling
South: Single Family Dwelling
East: Single Family Dwelling
West: Single Family Dwelling

Existing Zoning: RS-1 [Single Family Residential] [No change required]

Present Official Community Plan Designation: Low Density Residential [No change required]

Zoning

The subject property is in the Single Family Residential [RS-1] Zone. The Front Setback siting requirement for a Principal Building in the RS-1 Zone is that no Principal Building shall be located 7.5 metres of the Front Lot Line. A variance is required as the proposed porch will be 6.8 metres from the front Lot Line. In September 2018 the Esquimalt Board of Variance approved the relaxation of the Western Side Setback from the required 3.0 metres to 2.1 metres however variance to the total Setback of all Side Yards is required from 4.5 metres to 3.6 metres. The proposal meets all other requirements of the zone including Floor Area Ratio and Lot Coverage.

Parking

The enclosure of the carport will remove the parking space beyond the front face of the building therefore a parking variance is required.

Comment from Other Departments

Engineering Services: We have reviewed the plans submitted with the application for a Development Variance Permit at 1217 Juno Street. The proposed Variance does not have an impact on the Engineering aspects of this property. Engineering has no requirements for this development variance permit application.

Building Inspection: No concerns. Subject to review for Building Code and Bylaw compliance at time of Building Permit application.

Fire Services: No Concerns

Public Notification

As this is a development variance permit application, should it proceed to Council, a notice will be mailed to tenants and owners of properties within 50 metres (164 feet) of the subject property.

ALTERNATIVES:

1. Forward the application for a Development Variance Permit to Council with a **recommendation of approval including reasons for the recommendation.**
2. Forward the application for a Development Variance Permit to Council with a **recommendation of approval including specific conditions and including reasons for the recommendation.**
3. Forward the application for a Development Variance Permit to Council with a **recommendation of denial including reasons for the recommendation.**

1217 JUNO st, ESQUIMALT, BC, V9H 5J9



MAVERICKS

411-797 Tye Rd
Victoria, BC
V9A 7R4
tel: (250) 634-0196

ATELIER
OBLIK inc.
210 Choumontel, Sainte-Therese,
Quebec, J7E 5H5
tel: (514) 704-5221



PROJECT DATA:

SCOPE: EXTERIOR AND INTERIOR WORK.
TRANSFORM EXISTING CARPORT INTO
ENCLOSED LIVING AREA. INCLUDE
WITH OFFICE SPACE AND ENTRANCE.
SLAB ON GRADE CONSTRUCTION.
INFLOOR HEAT.

BASEMENT: NO CHANGE. (1023.375 sq.ft
GROSS)

ENTRANCE LEVEL: CARPORT
CONVERSION. (256.5 sq.ft GROSS)

UPPER FLOOR: NO CHANGE. (1054.5 sq.ft
GROSS)

PROJECT ADDRESS: 1217 JUNO ST.
ESQUIMALT, BC, V9A 5J9

ZONING DESCRIPTION: RS-1 SINGLE
FAMILY RESIDENTIAL
ESQUIMALT DISTRICT

SETBACK INFO

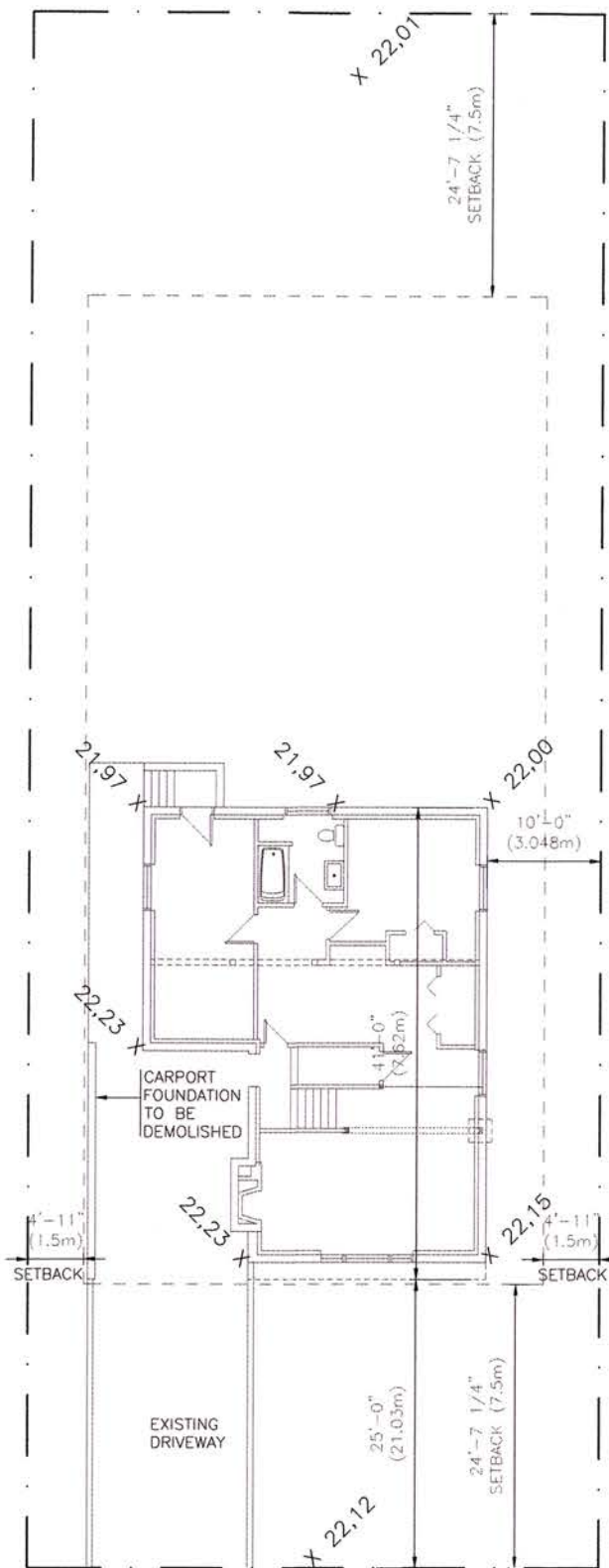
REQUIRED: 24'-6" PROPOSED: 22'-6"
FRONT STAIRS <400mm, EXCLUDED FROM SETBACK

VARIANCE REQUEST

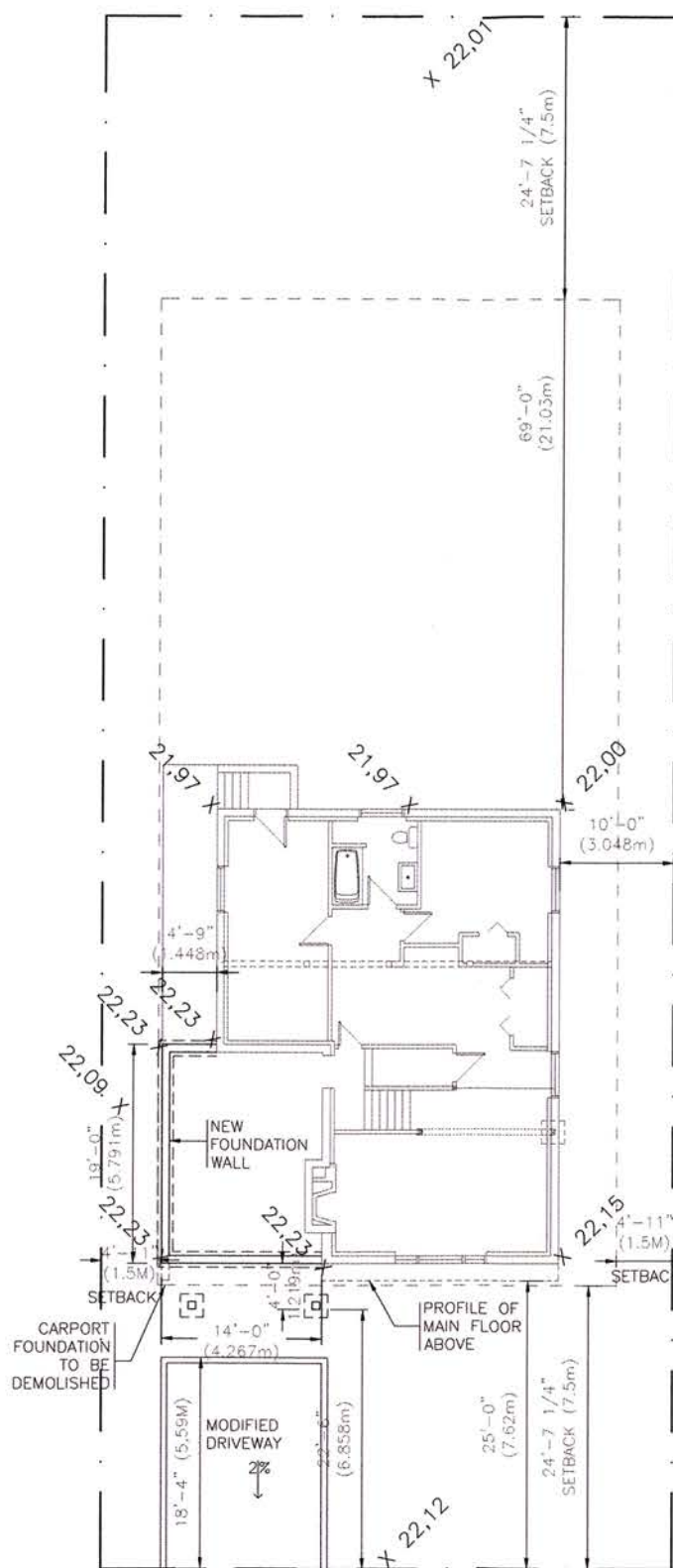
REQUIRED: 2ft PROJECTION INTO FRONT SETBACK
RELAXATION (PENDING)
REQUIRED: RELAXATION OF DRIVEWAY PARKING
ALLOWED IN FRONT OF RESIDENCE.
(PENDING)

DRAWING LIST

A-1 SITE PLAN
A-2 PLAN AND ELEVATIONS



EXISTING FOUNDATION PLAN
1/16" = 1'-0"



PROPOSED FOUNDATION PLAN
1/16" = 1'-0"

SEAL

12		
11		
10		
9		
8		
7		
6		
5		
4		
3		
2	VARIANCE	2019-03-19
1	PRELIMINARY	2019-02-23

NO. DATE DESCRIPTION
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AND BE RESPONSIBLE FOR ALL DIMENSIONS AND
CONDITIONS ON PROJECT AND THIS OFFICE SHALL BE
INFORMED OF ANY VARIATIONS FROM DIMENSIONS
AND CONDITIONS SHOWN ON THE DRAWING. DO NOT
SCALE DRAWING.

JUNO CARPORT

PROJECT

PRELIMINARY
SITE
PLAN
TITLE

ARCHITECTURE

TRADE

RG.S. 1/8" = 1'-0" K0175
DRAW BY SCALE FILE NO

KM
REVIEW BY

A1/2

APPROVED BY

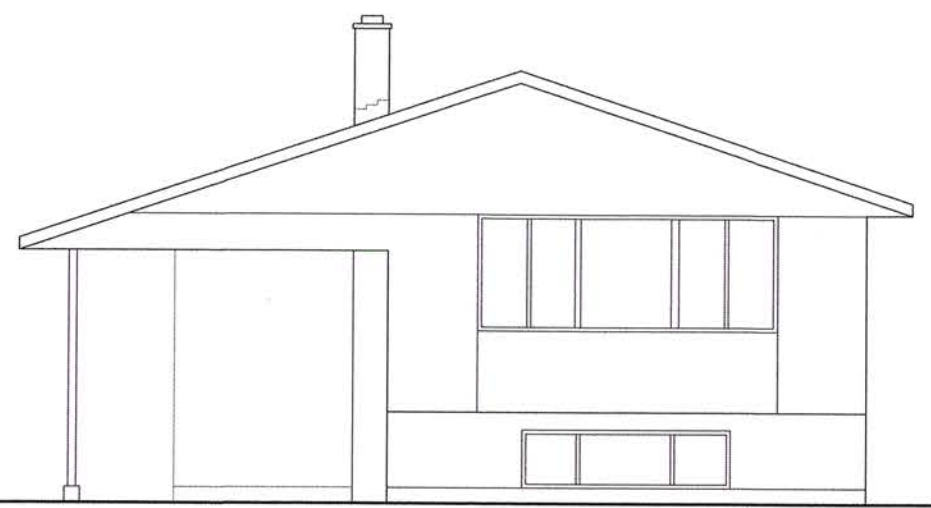
1217 JUNO st, ESQUIMALT, BC, V9H 5J9



MAVERICKS

411-797 Tye Rd
Victoria, BC
V8A 7R4
tel: (250) 634-0196

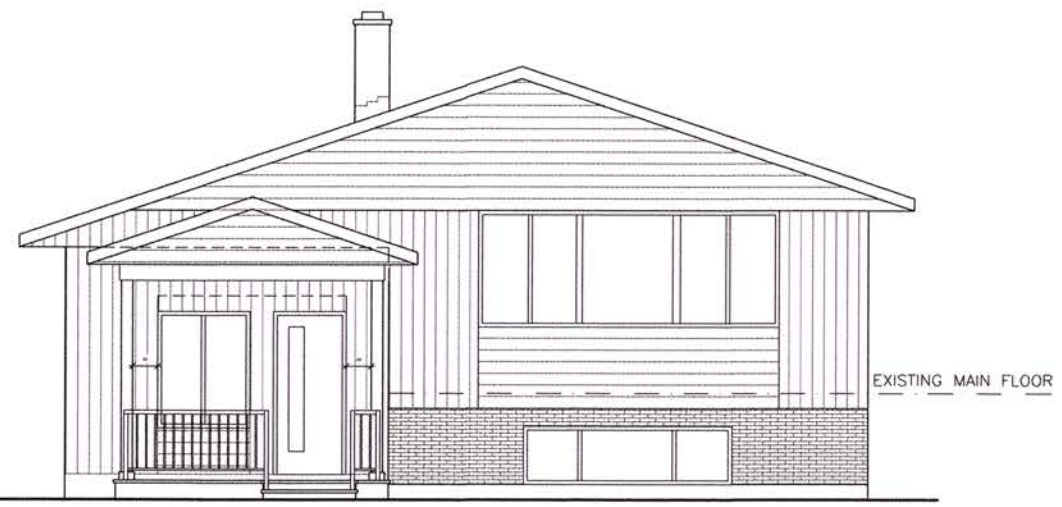
ATELIER
OBLIK inc.
210 Choumontel, Sainte-Therese,
Quebec, J7E 5H5
tel: (514) 704-5221



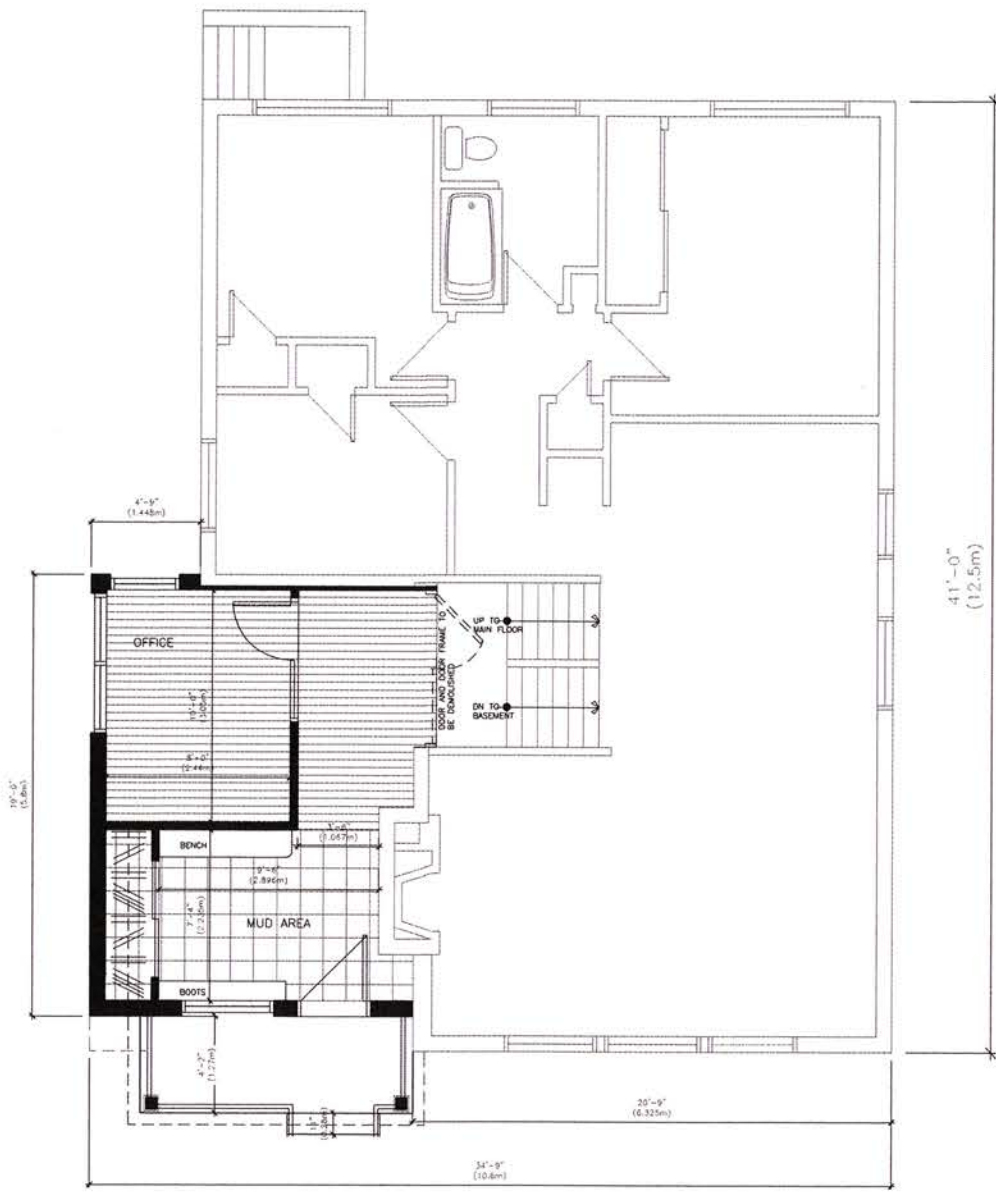
EXISTING FRONT ELEVATION
1/4"=1'-0"



PROPOSED LEFT SIDE ELEVATION
1/4"=1'-0"



PROPOSED FRONT ELEVATION
1/4"=1'-0"



PROPOSED MAIN FLOOR PLAN
1/4"=1'-0"

SEAL

NO.	DATE	DESCRIPTION
12		
11		
10		
9		
8		
7		
6		
5		
4		
3		
2	VARIANCE	2019-03-19
1	PRELIMINARY	2019-02-23

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INFORMED OF ANY VARIATIONS FROM DIMENSIONS
AND CONDITIONS SHOWN ON THE DRAWING. DO NOT
SCALE DRAWING.

JUNO CARPORT

PROJECT

PRELIMINARY
PLAN
ELEVATIONS
TITLE

ARCHITECTURE

TRADE		
RG.S. DRAW BY	1/4"=1'-0" SCALE	K0175 FILE NO

KM
REVIEW BY

A2/2

APPROVED BY



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

APC Meeting: May 21, 2019

STAFF REPORT

DATE: May 16, 2019

TO: Chair and Members of the Advisory Planning Commission

FROM: Janany Nagulan, Planner
Bill Brown, Director of Development Services

SUBJECT: DEVELOPMENT VARIANCE PERMIT
502 Paradise Street
[PID: 009-173-242, Lot 37, Block F, Section 11, Esquimalt District, Plan 292]

RECOMMENDATION:

That the Esquimalt Advisory Planning Commission recommends to Council that the application for a Development Variance Permit, bringing the existing principal building into compliance to allow for the reconstruction of the front stairs and construction of a porch roof as per plans stamped "Received April 5, 2019" and BC Surveyor Site Plan prepared by Wey Mayenburg Land Surveying Inc. stamped "Received April 5, 2019" and to include the following variances to the Zoning Bylaw 1992, No. 2050, for the property located at 502 Paradise Street [PID:009-173-242, Lot 37, Block F, Section 11, Esquimalt District, Plan 292] be forwarded to Council with a **recommendation to either approve, approve with conditions, or deny the application; and provide reasons for the chosen recommendation.**

Zoning Bylaw, 1992, No.2050, Section 34 (9)(a)(i) – Setback Requirements – Principal Building – Front Setback. A 4.2 metre decrease to the requirement that no principal building shall be located within 7.5 metres of the Front Lot Line [i.e. from 7.5 metres to 6.8 metres]

Zoning Bylaw, 1992, No. 2050, Section 34 (9)(a)(ii) – Setback Requirements – Principal Building – Side Setback. A 0.8 metres decrease to the requirement that no principal building shall be located within 1.5 metres of an Interior Lot Line. [i.e. from 1.5 metres to 0.7 metres]

BACKGROUND:**Purpose of the Application**

The applicant is seeking to reconstruct the front stairs and construct a porch roof for the existing principal building on the property which is currently non-conforming. The current Front Setback is 3.3 metres and the Western Side Setback is 0.7m. For the applicant to make these changes, the existing dwelling must come in to compliance therefore a Development Variance Permit is required.

Context

Applicant/ Owner: Helen Edwards and Howard Debeck

Property Size: Metric: 634 m² **Imperial:** 6824.32 ft²

Existing Land Use: Single Family Dwelling

Surrounding Land Uses:

North:	Two Family Dwelling
South:	Two Family Dwelling
East:	Single Family Dwelling
West:	Single Family Dwelling

Existing Zoning: RS-1[Single Family Residential] [No change required]

Present Official Community Plan Designation: Low Density Residential [No change required]

Zoning

The subject property is in the Single Family Residential Zone [RS-1]. The requirements for a Principal Building in the zone states that no Principal Building shall be located 7.5 metres of the Front Lot Line and 1.5 metres of an Interior Side Lot Line, with the total Setback of all Side Yards not be less than 4.5 metres. The existing Principal Building is located 3.3 metres of the Front Lot Line and 0.7 metres from the Interior Lot Line. A variance will not be required for the total Setback of all Side Yards as the Eastern Side Setback is 4.0 metres from the Interior Lot Line for a total of 4.7metres for all Side Yards.

Comment from Other Departments

Engineering Services: We have reviewed the plans submitted with the application for a Development Variance Permit at 502 Paradise Street. The proposed Variance does not have an impact on the Engineering aspects of this property. Engineering has no requirements for this development variance permit application.

Building Inspection: No concerns. Subject to review for Building Code and Bylaw compliance at time of Building Permit application.

Fire Services: No Concerns

Parks & Recreation: No Concerns

Public Notification

As this is a development variance permit application, should it proceed to Council, a notice will be mailed to tenants and owners of properties within 50 metres (164 feet) of the subject property.

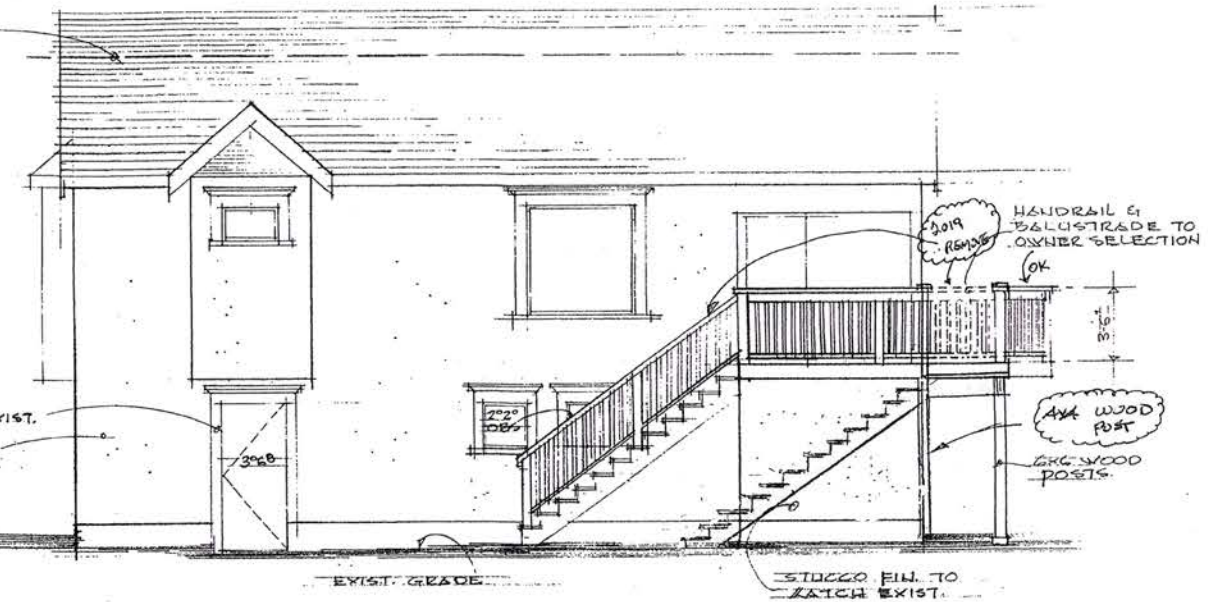
ALTERNATIVES:

1. Forward the application for a Development Variance Permit to Council with a **recommendation of approval including reasons for the recommendation.**
2. Forward the application for a Development Variance Permit to Council with a **recommendation of approval including specific conditions and including reasons for the recommendation.**
3. Forward the application for a Development Variance Permit to Council with a **recommendation of denial including reasons for the recommendation.**



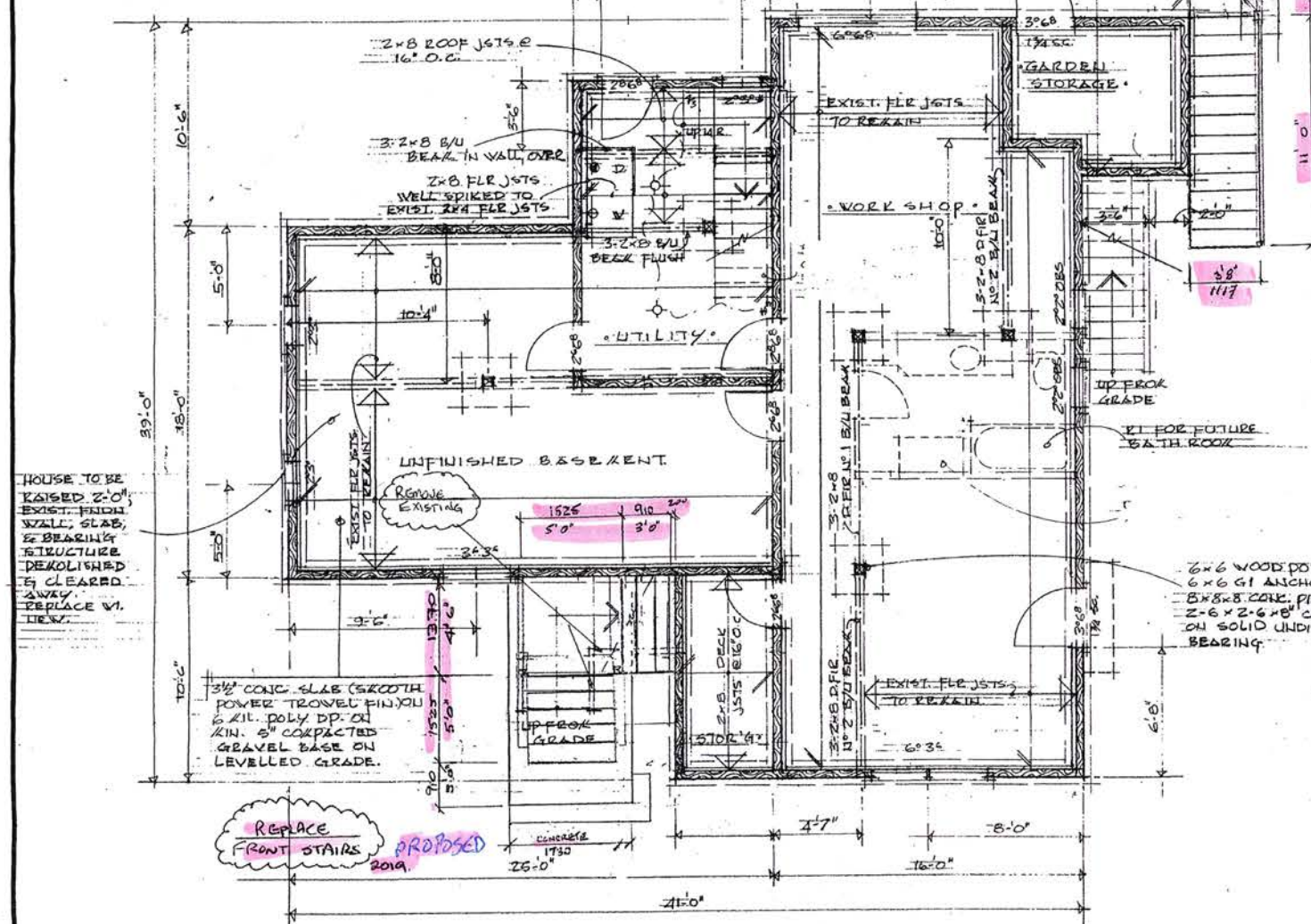
REPLACE FRONT STAIRS AND SHED ROOF
PROPOSED 2019

SOUTH ELEVATION
1/4" = 1'-0"



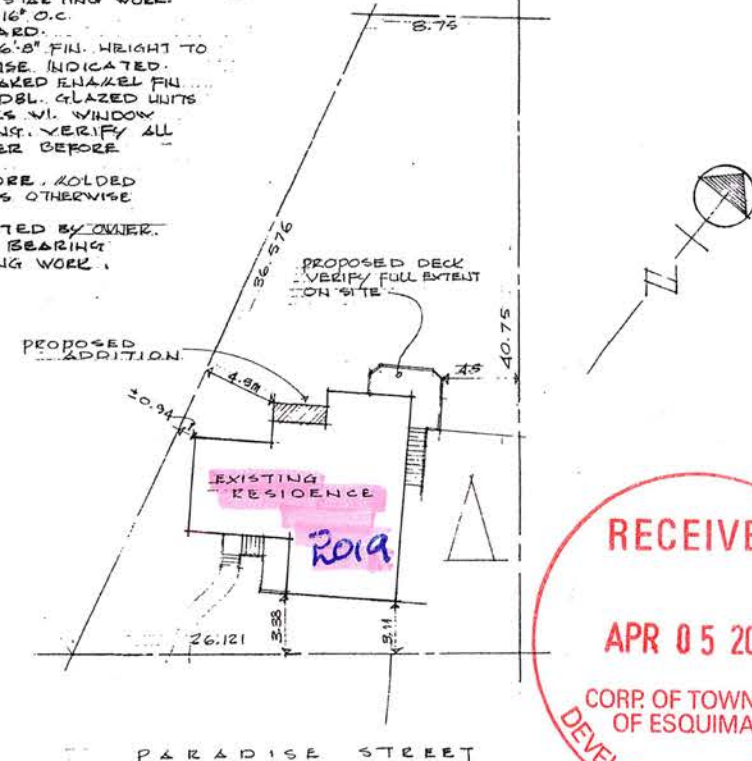
EAST ELEVATION
1/4" = 1'-0"

- GENERAL NOTES:**
- VERIFY ALL DIMENSIONS BEFORE STARTING WORK FROM DRAWINGS.
 - DIMENSIONS GIVEN TO FACE OF FRAMING IN NEW WORK; O/S ALL IN EXISTING WORK.
 - CONTRACTOR TO CONFIRM BEFORE FRAMING, FORMING OR DEMOLISHING.
 - GENERALLY MATCH EXIST. LINES, PLANES, PITCHES, & SURFACES, UNLESS OTHERWISE NOTED.
 - ALL FRAMING MATERIALS TO BE NO. 2 & BETTER (STANDARD & BETTER OR GRS) GRADE D. FIR OR HEK-FIR, UNLESS OTHERWISE NOTED.
 - AREA OF WORK GENERALLY - PROTECT EXIST. SURFACES TO REMAIN, CAREFULLY DEMOLISH & FILL NEW OPENINGS, MAKE GOOD ALL SURFACES TO MATCH EXIST.
 - CONFIRM LOCATIONS/TYPES OF ALL ELEC. OUTLETS & FIXTURES W/ OWNER BEFORE STARTING WORK.
 - EVT. & INT. WALLS 2x4 STUDS @ 16" O.C.
 - INT. FIN. 1/2" THICK GYP. WALLBOARD.
 - ALL WINDOW HEADS FRAMED @ 6" O/S FIN. HEIGHT TO MATCH DOORS UNLESS OTHERWISE INDICATED.
 - WINDOWS TO BE ALUM. FRAME, SAVED ENAMEL FIN. TO OWNERS APPROVAL. CW & DBL. GLAZED UNITS VERIFY ALL ROUGH OPENING SIZES W/ WINDOW MANUFACTURER BEFORE FRAMING. VERIFY ALL OPERATING WINDOWS W/ OWNER BEFORE ORDERING.
 - ALL INT. DOORS TO BE SOLID CORE, MOLDED PANEL TYPE, PRE-PRIMED UNLESS OTHERWISE INDICATED BY OWNER.
 - HEATING SYSTEM TO BE SELECTED BY OWNER.
 - CONTRACTOR TO VERIFY ALL BEARING CONDITIONS BEFORE STARTING WORK.



REPLACE FRONT STAIRS
PROPOSED 2019

LOWER FLOOR PLAN
1/4" = 1'-0"



SITE PLAN
1:200

LEGAL DESCRIPTION

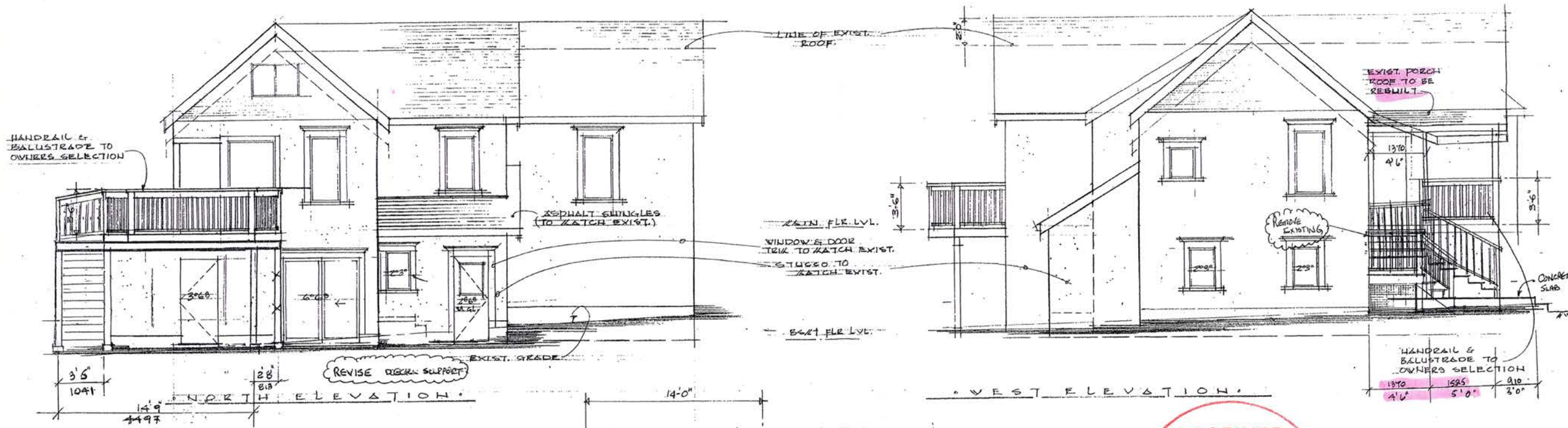
LOT - 37
BLOCK - F
SECTION - 11
ESQUIMALT DISTRICT
PLAN - 292



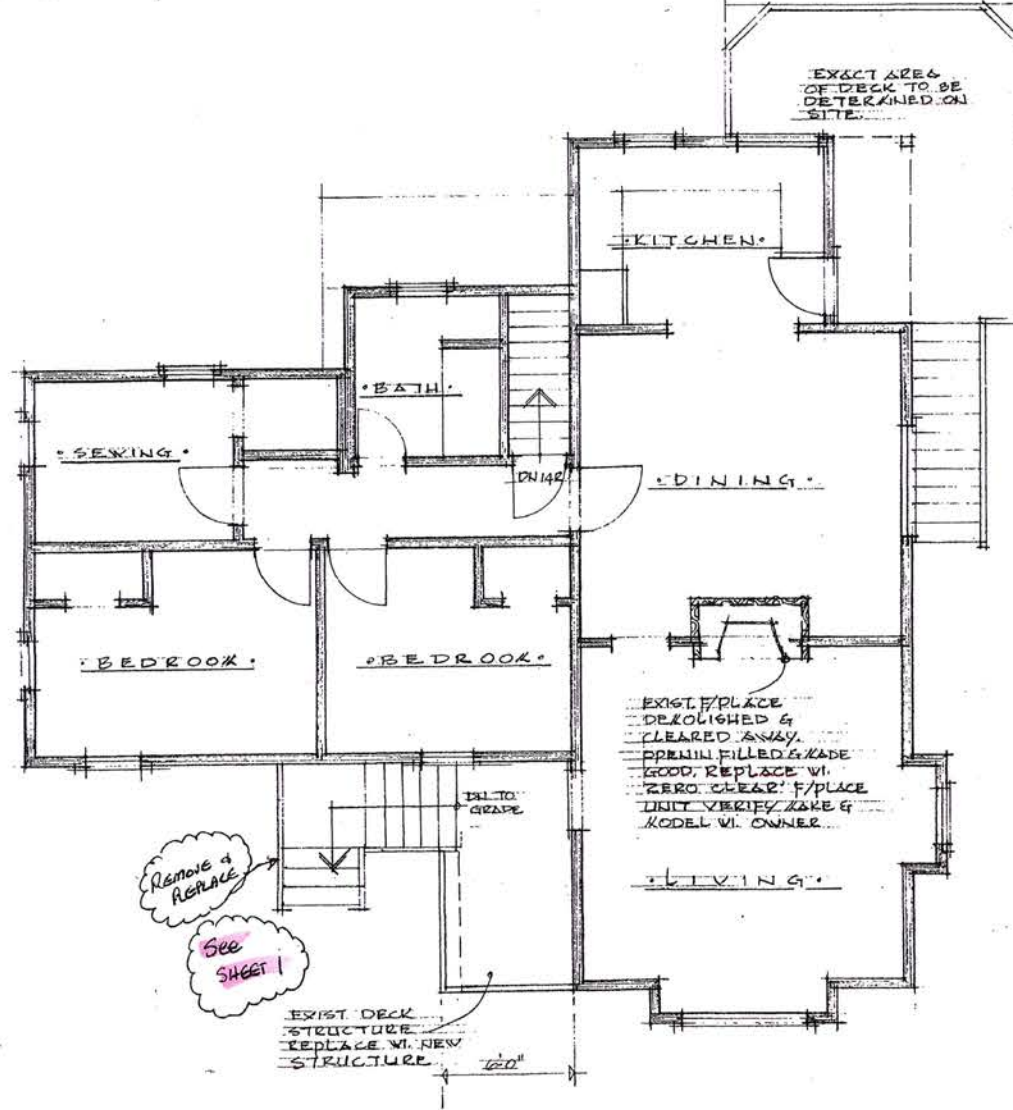
DESIGNER: APE
DESIGN: OWNER/BANKS
SCALE: AS NOTED
DATE: MAY 3, 2019
PROJECT: PROJ. N° 9125

PROPOSED ALTERATION TO RESIDENCE
FOR
MR. & MRS. H. DE BECK
LOWER FLOOR & SITE PLAN & ELEVATIONS
502 PARADISE ST.
VICTORIA B.C.

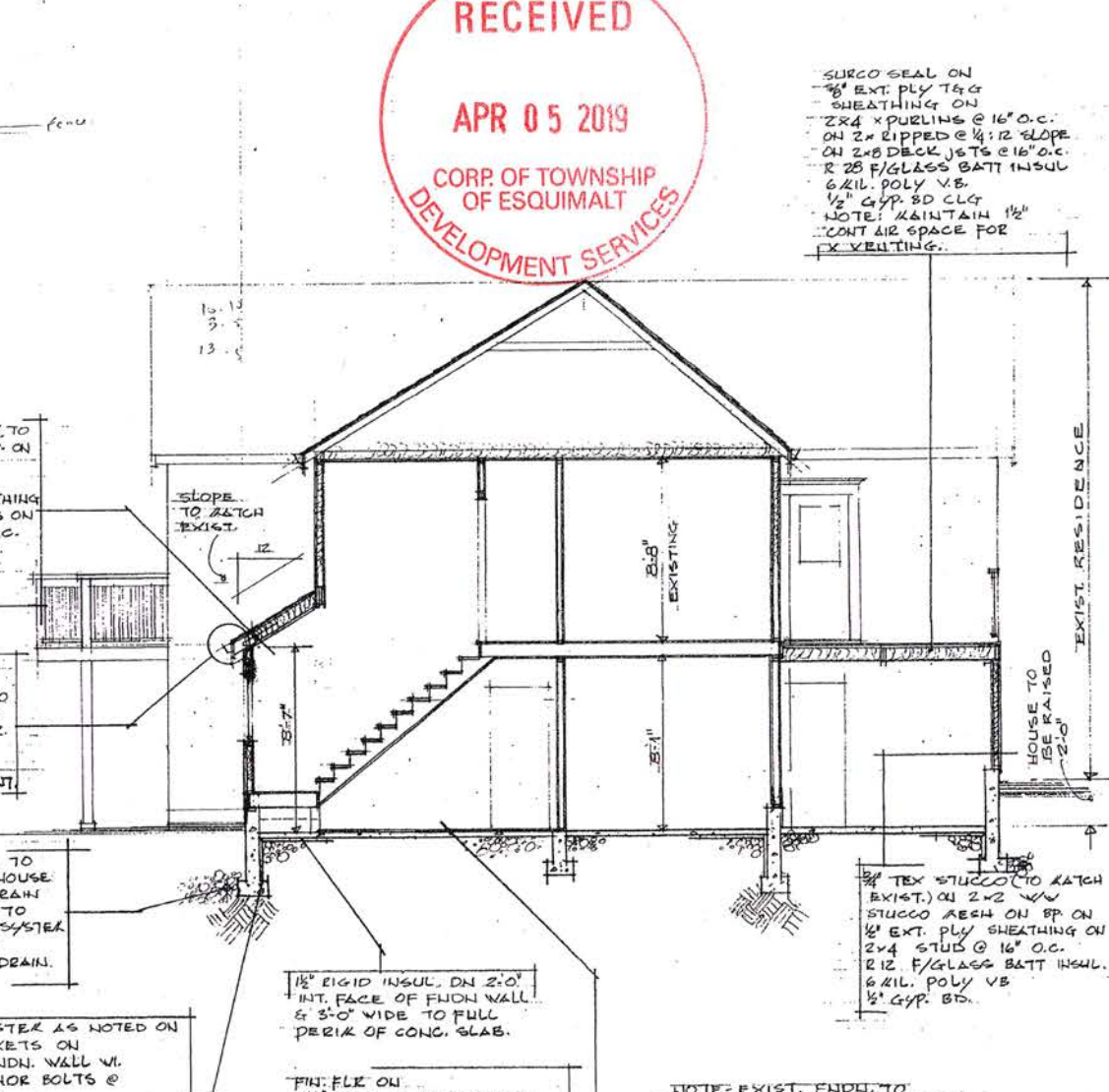
banks design
victoria 386-3331



DRAWN: AFR
 DESIGN: OWNER/BANKS
 SCALE: 1/4" = 1'-0"
 DATE: MAY 3, 91
 PROJ. NO. 9125
 DRUG NO.



MAIN FLOOR PLAN
 EXISTING



RECEIVED
APR 05 2019
 CORP. OF TOWNSHIP
 OF ESQUIMALT
 DEVELOPMENT SERVICES

SURCO SEAL ON
 2x8 EXT. PLY T&G
 SHEATHING ON
 2x4 PURLINS @ 16" O.C.
 ON 2x8 RIPPED @ 1/4" @ 12 SLOPE
 ON 2x8 DECK JSTS @ 16" O.C.
 R 28 F/GLASS BATT INSUL
 6x12 POLY VB.
 1/2" GYP. BD. CLG
 NOTE: MAINTAIN 1/2"
 CONT AIR SPACE FOR
 VENTING.

ASPHALT SHINGLES (TO
 MATCH EXIST.) ON EP ON
 6x12 POLY VEAVE
 PROTECTION ON
 1/2" EXT. PLY ROOF SHEATHING
 ON 2x4 SPACE PURLINS ON
 2x8 ROOF JSTS @ 16" O.C.
 R 28 F/GLASS BATT
 INSUL
 6x12 POLY VB.
 1/2" GYP. BD.

ALUM. GUTTER & EWL
 (TO MATCH EXIST.) ON
 2x8 FASCIA BD. TIED TO
 SOLID LEADER PIPES
 (SEPARATE FROM PEAK
 DRAIN).
 STUCCO SOFFIT W/
 2" CONT SCREENED VENT.

1/2" PERF. DRAIN TO
 FULL PERIM. OF HOUSE
 LAID IN 2IN. 8" DRAIN
 ROCK, CONNECT TO
 RM. COLLECTION SYSTEM
 IN SOLID PIPE &
 LEAD TO STORM DRAIN.

WALL SYSTEM AS NOTED ON
 SILL GASKETS ON
 8" CONC. FNDN. WALL W/
 1/2" 8 ANCHOR BOLTS @
 8" O.C.
 2 COATS ASPHALT EWL
 WATERPROOFING ON EXT. FACE
 8x16 STRIP FTNG. ON SOLID
 UNDIST. BEARING.

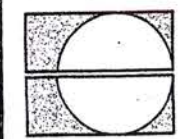
1/2" RIGID INSUL. DN 2'-0"
 INT. FACE OF FNDN WALL
 & 3'-0" WIDE TO FULL
 PERIM. OF CONC. SLAB.

FIN. FLR ON
 3/4" CONC. SLAB (SMOOTH
 POWER TROWEL FIN.) ON
 6x12 POLY O.P. ON
 2IN. 5" COMPACTED GRAVEL
 BASE ON LEVELLED GRADE.

NOTE: EXIST. FNDN. TO
 BE DEMOLISHED &
 CLEARED SWAY
 HOUSE TO BE RAISED
 2'-0"

PROPOSED ALTERATION TO RESIDENCE

banks design



MR & MRS H. DE BECK
 502 PARADISE ST.
 VICTORIA 386-3331

Site Plan Of:
Lot 37, Block F, Section 11,
Esquimalt District, Plan 292.
P.I.D. 009-173-242



Scale = 1:250

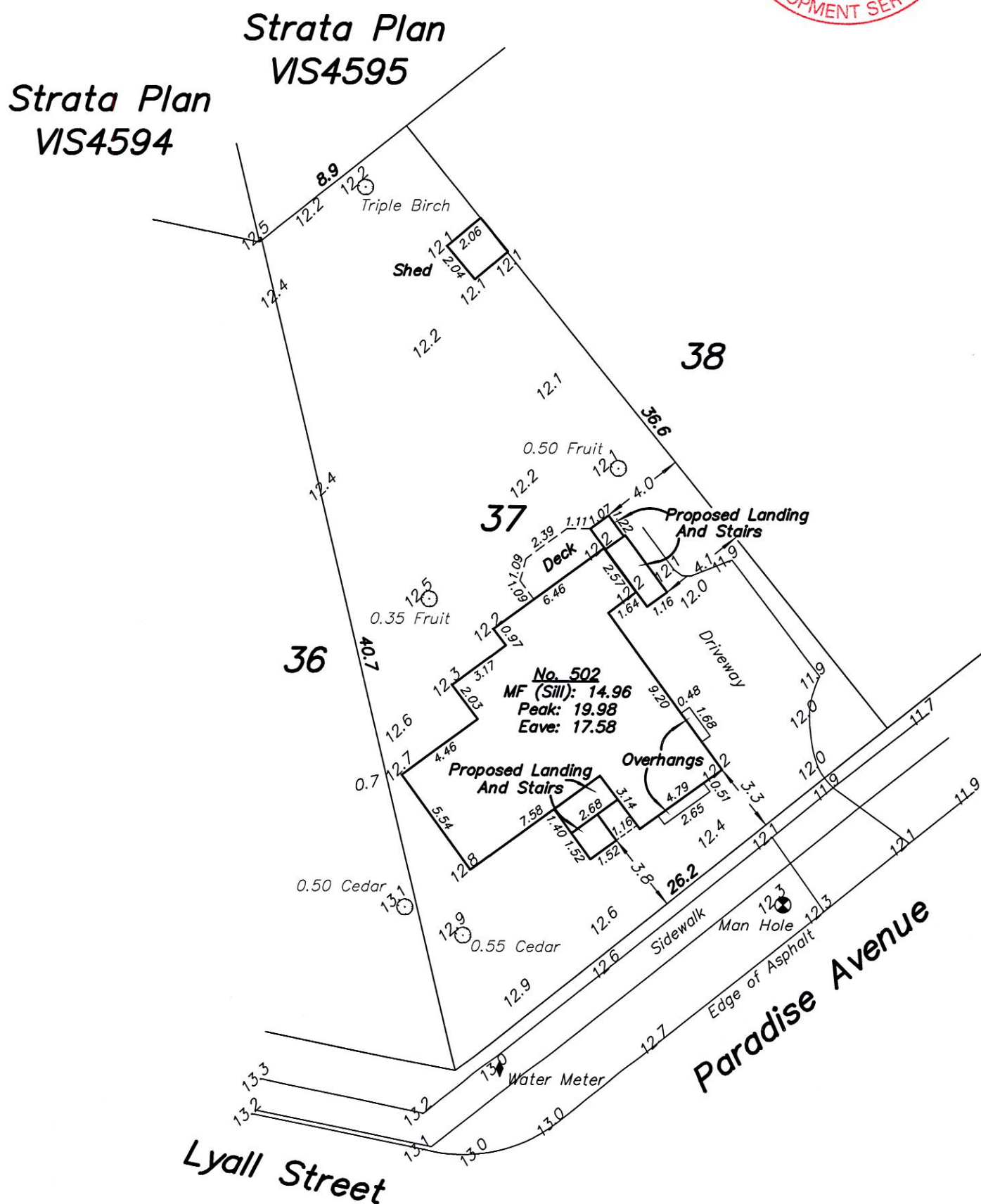
Dated this 7th day of August, 2018.

Distances and elevations shown are in metres.

Elevations are based on geodetic datum CVD28BC and derived from OCM 84H0144.

This site plan is for building and design purposes and is for the exclusive use of our client.

This document shows the relative location of the surveyed structures and features with respect to the boundaries of the parcel described above. This document shall not be used to define property lines or property corners.



The subject property is affected by the following registered documents:
123456G, FH456789, FV192837.

Wey Mayenburg Land Surveying Inc.

www.weysurveys.com
 #4-2227 James White Boulevard
 Sidney, BC V8L 1Z5
 Telephone (250) 656-5155
 File: 180233\SIT\LE



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

APC Meeting: May 21, 2019

STAFF REPORT

DATE: May 15, 2019

TO: Chair and Members of the Advisory Planning Commission

FROM: Alex Tang, Planner
Bill Brown, Director of Development Services

SUBJECT: **Development Permit and Development Variance Permit Application**
10-300 Plaskett Place
[PID 017-658-187 Strata Lot 10, Section 11, Esquimalt District, Strata Plan VIS2246, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1]

RECOMMENDATION:

That the Esquimalt Advisory Planning Commission recommends to Council that the application for a Development Permit and a Development Variance Permit, authorizing construction of a patio deck as illustrated in the plans prepared by Java Designs, and sited in accordance with the BC Land Surveyor's Site Plan prepared by Michael E. Claxton, both stamped "Received April 9, 2019", and including the following variances for the property located at 10-300 Plaskett Place [PID 017-658-187 Strata Lot 10, Section 11, Esquimalt District, Strata Plan VIS2246, together with an interest in the common property in proportion to the unit entitlement of the strata lot as shown on Form 1] be forwarded to Council with a recommendation **to either approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

Zoning Bylaw, 1992, No. 2050, Section 34 (8)(a) – Lot Coverage: A 1.2% increase to the requirement that all Principal Buildings, Accessory Buildings and Structures combined, shall not cover more than 30% of the Area of a Parcel [i.e. from 30% to 31.2%].

Zoning Bylaw, 1992, No. 2050, Section 34 (9)(b)(i) – Siting Requirements – Accessory Building – Front Setback: Exemption from the requirement that no Accessory Building shall be located in front of the front face of the Principal Building.

Zoning Bylaw, 1992, No. 2050, Section 34 (9)(a)(ii) – Siting Requirements – Accessory Building – Side Setback: A 1.1 metre decrease to the requirement that no Accessory Building shall be located within 1.5 metres of an Interior Side Lot Line [i.e. from 1.5 metres to 0.4 metre].

Zoning Bylaw, 1992, No. 2050, Section 34 (9)(a)(iv) – Siting Requirements – Accessory Building – Building Separation: Exemption from the requirement that no Accessory Building shall be located within 2.5 metres of a Principal Building.

BACKGROUND:**Purpose of the Application:**

The applicant is proposing to construct a new deck that extends from an existing upper patio area over an existing lower patio area. As the proposed deck is a separate, self supporting structure, it is governed in the Zoning Bylaw as an Accessory Building.

This site is located within Development Permit Area No. 1 - Natural Environment, Development Permit Area No. 2 – Protection of Development From Hazardous Conditions, No. 3 – Enhanced Design Control Residential, No. 7 - Energy Conservation and Greenhouse Gas Reduction and No. 8 - Water Conservation of the Township's Official Community Plan. Correspondingly, the Development Permit application will respect the guidelines of these areas where applicable.

Context

Applicant/Owner: Gary and Didinell Brennan

Property Size: Metric: 564 m² Imperial: 6076 ft²

Existing Land Use: Single Family Residential

Surrounding Land Uses:

North: Single Family Residential

South: Single Family Residential

West: Single Family Residential

East: Single Family Residential

Existing Zoning: RS-1 [Single Family Residential] [no change required]

Zoning

In Zoning Bylaw, 1992, Bylaw No. 2050 (as amended) defines an "Accessory Building" as a Building or Structure:

- (1) Having an area greater than 1.0 square metre;
- (2) Located on the same Parcel as the Principal Building yet subordinate in area and extent, and whose purpose is customarily incidental to the Principal Building;
- (3) Having no shower/bathtub or cooking facilities.

Hence, the proposed deck is a structure governed as an Accessory Building in the Zoning Bylaw.

Lot Coverage: The proposed deck has an area of 15 square metres and adds 2.6% to the existing lot coverage of 28.6%. The overall proposed lot coverage is at 31.2% which is 1.2% more than the allowed 30%.

Front Setback: The proposed deck is in front of the front face of the Principal Building. As the deck is governed as an Accessory Building, it is not allowed to be sited in front of the Principal Building.

Side Setback: The proposed deck has a interior side setback of 0.4 metre, compared to the required 1.5 metres.

Building Separation: The proposed deck has no separation from the Principal Building compared to the required 2.5 metres.

Official Community Plan

The proposed deck is inconsistent with the following guideline from Development Permit Area No. 1 – Natural Environment as the deck is considered a structure:

- 18.5.1.3 New Buildings/ structures shall not be located within 10 m of the high watermark of the Strait of Juan de Fuca.

There exists another guideline that states:

- 18.5.1.6 Variances to 'Building Height' and 'Siting Requirements' will be considered where natural areas and trees are being protected.

This house has already been built with a backyard that consists mostly of a rocky shoreline so the opportunities to protect trees and natural areas may be limited. Nonetheless, the adherence to this guideline can provide an opportunity to achieve consistency with the Development Permit Area guidelines.

Other guidelines that may be applicable include:

- 19.5.4 The use of construction materials that may leach toxic chemicals over time into the land or water should be avoided.
- 24.5.2.11 In exposed marine locations select durable materials that will withstand weather and sea spray, to ensure low maintenance costs and infrequent replacement needs.

Many of the Development Permit Area guidelines are not applicable for this proposed deck that will be covering an existing outdoor impermeable patio.

Comments From Other Departments

The plans for this proposal were circulated to other departments and the following comments were received:

Building Inspection: Building to be constructed to requirements of BC Building Code 2018 and municipal bylaws. Applicant must address all issues contained within the Township Development Protocol should application be approved. Plans will be reviewed for compliance with BC Building Code upon submission of a Building Permit application.

Engineering Services: Engineering staff has no concerns with this application.

Parks Services: Parks Services staff has no concerns with this application.

Fire Services: Fire Services staff has completed a preliminary review of the proposed plans and have no concerns at this time.

Public Notification

As this is a Development Variance application, should it proceed to Council, notice would be mailed to tenants and owners of properties within 50m (164ft) of the subject property.

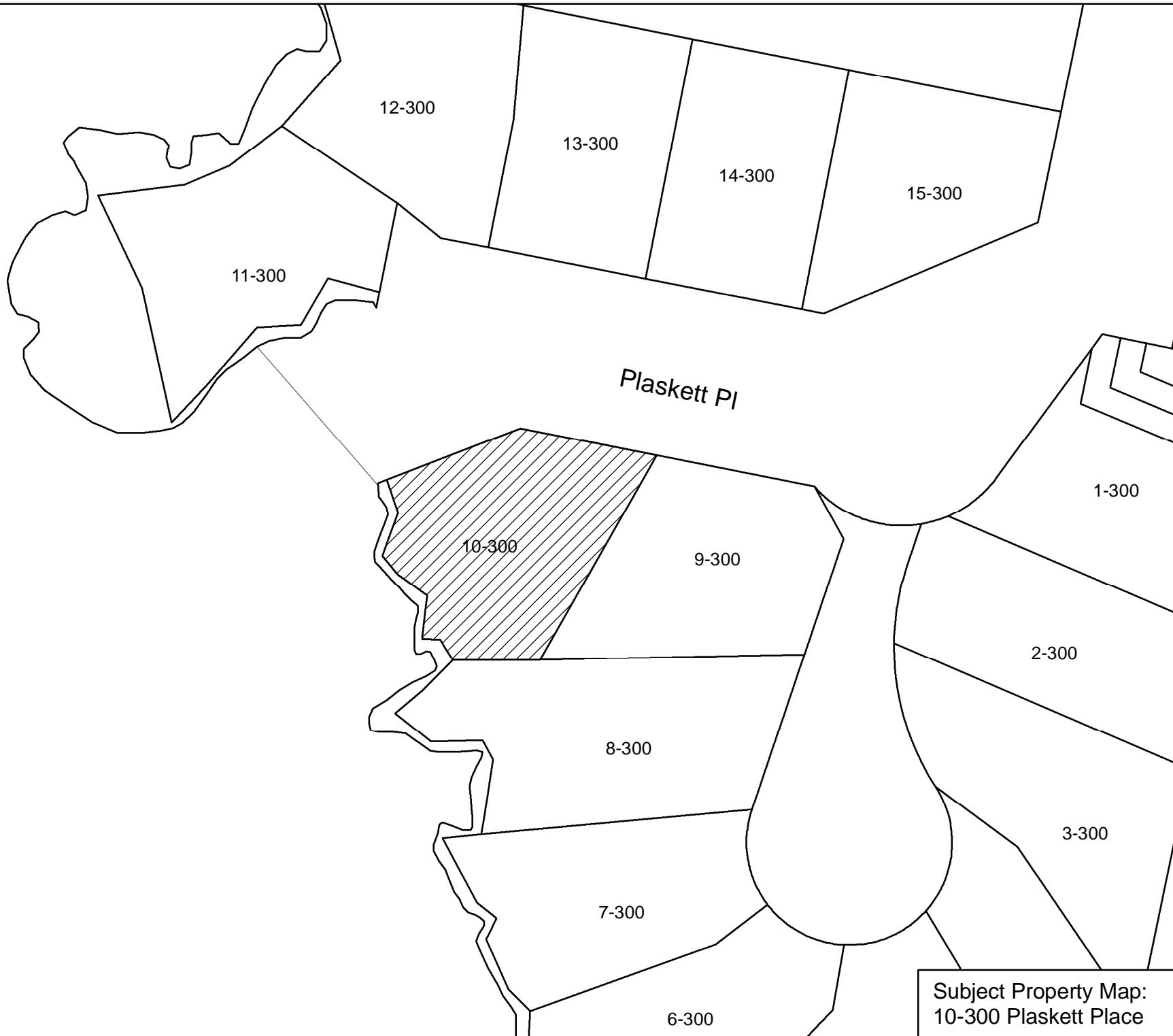
Questions for Consideration

What concerns, if any, do you have in regards to the construction of a structure, albeit a deck and not an enclosed building intended for habitation, within 10 metres of the high water mark of the Strait of Juan de Fuca?

What opportunities are there to protect natural areas on the subject property?

ALTERNATIVES:

1. Forward the application for a Development Permit and a Development Variance Permit to Council with a **recommendation of approval including reasons for the recommendation.**
2. Forward the application for a Development Permit and a Development Variance Permit to Council with a **recommendation of approval including specific conditions and including reasons for the recommendation.**
3. Forward the application for a Development Permit and a Development Variance Permit to Council with a **recommendation of denial including reasons for the recommendation.**



Subject Property Map:
10-300 Plaskett Place



12-300

13-300

14-300

15-300

11-300

PLASKETT PL

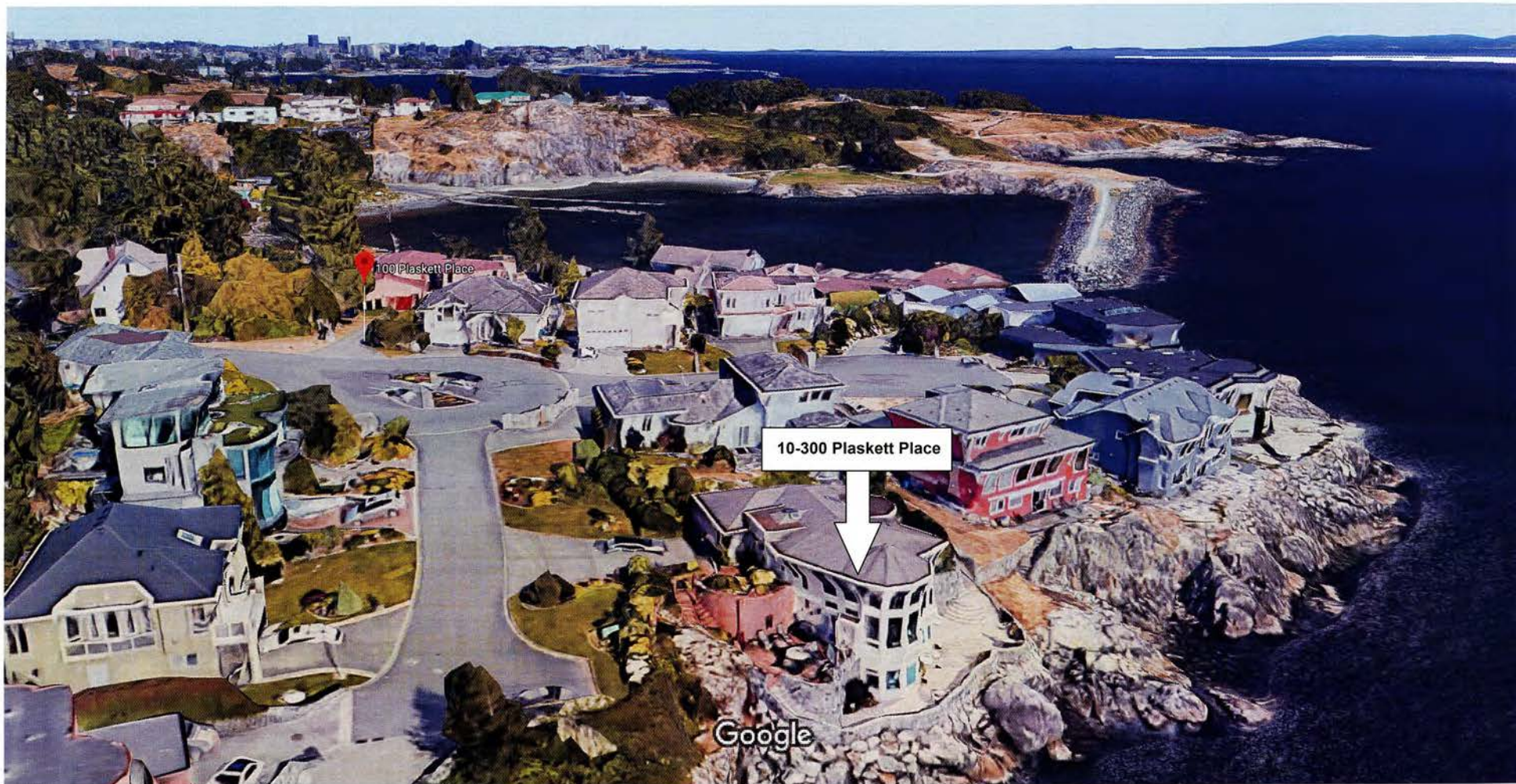
10-300

9-300

2-300

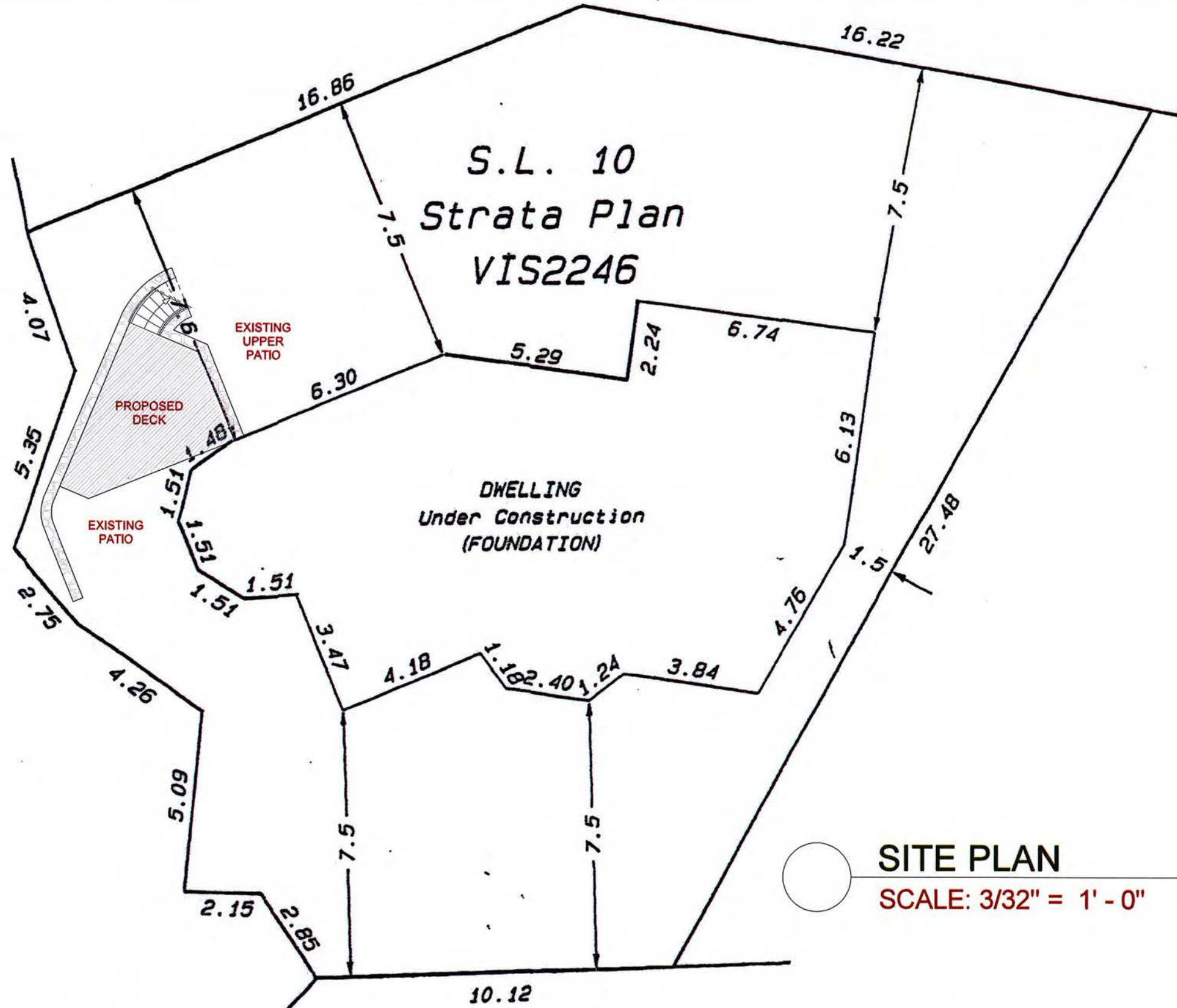
8-300

7-300



RECEIVED
APR 09 2019
CORP OF TOWNSHIP
OF ESQUIMALT
DEVELOPMENT SERVICES





SITE PLAN
SCALE: 3/32" = 1' - 0"

ISSUE DATE:
APRIL 09, 2019

DRAWN BY:
KYLE LEGGETT







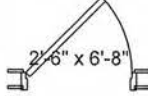
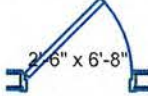



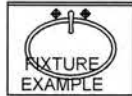


DRAWING NAME:
SITE PLAN

CUSTOMER:
GARY AND DIDI BRENNAN

ADDRESS:
#10 300 PLASKETT PLACE,
ESQUIMALT



RENOVATION LEGEND

<u>EXISTING:</u>		<u>NEW:</u>			
	EXT. 2X4 WALL		NEW 2X4 WALL		NEW RAILING
	EXT. 2X6 WALL		NEW 2X6 WALL		NEW COUNTER
	EXT. DOOR		NEW DOOR	<u>DEMO:</u>	
	EXT. WINDOW		NEW WINDOW		REMOVE WINDOW
	EXT. FIXTURE		NEW FIXTURE		REMOVE WALL



ISSUE DATE:
APRIL 09, 2019

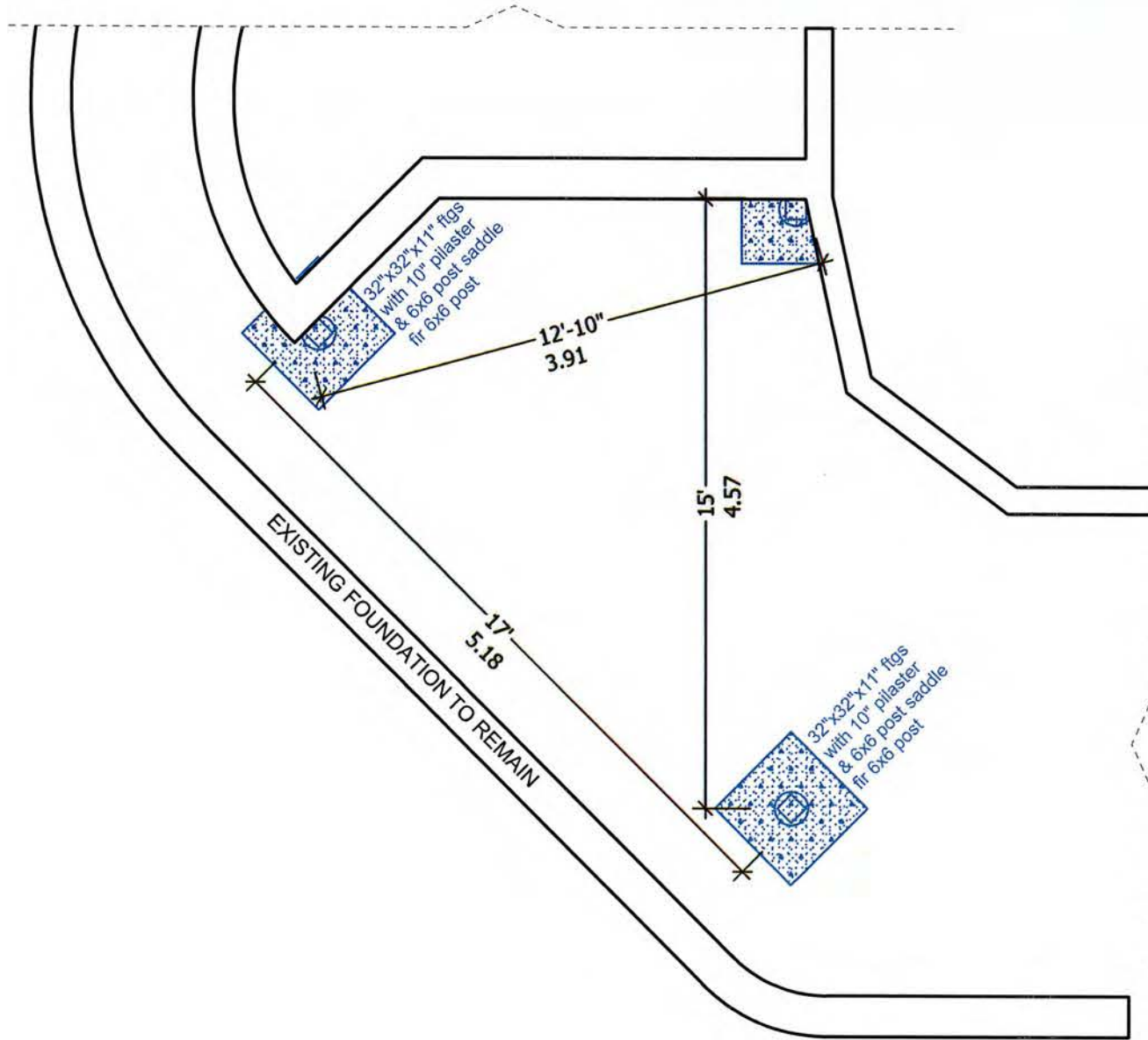
DRAWN BY:
KYLE LEGGETT

DRAWING NAME:
RENOVATION LEGEND


CUSTOMER:
GARY AND DIDI BRENNAN

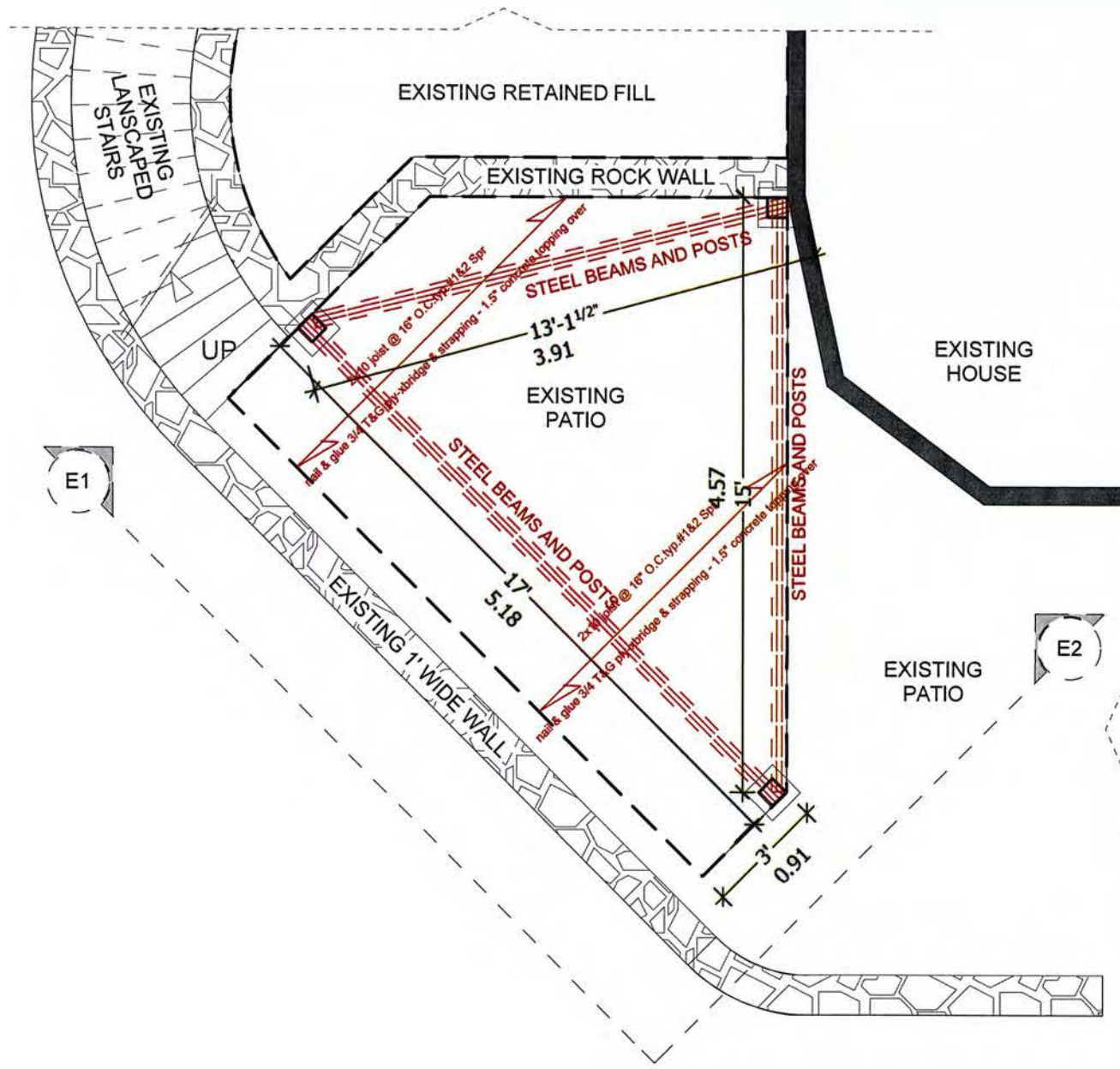
ADDRESS:
#10 300 PLASKETT PLACE,
ESQUIMALT

 **JAVA DESIGNS**
WHERE LINES ON PAPER BECOME WALLS ON SITE
PH 250.590.2468 FX 250.590.4577 www.javadesigns.ca



○ PROPOSED FOUNDATION PLAN
 SCALE: 1/4" = 1' - 0"

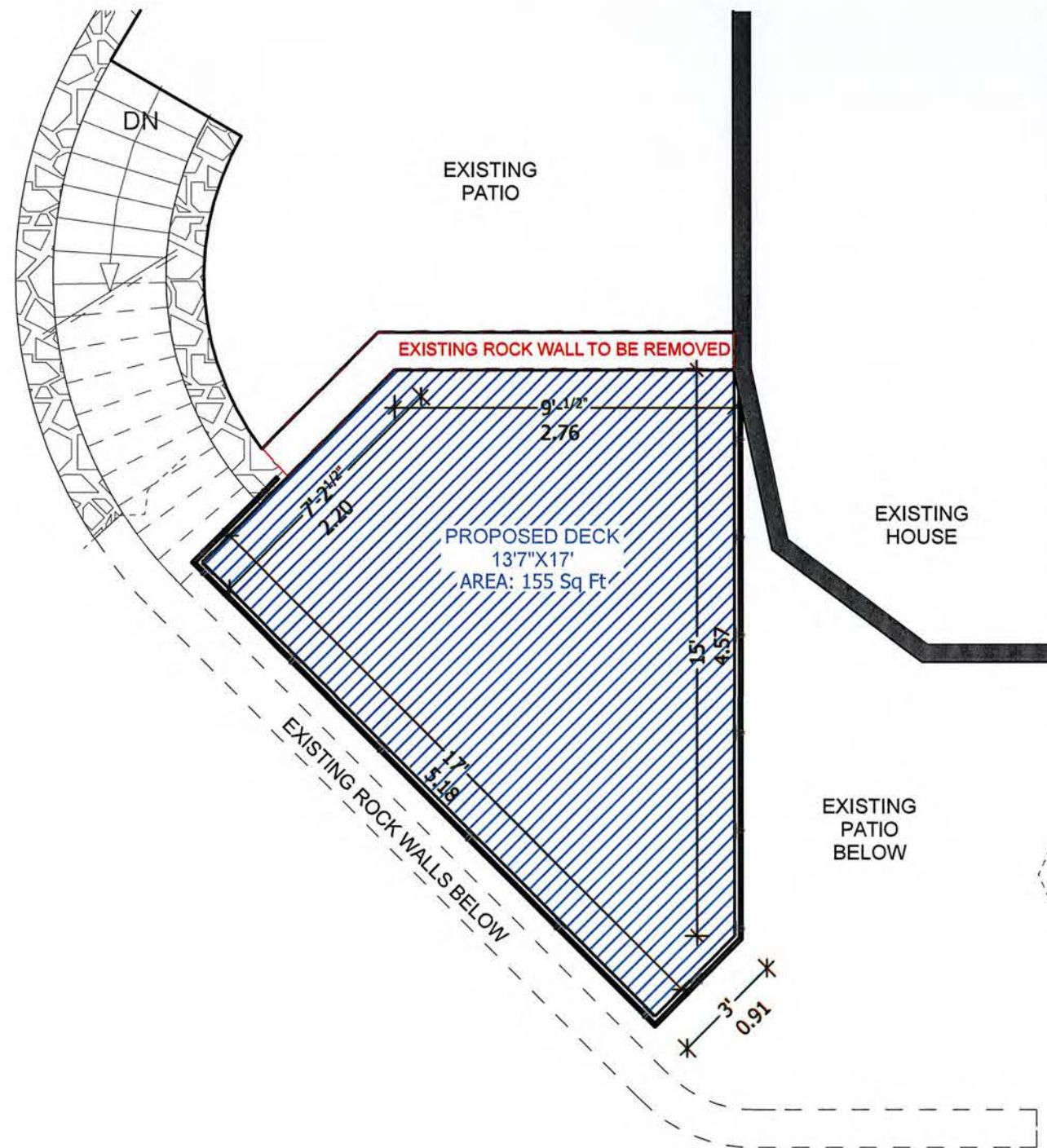
ISSUE DATE: APRIL 09, 2019	DRAWN BY: KYLE LEGGETT	DRAWING NAME: PROPOSED FOUNDATION PLAN	CUSTOMER: GARY AND DIDI BRENNAN	ADDRESS: #10 300 PLASKETT PLACE, ESQUIMALT	 JAVA DESIGNS WHERE LINES ON PAPER BECOME WALLS ON SITE PH 250.590.2468 FX 250.590.4577 www.javadesigns.ca
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PROPOSED LOWER FLOOR/ PATIO
SCALE: 1/4" = 1' - 0"

ISSUE DATE: APRIL 09, 2019	DRAWN BY: KYLE LEGGETT	DRAWING NAME: PROPOSED LOWER FLOOR/ PATIO	CUSTOMER: GARY AND DIDI BRENNAN	ADDRESS: #10 300 PLASKETT PLACE, ESQUIMALT
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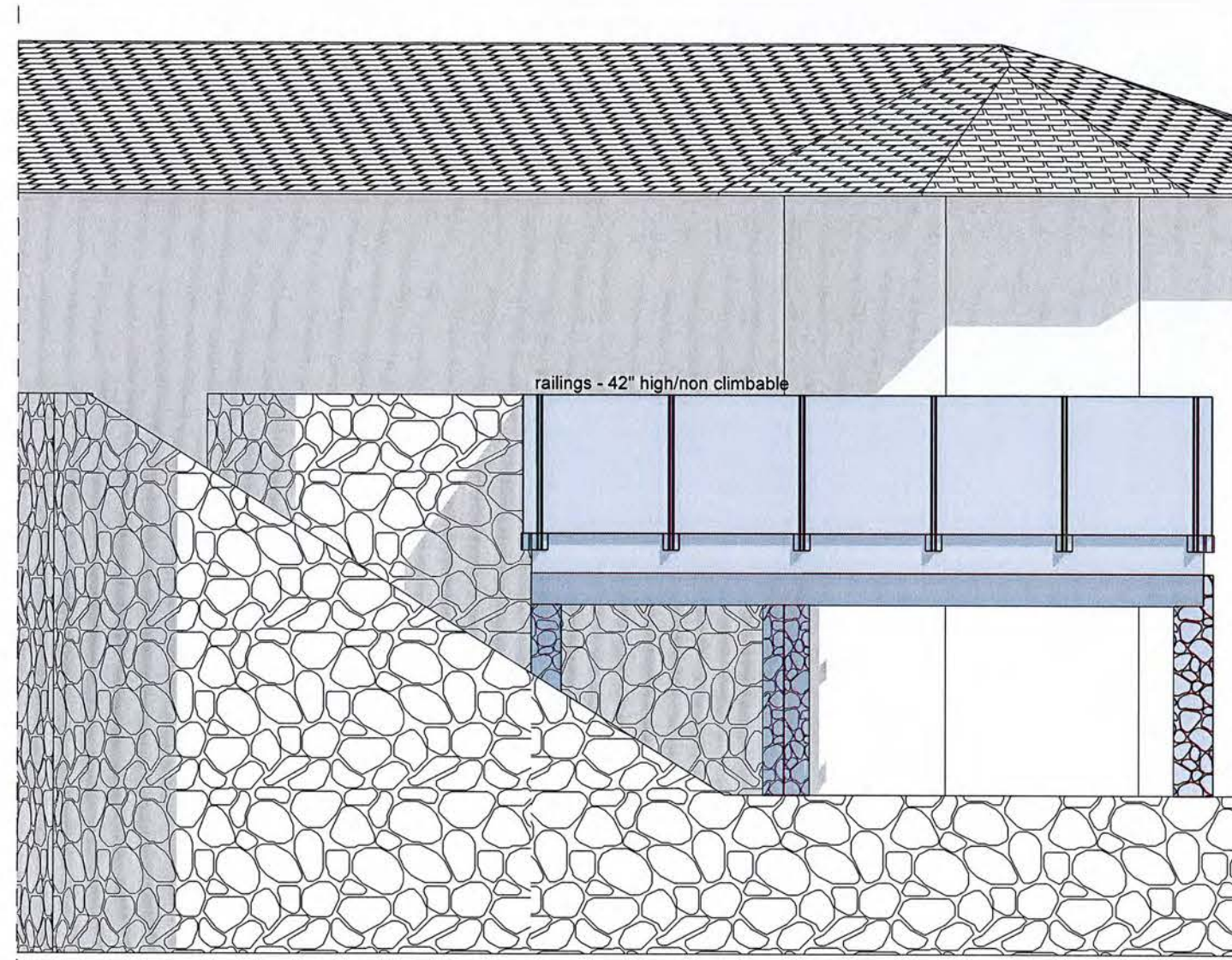




○ PROPOSED UPPER FLOOR/ PROPOSED DECK
SCALE: 1/4" = 1' - 0"

ISSUE DATE: APRIL 09, 2019	DRAWN BY: KYLE LEGGETT	DRAWING NAME: PROPOSED UPPER FLOOR/ PROPOSED DECK	CUSTOMER: GARY AND DIDI BRENNAN	ADDRESS: #10 300 PLASKETT PLACE, ESQUIMALT
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E1 **PROPOSED WEST ELEVATION**
SCALE: 1/4" = 1' - 0"

ISSUE DATE:
APRIL 09, 2019

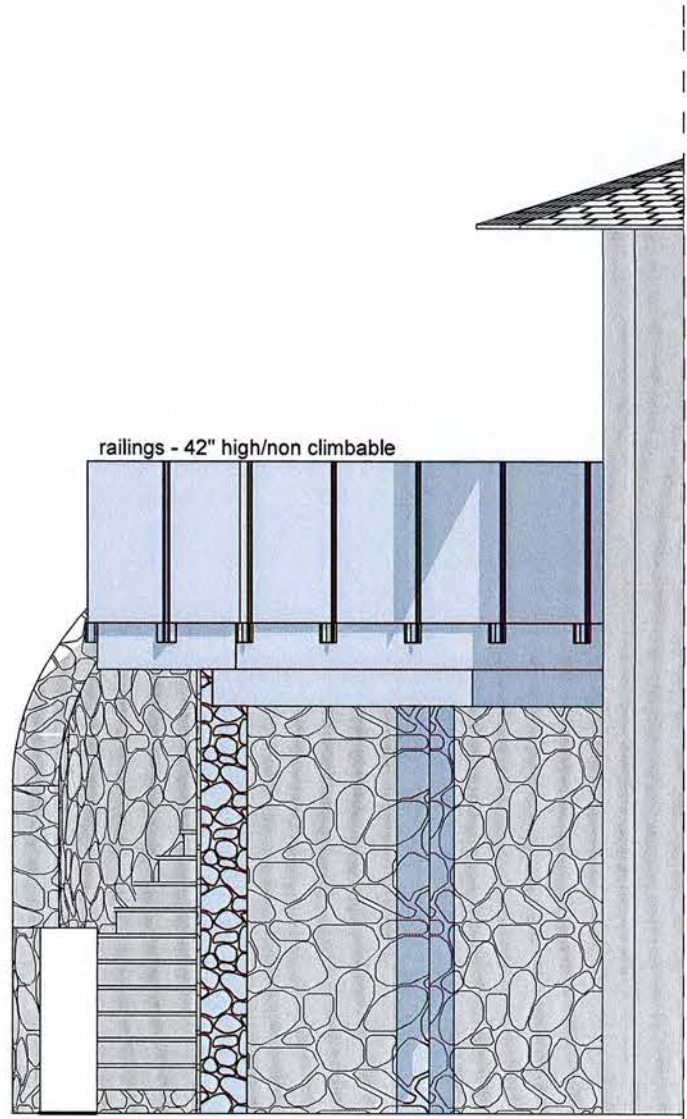
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KYLE LEGGETT

DRAWING NAME:
PROPOSED WEST ELEVATION

CUSTOMER:
GARY AND DIDI BRENNAN

ADDRESS:
#10 300 PLASKETT PLACE,
ESQUIMALT

 **JAVA DESIGNS**
WHERE LINES ON PAPER BECOME WALLS ON SITE
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E2

PROPOSED SOUTH ELEVATION

SCALE: 1/4" = 1' - 0"

ISSUE DATE:
APRIL 09, 2019

DRAWN BY:
KYLE LEGGETT

DRAWING NAME:
PROPOSED SOUTH ELEVATION

CUSTOMER:
GARY AND DIDI BRENNAN

ADDRESS:
#10 300 PLASKETT PLACE,
ESQUIMALT

JAVA DESIGNS
WHERE LINES ON PAPER BECOME WALLS ON SITE
PH 250.590.2468 FX 250.590.4577 www.javadesigns.ca

B.C. Land Surveyor's
Site Plan of
Strata Lot 10, Section 11, Esquimalt
District, Strata Plan VIS2246
PID No. 017-658-187

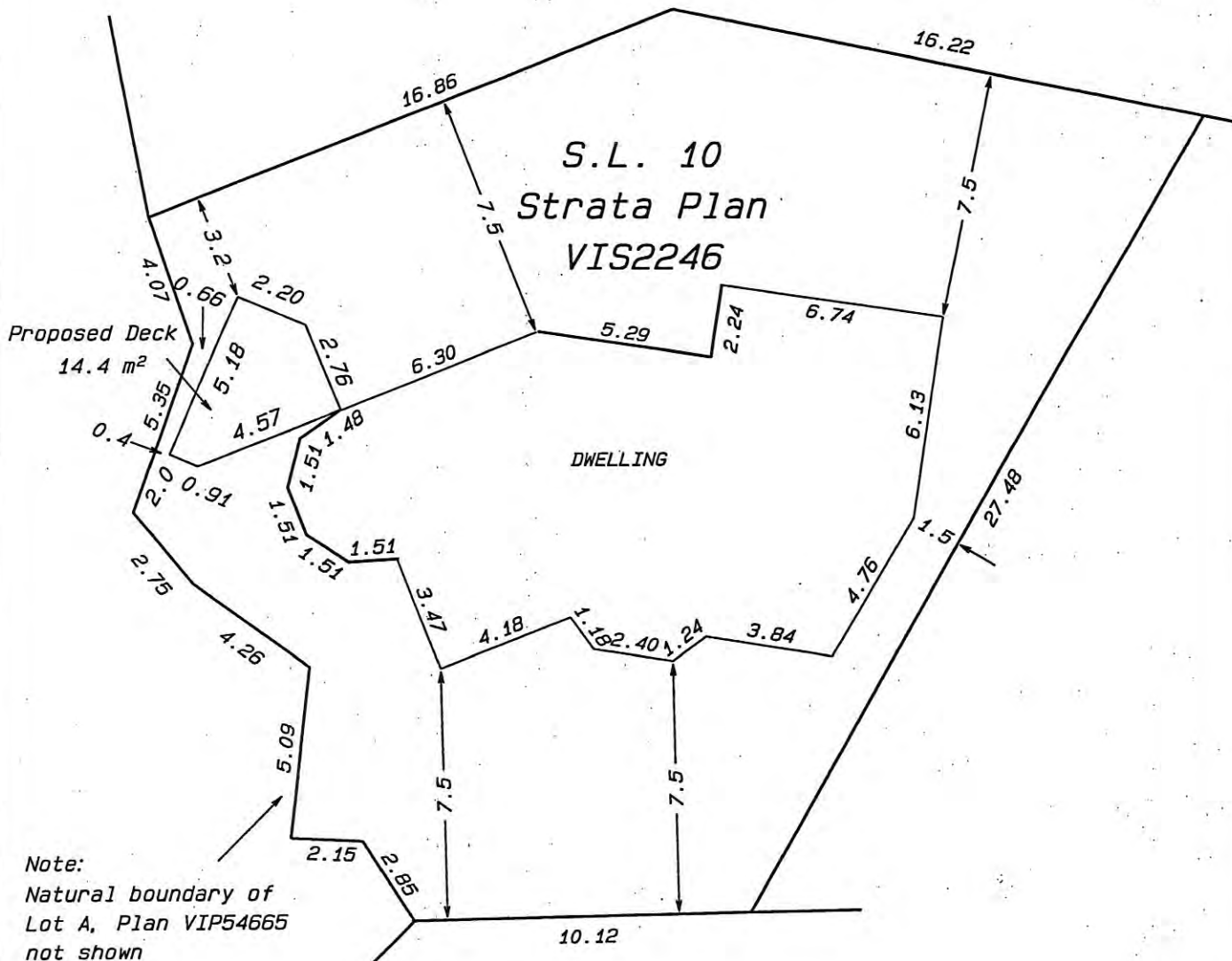
SCALE 1:200

ALL DISTANCES ARE IN METRES
 LOT DIMENSIONS ARE DERIVED FROM REGISTERED PLANS

CIVIC ADDRESS: 10-300 PLASKETT PLACE, VICTORIA, B.C.



PLASKETT PLACE



THIS PLAN IS NOT VALID UNLESS
 ORIGINALLY SIGNED AND SEALED OR
 WITH AN ELECTRONIC SIGNATURE ATTACHED

Proposed deck dimensions from
 design plan prepared by Java Designs
 dated March 19, 2019

THIS DOCUMENT IS FOR A DEVELOPMENT VARIANCE APPLICATION
 AND NOT TO BE USED FOR THE RE-ESTABLISHMENT
 OF PROPERTY BOUNDARIES

Former File 19463 Dec 1999

MICHAEL E. CLAXTON
 B.C. & CANADA LANDS SURVEYOR
 201-721 VANALMAN AVENUE
 VICTORIA, B.C. V8Z3B6
 PHONE 479-2258 FAX 479-3831
 FILE 19473 DVP mec

Michael
 Claxton
 TETGQA

Digitally signed by Michael Claxton
 TETGQA
 DN: c=CA, cn=Michael Claxton
 TETGQA, o=BC Land Surveyor,
 ou=Verify, ID at www.juricert.com/
 LKUP.cfm?id=TETGQA
 Date: 2019.04.08 13:33:19 -07'00'



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

APC Meeting: May 21, 2019

STAFF REPORT

DATE: May 15, 2019

TO: Chair and Members of the Advisory Planning Commission

FROM: Alex Tang, Planner 1
Bill Brown, Director of Development Services

SUBJECT: **Development Variance Permit Application**
835 Dunsmuir Road
[PID 030-709-156 Parcel A (Being a consolidation of Lots 2 and 3, see CA7353737), Section 11, Esquimalt District, Plan 9757]

RECOMMENDATION:

The Esquimalt Advisory Planning Commission recommends that the Development Variance Permit application for the following variance to Zoning Bylaw, 1992, No. 2050, as shown on the architectural plan of the parking level, provided by Praxis Architects Inc., stamped "Received May 15, 2019", for the proposed 5 storey 32 unit apartment building at 835 Dunsmuir Road [PID 030-709-156 Parcel A (Being a consolidation of Lots 2 and 3, see CA7353737), Section 11, Esquimalt District, Plan 9757] be forwarded to Council with a recommendation **to either approve, approve with conditions, or deny the application including reasons for the chosen recommendation.**

Zoning Bylaw, 1992, No. 2050, Section 67.95. (9)(a)(v) – Siting Exceptions – Principal Building - A 0.7 metre decrease to the requirement that no principal building shall be located within 2.0 metres of the northern Exterior Side Lot Line to accommodate the parking structure situated below the First Storey [i.e. from 2.0 metres to 1.3 metre]

BACKGROUND:

Purpose of the Application:

The applicant is requesting a variance to Zoning Bylaw, 1992, No. 2050, for a decrease in the setback to the Exterior Side Lot Line for the parking structure. The structural engineer has determined that additional structures were required in the parking level which resulted in the encroachment into the setback to the Exterior Side Lot Line. The setbacks at the First Storey and above will not change as a result of this requested variance.

Context

Applicant: Praxis Architects Inc. [Heather Spinney]
Owner: D.E. Mann Properties Ltd., Inc.No. BC1125695
Property Size: Metric: 1526 m² Imperial: 16427 ft²

Existing Land Use: Vacant Residential

Surrounding Land Uses:

North: Single Family Residential
South: Multiple Family Residential [4 storeys]
West: Multiple Family Residential [3 storeys]
East: Multiple Family Residential [4 storeys]

Existing Zoning: Comprehensive Development District No. 108 [CD No. 108]

Zoning

Comprehensive Development District No. 108 as part Zoning Bylaw, 1992, No. 2050, Amending Bylaw No. 2924, was adopted by Council on February 25th, 2019 to govern this development. Within this zone, the setback to the Exterior Side Lot Line is 7.5 metres; additionally, a siting exception allows for a setback of 2.0 metres for the parking structure below the First Storey.

The applicant has written a letter describing the need of a reduced setback to the Exterior Side Lot Line for the parking structure. This need is based on the requirement of additional structures at the parking level as determined by the structural engineer. The applicant is requesting a reduction of the setback to the Exterior Side Lot Line from 2.0 metres to 1.3 metres.

Public Notification

As this is an Development Variance Permit application, should it proceed to Council, notice would be mailed to tenants and owners of properties within 50m (164ft) of the subject property.

ALTERNATIVES:

1. Forward the application for a Development Variance Permit to Council with a **recommendation of approval including reasons for the recommendation.**
2. Forward the application for a Development Variance Permit to Council with a **recommendation of approval including specific conditions and including reasons for the recommendation.**
3. Forward the application for a Development Variance Permit to Council with a **recommendation of denial including reasons for the recommendation.**



Township of Esquimalt
1229 Esquimalt Road
Esquimalt, BC V9A 3P1

May 14, 2019

RE: 835 DUNSMUIR ROAD – DEVELOPMENT VARIANCE PERMIT

Dear Mr. Brown and Mr. Tang,

Following approval of rezoning and development permit for the above-noted property, the project has progressed into working drawings in preparation for building permit application.

Through the process of coordinating with the consultant team and having realistic feedback from the structural engineer, additional structure was required in the parkade which had a significant impact on our parking layout. We have outlined the effect of two additional columns and the resulting additional required stall width on the attached drawing A1.1 Plan – Parking.

It is regrettable that an encroachment into the approved 2.0m setback for the parking structure situated below grade only came to light recently.

We respectfully request consideration of a reduction in the northern exterior setback to 1.3m for the portion of the parkade west of the ramp as shown on the attached drawing.

In regards to lot coverage, the principal building including a parking structure is within the allowable 86%.

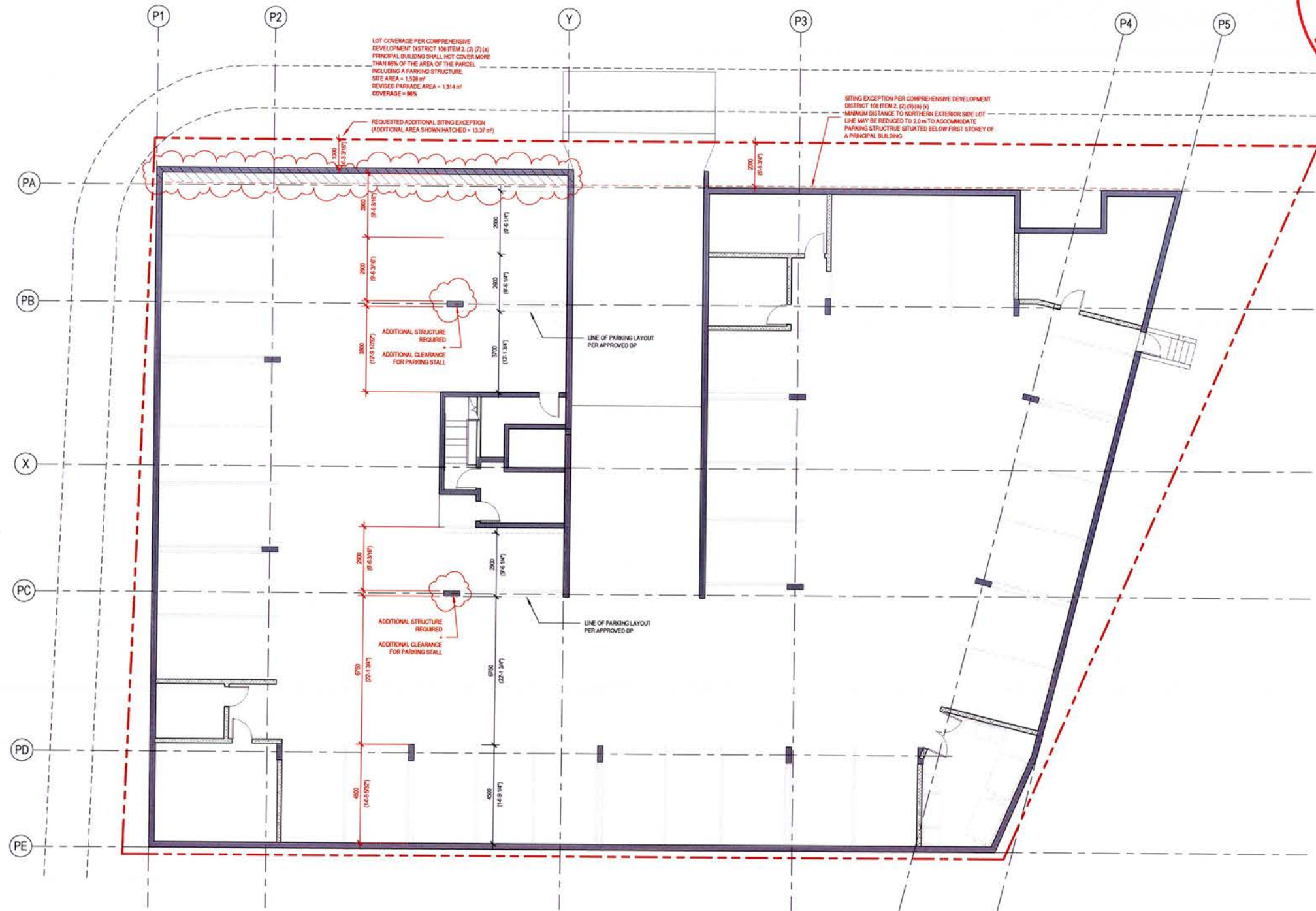
If any additional information is required to support this request, please do not hesitate to contact me.

Sincerely,

Heather Spinney, Architect AIBC
Principal, Praxis Architects Inc.



All of the documents prepared by Praxis Architects Inc. or on behalf of Praxis Architects Inc. in the connection with the project are instruments of service for the execution of the project. Praxis Architects Inc. retains the property and copyright in these documents, whether the project is executed or not. These documents may not be used on any other project, nor be reproduced, without prior written agreement of Praxis Architects Inc.
Do not scale the drawings.



1 PLAN - PARKING
1 : 100



NO.	DATE	ISSUE
1	2019.05.14	DEVELOPMENT VARIANCE PERMIT

PRAXIS ARCHITECTS INC.
Robert Rochelleau, Architect, AIBC
Heather Spinney, Architect, AIBC
Suite 401 - 1241 Esquimalt Road, Victoria, BC V8A 2P2
Tel: 250-432-2702 - Fax: 250-432-2701
www.praxisarchitects.com

PROJECT TITLE		
DUNSMUIR REDEVELOPMENT		
835 DUNSMUIR ROAD		
SHEET TITLE		
PLAN - PARKING		
DATE	2019.05.14	SHEET NO.
PROJECT NO.	17-012	A1.1
SCALE	1 : 100	
DRAWN BY	KD	



CORPORATION OF THE TOWNSHIP OF ESQUIMALT

Municipal Hall, 1229 Esquimalt Road, Esquimalt, B.C. V9A 3P1
Telephone (250) 414-7100 Fax (250) 414-7111

APC Meeting: May 21, 2019

STAFF REPORT

DATE: May 15, 2019

TO: Chair and Members of the Advisory Planning Commission

FROM: Trevor Parkes, Senior Planner
Bill Brown, Director of Development Services

SUBJECT: **Rezoning Application - 937 Colville Road**
PID 003-679-144, Lot 4, Block 1, Section 10, Esquimalt District, Plan 6277

RECOMMENDATION:

The Esquimalt Advisory Planning Commission [APC] recommends to Council that the application for a rezoning, authorizing six (6) townhouse dwelling units as sited on the survey plan prepared by Island Land Surveying Ltd., stamped "Received May 14, 2019" and incorporating the height and massing consistent with the architectural plans provided by Christine Lintott Architects, stamped "Received April 23, 2019", detailing the development proposed to be located at 937 Colville Road [PID 003-679-144, Lot 4, Block 1, Section 10, Esquimalt District, Plan 6277], be forwarded to Council with a recommendation to either **approve, approve with conditions, or deny the application; and provide reasons for the recommendation.**

BACKGROUND:

Purpose of the Application

The applicant is requesting a change in zoning from the current Single Family Residential [RS-1] zone to a Comprehensive Development District zone to accommodate the proposed six (6), two bedroom, strata dwelling units, to be built in a single, stacked style townhouse building on the subject property.

The existing house would be demolished and the new building containing the six dwelling units would be constructed. The Official Community Plan 'Proposed Land Use Designation' for this area is Townhouse Residential. Should the rezoning be approved, the form and character of the buildings and landscaping would be controlled by a Development Permit that would be considered by Council at a future date.

Evaluation of this application should focus on issues relevant to zoning such as the appropriateness of the proposed height, density and massing, proposed unit sizes, siting, setbacks, lot coverage, useable open space, how the building relates to adjacent and surrounding sites and whether the proposed uses are appropriate and consistent with the overall direction contained within the Official Community Plan.

Context**Applicant:** Ryan Jabs, Lapis Homes Ltd.**Owners:** Ryan Jabs, Lapis Homes Ltd., Inc. No. BC1063135 and
Jesse Cooke, 1156835 B.C. Ltd., Inc., No. BC1156835**Property Size:** Metric: 640.5 m² Imperial: 6894.6 ft²**Existing Land Use:** Single Family Dwelling**Surrounding Land Uses:****North:** Two Family Residential [RD-3]**South:** Single Family Residential [CD-32]**West:** Townhouse Residential [CD-112]**East:** Single Family Residential / Meditation Centre [CD-70]**Existing Zoning:** Single Family Residential [RS-1]**Proposed Zoning:** CD [Comprehensive Development District]**Present OCP Designation:** Low Density Residential**Proposed OCP Designation:** Townhouse Residential [no change required]**Comments From Other Departments**

The plans for this proposal were circulated to other departments and the following comments were received by the APC submission deadline:

Building Inspection: Construct to current BC Building Code and Municipal Building Code Bylaw, 2002, No. 2538. Subject to code and bylaw review at time of building permit application.**Engineering Services:** Engineering has completed a preliminary review of the proposed development at 937 Colville Road. The developer should be aware that they may be required to provide Works and Services up to the road centre line. At a minimum new curb, gutter and sidewalks along the frontage of the proposed development maybe required. The development is to have sewer, drain, catch basin and water service connections, as well as underground hydro, telephone, and cable. Additional review and comments will be provided upon receipt of detailed engineering drawings. All proposed Works and Services shall be as per Bylaw, 1997, No. 2175. The applicant is responsible for retaining the services of qualified professional for the design and construction supervision of all Works and Services, including construction costs, engineering fees, administrative fees and as indicated in Bylaw No. 2175.**Fire Services:** Sprinklers will be required for this building as per Building Regulation Bylaw 2017, No. 2899. A fire department lockbox will be required for the common room (bike storage), and a pre-construction safety plan will be required at building permit stage.**Comments From the Design Review Committee**

The Esquimalt Design Review Committee considered this application for rezoning at the regular meeting held on Thursday, February 13, 2019.

The DRC recommended to Council that the application **be referred back to the Design Review Committee with a design more consistent with the RM-3 Zone, specifically for lot coverage and height, and with parking more consistent with the neighbouring**

development, for the reason of better integrating the development into the neighbourhood.

Zoning

In keeping with other townhouse projects, the proposed Comprehensive Development District zone would contain the following uses: townhouse residential and home occupation.

Density, Lot Coverage, Siting and Setbacks: The following chart compares the setbacks, lot coverage and floor area ratio of this proposal with the requirements of the RM-3 [High Density Townhouse Residential Zone]:

	RM-3 [High Density Townhouse Residential]	Proposed CD Zone 937 Colville Road
Floor Area Ratio [F.A.R.]	0.60	0.69
Lot Coverage	25%	32%
Setbacks <ul style="list-style-type: none"> • Front (Colville Rd.) • Rear (South) • Side (West.) • Side (East) 	7.5 m 7.5 m 4.5 m 4.5 m	0 m to stairs (2.6 m to foundation) 11.84 m to stairs 3.7 m 3.1 m to building (0.98 m to balcony)
Building Height	9.0 m	9.5 m
Off Street Parking	2 spaces/ dwelling unit	0.67 spaces/ dwelling unit, 5 small car spaces [1 dedicated as visitor parking]
Usable Open Space	48.04 m ² / 640.5 m ² [7.5% of the area of the parcel]	0 m ² [0 %] conforming to terms of Zoning Bylaw, 1992, No. 2050 A small rain garden with bench is proposed in south-east corner of lot.

The F.A.R. of the proposal at 0.69 is greater than the 0.60 F.A.R. permitted in the Multiple Family Residential [RM-3] [high density townhouse / low density apartment] zone. The Official Community Plan allows for consideration of up to 0.70 F.A.R. for Townhouses. This lot is in an area designated for Townhouse Residential on the 'Proposed Land Use Designations Map'.

The proposed Lot Coverage at 32% is greater than the maximum 25% permitted in the RM-3 zone.

This proposal requires the complete elimination of the front setback (for the stairs) and a considerable reduction (2.36 metres) to the building, from the 7.5 metre setback requirements of the RM-3 zone. Staff note that the proposed elimination of the front setback would position the building in a manner notably inconsistent with other buildings on this section of Colville Road. Adjacent buildings are setback approximately, 6.0 metres, 7.5 metres and 4.0 metres from the Colville Road lot line which would result in this building intruding in to the established development pattern of the block. It is also noteworthy that the applicant, in response to DRC comments removed the eastern portion of the front stair assembly and proposes landscaping would replace the structure to soften the effect of the building at the street. Proposed side setbacks are also considerably reduced in this proposal.

The height of the proposed building at 9.5 metres is just above the maximum 9.0 metres provided in the RM-3 zone. The lower floor is partially located below grade on the west elevation and a full three storeys along the east elevation. In response to concerns regarding height the applicant has provided an updated rendering detailing the proposed building in context with the recently approved 10 unit townhouse project to the west.

The 'Useable Open Space'; as defined in the zoning bylaw, excludes areas used for front yards and parking, and areas with any dimension less than 6.0 metres. This proposal's provision of small private patios and decks does not meet the 7.5 % Useable Open Space requirement contained in the RM-3 zone. There is however a small outdoor siting area (including a rain garden) proposed for the south-east corner of the site. As it measures approximately 5.5 metres by 8.0 metres, (about 44 square metres) it would not meet the definition of Useable Open Space but it could function as such. There is also a public park and playground space located within 100 metres of this site.

Parking and Maneuvering

There is one building with six, two-bedroom dwelling units being proposed. There are no garages. Five parking spaces are being proposed at the rear of the site. The parking bylaw allows for up to 50% of parking spaces to be small car sized spaces; this proposal has only one full size space and four small car spaces. Four of the spaces are being dedicated to units while the one full size space is being dedicated for visitors to the site. Two units, the smaller basement units, would not have a parking space on site.

Parking Bylaw 1992, No. 2011 requires the minimum width of a maneuvering aisle accessing one bank of parking to be at least 6.75 metres. Initially, this proposal failed to satisfy this requirement as the maneuvering aisle was only 5.7 metres wide as measured from the end of the proposed parking space to the wall of the building. In response to staff comments the applicant revised the site plan and the maneuvering aisle is now compliant with the Parking Bylaw standard. There is also a turnaround zone at the eastern end of the aisle that would assist with on-site maneuverability. The driveway which is 3.7 metres wide is located over a statutory right of way. Due to the depth of this lot, the fire department would not place a fire truck on the private property.

There is regular bus service in the vicinity with BC Transit routes # 24 and # 26 on Lampson Street, and with route #14 on Craigflower Road. The Walk Score for this area is '64 – Somewhat walkable'. A car share car is proposed for the development directly west of this parcel, and the applicant has committed to provide memberships, attached to the each unit in perpetuity, for use by occupants of this building.

This location is close to the E & N Rail Trail - cycling and pedestrian trail. Common indoor bicycle racks for residents are proposed to be located on Level 1 of the building, which will offer some limited security for bicycles. No storage lockers are proposed in the building.

A report by Watt Consulting Group has been supplied to support this application (attached).

Official Community Plan (OCP)

This site is designated 'Townhouse Residential', as illustrated on the "Proposed Land Use Designation" map (Official Community Plan Bylaw, 2018, No. 2922 - Schedule B).

The Official Community Plan supports the expansion of housing types in residential areas. The immediate neighbourhood contains a mix of single family, two-family, townhouse and multi-family housing types. The following policies would be considered in the evaluation of this proposal.

Section 5 Housing & Residential Land Use contains policies that are intended to ensure that concerns such as tree protection, parking, traffic, noise, effects on neighbouring properties, and neighbourhood character are addressed.

- Policy - Consider new townhouse residential proposals with a Floor Area Ratio of up to 0.70, and up to three storeys in height, in areas designated 'Townhouse Residential' on the "Proposed Land Use Designation Map", provided the design responds effectively to both its site and surrounding land uses.
- Policy - Support the development of a variety of housing types and designs to meet the anticipated housing needs of residents. This may include non-market and market housing options that are designed to accommodate young and multi-generational families, the local workforce, as well as middle and high income households.

5.2 Low Density Residential Redevelopment

OBJECTIVE: Strive for redevelopment and infill development that improves and enhances the appearance and livability of neighbourhoods and the community as a whole.

- Policy - Proposed redevelopment or infill within present low density residential land use designated areas should be built to high quality design and landscaping standards and respond sensitively to existing neighbourhood amenities.

5.4 Affordable Housing

OBJECTIVE: To encourage a range of housing by type, tenure and price so that people of all ages, household types, abilities and incomes have a diversity of housing choice in Esquimalt.

- Policy - Encourage the provision of missing middle housing types such as two-unit dwellings (duplexes), townhouses and small lot infill as one avenue to address housing affordability.

Development Permit Guidelines

Should this application for rezoning be approved by Council a Development Permit required prior to a building permit being issued; therefore, applicants are urged to consider the DP guidelines early in the process. Many DPA guidelines require that the zoning issues (useable open space, lot coverage, height, density, massing, siting, setbacks, parking, how the building relates to adjacent homes) and natural area / tree protection be considered in order to be able to fulfill the guidelines for a development site.

This site is included within four development permit areas: Development Permit Area No. 1 – Natural Environment, Development Permit Area No. 6 - Multi-Unit Residential, Development Permit Area No. 7 – Energy Conservation and Greenhouse Gas Reduction, and Development Permit Area No. 8 – Water Conservation.

Green Building Features

The applicant has completed the Esquimalt Green Building Checklist [attached].

Public Notification

As this is a rezoning application should it proceed to a Public Hearing, a notice would be mailed to tenants and owners of properties within 100m (328ft) of the subject property. One sign, indicating that the property is under consideration for a change in zoning, has been installed on the Colville Road frontage. This sign would be updated to include the date, time, and location of the Public Hearing. Additionally, notice of the Public Hearing would be placed in two editions of the Victoria News.

Applicant's neighbourhood meeting

The applicant has indicated that they held a public meeting on February 7, 2019 in order to comply with the public consultation procedures of Development Application Procedures and Fees Bylaw, No. 2791, 2012.

ALTERNATIVES:

1. Forward the application for Rezoning to Council with a **recommendation of approval including reasons for the recommendation.**
2. Forward the application for Rezoning to Council with a **recommendation of approval including specific conditions and including reasons for the recommendation.**
3. Forward the application for Rezoning to Council with a **recommendation of denial including reasons for the recommendation.**



Colville Rd

Lampson St

939

937

933

921

825

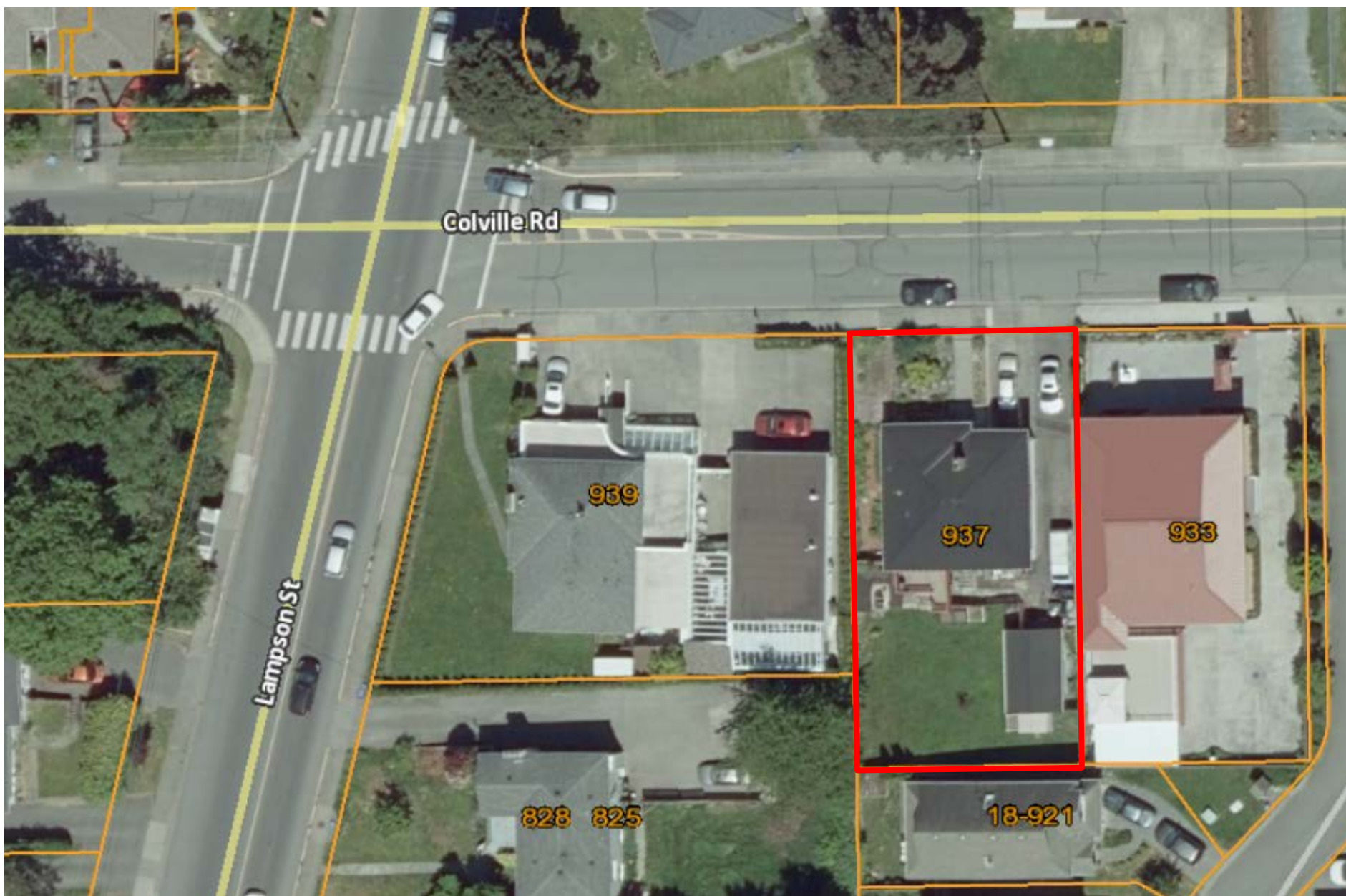
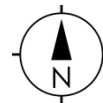
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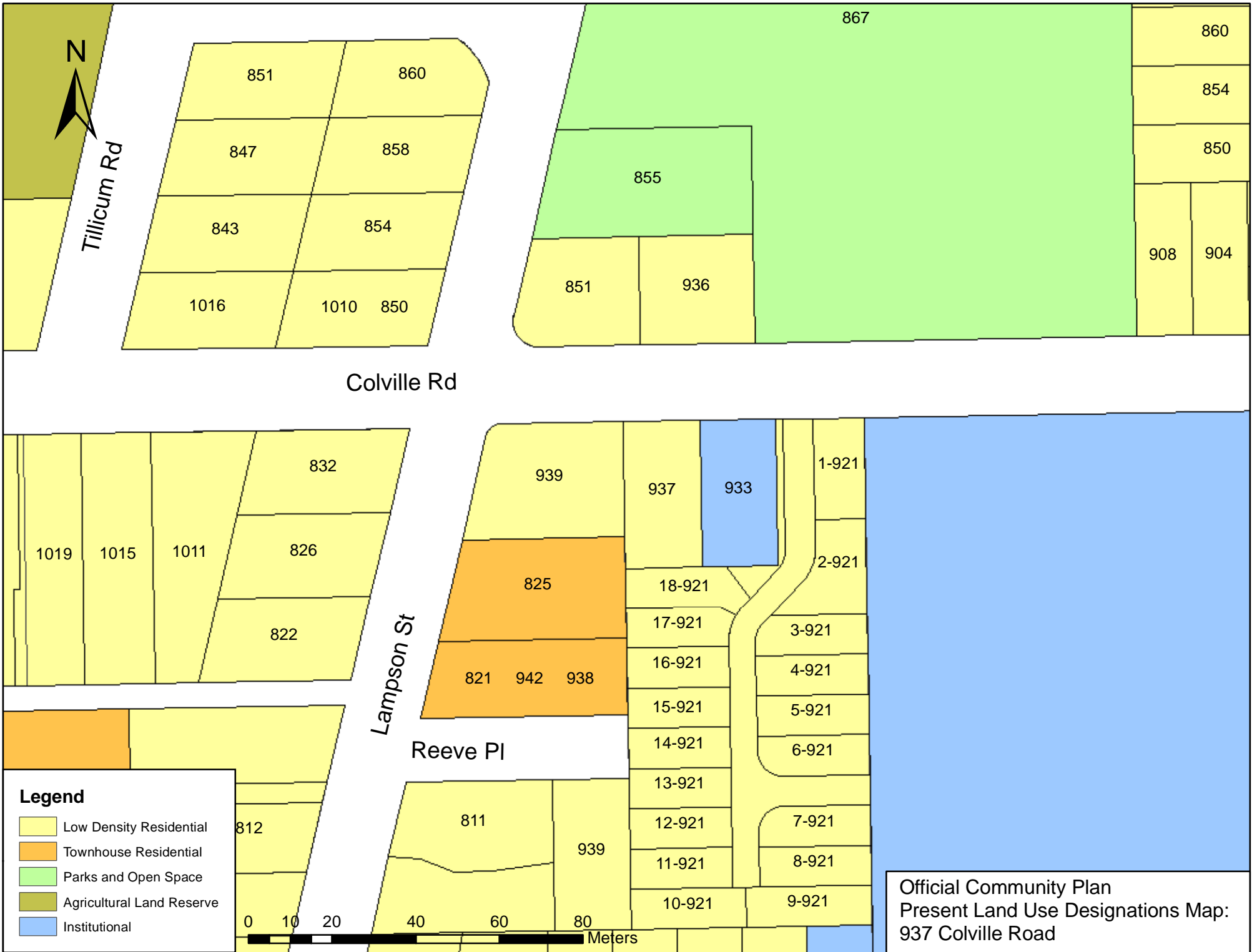
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921

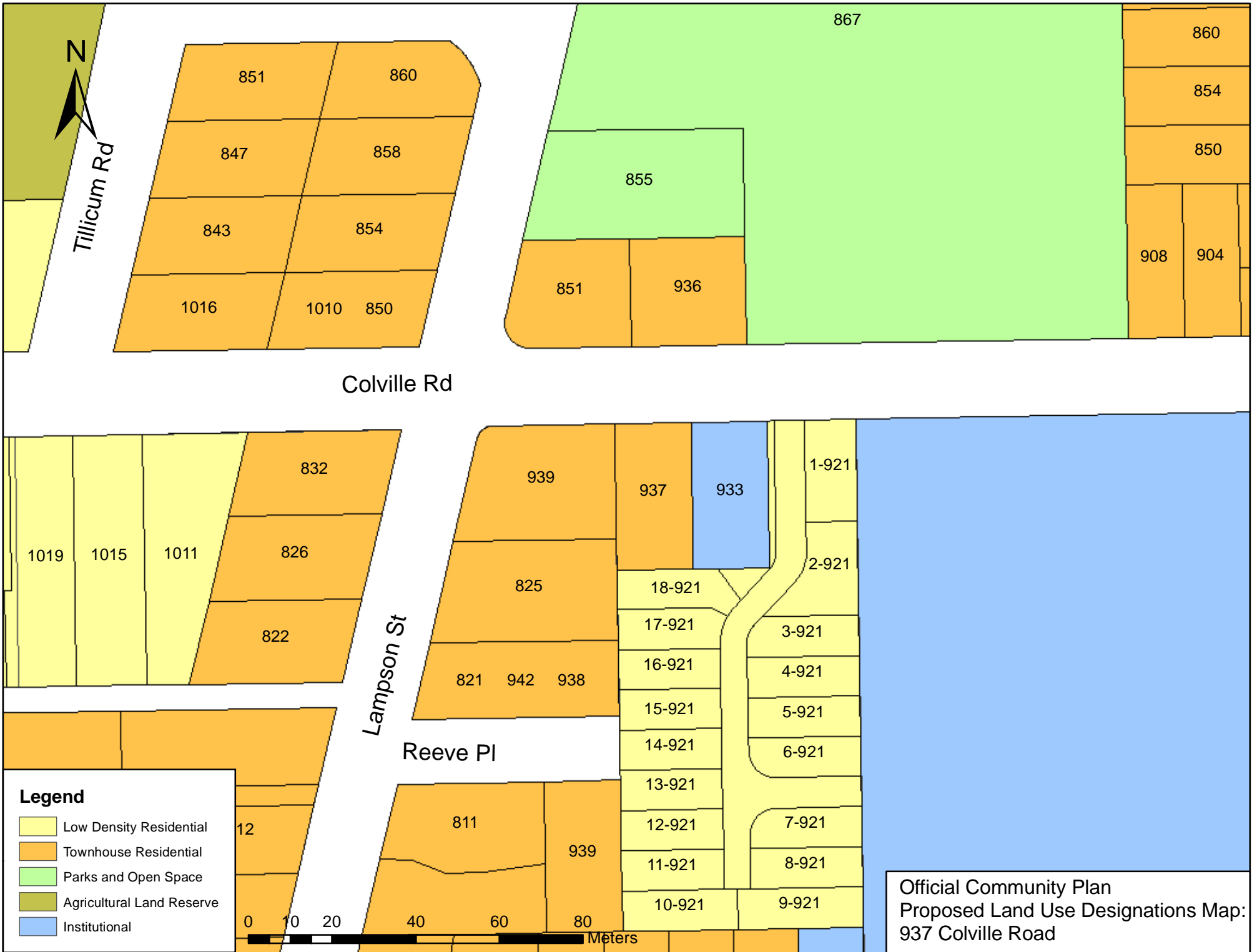
Subject Property Map:
937 Colville Road

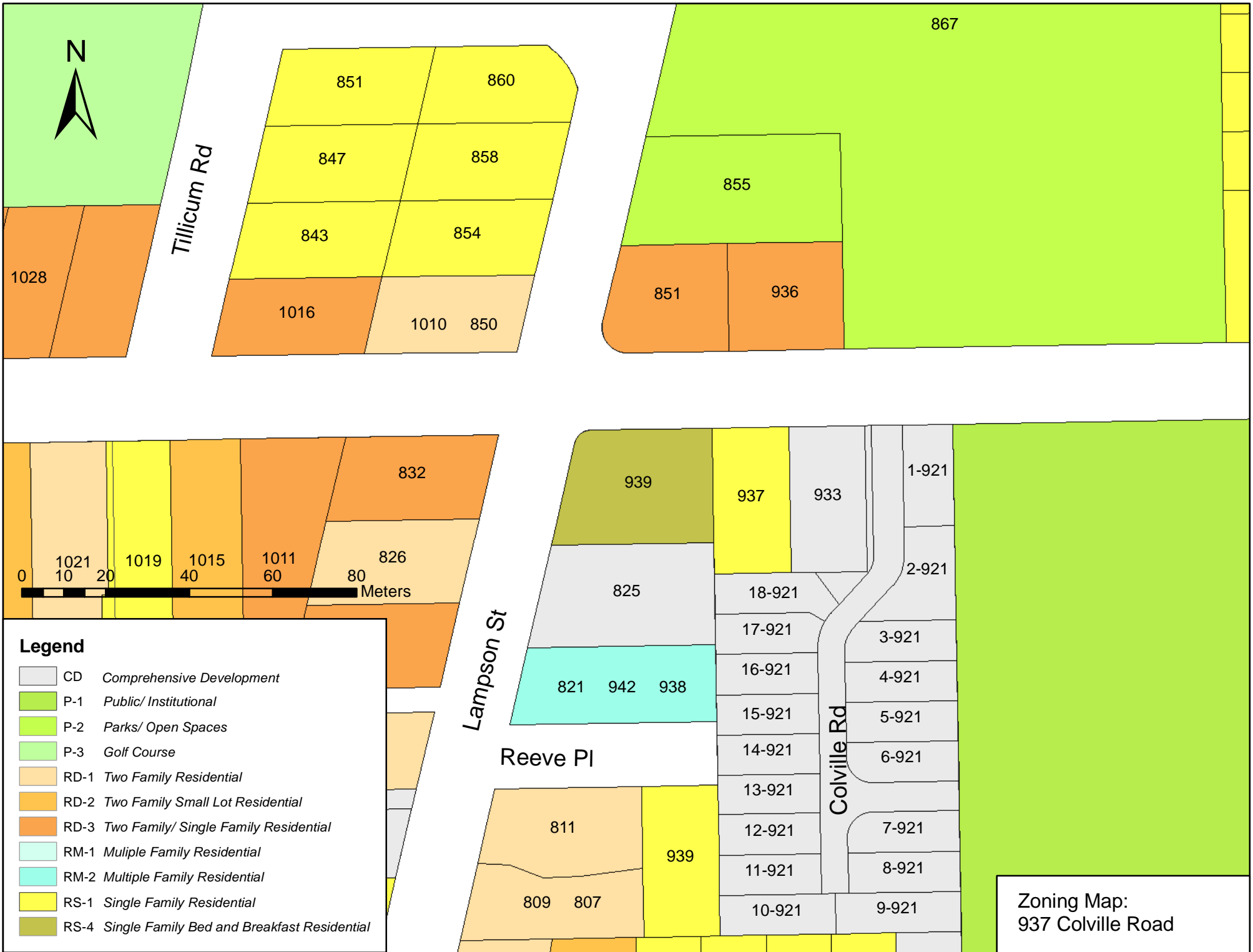
937 Colville Road - air photo





Official Community Plan
 Present Land Use Designations Map:
 937 Colville Road





Lapis Homes Ltd.
4291 Oakfield Crescent Victoria, BC, V8X4W4
Phone 250-413-7121
ryanjabs@laphomes.com
www.laphomes.com



November, 2018

Dear Mayor and Council,

We are the developers of this proposed six-two-bedroom unit townhouse project at 937 Colville Rd., and part of a small, family-run company called Lapis Homes. Our company has also proposed the 10-three-bedroom unit townhouse next door to this property, at 825 Lampson and 939 Colville, which is also making its way through rezoning.

We are writing to provide you with details about our proposal, as well as to let you know that we are available to meet with you if you'd like more information or have some specific questions about the project in advance of a council meeting.

Design Made for People

937 Colville is approximately 7,000 square feet large and is currently zoned single family. As a single-family home, it's right between our proposed 10-unit development to the west, the Buddhist temple to the east, and the 18-unit townhouse development to the south and to the east. In addition, the municipality has an underground right of way running along the western three meters of the property.

We feel that we have come up with a proposal that works well for this lot and for the mix of housing in the neighbourhood – a proposal that will create people-sized, gentle density that is consistent with the Township's Official Community Plan.¹

For example, the building is set closer to the sidewalk, with the entrances, great rooms, balconies and patios of the front units facing out – with the eyes of the residents oriented towards the street. And while a little different than some proposals, this type of design encourages residents to be more engaged with the daily activity of kids playing in the park across the street, and with neighbours and students walking by.

As noted on page 122 of the OCP, designing this way helps “create street activity and association among neighbours,” encouraging a healthier, safer and more engaged community.

In addition, moving the building closer to the street allows us to put people before their cars.

Traditionally, we've designed houses for cars, with larger setbacks that were used for pavement and parking, rather than for people. Moving the building towards the street allows us to tuck the parking in the back, hidden from view by the townhouse and landscaping (OCP – Pg 93, #5). This is not only more visually pleasing, it focuses the development around the people living there, rather than their cars.

¹ Esquimalt *Official Community Plan*, pgs 93-94 and 121 to 122
[https://www.esquimalt.ca/sites/default/files/docs/business-development/OCP/2018/toe adopted official community plan 2018 0.pdf](https://www.esquimalt.ca/sites/default/files/docs/business-development/OCP/2018/toe%20adopted%20official%20community%20plan%202018%200.pdf)

The Official Community Plan, Form & Character:

As noted above, this property is surrounded by a number of existing townhouses, as well as our proposed 10-unit development next door. In addition, both the current and the previous Official Community Plans designate this property as townhouse residential.

And while we're not proposing a cookie cutter box, these townhouses will have long-term appeal, fitting well with the buildings around it. Our Architect, Christine Lintott, has designed the building to look like many of the larger, character houses in the neighbourhood, taking elements of the neighbouring buildings into our design so that it fits quite nicely in the mix and style of housing in this small section of Colville.

For example, we've gone with gabled peaks, wood-like siding and shingles, large living room windows and traditional design elements, which match the form and character of the neighbourhood.

And at two-and-a-half storeys, the building will transition well from the proposed 3-storey development on the corner to the two-storey Buddhist temple next door.

Smaller homes, more options:

This proposal will also provide a more affordable mix of new townhouse housing in the neighbourhood.

At 825 Lampson and 939 Colville next door, for example, we're proposing 10 family-style townhouses with three-bedrooms and a bonus flexroom. Those units will be great for established professionals with younger or growing families and are similar to the other townhouses and single-family buildings in the area.

But they're also twice as large, more expensive to build – and more expensive to buy.

These two-bedroom units will compliment and be more affordable than the larger three-bedroom townhouses next door. They'll also provide housing for a broader mix of people in the neighbourhood – such as young working professionals, with future plans for a family (whose options currently are moving to Langford or buying a condo), or for downsizing retirees.

We also believe these units will be well received by the Esquimalt (and the greater Victoria) market. For example, according to 2016 Statistics Canada data, 76% of homes in Esquimalt had only one or two people living in them. Despite the large percentage of one and two-person households, only 62% of private houses, condos and townhouses in the Township had two or fewer bedrooms.

These units will help close that gap by providing more options in a people-sized townhouse building, rather than in a larger condo building.

Cycling, Modo and parking:

Parking is often one of the more controversial elements of any development, and we're asking for a variance to build fewer parking spots than units. From our perspective, providing fewer parking spots with strong alternative transportation options is a good thing that should help meet the Township's goals (cleaner

environment, less congestion, possibly a stronger local economy²), as well as benefit the residents (better health for those who cycle or walk³, lower housing costs).

Following a trend in many cities across North America, Seattle recently reduced parking requirements in multifamily buildings near good transit services.⁴ This decision was based in part on a 2015 report they commissioned. In that report, they found that 40% of parking in multi-family dwelling was not being used. In fact, the researchers noted that having an oversupply of parking actually encourages people to buy and rely on vehicles when making future transportation decisions.

If parking is not available, the King County researchers note, people will choose other forms of transportation.⁵

The Lampson Park neighbourhood is well connected to cycling trails, sidewalks, good transit services, and groceries (Wholesale club). It's also close to the Esquimalt base, the township centre and downtown Victoria, making it ideal for people who choose not to have a car.

We want to encourage these decisions.

We're going to support people to use alternative transportation in a number of ways:

- Build a 10-bike lock-up room inside the building for residents to securely store their bikes.
- Install a bike rack out front for guests and residents making a quick stop at home.
- Providing annual transit passes to the two units that won't have dedicated parking.
- Give every unit a membership to the MODO car share network, with easy access to the MODO vehicle at the development next door (if it's approved).
 - Research shows that multi-family developments that include car share memberships can reduce the demand to own their own vehicle, particularly when the site is well supported by transit, cycling and walking options.⁶

To further support this request, we hired Watt Consulting to review the traffic impact of our two projects (The report is attached to our application and concluded the traffic impact would be minimal).

While Watt's focus was on traffic volumes from the two developments, they also discussed parking requirements for this proposal, in combination with the 10-unit townhouse that we're proposing at 939 Colville and 825 Lampson (16 total new units). This included referencing a study that they did for a 16-unit townhouse project in Saanich, which suggested similar developments in the city had an average parking demand of 0.85 vehicles per unit. This is equivalent to 14 spaces for the 16 units in the two projects that we're proposing (five spaces for the six-unit and nine spaces for the 10 unit).

² *Consumer Behaviour and Travel Choices*: http://kellyjclifton.com/Research/EconImpactsofBicycling/OTREReport-ConsBehavTravelChoices_Nov2012.pdf; more readable summary: <https://www.citylab.com/transportation/2012/12/cyclists-and-pedestrians-can-end-spending-more-each-month-drivers/4066/>

³ British Medical Journal, *Active Commuting is Beneficial for Health*: <https://www.bmj.com/content/357/bmj.j1740>; Medical News Today summary article: <https://www.medicalnewstoday.com/articles/317058.php>

⁴ *Seattle cuts parking requirements near transit [...]*: <http://www.capitolhillseattle.com/2018/04/seattle-cuts-parking-requirements-near-transit-unbundles-costs-for-carless-tenants-in-new-buildings/>

⁵ <http://metro.kingcounty.gov/programs-projects/right-size-parking/pdf/rsp-final-report-8-2015.pdf>

⁶ Martin & Shaheen. 2011. The Impact of Carsharing on Household Vehicle Ownership. Access Magazine. Available at: <https://www.accessmagazine.org/spring-2011/impact-carsharing-household-vehicle-ownership/>

In Watt's report for us, they went further to note that "in previous studies where carshare memberships are provided and a carshare vehicle is easily accessible, it is Watt's experience that a 10-15% reduction in parking demand is expected."

The final benefit we'd like to highlight is that reducing the number of parking spots will also lower the cost for the units without cars, as we will charge lower prices for those units. This means that people who choose not to have a car won't have to subsidize the cost of building parking spots for people in the building that have a vehicle.

Building Green

Vehicle traffic is one of the largest contributors to an individual's carbon footprint in North America. And as noted above, with Modo memberships, good bike storage and transit passes, we're taking steps to make it as easy as possible to help future buyers use their cars as little as possible.

In addition, we're planning to take the following steps to achieve a greener development:

- Construct the building to at least Step 2 and possibly Step 3 in the step code.
- Install two outdoor chargers for electric vehicles in the parking area.
- Build a rain garden to collect and handle the rain water runoff from the roof – reducing the building's impact on the storm system.
- Install pavers, rather than concrete, for all vehicle hardscaping.
- Wire the building for future solar panels.
- Install a number of new trees and shrubs throughout the property.

Affordable Housing:

As noted above, these units will provide more affordable options for people looking for townhouses in Esquimalt. However, with the costs of land and increasing costs of construction, they will still be out of reach of many who are looking for housing in the community (although, over time, they will become more affordable as the building ages).

As I've noted in my previous application, as a small developer proposing smaller people-sized projects, it's challenging to build affordably or incorporate affordable housing units into my projects.

However, I still can contribute to affordable housing in the region.

As I've mentioned to previous councils, out of our profits from our earlier project in Esquimalt at 521 Foster, we donated \$60,000 to the Anglican Diocese towards the 24-unit affordable seniors housing project that they're working on in Esquimalt.

And as a good corporate citizen, Lapis Homes will continue to contribute out of our profits to affordable housing in the communities where we work.

Wrap

We are excited about this proposal – and we're hoping you will be too. It's an opportunity to build a unique, people-sized development to a neighbourhood that could use this mix of housing.

Thank you for taking the time to read through this letter and for reviewing our proposal. We look forward to discussing this project with you and with your staff in the coming months.

Take care,



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"If you plan cities for cars and traffic, you get cars and traffic. If you plan for people and places, you get people and places." - Fred Kent, Project for Public Spaces

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April 18, 2019

Regarding: Recommendation from Design Review Committee on our 937 Colville proposal

Dear Mayor and Council,

We are writing to respond and provide context to the recommendations made by the Design Review Committee on February 13, 2019, regarding our six, two-bedroom-unit townhouse proposed for 937 Colville Rd., as we believe this proposal is in line with the Official Community Plan and will be a good fit for the neighbourhood.

I've addressed the main recommendation – to design a townhouse consistent with the RM-3 Zone – off the top and then outlined in a table on the second page with how we've adjusted our proposal to address the other comments that the committee made during the meeting.

RM-3 zone

The main recommendation we received from the committee is to design a townhouse that is more "consistent with the RM-3 zone, specifically for lot coverage and height."

This recommendation took us a bit by surprise, as the RM-3 bylaw is very restrictive and from our knowledge has not been used for any townhouse development in the last 35 years. If we were to use it at 937 Colville, we'd only be able to add around 180 square feet of lot coverage to the existing house and garage, which would mean neither this nor any townhouse project could be built at this site.

In fact, from our scan, the last and perhaps only time the RM-3 zone was used in Esquimalt was for the 3.5 acre, 1985-built, CRD-operated Firgrove property at 741 Lampson, which has one townhouse unit for every 5,000 square feet of land.

Our proposal, on the other hand, is very much consistent with the townhouse proposals that have been approved in Esquimalt in the last 30 years.

It has a smaller or similar floor area ratio (without the attic storage) and similar lot coverage to the 15-unit M'akola-owned townhouse at 735 Admirals; the four-unit 2001-built townhouse at 733 Sea Terrace; the 2008-built six-unit townhouse at 1105 Esquimalt; the 2011-built four-unit townhouse at 617 Admirals; the 2015-built 10-unit townhouse at 1060 Tillicum; my 2017-built four-unit townhouse at 519 Foster; the newly built 12 unit townhouse at 618 Lampson; the 5-unit townhouse that's under construction at 1052 Tillicum, as well as my planned development for 939 Colville and 825 Lampson.

It's also close in listed height to nearly all of these developments, but in reality will be lower, as it sits 2.5 storeys high on the low side of the hill (this may be confusing, but please ask me to clarify if it is, as it has to do with how building height is calculated).

I believe that the committee’s recommendation may have come in part from a street scape representation that we used, which did not accurately capture the look and feel of our proposed development – mainly because it did not include landscaping or show our future 10-unit townhouse next door. With the revised streetscape (attached), you can see how the height falls from three storeys on the corner, to the proposed 2.5 storeys at 937 Colville, to the 2-storey Buddhist temple next door.

Without repeating everything in my original application letter – the proposed building is designed to mimic the houses in the surrounding community and will look quite a bit like a converted larger home, with two-bedroom units that we believe will fit well with the mix of housing in the Lampson and Colville area.

Design Review Committee – other comments/recommendations

The committee also provided a few other thoughts and considerations for the proposal we submitted to them for review, which we’ve mostly agreed with and adjusted our proposal as shown below:

Comment/Recommendation	Response
The front of the building has too many stairs.	Agree: The original proposal included two sets of stairs up to the landing at the front of the building. We’ve revised the proposal to include only one set of stairs and replaced the second set with a magnolia and a cedar and are framing the building with three laurels. In addition, we’re proposing a planter with an evergreen hedge in front of the remaining set of stairs. These changes will soften the site, provide some natural beauty and help shade the sidewalk.
Landscape maintenance needs to access east side of building	Agree: We originally included too much foliage on the east side of the building, which would have made it a challenge for a groundskeeper to maintain. We removed a small amount of plantings and added stepping stones from the north and south side of the building for maintenance.
Rear retaining wall/fence was too large (and stark)	Agree: The original streetscape didn’t include landscaping nor anything to mask the retaining wall and fence at the rear of the property. This starkness added to the perception of a too large building. We’ve shown trees along the drive, and trees & vines along the back wall, which will eliminate the wall from view and considerably soften the parking area.
Parking spaces are too small	We’ve discussed this with Esquimalt planning and in order to achieve as much greenery as possible, we’re making the visitor space a large-vehicle space: 2.9 metres wide, 5.5 metres long. This will allow access for a moving truck or other delivery vehicles. The remaining spaces will be 2.4 meters wide by 4.5 metres long, which is the minimum size for multifamily parking – both promoting lower carbon footprint vehicles and allowing us to plant trees along the rear property line.
The parking area could be softened with a pergola	Agree: We appreciated this suggestion. However, in discussion with planning, a pergola would make it challenging to maneuver back there and reduce the size of the common area. To achieve a similar look, we’ve gone with vines and trees along the back wall to soften the area.
Grading at the east side of the property may not work	Agree: We had originally proposed the raingarden to run right along the edge of our property line which would have been a challenge to retain. We’ve made it smaller and pulled it off of the property line by nearly two metres, giving us plenty of space to grade out towards next door.

We are quite excited about this proposal, as it will bring a unique and needed type of housing to the neighbourhood. We believe that the emphasis on alternative modes of transportation and the connection to the community that can be created by this type of close-to-the-curb design will encourage socially (and physically) healthier communities – a policy that Esquimalt has formally begun to explore through a BC Healthy Communities’ grant.

We believe our proposal could be one of the first of many housing developments that advances this happy and healthy city goal.

Again, I want to thank you for the time you’ve taken to read this letter and to consider this proposal. As always, I am available at any time on my cell (250-413-7121) or by email (ryanjabs@lapishomes.com) if you would like to discuss any of this.

Take care,



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LAPIS HOMES

Streetscape for the proposed 6-unit townhouse at 937 Colville.





GREEN BUILDING CHECKLIST

The purpose of this Checklist is to make property owners and developers aware of specific green features that can be included in new developments to reduce their carbon footprints to help create a more sustainable community.

Creating walkable neighbourhoods, fostering green building technologies, making better use of our limited land base and ensuring that new development is located close to services, shops and transit are some of the means of achieving sustainability.

The Checklist which follows focuses on the use of **Green Technologies** in new buildings and major renovations. The Checklist is not a report card, it is a tool to help identify how your project can become 'greener' and to demonstrate to Council how your project will help the Township of Esquimalt meet its sustainability goals. It is not expected that each development will include all of the ideas set out in this list but Council is looking for a strong commitment to green development.

There are numerous green design standards, for example, Built Green BC; LEED ND; Living Building Challenge; Green Shores; Sustainable Sites Initiative. Esquimalt is not directing you to follow any particular standard, however, you are strongly encouraged to incorporate as many green features as possible into the design of your project .

As you review this checklist, if you have any questions please contact **Development Services** at 250.414.7108 for clarification.

**New development is essential to Esquimalt.
We look forward to working with you
to ensure that development is
as green and sustainable as possible.**

Other documents containing references to building and site design and sustainability, which you are advised to review, include:

- Esquimalt's Official Community Plan
- Development Protocol Policy
- Esquimalt's Pedestrian Charter
- Tree Protection Bylaw No. 2664
- A Sustainable Development Strategic Plan for the Township of Esquimalt

Adopted on January 10th, 2011



“One-third of Canada’s energy use goes to running our homes, offices and other buildings. The federal government’s Office of Energy Efficiency (Natural Resources Canada) reports that a corresponding one-third of our current greenhouse gas (GHG) emissions come from the built environment.”
 [Green Building and Development as a Public Good, Michael Buzzelli, CPRN Research Report June 2009]

Please answer the following questions and describe the green and innovative features of your proposed development. Depending on the size and scope of your project, some of the following points may not be applicable.

Green Building Standards

Both energy use and emissions can be reduced by changing or modifying the way we build and equip our buildings.

1	Are you building to a recognized green building standard? If yes, to what program and level? <small>Expect to build to at least step code 2 or equivalent. See next q.</small>	<input checked="" type="checkbox"/> Yes	No
2	If not, have you consulted a Green Building or LEED consultant to discuss the inclusion of green features? <small>I've had preliminary discussions with my architect and energy consultant about the systems needed to achieve higher step codes. We're considering a combination of heating, ventilation and insulation systems to meet higher steps.</small>	<input checked="" type="checkbox"/> Yes	No
3	Will you be using high-performance building envelope materials, rainscreen siding, durable interior finish materials or safe to re-use materials in this project? If so, please describe them. <small>Yes, rainscreen and durable cement siding</small>	<input checked="" type="checkbox"/> Yes	No
4	What percentage of the existing building[s], if any, will be incorporated into the new building? <small>We'll look to reuse fill, rock and landscaping materials as much as possible. We'll also look to find places for the appliances, furnace and some of the fixtures pre demolition.</small>	<u>0-10</u> %	
5	Are you using any locally manufactured wood or stone products to reduce energy used in the transportation of construction materials? Please list any that are being used in this project. <small>Yes. Fill will come from on site, as well as local quarries, wood will be sourced through Slegg</small>		
6	Have you considered advanced framing techniques to help reduce construction costs and increase energy savings? <small>I'll work with my framer to take advantage of advanced framing techniques wherever it makes sense and to improve insulation.</small>	<input checked="" type="checkbox"/> Yes	No
7	Will any wood used in this project be eco-certified or produced from sustainably managed forests? If so, by which organization? <small>Wherever possible. We will be most likely be sourcing wood locally through Slegg</small> For which parts of the building (e.g. framing, roof, sheathing etc.)? _____		
8	Can alternatives to Chlorofluorocarbon's and Hydro-chlorofluorocarbons which are often used in air conditioning, packaging, insulation, or solvents] be used in this project? If so, please describe these. <small>We are considering our heating sources and whether to use a heat pump to meet step code requirements. If we do use a heat pump with AC, we will look at alternatives.</small>	Yes	No
9	List any products you are proposing that are produced using lower energy levels in manufacturing. <small>Engineered wood flooring, ceramic tiles for backsplashes and bathrooms.</small>		
10	Are you using materials which have a recycled content [e.g. roofing materials, interior doors, ceramic tiles or carpets]? <small>Will look at using recycled materials wherever it makes sense.</small>	Yes	No
11	Will any interior products [e.g. cabinets, insulation or floor sheathing] contain formaldehyde?	Yes	<input checked="" type="checkbox"/> No

Water Management

The intent of the following features is to promote water conservation, re-use water on site, and reduce storm water run-off.

Indoor Water Fixtures

12	Does your project exceed the BC Building Code requirements for public lavatory faucets and have automatic shut offs?	Yes	<input checked="" type="radio"/>	No	<input type="radio"/>
13	For commercial buildings, do flushes for urinals exceed BC Building Code requirements?	Yes	<input type="radio"/>	No	<input type="radio"/>
14	Does your project use dual flush toilets and do these exceed the BC Building Code requirements? We have not yet chosen toilets, but they will be low-flow.	Yes	<input checked="" type="radio"/>	No	<input type="radio"/>
15	Does your project exceed the BC Building Code requirements for maximum flow rates for private showers? We will likely use low flow showers but will determine in pre-construction phase	<input checked="" type="radio"/>	Yes	No	<input type="radio"/>
16	Does your project exceed the BC Building Code requirements for flow rates for kitchen and bathroom faucets? We will use low flow faucets and try to exceed the code.	<input checked="" type="radio"/>	Yes	No	<input type="radio"/>

Storm Water

17	If your property has water frontage, are you planning to protect trees and vegetation within 60 metres of the high water mark? [Note: For properties located on the Gorge Waterway, please consult Sections 7.1.2.1 and 9.6 of the Esquimalt Official Community Plan.]	Yes	<input type="radio"/>	No	<input checked="" type="radio"/>	N/A
18	Will this project eliminate or reduce inflow and infiltration between storm water and sewer pipes from this property? See proposed rain garden/swale in landscaping plan	<input checked="" type="radio"/>	Yes	No	<input type="radio"/>	N/A
19	Will storm water run-off be collected and managed on site (rain gardens, wetlands, or ponds) or used for irrigation or re-circulating outdoor water features? If so, please describe. Storm water will be collected in rain garden, which will reduce impact on storm system.	<input checked="" type="radio"/>	Yes	No	<input type="radio"/>	N/A
20	Have you considered storing rain water on site (rain barrels or cisterns) for future irrigation uses? We are considering it for the southeast corner of the building, but it would be a challenge due to layout.	<input checked="" type="radio"/>	Yes	No	<input type="radio"/>	N/A
21	Will surface pollution into storm drains will be mitigated (oil interceptors, bio-swales)? If so, please describe. Yes, through the rain garden	<input checked="" type="radio"/>	Yes	No	<input type="radio"/>	N/A
22	Will this project have an engineered green roof system or has the structure been designed for a future green roof installation?	Yes	<input type="radio"/>	No	<input checked="" type="radio"/>	N/A
23	What percentage of the site will be maintained as naturally permeable surfaces?	15-20		%		

Waste water

24	For larger projects, has Integrated Resource Management (IRM) been considered (e.g. heat recovery from waste water or onsite waste water treatment)? If so, please describe these.	Yes	<input type="radio"/>	No	<input checked="" type="radio"/>	N/A
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Natural Features/Landscaping

The way we manage the landscape can reduce water use, protect our urban forest, restore natural vegetation and help to protect the watershed and receiving bodies of water.

25	Are any healthy trees being removed? If so, how many and what species? _____ Could your site design be altered to save these trees? Have you consulted with our Parks Department regarding their removal?	Yes	<input checked="" type="radio"/>	No	<input type="radio"/>	N/A
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26	Will this project add new trees to the site and increase our urban forest? If so, how many and what species? <u>Yes. We will propose a number of new trees for the site</u> Currently working with my landscaping designer on detailed plan	<input checked="" type="checkbox"/> Yes	No	N/A
27	Are trees [existing or new] being used to provide shade in summer or to buffer winds? They will, both from the neighbouring new development, as well as trees planted on this lot.	Yes	No	N/A
28	Will any existing native vegetation on this site be protected? If so, please describe where and how. _____	Yes	No	<input checked="" type="checkbox"/> N/A
29	Will new landscaped areas incorporate any plant species native to southern Vancouver Island? We will look to incorporate native plants, mixed in with non-native to provide visual interest	<input checked="" type="checkbox"/> Yes	No	N/A
30	Will xeriscaping (i.e. the use of drought tolerant plants) be utilized in dry areas?	Yes	No	<input checked="" type="checkbox"/> N/A
31	Will high efficiency irrigation systems be installed (e.g. drip irrigation; 'smart' controls)?	<input checked="" type="checkbox"/> Yes	No	N/A
32	Have you planned to control invasive species such as Scotch broom, English ivy, Himalayan and evergreen blackberry growing on the property?	<input checked="" type="checkbox"/> Yes	No	N/A
33	Will topsoil will be protected and reused on the site?	<input checked="" type="checkbox"/> Yes	No	N/A

Energy Efficiency

Improvements in building technology will reduce energy consumption and in turn lower greenhouse gas [GHG] emissions. These improvements will also reduce future operating costs for building occupants.

34	Will the building design be certified by an independent energy auditor/analyst? If so, what will the rating be? <u>Step 2 of the step code at minimum. We consult to reach step 3.</u>	<input checked="" type="checkbox"/> Yes	No	N/A
35	Have you considered passive solar design principles for space heating and cooling or planned for natural day lighting? <u>Yes, and we will consider this with our energy advisor. We are restricted by the site size and layout, however.</u>	<input checked="" type="checkbox"/> Yes	No	N/A
36	Does the design and siting of buildings maximize exposure to natural light? What percentage of interior spaces will be illuminated by sunlight? <u>50-70</u> % Units are smaller and will have plenty of windows in living areas to maximize natural light.	<input checked="" type="checkbox"/> Yes	No	N/A
37	Will heating and cooling systems be of enhanced energy efficiency (ie. geothermal, air source heat pump, solar hot water, solar air exchange, etc.). If so, please describe. <u>We will likely use a heat pump. This will be determined during step code analysis. But at minimum, we will reach step code 2.</u> If you are considering a heat pump, what measures will you take to mitigate any noise associated with the pump? <u>Pump will likely be placed in insulated bike lock-up or hung from insulated rafters</u>	<input checked="" type="checkbox"/> Yes	No	N/A
38	Has the building been designed to be solar ready?	<input checked="" type="checkbox"/> Yes	No	N/A
39	Have you considered using roof mounted photovoltaic panels to convert solar energy to electricity? Not cost effective at this time. But will be convertible as price of solar panels come down.	<input checked="" type="checkbox"/> Yes	No	N/A
40	Do windows exceed the BC Building Code heat transfer coefficient standards? Will determine exact windows as part of our step code analysis. Likely to be higher end double pane windows.	<input checked="" type="checkbox"/> Yes	No	N/A
41	Are energy efficient appliances being installed in this project? If so, please describe. <u>We will likely install some or all energy star appliances within budget.</u>			
42	Will high efficiency light fixtures be used in this project? If so, please describe.	<input checked="" type="checkbox"/> Yes	No	N/A
43	Will building occupants have control over thermal, ventilation and light levels?	<input checked="" type="checkbox"/> Yes	No	N/A
44	Will outdoor areas have automatic lighting [i.e. motion sensors or time set]?	<input checked="" type="checkbox"/> Yes	No	N/A
45	Will underground parking areas have automatic lighting?	Yes	No	<input checked="" type="checkbox"/> N/A

Air Quality

The following items are intended to ensure optimal air quality for building occupants by reducing the use of products which give off gases and odours and allowing occupants control over ventilation.

46	Will ventilation systems be protected from contamination during construction and certified clean post construction?	<input checked="" type="checkbox"/> Yes	No	N/A
47	Are you using any natural, non-toxic, water soluble or low-VOC [volatile organic compound] paints, finishes or other products? If so, please describe. <u>Paints and adhesives.</u>	<input checked="" type="checkbox"/> Yes	No	N/A
48	Will the building have windows that occupants can open?	<input checked="" type="checkbox"/> Yes	No	N/A
49	Will hard floor surface materials cover more than 75% of the liveable floor area? This is possible, but we will likely go with carpet on bedroom floors to reduce noise and sound transfer.	Yes	<input checked="" type="checkbox"/> No	N/A
50	Will fresh air intakes be located away from air pollution sources?	<input checked="" type="checkbox"/> Yes	No	N/A

Solid Waste

Reuse and recycling of material reduces the impact on our landfills, lowers transportation costs, extends the life-cycle of products, and reduces the amount of natural resources used to manufacture new products.

51	Will materials be recycled during demolition of existing buildings and structures? If so, please describe. <u>We will use as much of the rock, fill and leave retaining walls where possible. We will also look to hold a demo sale or contract a salvager to remove reusable materials.</u>	<input checked="" type="checkbox"/> Yes	No	N/A
52	Will materials be recycled during the construction phase? If so, please describe. <u>Framing wood will be reused for building, as well as rock and fill from site.</u>	<input checked="" type="checkbox"/> Yes	No	N/A
53	Does your project provide enhanced waste diversion facilities i.e. on-site recycling for cardboard, bottles, cans and or recyclables or on-site composting?	Yes	No	<input checked="" type="checkbox"/> N/A
54	For new commercial development, are you providing waste and recycling receptacles for customers?	Yes	No	<input checked="" type="checkbox"/> N/A

Green Mobility

The intent is to encourage the use of sustainable transportation modes and walking to reduce our reliance on personal vehicles that burn fossil fuels which contributes to poor air quality.

55	Is pedestrian lighting provided in the pathways through parking and landscaped areas and at the entrances to your building[s]?	<input checked="" type="checkbox"/> Yes	No	N/A
56	For commercial developments, are pedestrians provided with a safe path[s] through the parking areas and across vehicles accesses?	Yes	No	<input checked="" type="checkbox"/> N/A
57	Is access provided for those with assisted mobility devices? We'll have a ramp coming down from the front of the building to the first floor for both mobility devices and bicycles.	<input checked="" type="checkbox"/> Yes	No	N/A
58	Are accessible bike racks provided for visitors?	<input checked="" type="checkbox"/> Yes	No	N/A
59	Are secure covered bicycle parking and dedicated lockers provided for residents or employees? YES! We'll have excellent secured covered parking for bicycles.	<input checked="" type="checkbox"/> Yes	No	N/A
60	Does your development provide residents or employees with any of the following features to reduce personal automobile use [check all that apply]: <input checked="" type="checkbox"/> transit passes <small>We'll provide one-year transit passes for two units without park spots.</small> <input checked="" type="checkbox"/> car share memberships <small>Car share will be located next door (pending approval of our proposal for 825 Lampson/939 Colville.</small> <input checked="" type="checkbox"/> shared bicycles for short term use <small>I have reached out to U-bike about potentially locating a neighbourhood bike-share bike on site.</small> <input type="checkbox"/> weather protected bus shelters <input checked="" type="checkbox"/> plug-ins for electric vehicles			

Is there something unique or innovative about your project that has not been addressed by this Checklist? If so, please add extra pages to describe it.

MEMORANDUM

To: Ryan Jabs – Lapis Homes
From: Tom Baumgartner, M.Sc., P.Eng.
Tanner Vollema, EIT
Our File #: 2503.B01
Project: Colville & Lampson Developments
Date: August 17, 2018
RE: Transportation Review



1.0 INTRODUCTION

Watt Consulting Group was retained by Lapis Homes to conduct a transportation review for two proposed developments located at the southeast corner of the Lampson Street and Colville Road intersection in Esquimalt, BC. The proposed developments include a 6-unit two-bedroom townhouse development at 937 Colville Road and a 10-unit three-bedroom townhouse development at 825 / 827 Lampson Street and 939 Colville Road. This memo will review the existing site conditions and characteristic, the existing operations of the Lampson Street / Colville Road intersection, the location of the proposed accesses, and the predicted trip generation and parking requirements of the proposed developments. **Figure 1** shows the location of the proposed development sites.



Figure 1: Proposed Development Location

2.0 EXISTING CONDITIONS

The proposed development sites are located on the southeast corner of the Colville Road / Lampson Street intersection in Esquimalt, BC. The development sites are currently zoned as follows:

- 825 Lampson Street: CD-90 (Comprehensive Development No. 90);
- 827 Lampson Street: RS-4 (Single Family Bed and Breakfast Residential);
- 937 Colville Road: RS-1 (Single Family Residential); and
- 939 Colville Road: RS-4 (Single Family Bed and Breakfast Residential).

Adjacent land uses include Two-Family Residential (RD-1 & RD-3), Multiple Family Residential (RM-2), Parks and Open Space (P-2), and Comprehensive Development (CD-32 & CD-70). The proposed development sites are currently accommodating single-family residential homes. The Township of Esquimalt OCP (2018) has designated the site as Townhouse Residential in the Proposed Land Use Designations (Schedule B).

Lampson Street is a two-lane major road with a speed limit of 50km/h. Colville Road is a two-lane local road east of Lampson Street and a two-lane collector road west of Lampson Street. The speed limit on Colville Road is 50km/h with a 30km/h playground zone that begins 20m east of Lampson Street and extends west of Carrie Street. The Lampson Street / Colville Road intersection is two-way stop controlled with the stop control located on Colville Road.

2.1 Current Intersection Operations

The Lampson Street / Colville Road intersection was previously studied by Watt and determined to have a failing level of service (~100 sec delay) for Colville Road approaches in the AM and PM weekday peak hours. The operational level of the intersection is the result of existing traffic levels and traffic control; the proposed developments traffic will not have a significant effect on the intersection volumes (as indicated in Section 5).

3.0 SITE CHARACTERISTICS

The transportation options and services within proximity of the site are as follows:



SERVICES

The development sites are located 300 meters west of the Esquimalt High School and about one kilometer northeast of Rockheights Middle School. The nearest grocery stores are within an 8-10 minute walk and include Craigflower Foods (600 metres away) and Esquimalt Wholesale Club (700 metres away). There are several restaurants within a one-kilometer walk. The sites are located about one kilometer away from the light industrial park in the Devonshire / Viewfield Road area and provide access to employment and services.



TRANSIT

Lampson Street is a public transit route with the nearest southbound transit stop located 40 meters from the development sites and the nearest northbound transit stop located 100 meters from the development sites. These stops are currently serviced by Route 24 (Cedar Hill / Admirals Walk) and by Route 26 (Dockyard / UVic). Additional transit stops on Craigflower Road are located 300 metres north of the development sites and are serviced by Route 14 (Vic General / UVic). **Figure 2** shows the proximity of nearby transit stops.



WALKING

There are paved sidewalks on both sides of Lampson Street and Colville Road. The Lampson Street sidewalks are continuous for the length of the road. On Colville Road, the south sidewalk ends 500m east of the sites and the north sidewalk ends 500m west of the sites; the opposite sidewalks continue until the end of Colville Road. The multi-use E&N Rail Trail is located 100m south of the development sites. The Walkscore for the development sites is 64, which indicates that some errands can be accomplished on foot.¹



CYCLING

The E&N Rail Trail crosses Lampson Street less than 200 meters south of the development sites. The 17-kilometer trail runs from West Victoria to Langford and provides access to the Galloping Goose Trail and to downtown Victoria via the Esquimalt Road and Johnson Street bike lanes. Bike lanes are also located on Craigflower Road, approximately 300 meters north of the development sites. **Figure 2** shows the proximity of nearby cycling facilities.



CARSHARING

The Modo Car Cooperative (“Modo”) is the most popular carsharing service in Greater Victoria. In 2015, there were 23 cars and 800 members; as of March 2018, there are 60 Modo vehicles and 4,136 members across the Greater Victoria region, suggesting that Modo is growing in popularity.² The developer has proposed to provide a Modo membership for each unit in both developments as well as a dedicated Modo carshare vehicle space.

¹ Walkscore. <https://www.walkscore.com/score/939-colville-rd-victoria-bc-canada>

² Email correspondence with Modo's Business Development Manager on March 13, 2018.



Figure 2: Proximity of Site to Alternate Modes

4.0 ACCESS REVIEW

4.1 Corner Clearance

Driveway access to each of the developments will be off of Colville Road. The access to the 825 / 827 Lampson Street and 939 Colville Road development will be located 25 metres east of the Lampson intersection and the 937 Colville Road development access will be located 35 metres east of the Lampson intersection. Section 8.8 of the TAC *Geometric Design Guide for Canadian Roads (2017)* suggests a minimum corner clearance of 15 metres between an access and a stop-controlled major intersection. The proposed driveway locations for both developments exceed the recommended corner clearance distance.

4.2 Sightlines

The TAC Geometric Design Guide sets the criteria for minimum sightlines for a vehicle turning from a stop onto a 50km/h roadway at 105 metres for a left turn and 95 metres for a right turn. Looking east, the sightlines for both accesses are in excess of 200 meters; however, the sightline to the west is obstructed by a vertical crest curve located at the Lampson Street / Colville Road intersection which limits the sightline distance to approximately 45m for the 825 / 827 Lampson St and 939 Colville Rd development and 55 metres for the 937 Colville Road development (see **Table 1**).

TABLE 1: SIGHT LINE DISTANCES FOR PROPOSED DRIVEWAY ACCESSSES

Access	Movement	Posted Speed	Required Sight Distance (m)	Actual Sight Distance (m)	Achieved
825/827 Lampson St / 939 Colville Rd	Right Turn	50km/h	95	45	No
	Left Turn	50km/h	105	200+	Yes
937 Colville Rd	Right Turn	50km/h	95	55	No
	Left Turn	50km/h	105	200+	Yes

Considering that traffic turning off of Lampson Street onto Colville Road would have to slow down to speeds of less than 20km/h in order to negotiate the horizontal alignment, and eastbound Colville Road traffic must stop before crossing Lampson Street. The required sight distance for a stopped vehicle turning right onto a 20km/h road is 40m. The sightlines at the proposed accesses are sufficient to allow for a safe exit onto Colville Road. It is recommended that on-street parking is restricted near the accesses so sightlines are not further constrained.

5.0 TRIP GENERATION

New site trips were estimated from the Institute of Transportation Engineers (ITE) *Trip Generation Manual (10th Edition)*. The *Trip Generation Manual* provides trip rates for a wide variety of land uses gathered from actual sites across North America over the past 35 years. The trip generation results are summarized in **Table 2**.

TABLE 2: POST-DEVELOPMENT TRIP GENERATION

ITE Code	Land Use	Units	Trip Rate	Trips In	Trips Out	Total Trips
AM Peak Hour						
220	Multifamily Housing (Low-Rise)	16	0.46/unit	2	5	7
PM Peak Hour						
220	Multifamily Housing (Low-Rise)	16	0.56/unit	6	3	9

The proposed developments will generate 7 trips during the AM peak hour and 9 trips during the PM peak hour. The low volume of trips generated is expected to have a minimal effect on the surrounding traffic operations.

6.0 PARKING REQUIREMENTS

6.1 Proposed Parking Supply

The proposed 10-unit 825 / 827 Lampson Street and 939 Colville Road development will provide a total of 16 parking spots consisting of 10 garage spaces (one for each unit), five (5) visitor stalls, and one (1) Modo carshare.

The proposed 6-unit 937 Colville Road development will provide a total of five (5) parking spaces consisting of four (4) owner stalls and one (1) visitor stall. Two units will not have dedicated parking and will be expected to use alternative transportation.

Secure bicycle parking will also be provided at both developments. At the 825/827 Lampson Street and 939 Colville Road site, secure bicycle parking spaces are provided in each unit's garage. At the 937 Colville Road site, a shared secure storage space will be provided for 10 bikes. An outdoor bicycle lockup will be provided at each site for visitor use.

6.2 Parking Bylaw Requirements

The Township of Esquimalt Parking Bylaw No. 2011 requires townhouse developments to provide 2 parking spaces per dwelling unit, with 1 out of every 4 spaces designated as visitor parking. Under this bylaw, the developments would be required to provide 32 parking spaces; however, past experience in Esquimalt and similar communities has shown the parking demand to be lower than the bylaw requirement. Additionally, the proposed developments will be oriented towards alternative transportation, which will lower the demand for parking spaces.

6.3 Expected Parking Demand

Using the ITE *Parking Generation Handbook* (4th ed.), the expected parking generation rate for an urban townhouse development (using the ITE Land Use No. 221 – Low/Mid-rise Apartments) is 1.2 spaces per dwelling during the peak demand period (Weekdays from 10PM to 5AM). This would result in a peak parking demand of 19 spaces.

Although conducting a parking study was out of scope of this review, a previous parking study was conducted by Watt Consulting Group in August 2017 for a 16-unit townhouse development in the District of Saanich. The Saanich development is similar in the number and type of units, geographic context, and site characteristics (on a transit route, near to cycle facilities, and a similar walk score of 66). During the study, observations were conducted of the parking demand at several representative townhouse sites. The observations suggest an average parking demand of 0.85 vehicles per unit (see summary in **Table 3**). Applied to the proposed developments, this rate would result in a peak parking demand of 14 spaces.

TABLE 3: PARKING DEMAND OBSERVATIONS (SAANICH PARKING STUDY)

Site	Units	Parking Demand (vehicles / unit)
2633 Shelbourne Street	8	0.75
1827 Fairfield Road	4	1.00
229 Ontario Street	13	0.69
242 Ontario Street	9	0.67
245 Ontario Street	9	1.22
290 Superior Street	7	0.71
130 Niagara Street	14	0.93
Average		0.85

6.4 Parking Demand Reduction

The developer is proposing to reduce the parking demand by prioritizing alternative modes of transportation. In addition to the secure bicycle parking, the developer is proposing to provide a MODO carshare membership to each unit and will include a dedicated MODO carshare vehicle parking space. Access to carsharing programs have been shown to reduce vehicle ownership and lower parking demand. Several municipalities have introduced regulations allowing a reduction in parking requirements where carshare vehicles are easily accessible, including the Cities of Vancouver, New Westminster, Coquitlam, and Richmond. In previous studies where carshare memberships are provided and a carshare vehicle is easily accessible, it is Watt's experience that a 10-15% reduction in parking demand is expected.

7.0 SUMMARY & CONCLUSIONS

The proposed 6-unit two-bedroom townhouse at 937 Colville Road and 10-unit three-bedroom townhouse developments at 825 / 827 Lampson Street and 939 Colville Road in Esquimalt are not expected to incur a significant impact on the surrounding transportation network.

The proposed development will generate few vehicle trips volume of trips and provide minimal off-street parking. This is supported by:

- Pedestrian infrastructure and proximity to schools and commercial areas;
- Nearby transit stops servicing routes to downtown Esquimalt, downtown Victoria, and to the University of Victoria;
- Bike parking (secure and bike racks) and nearby access to Craigflower Road bicycle lanes and the E&N Rail Trail; and
- A dedicated MODO carshare parking space and MODO carshare membership for each unit.

To: Ryan Jabs – Lapis Homes

August 17, 2018

Re: 2503.B01 - Colville & Lampson Developments - Transportation Review

Page 8

There is adequate sightline distance for the proposed development accesses, however on-street parking should be restricted to provide sufficient sightline to the Lampson Street intersection.

Please contact me if there are any questions or comments at 778-313-1014 (ext 431). Thank you.

Sincerely,

Watt Consulting Group



Tom Baumgartner, M.Sc., P.Eng.
Transportation Engineer

937 Colville Road

APPLICATION FOR REZONING & DEVELOPMENT PERMIT

LEGAL DESCRIPTION: LOT 4, BLOCK 1, SECTION 10, ESQUIMALT DISTRICT, PLAN 6277



SITE COVERAGE

STRUCTURE	217.02/
LOT AREA	640.6
	0.339
34.0%	

OPEN SITE SPACE

STRUCTURE	217.02+
PARKING LOT & DRIVEWAY	318.5
TOTAL	536 m. sq.
84.0%	

LOT AREA

LOT AREA	640.6
TOTAL ABOVE	536
OPEN SITE SPACE	104.6 m. sq.
16.3%	

OPEN SITE SPACE

LOT AREA	104.6/
LOT AREA	640.6
	0.163
16.3%	

TOTAL RESIDENTIAL UNIT AREAS
(MEASURED IN INSIDE FACE OF EXTERIOR WALLS, INTERIOR STAIRS NOT DOUBLE COUNTED, ATTIC STORAGE FLOORS INCLUDED)

UNIT 1	72.3 SQ.M./778 SQ.FT.
UNIT 2	72.7 SQ.M./782 SQ.FT.
UNIT 3	65.2 SQ.M./701 SQ.FT.
UNIT 4	64.9 SQ.M./698 SQ.FT.
UNIT 5	73.1 SQ.M./787 SQ.FT.
UNIT 6	72.5 SQ.M./780 SQ.FT.

TOTAL FLOOR AREAS
(MEASURED IN INSIDE FACE OF EXTERIOR WALLS)

LEVEL 1	130.03 SQ.M./1400 SQ.FT.
LEVEL 2	145.80 SQ.M./1569 SQ.FT.
LEVEL 3	145.00 SQ.M./1560 SQ.FT.
ATTIC FLOOR	46.84 SQ.M./504 SQ.FT.
TOTAL FLOOR AREA	467.67 SQ.M./5033 SQ.FT.

HALF STOREY (SCHEDULE A DEFINITION)
THAT PART OF ANY BUILDING WHOLLY OR PARTLY WITHIN THE FRAMING OF THE ROOF, WHERE THE HABITABLE FLOOR AREA IS NOT MORE THAN 70% OF THE GROUND FLOOR AREA OF THE BUILDING.
CALCULATION:
46.84 SQ.M. ATTIC STORAGE FLOOR AREA
130.03 SQ.M. MAIN FLOOR AREA
= 36.0%

REZONING PROJECT INFORMATION TABLE

	PROPOSED	EXISTING ZONING
ZONING	SITE SPECIFIC	RS-1 SINGLE FAMILY RESIDENTIAL
SITE AREA	640.6 sq. m.	640.6 sq. m.
TOTAL FLOOR AREA	467.67 sq. m.	-
COMMERCIAL FLOOR AREA	N/A	N/A
FLOOR SPACE RATIO	0.73:1 (incl. attic) 0.66:1 (excl. attic)	0.35:1
SITE COVERAGE	34.0%	30.0%
OPEN SITE SPACE	16.3%	-
HEIGHT OF BUILDING	9.48m	7.3m
NUMBER OF STOREYS	2 1/2	2 STOREYS WITH NO BASEMENT 1 1/2 STOREYS WITH BASEMENT
PARKING STALLS ON SITE	5 (4 Residential & 1 Visitor)	1 SPACE PER DWELLING UNIT
BICYCLE PARKING NUMBER	12 - CLASS 1 BICYCLE STORAGE 2 - CLASS 2 BICYCLE PARKING	-
BUILDING SETBACKS		
FRONT YARD (SOUTH)	2.37m TO MAIN LEVEL BLDG FACE 0.45m TO STAIR	7.5m
REAR YARD (NORTH)	13.4m TO MAIN LEVEL BLDG FACE 11.84m TO STAIR	7.5m
SIDE YARD (EAST)	3.11m TO MAIN LEVEL BLDG FACE 0.98m TO BALCONY	3.6m
SIDE YARD (WEST)	5.40m TO MAIN LEVEL BLDG FACE 3.70m TO BALCONY	3.6m



RESIDENTIAL USE DETAILS	PROPOSED
TOTAL NUMBER OF UNITS	6
UNIT TYPE	2 BEDROOM
GROUND ORIENTED UNITS	2
MINIMUM UNIT FLOOR AREA	64.9 sq. m.
TOTAL RESIDENTIAL FLOOR AREA	467 sq. m.

CONTACTS

APPLICANT

LAPIS HOMES
250-413-7121

CONTACT: Ryan Jabs
ryanjabs@laphomes.com

ARCHITECT

CHRISTINE LINTOTT ARCHITECT
22-532 HERALD STREET
VICTORIA, BC V8W 1S6

250-384-1969

CONTACT: CHRISTINE LINTOTT
christine@lintottarchitect.ca

BUILDING CODE DATA

APPLICABLE BUILDING CODE:

BC BUILDING CODE 2012 EDITION
ALL WORK TO COMPLY WITH BCBC 2012 IN ALL INSTANCES
PART 9, GROUP C RESIDENTIAL OCCUPANCY CLASSIFICATION

BUILDING HEIGHT:
2 1/2 STOREYS

BUILDING CODE 2012 DEFINITION OF STOREY
THAT PORTION OF A BUILDING THAT IS SITUATED BETWEEN THE TOP OF ANY FLOOR AND THE TOP OF THE NEXT FLOOR ABOVE IT, AND IF THERE IS NO FLOOR ABOVE IT, THAT PORTION BETWEEN THE TOP OF SUCH FLOOR AND THE CEILING ABOVE IT.

FIRE SEPARATIONS:
FLOOR ASSEMBLY BETWEEN LOWER & MAIN LEVEL
AND DEMISING WALLS BETWEEN MAIN UNITS
AND BETWEEN MAIN UNITS & COMMON EXIT
1 HOUR (ARTICLE 9.10.9.14.3)

DWELLING UNITS THAT CONTAIN 2 OR MORE STOREYS INCLUDING BASEMENTS
SHALL BE SEPARATED FROM THE REMAINDER OF THE BUILDING BY A
FIRE SEPARATION HAVING A FIRE RESISTANCE RATING OF NOT LESS THAN 1 HOUR.

FLOOR ASSEMBLY BETWEEN MAIN & UPPER LEVEL
AND DEMISING WALLS BETWEEN MAIN UNITS
AND BETWEEN MAIN UNITS & COMMON EXIT
0 HOUR (ARTICLE 9.10.9.4.2)
FLOOR ASSEMBLIES CONTAINED WITHIN DWELLING UNITS
NEED NOT BE CONSTRUCTED AS FIRE SEPARATIONS

CLASSIFICATION:

3.2.2.52. GROUP C, UP TO 3 STOREYS
1) A BUILDING CLASSIFIED AS GROUP C IS PERMITTED TO CONFIRM TO SENTENCE (2)
PROVIDED:

a) IT IS NOT MORE THAN 3 STOREYS IN BUILDING HEIGHT, AND
b) IT HAS A BUILDING AREA NOT MORE THAN THE VALUE IN TABLE 3.2.2.52.

NO. OF STOREYS
3

FACING 1 STREET
600 SQ.M.
2) THE BUILDING REFERRED TO IN SENTENCE (1) IS PERMITTED TO BE OF COMBUSTIBLE CONSTRUCTION OR NONCOMBUSTIBLE CONSTRUCTION USED SINGLY OR IN COMBINATION, AND
a) EXCEPT AS PERMITTED BY SENTENCE (3) AND (4), FLOOR ASSEMBLIES SHALL BE FIRE SEPARATIONS WITH A FIRE-RESISTANCE RATING NOT LESS THAN 45 MIN,
b) MEZZANINES SHALL HAVE, IF OF COMBUSTIBLE CONSTRUCTION, A FIRE RESISTANCE RATING NOT LESS THAN 45 MIN, AND
c) LOADBEARING WALLS, COLUMNS AND ARCHES SHALL HAVE A FIRE RESISTANCE RATING NOT LESS THAN THAT REQUIRED FOR THE SUPPORTED ASSEMBLY.

3) IN A BUILDING THAT CONTAINS DWELLING UNITS THAT HAVE MORE THAN ONE STOREY, SUBJECT TO THE REQUIREMENTS OF SENTENCE 3.3.4.2.(3), THE FLOOR ASSEMBLIES, INCLUDING FLOORS OVER BASEMENTS, WHICH ARE ENTIRELY CONTAINED WITHIN THESE DWELLING UNITS, SHALL HAVE A FIRE RESISTANCE RATING NOT LESS THAN 45 MIN BUT NEED NOT BE CONSTRUCTED AS FIRE SEPARATIONS.

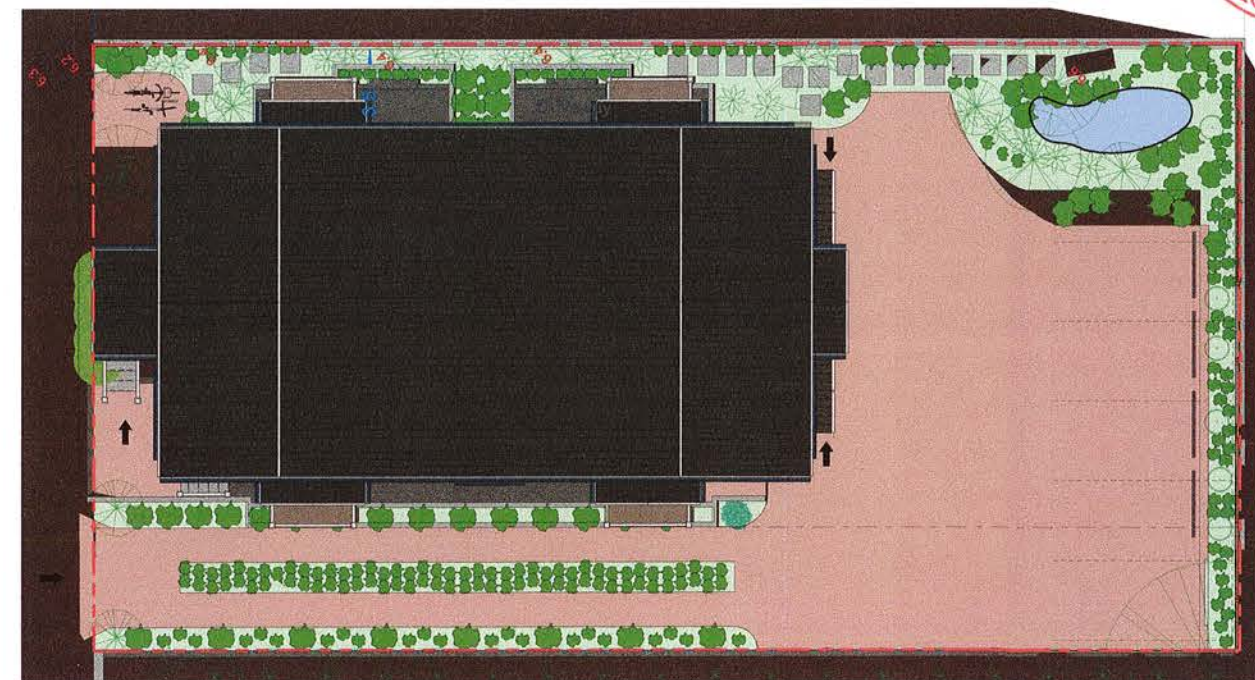
4) IN A BUILDING IN WHICH THERE IS NO DWELLING UNIT ABOVE ANOTHER DWELLING UNIT, THE FIRE-RESISTANCE RATING FOR FLOOR ASSEMBLIES ENTIRELY WITHIN DWELLING UNIT IS WAIVED.

FIRE ALARM:

9.10.18.2. FIRE ALARM SYSTEM REQUIRED
5) A FIRE ALARM SYSTEM IS NOT REQUIRED IN A RESIDENTIAL OCCUPANCY WHERE AN EXIT OR PUBLIC CORRIDOR SERVES NOT MORE THAN 4 SUITES OR WHERE EACH SUITE HAS DIRECT ACCESS TO AN EXTERIOR EXIT FACILITY LEADING TO GROUND LEVEL.

CONSTRUCTION:

COMBUSTIBLE
SPRINKLERED - NO



2 Site Cover
1 : 100



Christine Lintott Architects



Suite 1 - 854 Queen Street, Victoria, BC V8T 1M5
Telephone: 250.384.1969
www.lintottarchitect.ca

Issue Date

Revision No. Description Date

Consultant

937 Colville Road

Enter address here

Cover Sheet

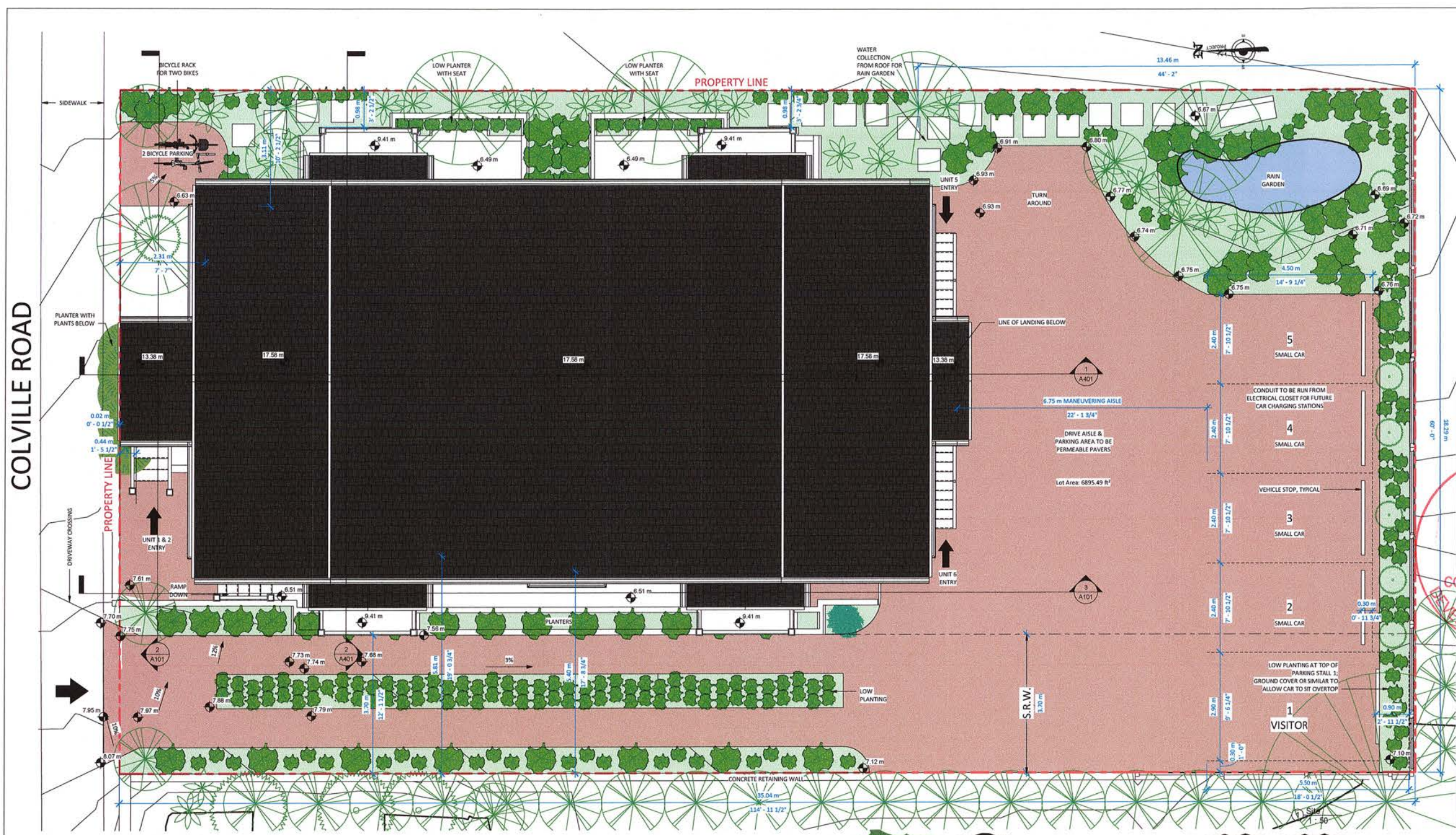
Date 2019-04-10 3:39:00 PM

Drawn by JJ/LA

Checked by CL

A000

Scale As indicated



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 Telephone: 250.384.1909
 www.lintottarchitect.ca

Issue _____ Date _____

Floor Areas:
 *Excludes secure bike storage and common exit stair
 Level 1 1452 SF
 Level 2 1606 SF
 Level 3 1708 SF
 Site Area: 6895.5 SF

Revision No. Description Date



Consultant _____

937 Colville Road

Enter address here

Site Plan

Date 2019-04-10 3:39:22 PM

Drawn by JJ/LA

Checked by CL

A002

Scale As indicated



921 -1 COLVILLE RD.

933 COLVILLE RD.

937 COLVILLE ROAD

939 COLVILLE RD.

STREET CAPTURE

NOTES:
STAIRS, RAMPS, HANDRAILS AND GUARDS TO CONFORM IN ALL INSTANCES TO SECTION 9.8. OF THE BRITISH COLUMBIA BUILDING CODE 2012

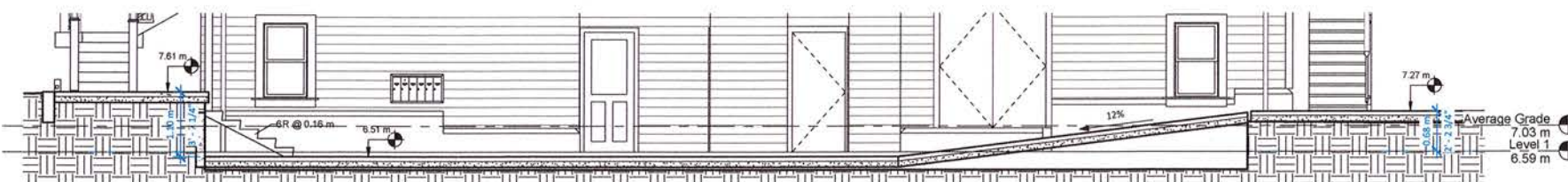
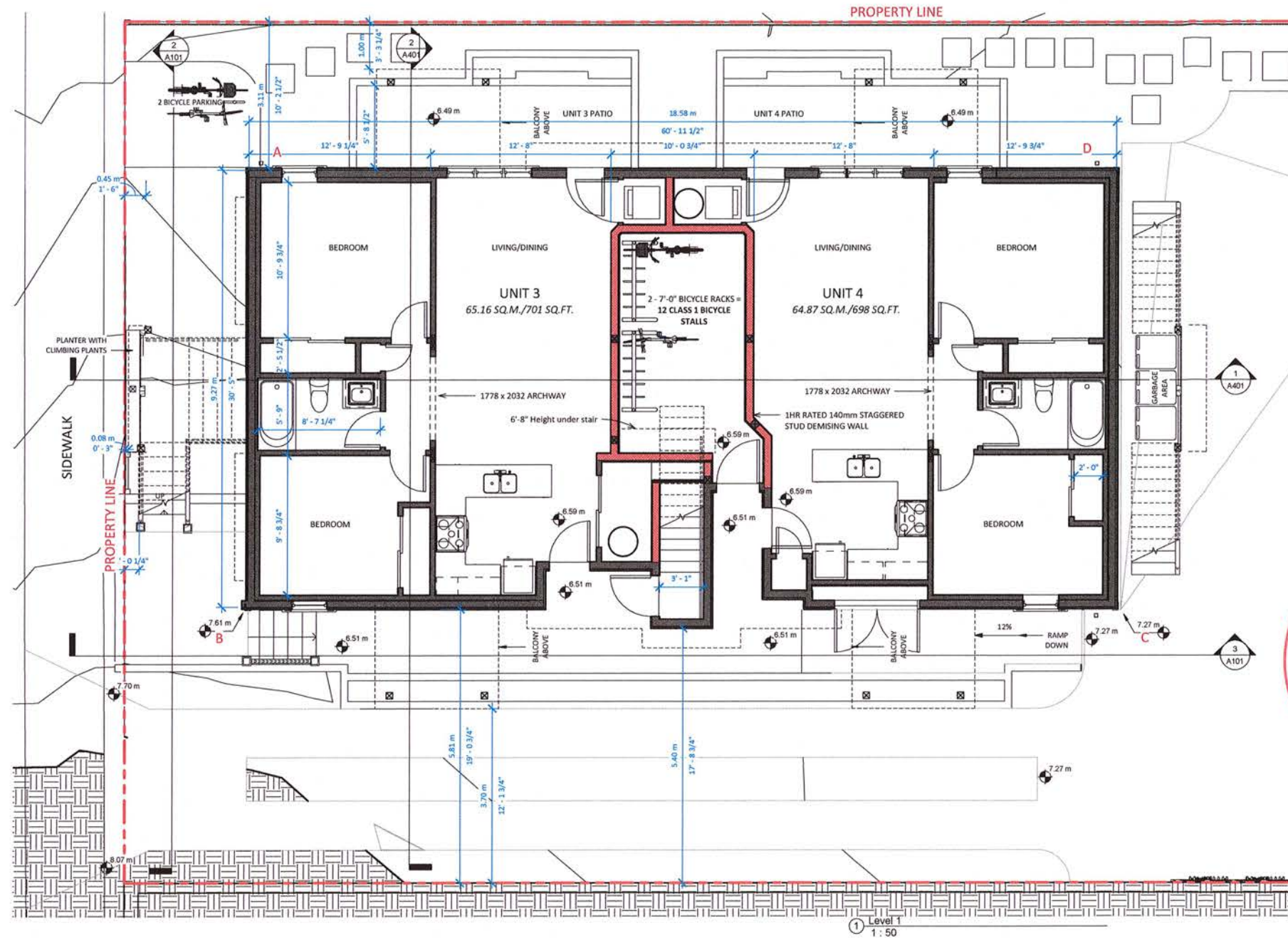
SECOND EXIT STAIR PROVIDED IN RESULT OF:

- 9.9.9.1. TRAVEL LIMIT TO EXITS OR EGRESS DOORS
1) EXCEPT AS PROVIDED IN SENTENCE (2) AND (3), EVERY DWELLING UNIT CONTAINING MORE THAN 1 STOREY SHALL HAVE EXITS OR EGRESS DOORS LOCATED SO THAT IT SHALL NOT BE NECESSARY TO TRAVEL UP OR DOWN MORE THAN 1 STOREY TO REACH A LEVEL SERVED BY
a) AN EGRESS DOOR TO A PUBLIC CORRIDOR, ENCLOSED EXIT STAIR OR EXTERIOR PASSAGEWAY, OR
b) AN EXIT DOORWAY NOT MORE THAN 1.5 m ABOVE ADJACENT GROUND LEVEL

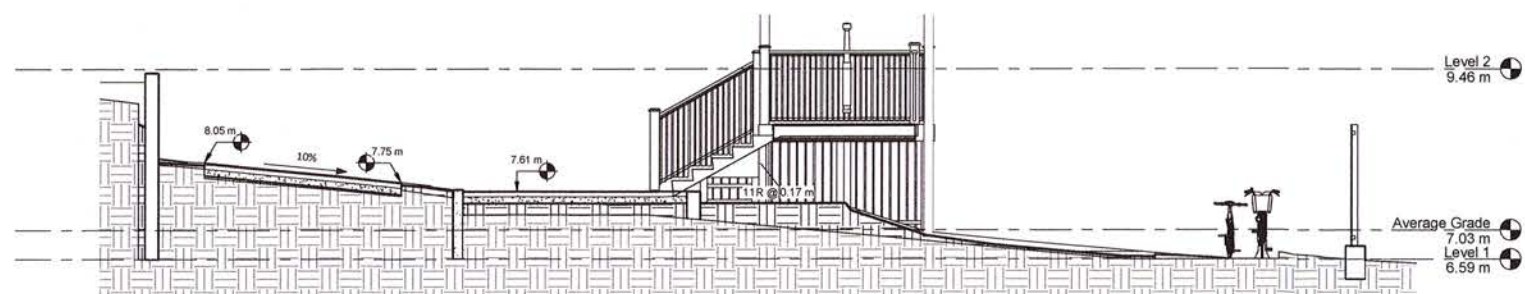
- 9.9.9.3. SHARED EGRESS FACILITIES
1) A DWELLING UNIT SHALL BE PROVIDED WITH A SECOND EXIT AND SEPARATE MEANS OF EGRESS WHERE AN EGRESS DOOR FROM THE DWELLING UNIT OPENS ONTO
a) AN EXIT STAIRWAY SERVING MORE THAN ONE SUITE,
b) A PUBLIC CORRIDOR
i) SERVING MORE THAN ONE SUITE, AND
ii) SERVED BY A SINGLE EXIT,
c) AN EXTERIOR PASSAGEWAY
i) SERVING MORE THAN ONE SUITE,
ii) SERVED BY A SINGLE EXIT STAIRWAY <OR RAMP,> AND
iii) MORE THAN 1.5 m ABOVE ADJACENT GROUND LEVEL, OR
d) A BALCONY
i) SERVING MORE THAN ONE SUITE,
ii) SERVED BY A SINGLE EXIT STAIRWAY <OR RAMP,> AND
iii) MORE THAN 1.5 m ABOVE ADJACENT GROUND LEVEL.

AVERAGE GRADE:
A & B (6.75m + 7.44m) / 2 x 9.27m = 65.7
B & C (7.44m + 7.10m) / 2 x 18.57m = 135
C & D (7.10m + 6.84m) / 2 x 9.27m = 64.6
D & A (6.84m + 6.75m) / 2 x 18.57m = 126.2
TOTAL = 391.5
391.5 / 55.68m perimeter = **7.03m**

LEGEND
1 HOUR FIRE RESISTANCE RATING



3 Side Entrance Section
1:50



2 Front Grade Section
1:50

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Level 1

Date 2019-04-10 3:39:25 PM

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Checked by CL

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Scale 1:50



Issue _____ Date _____



Revision

No. Description Date

No.	Description	Date

Consultant

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937 Colville Road

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Levels 2 & 3

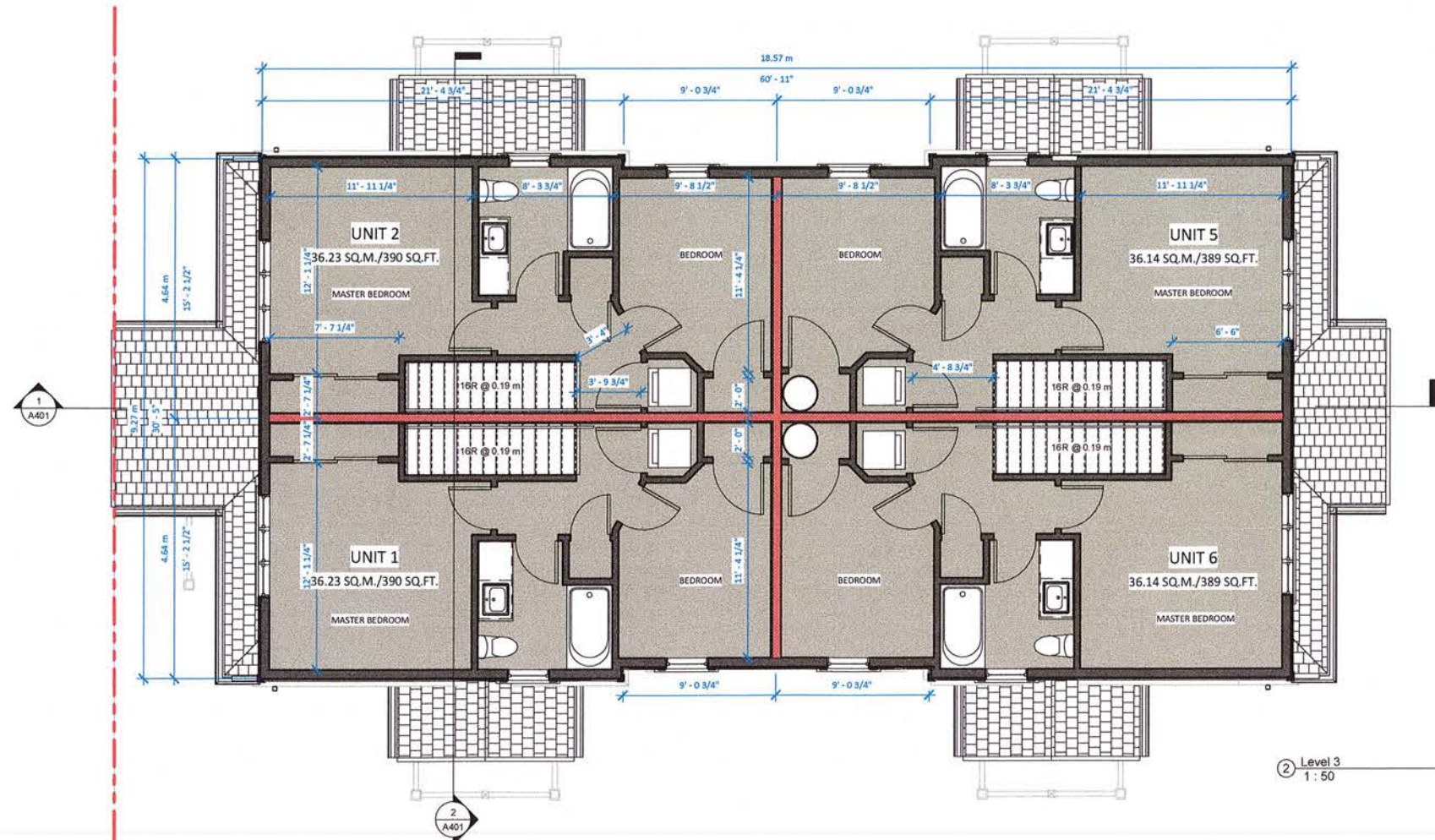
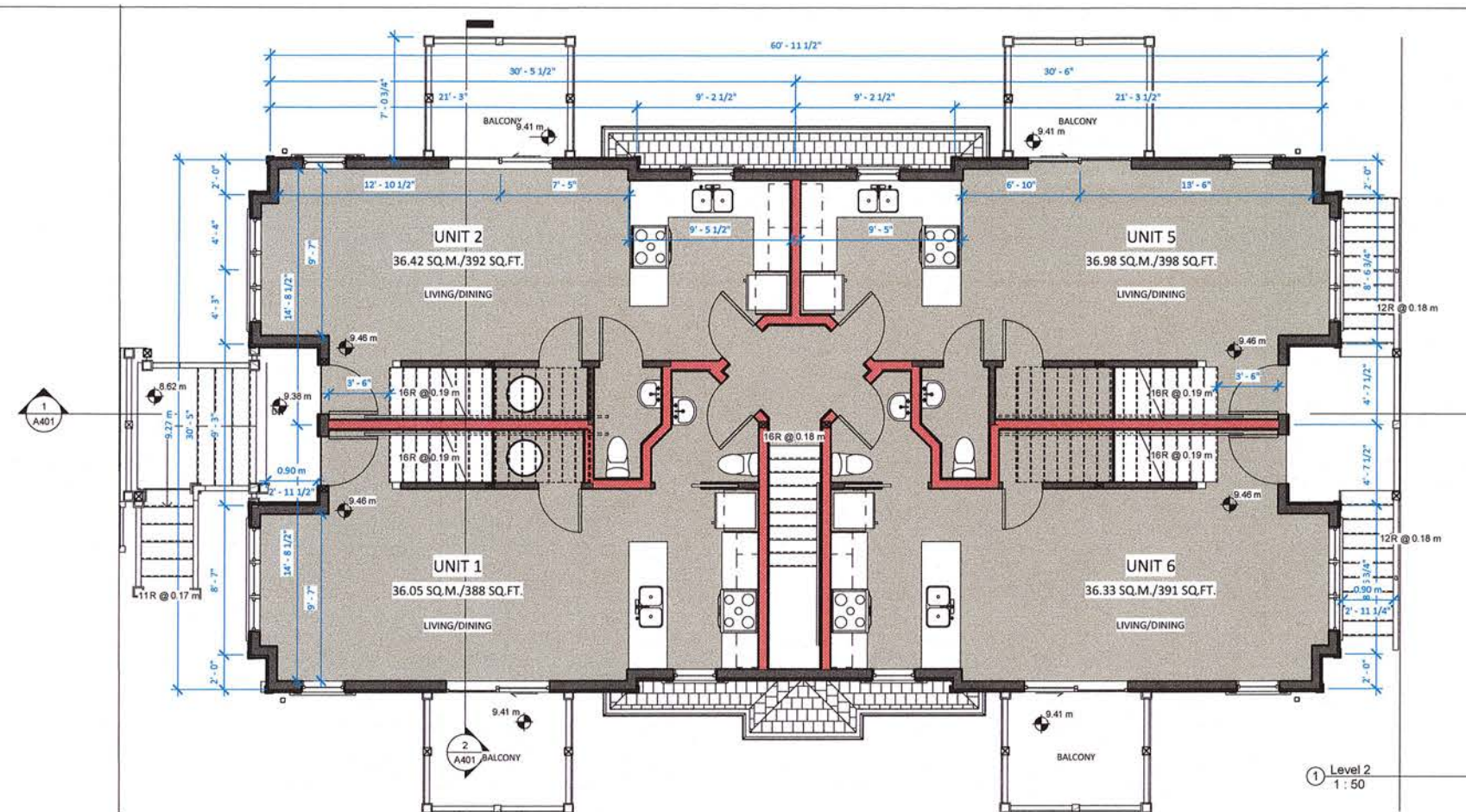
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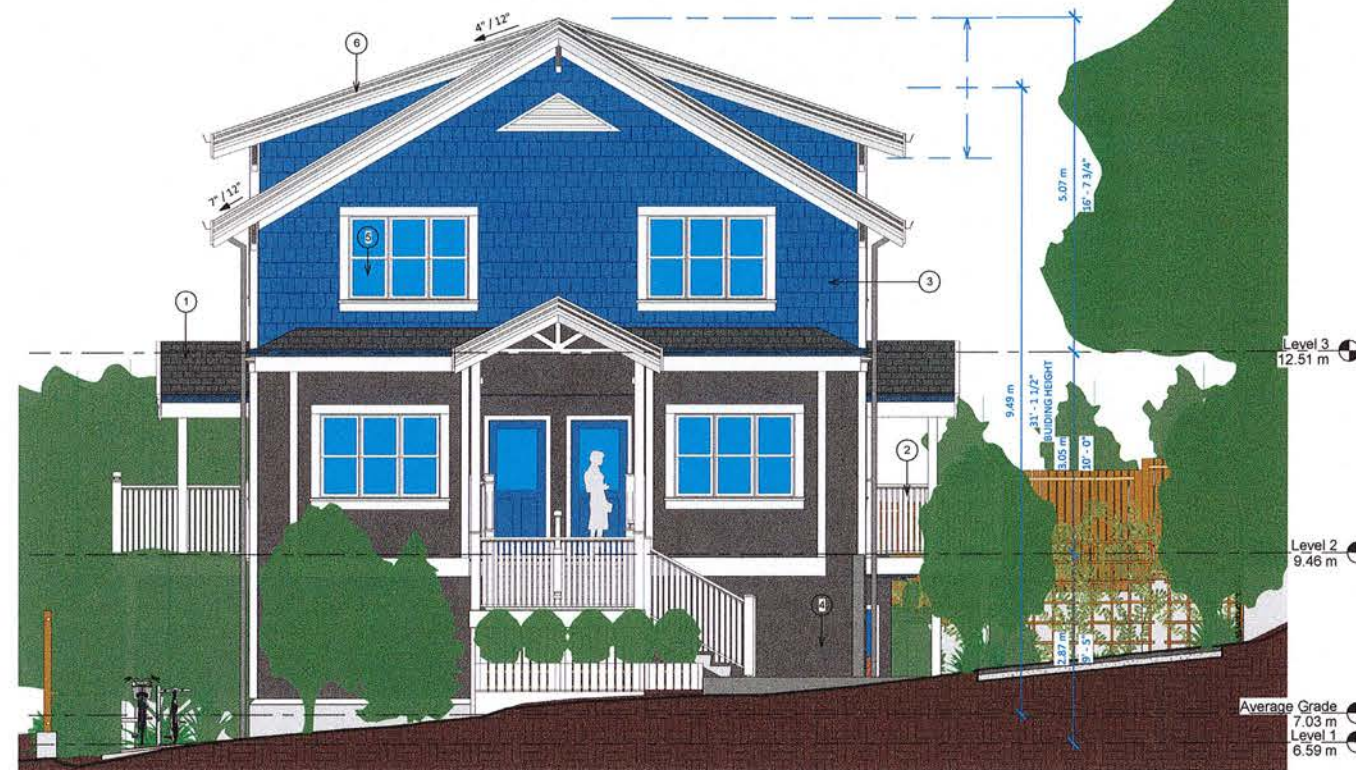
Drawn by JJ/LA

Checked by CL

A102

Scale 1 : 50





Christine Lintott Architects

Suite 1 - 854 Quays Avenue, Victoria, BC V8T 1M5
 Telephone: 250.384.1969
 www.lintottarchitect.ca

Issue _____ Date _____

Material Legend	
Key Value	Keynote Text
1	Asphalt Shingle Roof
2	Wood Railing, Painted
3	Fibre Cement Shingles
4	Fiber Cement Panel, 6" reveal
5	Vinyl Windows
6	Decorative Wood Fascia

Revision

No.	Description	Date



937 Colville Road

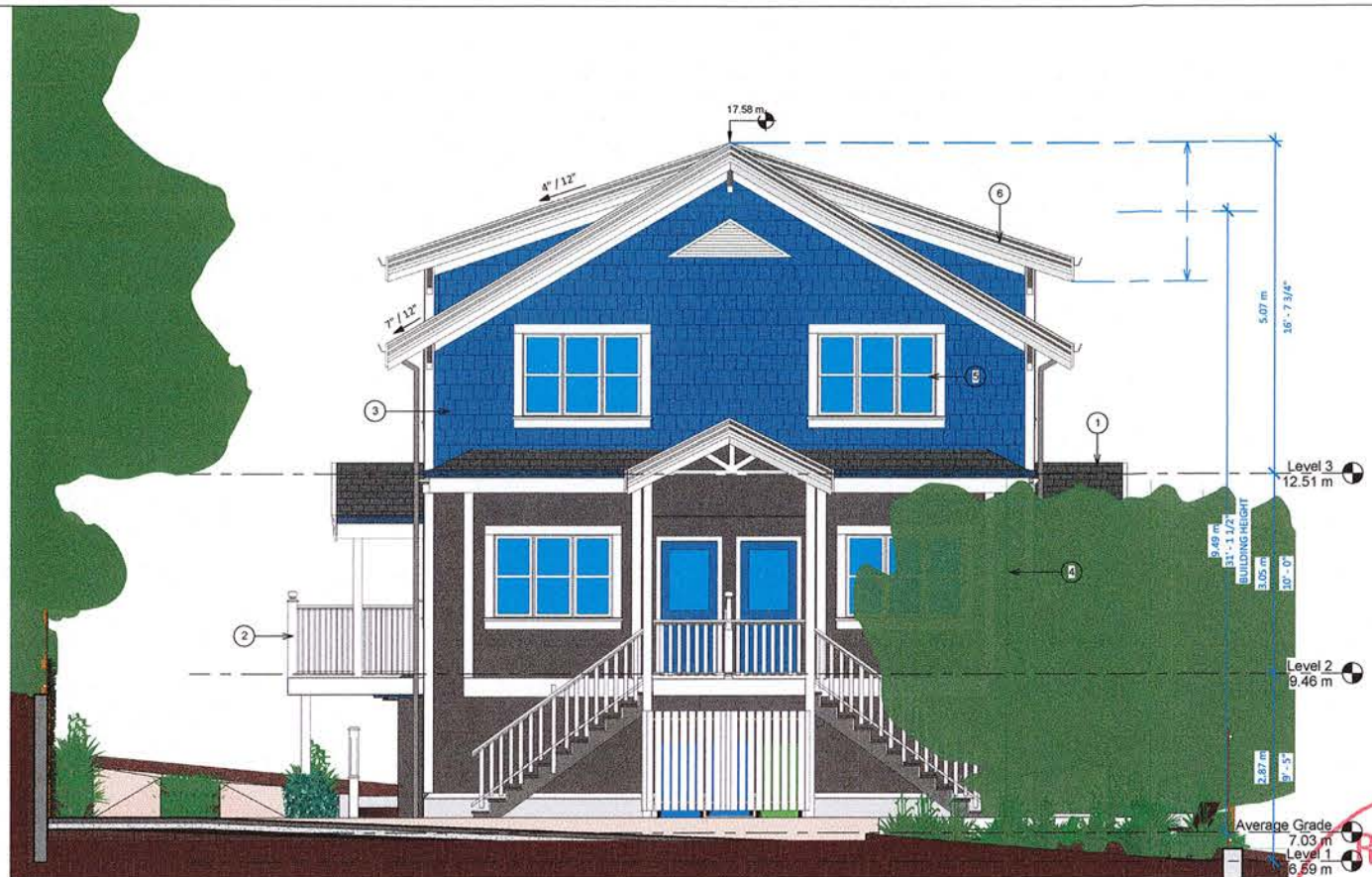
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North & West Elevation

Date 2019-04-10 3:40:01 PM
 Drawn by JJ/LA
 Checked by CL

A301

Scale 1:50



Christine Lintott Architects
 Suite 1 - 854 Queen Avenue, Victoria, BC V8T 1M5
 Telephone: 250.384.1969
 www.lintottarchitect.ca

Issue _____ Date _____

Material Legend	
Key Value	Keynote Text
1	Asphalt Shingle Roof
2	Wood Railing, Painted
3	Fibre Cement Shingles
4	Fiber Cement Panel, 6" reveal
5	Vinyl Windows
6	Decorative Wood Fascia

Revision No. Description Date



2 South
1:50



1 East
1:50

Consultant _____

937 Colville Road
 Enter address here

South & East Elevation

Date 2019-04-10 3:40:32 PM
 Drawn by JJ/LA
 Checked by CL

A302

Scale 1:50

Christine Lintott Architects



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Telephone: 250.384.1969
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Issue Date



Revision

No. Description Date

Consultant

937 Colville Road

Enter address here

Sections

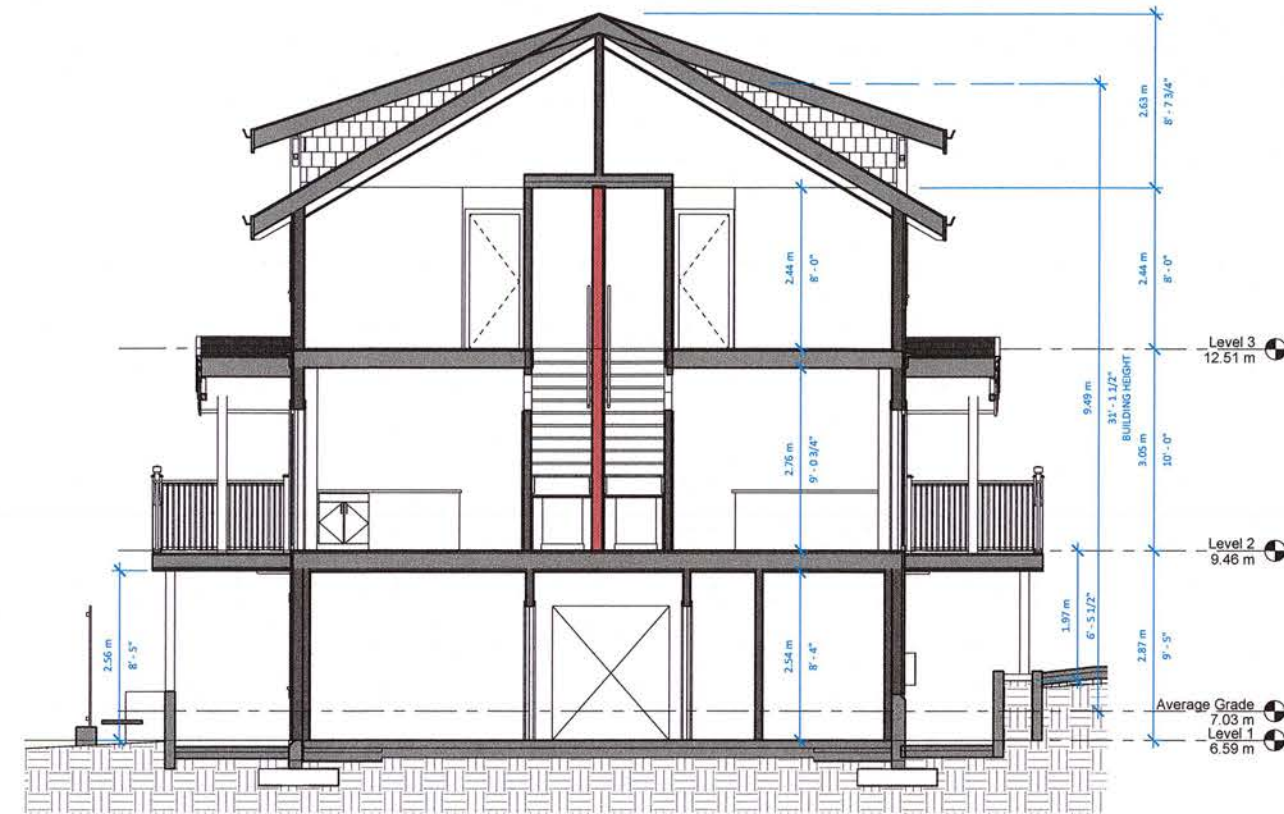
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Drawn by JJ/LA

Checked by CL

A401

Scale 1 : 50

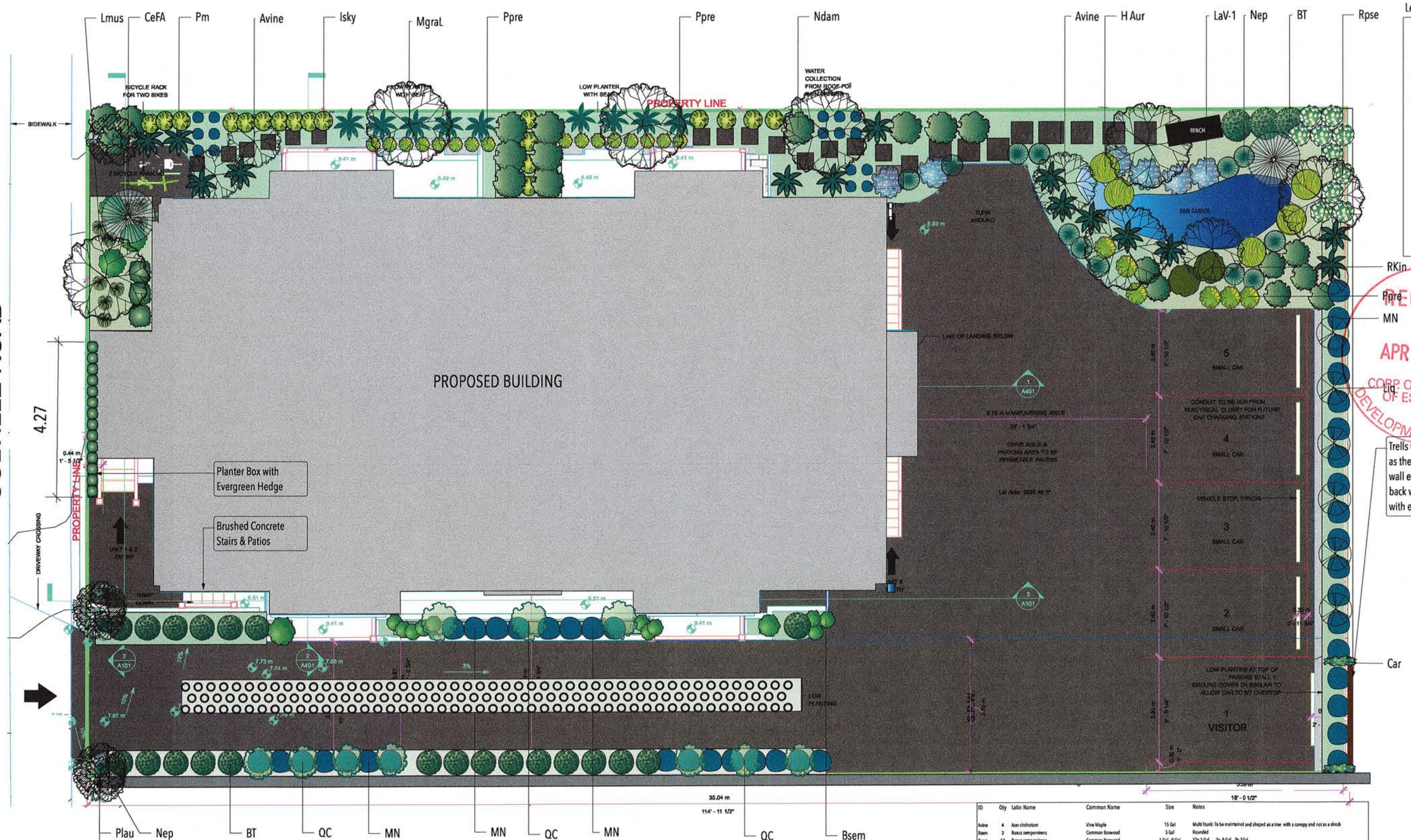


② Building Cross Section
1 : 50



① Building Section
1 : 50

COLVILLE ROAD



Legend

- Brushed Concrete
- Permeable Pavers
- Garden Beds
- Trough Planters
- Rain Garden
- Wood Trellis

RECEIVED
 APR 23 2019
 CORP. OF TOWNSHIP OF ESQUIMALT
 DEVELOPMENT SERVICES

Trellis will extend as far as the rear retaining wall extends along the back wall/fence line with evergreen vines

3x Standard Evergreen Laurels

ID	Qty	Latin Name	Common Name	Size	Notes
Avine	4	Acer crichatum	Vine Maple	15 Gal	Multi trunk: To be maintained and shaped as a tree with a canopy and not as a shrub
Bam	3	Buxus sempervirens	Common Boxwood	5 Gal	Rounded
BT	13	Buxus sempervirens	Common Boxwood	1 Gal - 8 Gal	12x 1 Gal - 2x 8 Gal - 9x 3 Gal
Car	24	Buxus sempervirens "Monrovia"	Green Tower Boxwood	2 1/2 Gal	
CeFA	2	Clematis integrifolia	Evergreen Clematis	5 Gal	To grow and cover trellis
CaC	2	Calluna sibirica "Tussock"	Heather-like Calluna	4" tall B&B	For Colville Street Planter: To be maintained as a hedge
CaC	3	Ceanothus thyrsiflorus "Victoria"	Victoria California Lilac	5" tall B&B	For Colville Street Planter: To be maintained as a hedge
CaC	14	Ceanothus thyrsiflorus "Victoria"	Victoria California Lilac	3" tall	For Colville Street Planter: To be maintained as a hedge
QC SM	143	Sagina subulna "Aurea"	Sandwich Moss	4"	Groundcover: Sub: Pests "Blue Star"
H Aur	5	Hibiscus rosa-sinensis "Aurea"	Golden Japanese Forest Grass	1 Gal	
Isky	18	Ilex crenata "Sky Pencil"	Sky pencil Japanese Holly	1 Gal	Sub: Ceanothus / Buxus "Green tower"
LaV-1	6	Larodendron argenteifolia "Hidcote Blue"	Hidcote Blue English Lavender	5" tall Min	
Liq	7	Liquidambar styraciflua "Stender silhouettes"	Silhouette Sweet Gum	1 Gal	
Lmus	9	Lonicera maackii	Big Blue Lily Tree	1 Gal	
MgraL	7	Magnolia grandiflora "Little Gem"	Dwarf Little Gem Magnolia	10x10 Gal	
MN	57	Muhlenbergia laevis	Dwarf Oregon Grape	1 Gal	Mass planting: Sub along parking Paspalum termitalis
MN	20	Muhlenbergia laevis	Heavenly Bamboo	5 Gal	
Nep	14	Nepenthes "Pillar"	Dwarf Carnation	1 Gal	
Plu	5	Prunus laurocerasus	English Laurel	4" tall B&B	Single trunk, tree shaped with thick canopy
Pm	9	Polystichum muratum	Sword Fern	3 Gal	
Pm	14	Polystichum muratum	Sword Fern	1 Gal	
Ppre	4	Prunus japonica "Prelada"	Dwarf Prunus Prelada	3 Gal	
Ppre	16	Prunus japonica "Prelada"	Dwarf Prunus Prelada	1 Gal	
QC	10	Quercus ssp. "Cinderella Spirit"	English Narrow Oak s	12 Gal	Sub: Most narrow width species available at planting time: Liquidambar
RKin	2	Ribes sanguineum "King Edward VI"	Flowering Currant	5 Gal	
Rkse	10	Rosa rugosa	Wild nectar Rose	1 Gal	Mass Planting



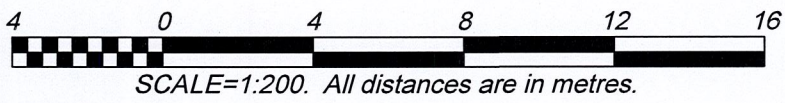
Address: 937 Colville Road
 APPLICATION FOR REZONING & DEVELOPMENT PERMIT

Imagine Garden Design & Landscapes
 4041 Raymond St N
 Victoria, B.C. V8Z 4L1
 250-885-4355

Sheet Title: Landscape Concept

Designed By: Lisa May
 Date: April 18th, 2019
 Scale: 1/4" = 1'

PROPOSED DEVELOPMENT UPON LOT 4, BLOCK 1, SECTION 10, ESQUIMALT DISTRICT, PLAN 6277.

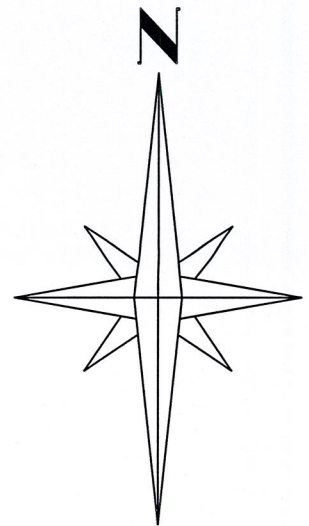


NOTE:

Proposed building position and shape shown is based upon digital files received from the project architect October 30th, 2018.

Lot dimensions, offsets, and area shown may vary upon completion of a comprehensive legal survey. Geodetic elevations shown are based upon observations to geodetic control monuments 84H0179 (Elevation= 6.162m) and 84H0253 (Elevation= 13.214m).

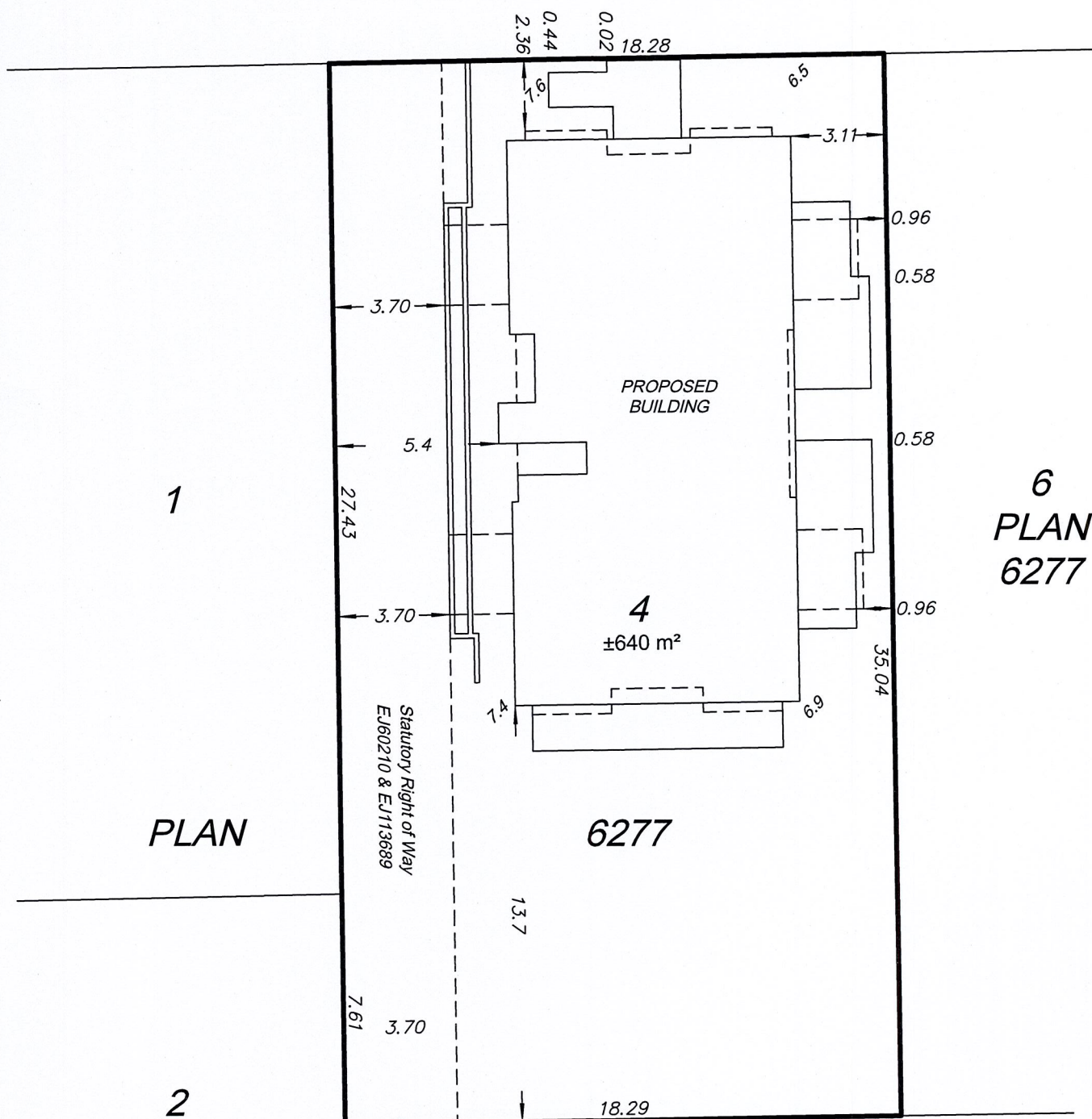
This plan is for building design & permit purposes only and is for the exclusive use of our client. This plan shall not be used to define property lines or property corners. Unregistered interests have not been included or considered.



LEGEND

Denotes natural grade to geodetic datum

COLVILLE ROAD



6
PLAN
6277

PLAN

6277

18
STRATA
PLAN VIS5131

File: 22-RJ-SP9
Date: May 14, 2019
Island Land Surveying Ltd.
#117-693 Hoffman Avenue
Victoria B.C. V9B 4X1
TEL 250.475.1515 fax 250.475.1516
www.islandsurveying.ca